



## MANAGING CITY PLANNING

## DEVELOPMENT CONTROL







# **DRAFT KUALA LUMPUR LOCAL PLAN 2040**

## **VOLUME 1: PART 1 DEVELOPMENT PLANNING CONTROL**



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Draft KLLP2040



## DRAFT PTKL2040



# Preface

Assalamualaikum Warahmatullahi Wabarakatuh

Greetings and Salam Malaysia MADANI,

Alhamdulillah, praise to Allah, the most gracious most gracious and the most merciful. I extend my gratitude and heartiest congratulations to Kuala Lumpur City Hall (KLCH) for having successfully prepared the Draft Kuala Lumpur Local Plan 2040 (KLLP2040).

The preparation of the plan is as stipulated under Section 13, Federal Territory (Planning) Act 1982 (Act 267). This plan also acts as the main document for any development planning in the Federal Territory Kuala Lumpur.

Kuala Lumpur is the Capital City that plays the role in transforming Malaysia to a developed country by 2040. KLCH has taken a proactive approach by outlining the transformation achievement level in Draft KLLP2040. Through the Kuala Lumpur City Plan 2020 (KLCP2020) that has been gazetted, the goal of Kuala Lumpur is to be a competitive city focusing on tourism and commercial sectors while elevating its position as a city in the world that focuses primarily on the financial and commercial sector.

Draft KLLP2040 will become an important document used as a basis in regulating municipal development in Kuala Lumpur. Therefore, the government is always committed in managing and implementing municipal development competently, efficiently, systematic and global standard.

Draft KLLP2040 takes into account global, national and Kuala Lumpur Federal Territory development policies so that planning and development in Kuala Lumpur is more relevant and holistic as well as becomes a reference at the international level.

The future planning of Kuala Lumpur implements the MADANI Malaysia Policy or (SCRIPT) based on Sustainability, Care and Compassion, Respect, Innovation, Prosperity and Trust in shaping a progressive society. This framework is also supported by the goals of Sustainable Development 2030 (SDGs) and the New Urban Agenda (NUA), which are the national commitments at the global level.

Besides, to realise Kuala Lumpur's attainment of Low Carbon City status by 2050, Draft KLLP2040 outlines detailed guidelines and initiatives to ensure its joint implementation with the city's residents.

The government is always attentive and conscientious in making sure the plan helps Kuala Lumpur achieve its goal in becoming a **City for All** by the year 2040.

Thank you and best regards.

**YB DR. ZALIHA BINTI MUSTAFA**  
**Minister in Prime Minister's Department**  
**(Federal Territories)**



# Preface

Assalamualaikum Warahmatullahi Wabarakatuh

Greetings and Salam Malaysia MADANI,

Praise to Allah SWT, the most gracious the most merciful, Alhamdulillah, Kuala Lumpur City Hall (KLCH) has successfully prepared the Draft Kuala Lumpur Local Plan 2040 (KLLP2040) which is the core of future planning for the development of Kuala Lumpur. The Draft KLLP2040 is in line with the 17 Sustainable Development Goals (SDGs) and New Urban Agenda (NUA) to ensure Kuala Lumpur will flourish dynamically, while meeting the needs of today and the days forward. The preparation of the Draft KLLP2040 also takes into account current government policy, the MADANI Malaysia Policy or (SCRIPT) by incorporating all the core elements of the MADANI Malaysia in its planning initiative.

Draft KLLP2040 is a comprehensive action plan that encompasses of all aspects of current and the future changes. These includes climate change, economic development, changes in demography, lifestyle, infrastructural needs, as well as sustainable urban management; and development.

Through this plan, various planning strategies, implementation proposals, and initiatives are outlined to form a flexible and orderly framework in line with changing market demand and the needs of the population that need to be balanced with the changing demographics of Kuala Lumpur's population in the future. Therefore, the Draft KLLP2040 would serve as the foundational main reference for all, especially stakeholders and related agencies in Kuala Lumpur's development planning.

The effect of the pandemic that has hit the whole world has created and approaches to new norm practices. Therefore, the Draft KLLP2040 also sets the direction of the city to be more liberal in governing Kuala Lumpur as well as addressing strategic issues and handling them effectively.

The proposal implementation of Draft KLLP2040 also emphasizes future projection and targets, considering feedback and surveys conducted throughout the preparation of the local plan.

Feedbacks, sharing and views been received throughout the preparation of Draft KLLP2040 truly reflected the aspiration of KLCH in creating **City for All**. This comprehensive effort is based on a holistic development vision with efficient governance, ensuring the development of Kuala Lumpur is driven by the active citizen engagement. The implementation of Draft KLLP2040 is seen as a comprehensive and balanced developmental stage, able to promote investment opportunities, employment and become a preferred destination in the future.

Lastly, I would like to congratulate the KLCH workforce government agencies, private bodies, NGOs and all of Kuala Lumpur residents who have contributed to the successful preparation of Draft KLLP2040. May KLCH in collaboration with all stakeholders, remain committed to empowering development strategies in Kuala Lumpur, strengthening confidence in carrying out the responsibilities entrusted towards the realisation of Kuala Lumpur, **City for All**.

Thank you.



**DATUK SR HJ. KAMARULZAMAN BIN MAT SALLEH**  
Mayor of Kuala Lumpur



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# 1.0

## INTRODUCTION







# 1.0 INTRODUCTION

## DRAFT KUALA LUMPUR LOCAL PLAN 2040

A local plan for Kuala Lumpur called the Draft Kuala Lumpur Local Plan 2040 (KLLP2040) has been prepared in accordance with the provision of Section 13 of the Federal Territories (Planning) Act 1982 (Act 267). It replaces the Kuala Lumpur City Plan 2020 (KLCP2020).

The Draft KLLP2040 is prepared in two (2) volumes:

### 1. VOLUME 1: Managing City Planning

This document focuses on urban planning management which includes planning control guidelines, land use zone and development intensity maps as well as use of land and building classes; and

### 2. VOLUME 2: Promoting City Development

This document focuses on the translation of KLSP2040 through planning strategies, implementation proposals and implementation initiatives as well as forming the basis for the details of planning control in Volume 1.

1-1

### THE RELATIONSHIP BETWEEN KUALA LUMPUR STRUCTURE PLAN 2040 (KLSP2040) AND DRAFT KUALA LUMPUR LOCAL PLAN 2040 (KLLP2040)

The Draft KLLP2040 is prepared to translate the goals and strategic direction of KLSP2040 into the form of a more detailed physical plan to achieve the vision of Kuala Lumpur City for All.

The translation of the Draft KLLP2040 is carried out based on planning strategies and implementation proposals through sustainable development and land use management. It is further translated into a map of land use zones and development intensity as well as statements to guide Kuala Lumpur's planning in the future.

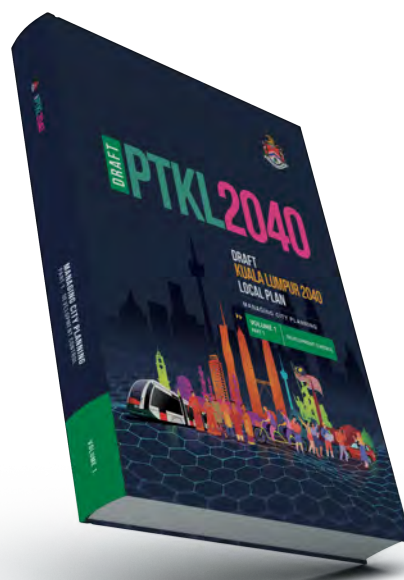
1-2

### APPLICATION OF DRAFT KUALA LUMPUR LOCAL PLAN 2040 (KLLP2040)

The Draft KLLP2040, after being gazetted, is applicable for the next 20 years or until a new replacement plan is prepared and adopted.

This plan applies to all areas within Kuala Lumpur that are under the jurisdiction of the Kuala Lumpur City Hall (KLCH) as the Local Authority. This plan also focuses on protected areas such as forests and hilly lands, heritage zones and buildings as well as government and private lands within Kuala Lumpur.

**Figure 1.1.1:**  
Volume1: Managing City Planning



The Mayor of Kuala Lumpur may at any time alter, add, revise, repeal or replace the whole or part of this local plan in accordance with Section 17 of the Federal Territory (Planning) Act 1982 (Act 267).

### 1-3.1 Change of Land Use Zone

All land plots in Kuala Lumpur have been designated with land use zones, therefore, permitted uses and activities must comply with the use class as stated in Volume 1: Part 3, Classes of Use of Land and Buildings 2024 (CULB2024).

Changes to land use zones other than those designated through the planning permission application process are not permitted. Land use zone changes are allowed only through the process in accordance with the provision of Section 17 of the Federal Territories (Planning) Act 1982 (Act 267) which states that the Mayor can alter, add, revise, repeal or replace KLLP2040.

However, if any application for planning permission is not in line with the land use zone, planning permission can still be granted according to the discretion implemented in accordance with the provisions of Section 22 of the Act (Planning) of the Federal Territory 1082 (Act 267) or any provision of the act that replaces it or subject to any stipulations set by the government.

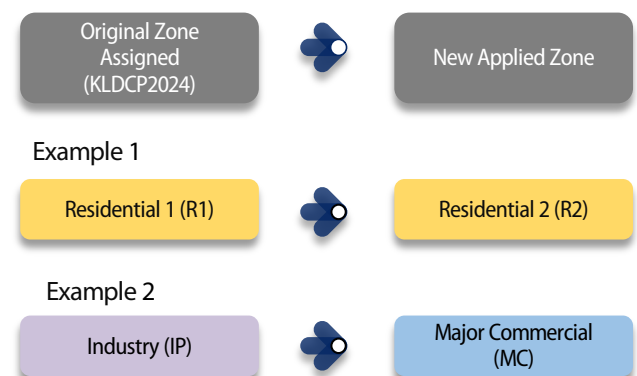
Changes to the land use zone must be made when the proposed development is not in line with the land use zone set in the Kuala Lumpur Development Control Plan 2024 (KLDCP2024), as shown in Figure 1.3.1. However, land and building use is permitted or permitted with conditions in the Classes of Use of Land and Buildings 2024 (CULB2024) are not required to go through the amendment process.

Additionally, the re-planning of an area involving infrastructure, utilities, transport, community facilities, or open space does not involve the process of changing the land use zone, which is subject to the breadth of the area not exceeding 30 percent of the developed area as shown in Figure 1.3.2. It is to ensure the planning maintains the original/basic use and takes into consideration the current and future needs of an area. The stated land use is supportive land use and not the main land use for an area.

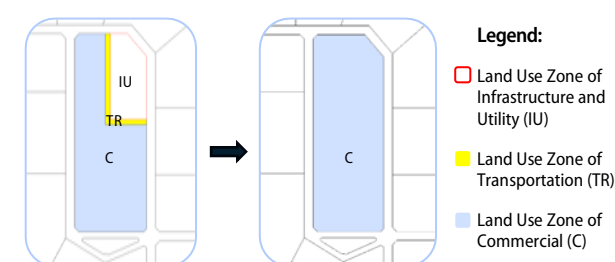
Other developments that do not involve the change process consist of two (2) land use zones, and the specific conditions for the exemption are as follows:

1. Land Use Zone - Institutional (INT):
  - a. Permissible development in the form of residing in line with any requirement by the government as well as subject to suitable planning including socioeconomic benefits; and
2. Land Use Zone - Private Open Space (OS3)
  - a. Development for the purpose of socioeconomic benefits (trade or residential component) for part of the site is subject to suitability involving aspects of site size, location, environment, and carrying capacity of an area.

**Figure 1.3.1:**  
Examples of Land Use Zone Changes that Require KLDCP2024 Alteration



**Figure 1.3.2:**  
**Examples of Area Replanning That Does Not Involve**  
**Land Use Zone Changes**



Change of Land Use Zone of Infrastructure and Utility (IU) and Transportation (TR) to Land Use Zone of Commercial not exceeding 30 percent of land area in same plot.

### BRIEF INFO

#### SCENARIO OF CHANGE OF LAND USE ZONE

- 1** Changes for the purpose of enabling development such as access to the site that involves amalgamation of lots with road reserves or existing roads in layout planning. Infrastructure related to layout replanning includes infrastructure and utilities, green linkage and green areas that are not bound to public use. For example, changing the Land Use Zone of Open Space 2 (OS2) to Transportation (TR) for the purpose of access to new development.
- 2** The development of affordable housing in Land Use Zone of Residential 3 (R3) is encouraged due to the provision of incentives for affordable housing. Nevertheless, this development does not require a change of Land Use Zone of Residential 3 (R3) to Land Use Zone of Residential 4 (R4).

### 1-3.2 Changes in Intensity

The Draft KLLP2040 has identified matters that do not require the process of changing the local plan as follows:

1. Lower development intensity compared to the stipulations in the Draft KLLP2040 in the same main land use; and
2. Development that increases intensity due to the provision of incentives in line with current guidelines or policies stipulated by the government.

## 1-4 CLASSES OF USE OF LAND AND BUILDINGS 2024 (CULB2024)

Classes of Use of Land and Buildings 2024 (CULB2024) allows more than one (1) class of use in each land use zone.

However, within the same land use zone, an application of planning permission must be made to change from one class to another as shown in Figure 1.4.1. The Mayor can determine forms of class changes that do not require approval of planning permission application by considering requirements or amendments that do not involve material changes. Nevertheless, there are two (2) scenarios that require the application of planning permission:

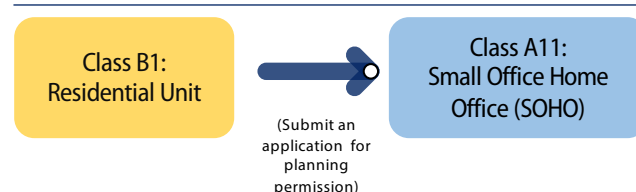
1. Intensification of use. If the activity involves increasing the intensity of use, then it requires the application of planning permission; and
2. Sensitivity involving activities related to entertainment, race, religion, social issues, safety and others.

Examples of CULB2024 changes are as follows:

1. Example 1: The same main class of use
  - a. Changing the use of Class A3 - Offices to Class A4 - Financial Institutions, involving banks in the main use class do not require an application for planning permission; and
  - b. Changing the use from Class A1 - General Retail to Class A5 - Food and Drink shall be subjected to the Licensing By Laws, restaurant use still requires the application of planning permission approval.
2. Example 2: Different main class of use
  - a. Changing of Class A3 - Office to Class B3 - Dormitory or Centralised Labour Quarters requires application for planning permission.

In addition, CULB2024 need to go through an alteration process if there is a new class of use which is incompatible with the existing class of use. However, an addition of types of activity within the same class will not involve an alteration process.

**Figure 1.4.1:**  
**Alteration in Class of Use in a Zone**



This document focuses on managing urban planning and contains three (3) main parts which are detailed as follows:

Table 1.5.1: Content of Draft KLLP2040 Volume 1

Documents	Content
	<p><b>Part 1: Development Control</b></p> <ol style="list-style-type: none"> <li>Chapter 1 explains the content and application of Volume 1;</li> <li>Chapter 2 explains the Land Use Zone Map which contains the principles for determining land use zones, classification, interpretation, types of land use zones and stipulation of special implementation for the Land Use Zone of Mixed Development (MX);</li> <li>Chapter 3 explains the principles for ascertaining and determining the intensity of development which are the plot ratio and building height control that are applicable in all areas in Kuala Lumpur;</li> <li>Chapter 4 explains the layers of planning control which are Planning Control of Layout Plan Plot Ratio, Planning Control for Side Corridors of River, Lake and Pond, Planning Control for Heritage Areas or Buildings, Planning Control for Height Control Zone Surrounding The Istana Negara, Planning Control for Visual Line of Sight Towards Landmarks, Planning Controls for Public Open Spaces in Private Schemes (OS4) and Planning Controls for Affordable Housing; and</li> <li>Chapter 5 explains planning incentives for development at areas of Transit Planning Zone (TPZ), Lot Amalgamation, Redevelopment and Affordable Housing (RMM).</li> </ol>
	<p><b>Part 2: Land Use Zoning and Intensity Map (KLDCP2024)</b></p> <ol style="list-style-type: none"> <li>Chapter 1 describes the introduction and method of use in Part 2; and</li> <li>Chapter 2 contains land use zones, development intensity, planning control layers and designated incentives area known as the Kuala Lumpur Development Control Plan 2024 (KLDCP2024).</li> </ol>
	<p><b>Part 3: Classes of Use of Land and Buildings 2024 (CULB2024)</b></p> <ol style="list-style-type: none"> <li>Chapter 1 explains the application of Classes of Use of Land and Buildings 2024 (CULB2024);</li> <li>Chapter 2 contains the interpretation of Classes of Use of Land and Buildings 2024 (CULB2024);</li> <li>Chapter 3 explains the classification of Classes of Use of Land and Buildings 2024 (CULB2024);</li> <li>Chapter 4 contains tables of General Classes of Use of Land and Buildings (General CULB);</li> <li>Chapter 5 contains tables of Special Classes of Use of Land and Buildings (Special CULB); and</li> <li>Chapter 6 is the descriptions of Classes of Use of Land and Buildings 2024 KLDCP2024 (CULB2024)</li> </ol>



Kuala Lumpur Development Control Plan 2024 is referred to as KLDCP2024 throughout this document. This plan translates various goals, planning strategies and implementation proposals of the Draft KLLP2040 in the aspects of spatial control and physical development. It contains detailed provision on aspects of development and land use in Kuala Lumpur as a guide for development planning by property owners, developers, investors, related agencies and society.

KLDCP2024 will also contribute to the growth and character of Kuala Lumpur through various provisions covering all aspects related to development in Kuala Lumpur. The provision includes measures that contribute towards sustainable built and natural environment, transportation, housing and social facilities well-being as well as creativity innovation in infrastructure and utilities as an effort to achieve Kuala Lumpur's goals for the next 20 years.

KLDCP2024 consists of several planning actions used to organise, control and manage physical development of land through development control processes and methods. Development of any land must refer to and comply with the detailed regulations and provisions in this document. In addition, applications for planning permission are also subject to meet the technical requirements of the KLCH and related agencies.

Based on KLDCP2024, applications for planning permission in the context of development plan must comply with the following five (5) main points:

1. Compliance with designated Land Use Zones;
2. Compliance with designated Development Intensity;
3. Compliance with Planning Control Layer requirements;
4. Compliance with the designated Development Incentives if relevant; and
5. Compliance with Classes of Use of Land and Buildings 2024 (CULB2024).

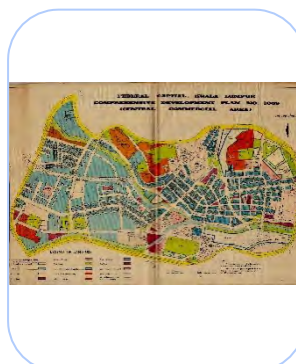
In addition, applications for planning permission must also comply with planning control policies and guidelines set by the Mayor from time to time.

The KLDCP2024 base map is based on the planning aspect where the determination of the actual lot boundaries is based on the land title deed.

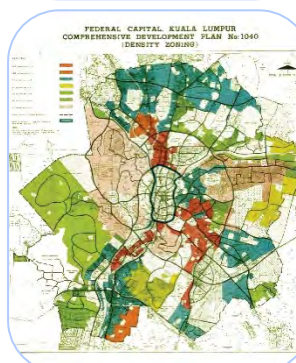
This plan replaces KLDCP 2015, KLCP 2020 and other plans replaced by KLDCP 2015 namely the Kuala Lumpur Comprehensive Development Plan (CDP) and the gazetted layout plan as well as other KLCH technical policies in the aspect of planning control. This includes:

1. CDP 1039 – Central Commercial Area;
2. CDP 1040 – Density Zoning;
3. CDP 1041 – Land Use Zoning;
4. Layout plans gazetted under the provisions of the Kuala Lumpur City (Planning) Act 1973 (Act 107) (refer to Appendix 1 for a list of gazetted layout plans); and
5. Federal Territories (Planning) (Zone and Density) Rules 1985.

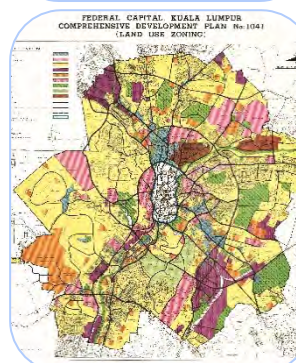
Nevertheless, documents 1 to 5 above including the approval of the development order in force continue to be the basis for determining development charges.



**Figure 1.6.1:**  
CDP 1039  
- Central Commercial Area



**Figure 1.6.2:**  
CDP 1040  
- Density Zoning



**Figure 1.6.3:**  
CDP 1041  
- Land Use Zoning

## COMPONENTS OF KUALA LUMPUR DEVELOPMENT CONTROL PLAN 2024 (KLDCP2024)

KLDCP2024 contains four (4) components consisting of two (2) maps, one (1) Planning Control Layer and one (1) designated incentives area to control the use and development intensity of the entire Kuala Lumpur area. The components are:

### 1. DCP1 - Land Use Zoning Map

DCP1 - Land Use Zoning Map controls the use of land and buildings in the Land Use Zoning Map (Figure 2.1.1, Chapter 2) which provides Land Use Classification information designated for each plot of land in Kuala Lumpur. This includes land zoned for residential, commercial, mixed development, industrial, institutional, cemetery, infrastructure and utilities, open space, community facilities and transportation.

The Land Use Zoning Map should be referred to concurrently with CULB2024 for details of the requirements and conditions of each application for planning permission. CULB2024 describes activities that are permissible, permissible with conditions and not permissible for each plot of land and building according to the designated land use zone.

### 2. DCP2 - Intensity Map

DCP2 - Intensity Map (Figure 3.1.1, Chapter 3) provides specific guidance on the development's intensity on a plot of land. The designated intensity is in two (2) forms, namely basic plot ratio and building height control.

Plot ratio controls are used for residential, commercial, mixed development, industrial and institutional land use zones. Meanwhile, for building height control is used for low-intensity residential, Traditional Village 1 (TV1) and commercial terraces.

The use of plot ratio or building height control is intended to:

1. Control the level of development at a site;
2. Promote equality in the planning of a site;
3. Enable variety of unit sizes that provide design flexibility to meet market and lifestyle needs;
4. Create livable and sustainable development; and
5. Create uniformity of typology and skyline with building height control.

### 3. Planning Control Layer

The Planning Control Layer is imposed in addition to land use zone control DCP1 - Land Use Zoning Map and intensity DCP2 - Intensity Map. This layer is important to ensure a quality of built environment, environmental protection and sensitivity of new development to the heritage buildings. The planning control layer is related to the following seven (7) aspects:

#### 1. Planning Control Layer of Layout Plan Plot Ratio

Refers to control over an area that has been granted plot ratio approval based on the development scheme.

#### 2. Planning Control Layer for Side Corridors, Lake and Pond

Refers to the control of all new development and redevelopment located adjacent to identified rivers, lakes and ponds.

#### 3. Planning Control Layer for Heritage Areas or Buildings

Refers to sites and buildings that must comply with the requirements and guidelines designated to conserve and maintain the special character in the Kuala Lumpur Heritage Zone.

#### 4. Planning Control Layer for Height Control Zone Surrounding The Istana Negara

Refers to building height control guidelines to preserve open vistas from and leading to Istana Negara.

#### 5. Planning Control Layer for Line of Sight Towards Landmarks

Refers to additional controls on the height, orientation and design of new buildings located within the visual line of sight to identified Kuala Lumpur landmarks.

#### 6. Planning Control Layer for Public Open Spaces in Private Schemes (OS4)

Refers to open spaces handed over to KLCH or specified through express condition in land title deed or through an undertaking agreement for public use and not only specific to the local community.

#### 7. Planning Control Layer for Affordable Housing (RMM)

Refers to redevelopment or new areas containing an affordable housing component that has been approved or agreed upon in principle.







# 2.0

## LAND USE ZONE





# 2.0 LAND USE ZONE

DRAFT KUALA LUMPUR LOCAL PLAN 2040

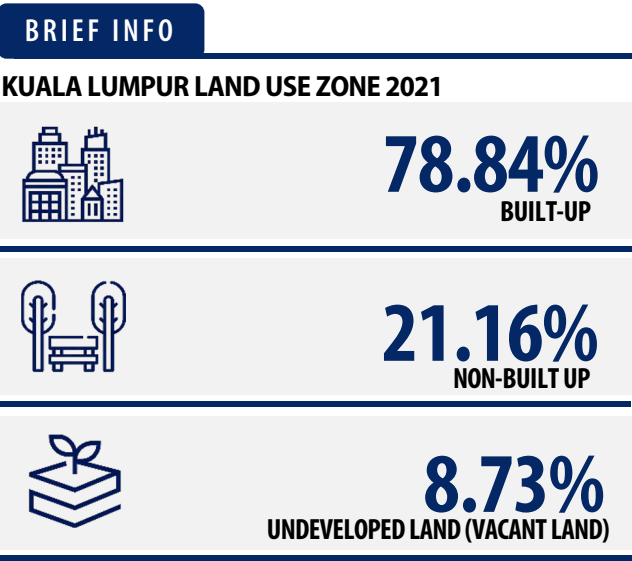
The Draft KLLP2040 has determined the land use zone for each plot of land in Kuala Lumpur with the aim to regulate land use, avoid land use conflicts and allow continuous development to take place in a sustainable manner.

Land use in Kuala Lumpur is classified into ten (10) main land uses which are further detailed into 25 land use zones shown on DCP1 - Land Use Zoning Map. The description and details of land use are outlined in Part 3, Classes of Use of Land and Buildings 2024 (CULB2024).

The determination of land use zones are based on 12 main aspects as shown in Table 2.1.1.



Aerial view of a rapidly developing Kuala Lumpur.



**Table 2.1.1:**  
**Aspects Considered in the Determination of Land Use Zones**

1	KLCP 2020 Land Use Zone
2	Addendum to KLCP 2020
3	Alteration 1, 2, 3 and 4 of KLCP 2020
4	Approval of land ownership by the Federal Territory Land Executive Committee or Issuance of Land Titles and Express Conditions of Land
5	Surrounding development approval trends
6	Current planning status of government lands, Mayor's land and decision of Federal Territory Land Executive Committee Meeting
7	Replanning of traditional villages, villages and Malay Reserve areas
8	Replanning of industrial areas
9	Replanning of Land Use Zone of Establish Residential (EH)
10	Verification of public facilities, institutional, open spaces, infrastructure and utilities sites during Technical Working Group (TWG) Session
11	Planning of development around new infrastructure including MRT3
12	Coordination of base map

## 2-1

## SUSTAINABLE LAND USE FOR KUALA LUMPUR

The Draft KLLP2040 emphasises sustainable land use management to meet present and future community's needs. KLDCP2024 contains a Land Use Zoning Map that illustrates the principles of sustainable land use as follows:

1. Strengthening the development of existing commercial and employment centres;
2. Meeting new economic needs that will improve Kuala Lumpur's position as a Global City;
3. Integrating land use development with the transport network, especially rail transport;
4. Conserving Special Area Management (SAM) Zone and disaster risk;
5. Encouraging infill development and redevelopment or rehabilitation of existing sites (brownfields) to be more economical;
6. Protecting building heritage areas and sites;
7. Improving physical environment in Malay Reserve area including Malay Agricultural Settlement (M.A.S) Kampong Bharu land, traditional villages, new villages and public housing areas;
8. Providing and protecting open spaces as well as public spaces;
9. Providing community facilities that promote fairness and social inclusion;
10. Integrating green infrastructure and utility with new development;
11. Conserving natural assets including water bodies and forest reserves;
12. Managing establish residential areas; and
13. Encouraging various types of housing development to accommodate the needs of all community groups.

## 2-2

## LAND USE ZONE AND PLANNING PERMISSIONS

All developments involving applications for planning permission must refer to Section 19 and Section 20 of the Federal Territories (Planning) Act 1982 (Act 267).

Section 19 of the Federal Territories (Planning) Act 1982 (Act 267) states that no one may carry out any development except in accordance with a development plan or a planning permission authorised by the Mayor.

Section 20 of the Federal Territories (Planning) Act 1982 (Act 267) states that work involving maintenance, repair or other changes in buildings without involving changes to the use of land or buildings, shape, height, floor area, changes that affect drainage and health systems or contravene any provision of the development plan does not require planning permission.

This also means that no planning permission will be granted if the proposed land use or activity is not permitted according to the land use zone as set out in DCP1 - Land Use Zoning Map in Volume 1: Part 2, Land Use Zoning and Intensity Map (KLDCP2024) and Volume 1: Part 3, Classes of Use of Land and Buildings 2024 (CULB2024) as stipulated in the Federal Territories (Planning) Act 1982 (Act 267).



Development outside Kuala Lumpur City Center.

DCP1 - Land Use Zoning Map (Figure 2.3.1) shows the distribution of 25 land use zones set in spatial form for the whole of Kuala Lumpur. This means that a plot of land can be developed only according to the designated land use zone. However, this right is granted only after the application for planning permission and approval is obtained. In contrast, the current use of the plot of land will apply until the application is made and approved or until an order is issued by the Kuala Lumpur's Mayor.

The designated land use zone is determined by its main function and use. In general, land use zone is divided into ten (10) main land use classifications as shown in Table 2.3.1. Meanwhile, Table 2.6.1 describes details of the 25 land use zones.

## BRIEF INFO

## LAND USE OF DRAFT KLLP2040



**10**  
MAIN LAND USE CLASSIFICATIONS



**25**  
LAND USE ZONES

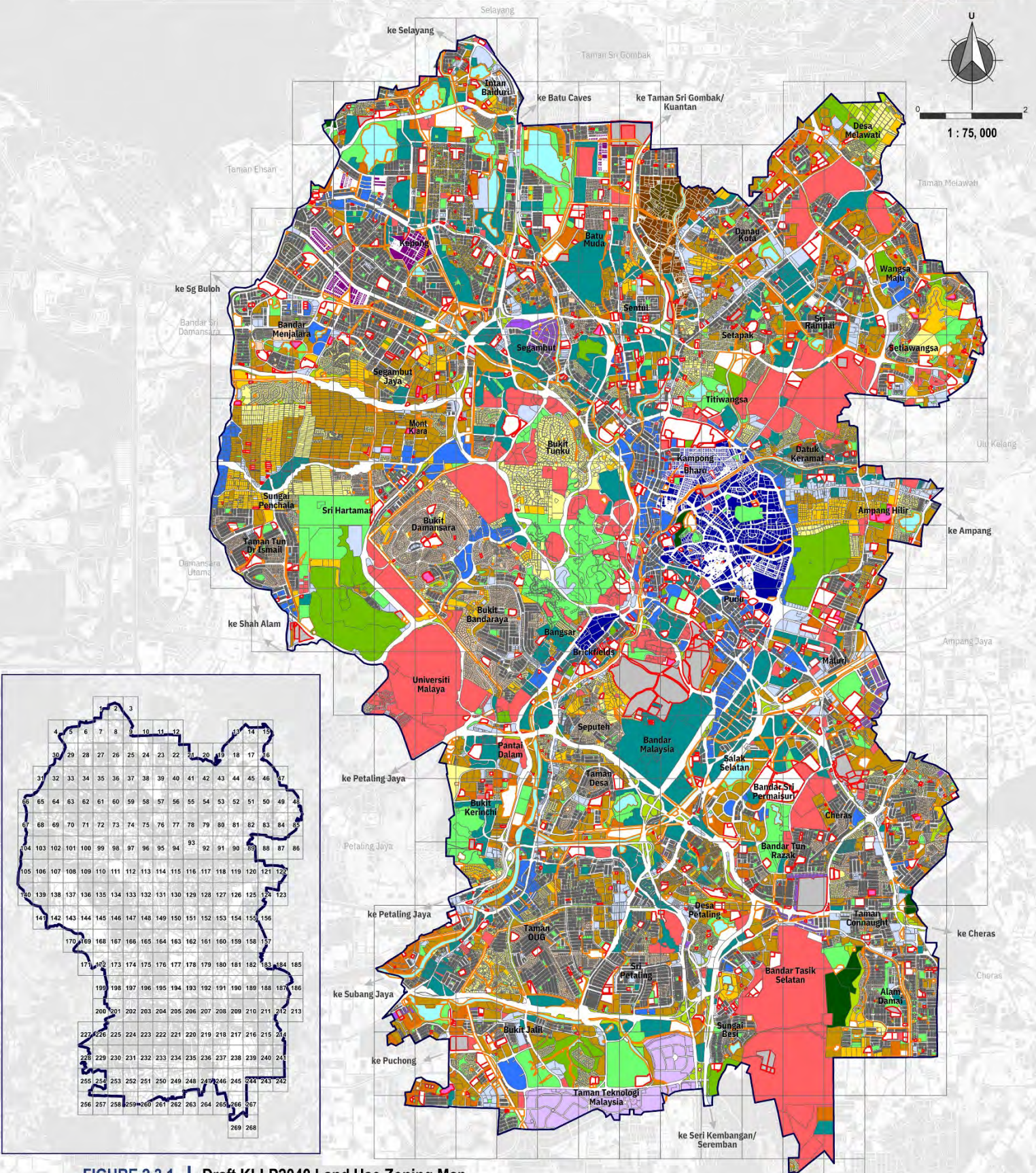
**Table 2.3.1: Classification of Land Use Zones in Kuala Lumpur**

Classification of Main Land Use	Land Use Zone	Abbreviation
1. Commercial	1. City Centre Commercial	CCC
	2. Major Commercial	MC
	3. Commercial	C
	4. Local Commercial	LC
2. Mixed Development	5. Mixed Development	MX
3. Industrial	6. Industry	IP
	7. Mixed Industry	MXI
	8. Technology Park	TP
4. Residential	9. Residential 1	R1
	10. Residential 2	R2
	11. Residential 3	R3
	12. Residential 4	R4
	13. Traditional Village 1	TV1
	14. Traditional Village 2	TV2
	15. Established Residential	EH
5. Institutional	16. Institution	INT
6. Community Facilities	17. Public Facilities	PF1
	18. Private Facilities	PF2
7. Cemetery and Mortuary Management	19. Cemeteries and Mortuary Management	CE
8. Open Space	20. Public Open Space 1	OS1
	21. Public Open Space 2	OS2
	22. Private Open Space	OS3
	23. Forest Reserve	FR
9. Infrastructure and Utilities	24. Infrastructure and Utilities	IU
10. Transportation	25. Transportation	TR









**FIGURE 2.3.1 | Draft KLLP2040 Land Use Zoning Map**

**Legend:**

**Commercial**

- City Centre Commercial (CCC)
- Major Commercial (MC)
- Commercial (C)
- Local Commercial (LC)

**Mixed Development**

- Mixed Development (MX)

**Industry**

- Industry (IP)
- Mixed Industry (MXI)
- Technology Park (TP)

**Residential**

- Residential 1 (R1)
- Residential 2 (R2)
- Residential 3 (R3)
- Residential 4 (R4)
- Established Residential (EH)
- Traditional Village 1 (TV1)
- Traditional Village 2 (TV2)

**Institutional**

- Institution (INT)

**Community Facilities**

- Public Facilities (PF 1)
- Private Facilities (PF 2)

**Cemetery**

- Cemetery and Mortuary Management (CE)

**Infrastructure and Utilities**

- Infrastructure and Utilities (IU)
- Water Bodies

**Open Space**

- Public Open Space 1 (OS1)
- Public Open Space 2 (OS2)
- Private Open Space (OS3)
- Forest Reserve (FR)

**Transportation**

- Transportation (TR)

**Others**

- Kuala Lumpur Boundary







In general, the principles for determining land use zones in the Draft KLLP2040 aim to prevent confusion during the implementation process and ensure that the zoning considers the actual and potential activities of related sites. The basic principles are as follows:

1. The replanning of land use zones considers factors of development potential, infrastructure improvements especially rail based, development pressure and new government policies and directions;
2. Coordination of the appropriateness of land use zones based on designated urban growth centre which are Primary Growth Centre, Secondary Growth Centre, District Growth Centre and Local Growth Centre;
3. The determination of Kuala Lumpur's planning concept for the next 20 years that focuses on the concentration of a compact city around transit stations. This will lead to the creation of a working, living and recreational community in an environment dependent on active mobility; and
4. Readjustment of land use zones such as Local Commercial (LC), Established Residential (EH), Traditional Village 1 (TV1), Traditional Village 2 (TV2), Residential 4 (R4), Private Facilities (PF2) and Public Open Space 2 (OS2) in line with current and future needs.



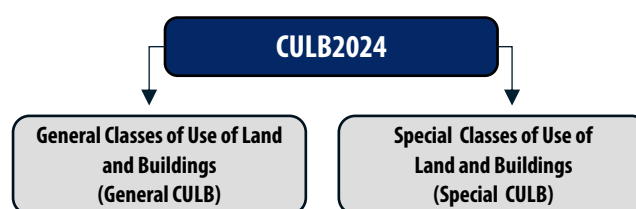
Location: Kampung Padang Balang

Distribution of housing in Kampung Padang Balang from an aerial view.

Land use zones are supported by a document of class of use that specifically designates the classes of use of land and buildings permissible with planning permission for each land use zone.

CULB2024 has nine (9) main categories of use of land and buildings and 67 designated classes of use. CULB2024 consists of two (2) main categories, namely the General Classes of Use of Land and Buildings (General CULB) and the Special Classes of Use of Land and Buildings (Special CULB) as shown in Figure 2.5.1.

**Figure 2.5.1:**  
**Component Classes of Use of Land and Buildings 2024 (CULB2024)**



General CULB involves 25 land use zones and Special CULB is categorised into two (2) as follows:

1. Specific Area Classes of Use of Land and Buildings (Specific Area CULB) involves nine (9) areas; and
2. Specific Village Classes of Use of Land and Buildings (Specific Village CULB) involves 25 villages.

DCP1 - This Land Use Zoning Map and CULB2024 should be referred to concurrently in determining the use of land and buildings, an ancillary or related activities that may be allowed to support or manage the main use.

CULB2024 is part of the development control and must be consistently referred to during planning approval process. CULB2024's regulation is based on the principle of encouraging appropriate or supporting use in the same zone to ensure more efficient and sustainable land use can be provided.

CULB2024 is related to the list of permissible uses and activities for the main uses in the land use zone. These uses and activities are defined as classes of activities that are compatible uses and activities that are not in conflict with the purpose and goals of the land use zone. In general, CULB2024 describes activities that are compatible with each other and have the same impact.

Land use zones, planning control layer and designated incentives area are shown on the Land Use Zone Map for each plot of land in Kuala Lumpur. An index map is provided in Volume 1: Part 2, Land Use Zone and Intensity Map (KLDGP2024) according to index for more detailed reference. The classification and detailed interpretation of land use zones are shown in Table 2.6.1 and 2.6.2.

Table 2.6.1:

Classification and Interpretation of Types of Land Use Zones in Kuala Lumpur

Land Use Zone	Abbreviation	Definition
<b>A. Commercial</b>		
1. City Centre Commercial	CCC	Refers to the highest hierarchy of commercial land use zones that serves as a centre for <b>financial, prime commercial, services and main employment</b> .
2. Major Commercial	MC	Refers to the commercial land use zone that serves as a <b>major commercial centre</b> that supports the City Centre Commercial Land Use Zone (CCC).
3. Commercial	C	Refers to commercial areas that are generally located on the main streets and at the centre of <b>major neighbourhoods</b> .
4. Local Commercial	LC	Refers to commercial land use zones that serves as <b>local service centres in residential areas</b> .
<b>B. Mixed Development</b>		
5. Mixed Development	MX	Refers to a <b>mixed</b> land use zone that <b>allows the commercial components</b> to support the basic use of residing component in a development.  Residing components, including residential units, quarters and serviced apartments, shall <b>not be less than 60 percent</b> of the total floor area.
<b>C. Industrial</b>		
6. Industry	IP	Refers to a land use zone for <b>manufacturing, production, services, research and related activities</b> . <b>Commercial activities that support major industrial activities</b> such as offices and promotional spaces not exceeding <b>20 percent</b> of the total floor area are <b>permissible</b> .
7. Mixed Industry	MXI	Refers to an industrial land use zone that allows commercial activities wherein <b>commercial activities shall not exceed 50 percent</b> of the total floor area. Residential activities except quarters and hostel as well as residential characterised commercial activities especially service apartment, SOHO and hotels are not permissible.
8. Technology Park	TP	Refers to a land use zone for <b>high-tech industries</b> , mainly focuses on <b>research and development (R&amp;D)</b> activities as well as other <b>supporting activities</b> that are regulated by agencies appointed by the government.  <b>Commercial activities and residing in Land Use Zone of Technology Park not exceeding 10 percent</b> of the total floor area is <b>permissible</b> .
<b>D. Residential</b>		
9. Residential 1	R1	Refers to a low-intensity residential zone, generally consisting of <b>landed residential with single ownerships</b> , such as detached houses, semi-detached houses and terrace houses <b>not exceeding 3 storeys</b> including landed strata with control height of 4 levels based on guidelines provision.  The existing residential building typology must be retained and harmonised with adjacent developments. Any changes in the building typology are permissible and considerable if it is compatible with the surrounding development planning and does not affect the capacity of infrastructure and public facilities capacity.

Table 2.6.1: Classification and Interpretation of Types of Land Use Zone in Kuala Lumpur (continued)

Land Use Zone	Abbreviation	Definition
<b>D. Residential</b>		
10. Residential 2	R2	Refers to a <b>medium-intensity residential</b> zone, generally consisting of multi-storey dwellings, multi-storey strata and town houses with a base plot ratio of up to 1:2.5.
11. Residential 3	R3	Refers to a <b>high-intensity residential</b> zone, generally consisting of multi-storey dwellings with a base plot ratio of up to 1:4.0.
12. Residential 4	R4	Refers to a specific residential land use zone for <b>affordable housing, including public housing</b> . Generally the base plot ratio for R4 is up to 1:6.0 with multi-storey residential typology.
13. Traditional Village 1	TV1	Refers to land use zone in <b>a part of Kampung Padang Balang only</b> that preserves the village characteristic and setting consisting of <b>detached or semi-detached houses not exceeding 3 storeys</b> with controlled of <b>Traditional Malay House design</b> .
14. Traditional Village 2	TV2	Refers to the residential land use zone in a village setting areas with various residential characteristics for part of villages within <b>Gombak Malay Reserve Area (MRA) only</b> , namely a part of Kampung Padang Balang, a part of Kampung Sungai Mulia, Kampung Banda Dalam, a part of Kampung Sungai Merali, a part of Kampung Puah Asal and a part of Kampung Chubadak Hulu.
15. Established Residential	EH	Refers to the land use zone of a well-planned <b>landed residential area with low intensity</b> , consisting of detached, semi-detached and terrace houses, of <b>good quality</b> and condition with complete provision of facilities and infrastructure that <b>will not experience drastic typological changes</b> during the development plan period.
<b>E. Institutional</b>		
16. Institution	INT	Refers to land use zone designated for the uses of <b>cultural and civic activities, as well as government and semi-government facilities</b> such as palaces, museums, galleries or memorials, courts, cultural centres, libraries, universities, colleges, training centres, research centres, military camps, security agency headquarters, care and rehabilitation centres, homeless transit centres, and other government-related offices as well as allow residential development in accordance with any requirement by the government as well as subject to planning suitability including socioeconomic benefits. These institutional activities are usually <b>owned or facilitated by the government, statutory bodies or non-governmental bodies</b> .
<b>F. Community Facilities</b>		
17. Public Facilities	PF1	Refers to land use zone designated for <b>educational, health, safety and emergency, and well-being facilities for the general public or local communities</b> as well as for other supporting activities (as ancillary uses) that require such facilities. Provision of PF1 facilities is based on catchment areas and owned by a government or statutory body as well as could be managed by private parties.
18. Private Facilities	PF2	Refers to the land use zone designated generally in the <b>development scheme's layout plan for the provision of community facilities and facilities</b> related to private education. Private entities can own, operate, and manage these facilities.
<b>G. Cemetery and Mortuary Management</b>		
19. Cemeteries and Mortuary Management	CE	Refers to the land use zone designated for the <b>management of Muslim and non-Muslim burials</b> . The cemetery area includes Muslim, Chinese, Hindu, Christian and other cemeteries.

**Table 2.6.1:**  
**Classification and Interpretation of Types of Land Use Zone in Kuala Lumpur (continued)**

Land Use Zone	Abbreviation	Definition
<b>H. Open Space</b>		
20. Public Open Space 1	OS1	Refers to the land use zone of an open space for <b>recreational activities such as leisure, games, sports or cultural activities</b> . OS1 includes recreational parks and open spaces surrendered by private development scheme or government bodies for <b>public use</b> as open space managed or maintained by KLCH or other relevant entities for public usage.  Activities or building's structure that support open space activities such as management office, food and beverage as well as facilities <b>shall not exceed 30 percents</b> from total open space area is permissible and subject to suitability and area of the plot without affected the function of particular open space.
21. Public Open Space 2	OS2	Refers to the land use zone of an open space that serves <b>various purposes such as nature recreational activities, buffer zone, suitable roundabout or traffic island, linear green area, including development excess areas that is used as green areas</b> such as slopes, and small land spaces in between developments including those surrender by private parties. OS2 can be managed or maintained by KLCH or other relevant entities.
22. Private Open Space	OS3	Refers to the land use zone of open space for <b>recreational activities such as park, playground, recreation ground, sports ground, golf course or cultural activities that are privately owned or privately managed</b> , wherein the public has <b>limited access</b> except with authorisation of usage. Development for the purpose of socio-economic benefits (commercial or residential component) for part of the site is subject to suitability involving aspects of site size, location, environment, and capacity of an area without affecting its original functionality except for sites that exist due to planning requirements.
23. Forest Reserve	FR	Refers to the land use zone of a <b>gazetted</b> forest reserve and forested areas managed or maintained by the relevant public authority. Activities or structure such as gazebo, kiosk and ancillary facilities need to be designed with suitable scale without affecting flora and fauna.
<b>I. Infrastructure and Utilities</b>		
24. Infrastructure and Utilities	IU	Refers to the land use and building intended for the provision of infrastructure and utilities such as <b>water supply, energy supply (electricity, gas, cooling systems and others), sewerage, telecommunications, drainage (irrigations, rivers, lakes and detention or retention pond), solid waste disposal</b> and other related infrastructure and utilities.
<b>J. Transportation</b>		
25. Transportation	TR	Refers to the land use zone designated for the provision of transportation facilities including <b>public transportation terminals, public transportation stations, Park and Ride facilities, parking facilities, transportation depots, road reserves, rail lines</b> and other related facilities permissible to support the transportation infrastructure system and services.

**Table 2.6.2:**  
**Planning Control Layer, Public Open Space within Private Scheme (OS4)**

Planning Control Layer	Abbreviation	Definition
<b>Open Space</b>		
*Public Open Space within Private Scheme	OS4	A multipurpose public open space located at ground level or podium and accessible to the public. OS4 <b>is only shown as a planning control layer in land use zone map</b> . OS4 is surrendered to KLCH through strata parcels or accessory parcels or specified through express conditions in the land title deed or determined through an undertaking agreement. The area of OS4 is not counted in the calculation of land use zones but is considered in the preparation of open space requirements.



## SPECIAL IMPLEMENTATION PROVISION FOR LAND USE ZONE OF MIXED DEVELOPMENT (MX)

Mixed development refers to development that permits more than one use or activity for a building or a group of buildings in a plot or lot of a developed land. Permitted uses or activities must be compatible with each other.

The Draft PTKL2040 designates the Land Use Zone of Mixed Development (MX) as a land use zone that permits commercial components to support the residing element in a development.

Land Use Zone of Mixed Development (MX) in Kuala Lumpur stipulates development with 60 percent of residing elements. However, the drastic provision of residential components for small-sized lots and industrial areas zoned as Land Use Zones of Mixed Development (MX) is complicated after taking into account the current feasibility, location suitability, social and environmental aspects.

This designation also aims to redevelop old and dilapidated areas to become more vibrant. It also facilitates the conversion of existing industrial areas to mixed development through provision of commercial elements at industrial areas that are eventually completed with residing elements.

The Draft KLLP2040 determines development in the Land Use Zone of Mixed Development (MX) as follows:

1. Development of not less than 60 percent of the total floor area as residing elements such as residential units, quarters and service apartments. However, hotel activities are not included as residing element; and
2. Development of not exceeding 40 percent of the total floor area as a commercial element such as offices, financial institutions, hotels and shopping centres.

### 2-7.1 Special Implementation For Land Use Zone of Mixed Development (MX)

The implementation provisions for Land Use Zone of Mixed Development (MX) are as follows:

1. Small-sized lots that are originally residential areas (less than 20,000 square feet) are exempted from providing residing elements subject to intensity control. Additionally, in order to avoid conflict with current activities, permitted commercial activities also need to be controlled. The control of commercial activities as shown in Table 2.7.1; or

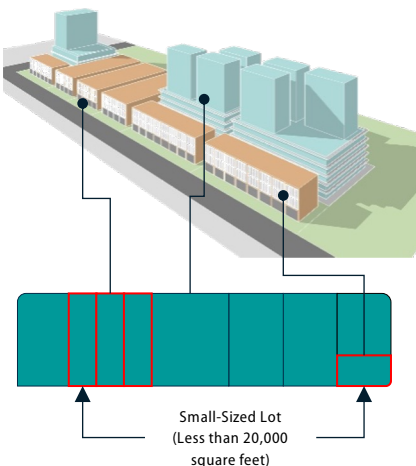
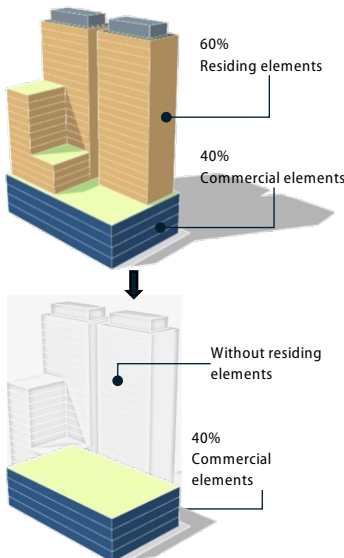
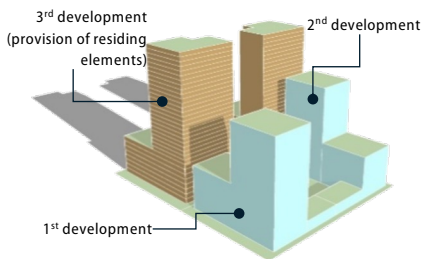
**Table 2.7.1:**  
**Control of Commercial Activities in Land Use Zones of Mixed Development (MX) for Lots Size Less Than 20,000 Square Feet**

CULB2024	CONDITION
A1 – General Retail	<ul style="list-style-type: none"><li>• Sale, display and storage of goods outside the building are not permissible.</li><li>• Shall not cause of nuisance and traffic congestion to the surrounding area.</li><li>• Limited only to accommodation (only applicable for Class A9 – Tourist Accommodation)</li><li>• Event activities that generate traffic or visitors in high capacity are not encouraged (only applicable for Class A18 – Exhibition Centre)</li></ul>
A2 – Business	
A3 – Office	
A4 – Financial Institution	
A7 – Health Services	
A9 – Tourist Accommodation	
A18 – Exhibition Centre	
<ul style="list-style-type: none"><li>• Classes of A5, A6, A8, A10 up to A17, A19 and A20 are not permissible.</li><li>• Other categories of Main Land Use Class shall refer to General CULB2024.</li></ul>	

2. Large-sized lots (over 20,000 square feet) are exempted from providing residing elements subject to not exceeding 40 percent of the maximum total floor area, including adoption of incentives; or
3. Lots designated as Land Use Zone of Mixed Development (MX) in active industrial areas, the exemption of residing elements may be allowed up to 50 percent of the industrial area being developed with commercial elements. If the development has reached 50 percent of the whole area, the next development must provide a residing elements. This provision does not apply to items 1 and 2.

## BRIEF INFO

## SPECIAL IMPLEMENTATION SCENARIO OF MIXED DEVELOPMENT LAND USE ZONE (MX)

Item 1	Item 2	Item 3
 <p>Small-Sized Lot (Less than 20,000 square feet)</p>	 <p>60% Residing elements 40% Commercial elements</p> <p>Without residing elements 40% Commercial elements</p>	 <p>3<sup>rd</sup> development (provision of residing elements)</p> <p>2<sup>nd</sup> development</p> <p>1<sup>st</sup> development</p> <div style="border: 1px dashed red; padding: 5px; margin-top: 10px;"> <p>The first 5 acres not subject to the provision of residing elements</p> <p>The next 5 acres need to provide residing elements not less than 60% GFA</p> <p><b>Site area: 10 acres</b></p> </div>
<p><b>Small-Sized Lot:</b> Lot size: Less than 20,000 square feet for originally residential area</p>	<p><b>Large-Sized Lot:</b> Lot size: Exceeding 20,000 square feet</p>	<p>Applicable for originally industrial area</p>
<p><b>Special Provision:</b> Exemption from the provision of residing elements</p>	<p><b>Special Provision:</b> Exemption from the provision of residing elements is subject to a maximum use of 40 percent of total floor space</p>	<p><b>Special Provision:</b></p> <ul style="list-style-type: none"> <li>The first 50 percent of industrial area is exempted from the provision of residing elements</li> <li>The next 50 percent of industrial area must provide residing elements</li> </ul>
<p><b>Height Control:</b> Not exceeding 3 storeys</p>	<p><b>Example of Calculation:</b></p> <p>Site area is 2 acres</p> <p>Plot Ratio including incentives is 1:6</p> <p>Maximum 40 percent GFA utilisation (exemption of residing element)</p> <p><b>Calculation Steps</b></p> <p>Plot area x 43,560 square feet x Plot Ratio x 40% maximum GFA = 2 acres x 43,560 sqft x 6 x 40% = <b>209,088 square feet</b></p> <p>If the commercial GFA exceeds 40 percent of the total floor space, residing element must be provided.</p>	<p><b>Example of Implementation:</b></p> <p>Site area is 10 acres</p> <p>First Development Order = Area of 2 acres</p> <p>Second Development Order = Area of 3 acres</p> <p>Third Development Order = Area of 5 acres</p> <p>The First and Second Development Orders are <b>not subject to</b> the provision of residing elements.</p> <p>The Third Development Order is <b>subject to</b> the provision of residing elements.</p>
<p>Permissible commercial activities refer to Table 2.7.1.</p>		

## BRIEF INFO

## INTEGRATED DEVELOPMENT CONCEPT

The concept of integrated development promotes a more integrated form of development where activities such as living and working spaces are combined. This concept is important to ensure that both commercial activities and residential activities are planned by considering needs of the main and supporting activities.

Integrated development refers to the combination of related retail or office or commercial or service or residential uses in the same building or site are as follows:

### 1. Vertical Mixed Use Development

One (1) single building structure in a designated plot of land with the upper floor used for residential activities or office use and part of the lower floor for retail or trade or service use.

### 2. Horizontally Mixed Use Development (Connected)

A combination of two (2) or more building structures in a designated plot of land that provides retail or commercial or service use and connected (physically or accessibility) to residing activity or office use.

### 3. Horizontal Mixed Use Development (Separate)

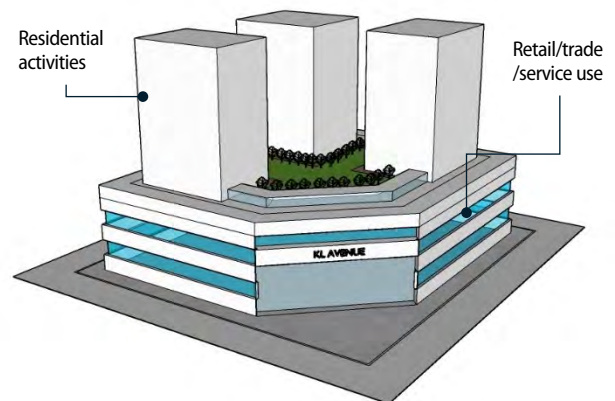
Two (2) or more building structures on a designated plot of land that provide retail, commercial or service uses and residing element physically separated.

The purposes of this zone are as follows:

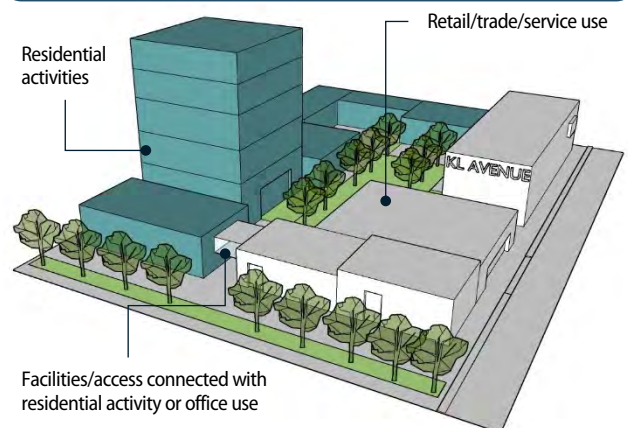
1. To encourage shared use of residential activities with commercial and other appropriate activities to create an active street atmosphere, enhance the viability of business and reduce vehicular traffic;
2. The elements of residential, commercial, non- residential uses and ancillary uses should be provided in such a way for it to be interdependent, integrated and complementary to each other;
3. To encourage development intensification in identified transit planning zones;
4. To provide suitable locations for high density residential use to be integrated with the overall mixed development; and
5. To encourage compatible use of activities near each other. It is also important to ensure mixed development applications for large plots or adjacent plots to meet the planning objectives of zoning and urban design. This includes consideration for the distribution of commercial space in a mixed development between predominantly residential activities. Preferably, commercial floor space must be located fronting the main road and at ground level to encourage street activity and public surveillance.

Any development with an integrated development concept must comply with the land use zone and CULB2024 as well as meeting the needs of community facilities at the development site. In appropriate locations, KLCH can impose conditions for the provision of community facilities to meet the needs of residents and surrounding population such as public libraries and sports facilities. The provision of green areas and larger open spaces can be imposed for large-scale mixed plot development to accommodate the needs of local community.

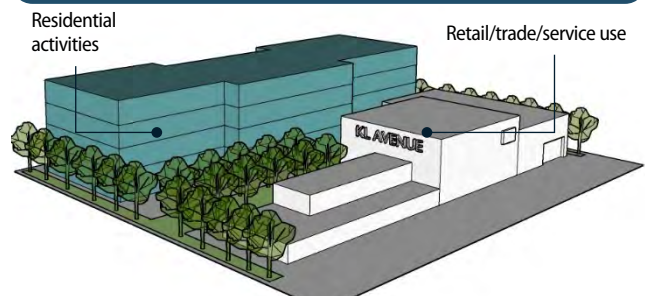
#### Vertical Mixed Use Development



#### Horizontally Mixed Use Development (Connected)



#### Horizontal Mixed Use Development (Separate)







# 3.0

## DEVELOPMENT INTENSITY





# 3.0 DEVELOPMENT INTENSITY

## DRAFT KUALA LUMPUR LOCAL PLAN 2040

The intensity of development can be defined as the capacity of land plot usage for a particular development. Determination of effective land plot capacity must be determined to ensure that development of Kuala Lumpur in the future is controlled.

Control of development intensity for urban activities in Kuala Lumpur is important to achieve a sustainable built environment. Planning will be regulated to ensure the provision of community and infrastructure facilities are capable to support growth and development as well as protecting the environment. The permitted intensity is also subject to compliance with the planning guidelines and incentives set by KLCH.

A development that has been granted an approval of Development Order (DO) with an intensity exceeding DCP2 - Intensity Map before gazettement, then the approval is applicable as the permitted intensity for the site. This intensity should not be used as a basis for the purpose of giving incentives in the Draft KLLP2040. Nevertheless, the approval must be consistent in terms of land use zones as set out in DCP1 - Land Use Map. The intensity set in DCP2 - Intensity Map must be adhered to and is subject to any designated incentives for a development that has been granted after the gazettement of the Draft KLLP2040.

The intensity of development in Kuala Lumpur will be controlled in two (2) forms as follows:

1. Plot ratio control for development of residential, commercial, mixed development, institutional and industrial; and
2. Height control for building of low intensity residential, Traditional Village 1 (TV1) and commercial terrace.

The determination of the intensity is subject to compliance with the overall technical requirements, policies and current guidelines set by KLCH. In circumstances where certain sites are not able to meet the requirements, a lower intensity should be considered at the planning permission stage. In addition, a development with a lower intensity than the designated in DCP2 - Intensity Map by owner intention is allowed.

### BRIEF INFO

#### DEVELOPMENT INTENSITY OF DRAFT KLLP2040



**Plot ratio control** for development of residential, commercial, mixed development, institutional and industrial



**Height control** for building of low intensity residential, Traditional Village 1 (TV1) and commercial terrace



Development of Fintech and Banking Zone located in Kuala Lumpur City Centre.



Mixed development around Setapak, Kuala Lumpur.

## DETERMINATION PRINCIPLES OF KLDCP2024 DEVELOPMENT INTENSITY

Development intensity determination is an important basis in controlling total floor area that directly affects the needs of community facilities, infrastructure and utilities, traffic generation and road capacity. The development intensity is subject to the conditions of compliance with the technical requirements that have been outlined.

The basic principles in determining the development intensity of KLDCP2024 are as follows:

1. Determination of the intensity is subject to site suitability factors (location, physical condition, accessibility, availability of community facilities, infrastructure and utility as well as compatibility with the surrounding area);
2. Use of Base Plot Ratio and Height Control as intensity control tools in KLDCP2024;
3. Determination of commercial development intensity for the first layer land plot along the main road considers the appropriateness of the lot size and improvement as well as capacity of the road infrastructure but excluding commercial terrace buildings as shown in Table 3.1.1. Nevertheless, the actual plot ratio setting for each plot along the main road is as shown in DCP2 - Intensity Map;
4. Determination of height control for commercial terrace buildings creates a uniform skyline and controls the typology in accordance with capacity of the infrastructure provided;
5. Changes in demographic trends of population such as percentage increase of elderly, DINK (Double Income No Kids) or YUPPIES (Young Urban Professionals) in Kuala Lumpur as well as purchasing power which affect the demand and supply of small houses including affordable housing;
6. Height control of low-intensity development which involve Land Use Zones of Residential 1 (R1), Traditional Village 1 (TV1) and Established Residential (EH);
7. Determination of Layout Plan Plot Ratio of one (1) overall development plot is shown with one (1) plot ratio only;
8. Replanning of the basic plot ratio of the petrol station with the surrounding development;

9. Requirement to provide detailed guidelines for low intensity residential in sloped areas with height control of three (3) storeys based on the height of each cluster viewed from all directions; and
10. If the landowner intends to use the Development Order (DO) approval based on density Person Per Acre (PPA) with higher floor area than the determination plot ratio of Draft KLLP2040, the intensity control for the plot is still based on density. However, if the owner intends to use intensity control based on plot ratio, it needs to comply with the designated plot ratio as well as incentives allowed based on the Draft KLLP2040.

Any development or existing land plot approved before the gazettment of Draft KLLP2040 that exceeds the basic plot ratio of the Draft KLLP2040 is calculated as the plot ratio set for the land plot. However, there are sites that have been approved with a higher plot ratio exceeding the Draft KLLP2040 will considers additional commitments of the development at that time such as infrastructure improvement, provision of affordable housing or contribution of community facilities outside the development site and other aspects of consideration during approval given. Nevertheless, this practice will no longer be continued in line with the preparation of the Draft KLLP2040 which takes into consideration the best planning practices.

**Table 3.1.1:**  
**Determination of Commercial Land Use Intensity in Specific Locations**

Plot Ratio	Location
Plot Ratio 1:8	1. Jalan Tun Razak
Plot Ratio 1:6	1. Jalan Klang Lama 2. Jalan Ampang 3. Jalan Genting Klang
Plot Ratio 1:5	1. Jalan Cheras
Plot Ratio 1:4	1. Jalan Puchong 2. Jalan Kepong 3. Jalan Ipoh 4. Jalan Syed Putra 5. Jalan Kuchai Lama 6. Jalan Gombak 7. Jalan Pahang

Note: The setting of this intensity principle is in general, different intensities are subject to planning suitability by considering location, physical condition, accessibility, availability of community facilities, infrastructure and utility as well as compatibility with the surrounding area.



The plot ratio is the relationship between an area of the site and the total floor area of the building on the site. Plot ratio control considers the floor area requirement limits imposed for land use zone of residential, commercial, mixed development, institutional and industrial.

The need to determine the plot ratio for land development in Kuala Lumpur are as follows:

1. To create balanced control between one development and another;
2. To ensure flexibility in development to achieve comprehensive development;
3. To determine the intensity of sustainable development that can prevent the negative effects of excessive development;
4. To create function and character that is required in the commercial zone of Kuala Lumpur;
5. To encourage generation of optimum activities to support city services such as public transport, community facilities and others;
6. To ensure overall objective of green urban architecture and design in Kuala Lumpur is achieved;
7. To meet the trend of small sized family where the current household size is 3.5 and estimated to be 3.0 by 2040;
8. To meet the increasing trend of population in Kuala Lumpur which is DINK, YUPPIES and senior citizens which shows a change in the demand for smaller residential sizes; and
9. To increase development viability whereby the plot ratio method allows more housing units to be built, especially if the unit size is smaller compared to the density method in the same site.

### 3-2.1 Plot Ratio Definition

Plot ratio means the ratio between the area of the floor space of the building and the area of the building plot as defined in Section 2 of the Federal Territories (Planning) Act 1982 (Act 267).

Intensity in KLDCP2024 is governed by the ratio of the base plot as set to the plot of land or lot for development. An increase in intensity can be subject to the incentives given but shall not exceed appropriate percentage of the basic plot ratio except for incentives for lot amalgamation and affordable housing development.

### 3-2.2 Use of Plot Ratio

The use of plot ratio as planning control measure for land use zones as per Table 3.2.1 and the absolute determination of use is obtained only after Development Order (DO) is granted.

This planning control applies to new development proposal and existing developments that involve the addition of building proposal or increase in floor area. Any development or existing land plot that has been approved in excess of the basic plot ratio allowed is calculated as the maximum plot ratio applicable to the land. However, this approved plot ratio shall not be considered as a basic plot ratio for the purpose of granting incentives in the Draft KLLP2040. If the base plot ratio, along with the incentive, exceeds the approved plot ratio, the plot is eligible for consideration with the maximum plot ratio set.

### 3-2.3 Determination of Plot Ratio

The determination of plot ratio is related to the land use strategy identified in the Draft KLLP2040. The land use strategy of Draft KLLP2040 which promotes sustainable and optimal land use to support the role and vision of Kuala Lumpur as a City For All.

Plot ratio control should consider the following aspects as follows:

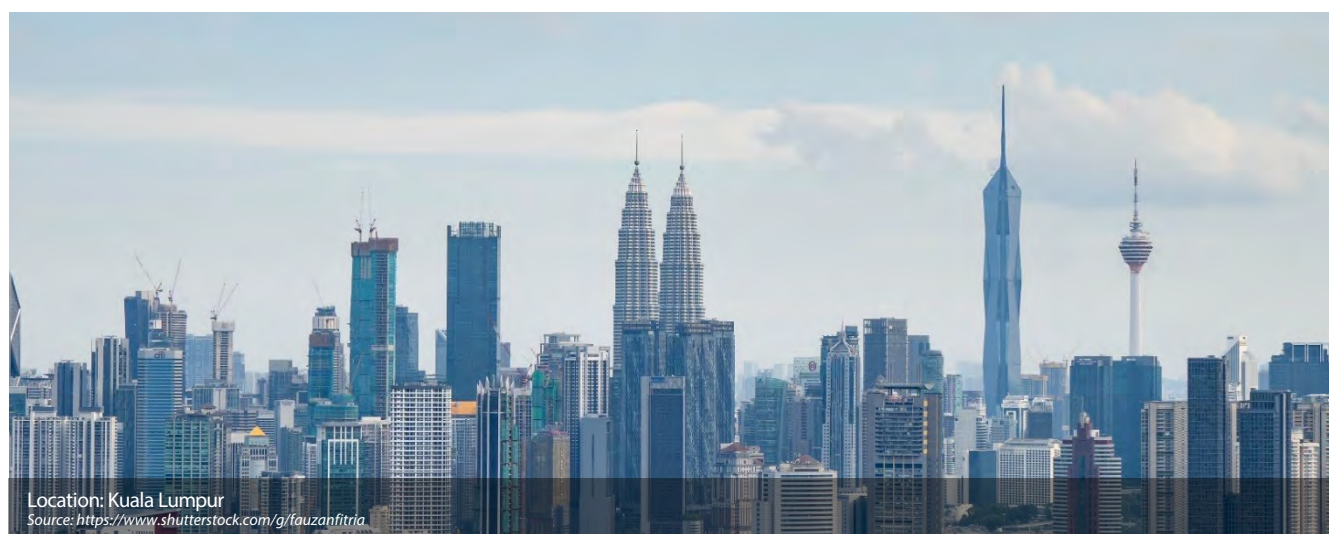
1. An increase in plot ratio is allowed above the basic plot ratio subject to incentives allocated based on Section 3 of the Federal Territories (Planning) Act, 1982 (Act 267) which is set during consideration of the Development Order (DO) approval. However, the determination of an additional plot ratio at a suitable minimum rate can be considered at the planning control level, subject to design factors that do not involve material changes; and
2. Development plots that use approved Layout Plan Plot Ratio can be redistributed subject to the principles outlined in the approved Layout Plan Plot Ratio.

**Table 3.2.1:**  
**Plot Ratio Control According to Land Use Zone**

Land Use Zone	Abbreviation	Basic Plot Ratio
City Centre Commercial	CCC	Up to 1:10
Major Commercial	MC	Up to 1:8
Commercial	C	Up to 1:8
Local Commercial	LC	Up to 1:2
Mixed Development	MX	Up to 1:8
Industrial	IP	Up to 1:4
Mixed Industry	MXI	Up to 1:4
Technology Park	TP	Up to 1:3
Residential 2	R2	Up to 1:2.5
Residential 3	R3	Up to 1:4
Residential 4	R4	Up to 1:6
Traditional Village 2	TV2	Up to 1:2
Institutional	INT	Up to 1:4

**Notes:**

1. Plot ratio does not apply to the Land Use Zone of Residential 1 (R1), Established Residential (EH), Traditional Village 1 (TV1), Open Space (OS1 to OS3), Community Facilities (PF1 to PF2), Cemetery and Mortuary Management (CE) and Transportation (TR) as well as Infrastructure and Utilities (IU). However, for the Land Use Zone of Community Facilities (PF1 to PF2), Cemetery and Mortuary Management (CE), Transportation (TR) and Infrastructure and Utilities (IU), development intensity is permitted if developed with building structures that are intended to support the particular land use zone or in accordance with CULB2024 and are compatible with surrounding development.
2. Determination of the actual plot ratio according to each plot should refer to DCP2 – Intensity Map;
3. Refer to Classes of Use of Land and Buildings 2024 (CULB2024) for Opens Space Land Use Zone (OS1 to OS3) for development of permitted building structure;
4. Height control applies to commercial terrace buildings and Land Use Zone of Residential 1 (R1), Traditional Village 1 (TV1) and Established Residential (EH); and
5. Layout Plan Plot Ratio applies to Land Use Zone of Technology Park (TP) and other plots that have been designated in the Draft KLLP2040.



Development in the city centre of Kuala Lumpur with skyscrapers seen from an aerial view.



## BRIEF INFO

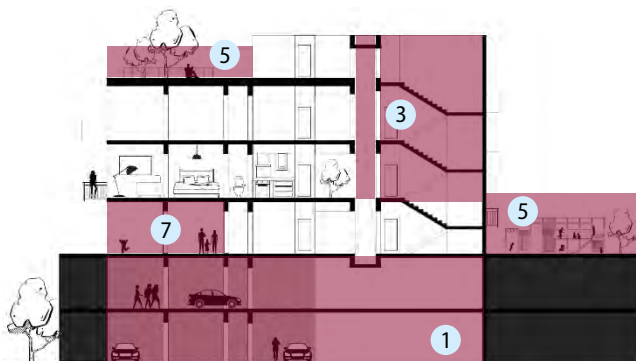
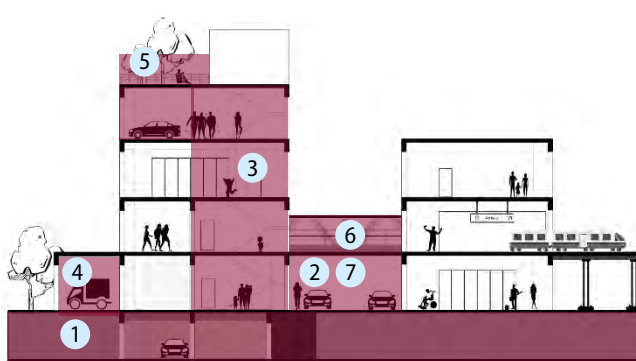
## REQUIREMENT OF FLOOR AREA

The Federal Territories (Planning) Act 1982 (Act 267) states that floor area is the total area of the floor space in a building measured from the outer wall and for shared walls, it is measured from the middle of the shared wall.

Gross floor area (GFA) is calculated for the purpose of determining plot ratio calculations. The Draft KLLP2040 stipulates that the GFA calculation method for commercial and residential buildings is similar. Its calculation and

determination generally takes into consideration saleable spaces and spaces for private use. Exemption from the GFA calculation generally refer to parking space, circulation, open space on the roof, circulation area, service area (corridor from the lift) or utilities and community facilities that are required or not required by KLCH and surrendered to the management body or regulatory agency.

**Figure 3.2.1: GFA Calculation for Commercial and Residential Buildings**

Floor Area Calculation	Illustration
<p><b>GFA includes:</b></p> <ol style="list-style-type: none"> <li>1. Saleable or lettable area, which refers to residential units or commercial space including accessory parcels excluding parking space;</li> <li>2. Corridors and covered passages that are not for public passage including lift lobbies;</li> <li>3. Open or semi-open spaces that are saleable or lettable areas such as air-conditioned ledge (subject to size of space), drying area, balcony;</li> <li>4. Support area that are not surrendered to management bodies such as gym, spa, laundry, canteen, restaurant, cafeteria, kitchen and bathroom; and</li> <li>5. Mechanical, service or utility rooms such as Air Handling Unit (AHU) room, riser room, motor room and water tank on a level other than the parking level.</li> </ol> <p><b>GFA does not include:</b></p> <ol style="list-style-type: none"> <li>1. Parking and circulation including any mechanical, electrical space in the parking area of the building;</li> <li>2. Covered or uncovered open parking area outside the building;</li> <li>3. Stairwells and lift shafts on levels other than the ground floor or lobby;</li> <li>4. Waiting area for commercial vehicles to unload goods;</li> <li>5. Garden spaces or residents' facilities provided at the roof or podium level in the open or semi-open setting;</li> <li>6. Pedestrian walkways connected to buildings or transit stations including any supporting activities;</li> <li>7. Pedestrian walkways within the building that serve as public walkways; and</li> <li>8. Floor space for hawker stalls, managed and operated by Joint Management Body (JMB) or Management Cooperation (MC) according to requirements of KLCH.</li> </ol> <p>KLCH deserves the right to determine the floor space status for spaces that are not included in the above list and should there be new requirements from time to time.</p>	 <p>Examples of GFA exemptions for residential buildings</p>  <p>Examples of GFA exemptions for commercial buildings</p>

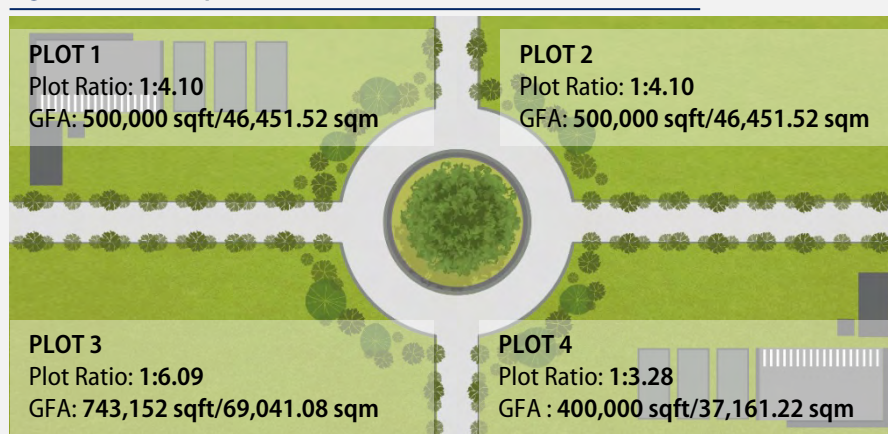
Note: The illustration is in reference to an example of GFA exemption.

## BRIEF INFO

## CALCULATION OF PLOT RATIO FOR SUB-DIVIDED PLOT

1. Refer to DCP2 – Intensity Map for plot ratio allowed on each plot of land for residential, commercial, mixed development, institutional and industrial buildings;
2. The total floor area (gross floor area, GFA) allowable for development on a plot of land is according to plot ratio allowed for that land or plot as set out in DCP2 - Intensity Map;
3. The GFA allowed based on the plot ratio can be distributed to the entire plot of land or to the sub-divided plots of land;
4. After obtaining approval for Development Order (DO), the plot of land that has been granted approval of a Development Order (DO) sets out the maximum amount of floor area allowed for building on the plot. The application for building plan must comply with the maximum floor area allowed as approved by the Development Order (DO).

Figure 3.2.2: Example of Sub-division for Lot 3839

**Calculation Step 1:****Calculation of overall GFA for Plot**

The designated Land Use Zone is **Mixed Development (MX)**

The total plot site area is **12.3 acres**

The plot ratio allowed is **1:4**

**Total Overall GFA**

$$\begin{aligned}
 &= \text{Plot Area} \times \text{Plot Ratio} \times 43,560 \text{ square feet} \\
 &= 12.3 \text{ acres} \times 4 \times 43,560 \text{ square feet} \\
 &= 2,143,152 \text{ square feet}
 \end{aligned}$$

**Calculation Step 2:****Calculation of GFA and plot ratio according to the sub-divided plot**

The planning permission application for Lot 3839 has been approved for four (4) new plots of land with maximum floor area allowed as stipulated. The details of the Land Use Zone of Mixed Development (MX) plot that has been subdivided are as follows:

Plot	Land Area (Acres)	Land Area (Square Feet)	Gross Floor Area (GFA) (Square Feet)	Gross Floor Area (GFA) (Square Metres)	Plot Ratio
Plot 1	2.8	121,968.00	500,000.00	46,451.52	4.10
Plot 2	2.8	121,968.00	500,000.00	46,451.52	4.10
Plot 3	2.8	121,968.00	743,152.00	69,041.08	6.09
Plot 4	2.8	121,968.00	400,000.00	37,161.22	3.28
Road reserve and Open Space	1.1	47,916.00	-	-	-
<b>TOTAL</b>	<b>12.3</b>	<b>535,788.00</b>	<b>2,143,152.00</b>	<b>199,105.34</b>	<b>4.00</b>

Building height control is a method of intensity control that determines the appropriate building height limit for development in commercial terrace areas and Land Use Zone of Residential 1 (R1), Traditional Village 1 (TV1) and Established Residential (EH).

### 3-3.1 Height Control for Commercial Terrace Building

The height control of commercial terrace buildings involves the Land Use Zone of City Centre Commercial (CCC), Major Commercial (MC), Commercial (C) and Local Commercial (LC).

Designated height control of commercial terrace buildings aims to coordinate the height of commercial terrace buildings according to the dominant height, the character of the heritage area, and compatibility with the capacity of infrastructure provided. Shops located in the corner lot or end lot in the commercial terrace buildings can be allowed under the corner treatment concept, where the height of the building in those lots may exceed the height of the shop building in the middle lot subject to the suitability of the building façade.

Figure 3.3.2 shows the intensity map for height control of commercial terrace buildings for each land use zone and Table 3.3.1 shows the building height control for commercial terraces building.

**Table 3.3.1: Height Control for Commercial Terrace Building**

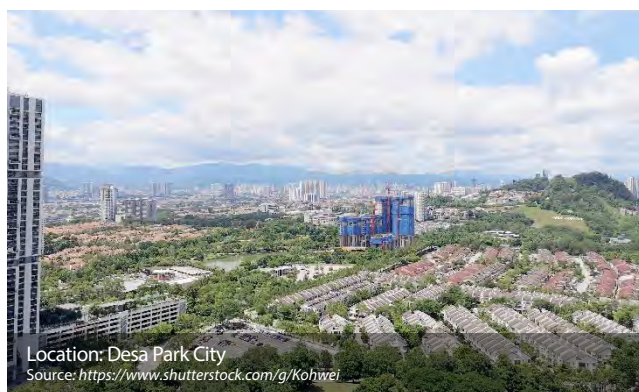
Land Use Zone	Building Height
City Centre Commercial (CCC)	<ol style="list-style-type: none"> <li>1. Building height up to 14 storeys;</li> <li>2. Building height up to 7 storeys for Secondary Heritage Zone areas;</li> <li>3. Building height up to 7 storeys for shop buildings, infill sites and lot redevelopment; and</li> <li>4. Building height up to 7 storeys for heritage areas with the retention of special character in terms of identity and lots on Jalan Doraisamy and Jalan Kamunting.</li> </ol>
Major Commercial (MC)	<ol style="list-style-type: none"> <li>1. Building height up to 14 storeys in the City Centre;</li> <li>2. Building heights up to 10 storeys for Special Character Zones area in Jalan Tuanku Abdul Rahman, Jalan Masjid India, Lorong Haji Taib, Jalan Chow Kit and Lorong Bunus;</li> <li>3. Building height up to 8 storeys outside the City Centre;</li> <li>4. Building height up to 7 storeys for Secondary Heritage Zone;</li> <li>5. Building height up to 7 storeys for Special Character Zones area in Jalan Pudu Lama;</li> <li>6. Building height up to 7 storeys for shop buildings, infill sites and redevelopment of lots in the heritage zone area in Jalan Tun Sambanthan, Jalan Sultan Abdul Samad, Lorong Ayer Kuning, Jalan Thambipillay and Jalan Vivekananda;</li> <li>7. Building height up to 3 storeys is limited to the front building area of SOGO Complex, Jalan Tuanku Abdul Rahman (Lot 22-36, Lot 190-191, Lot 20000 -20002); and</li> <li>8. Building height up to 3 storeys for the area along Jalan Tuanku Abdul Rahman (including the row of Dunhill shops) and the Old Gian Singh building on Jalan Tun Perak.</li> </ol>
Commercial (C)	<ol style="list-style-type: none"> <li>1. Building height up to 10 storeys in Jalan Bukit Candan Off Jalan Ipoh;</li> <li>2. Building height up to 6 storeys and necessity of maintaining the original façade of the building if it involves an old building of heritage interest;</li> <li>3. Building height of up to 3 storeys is limited to building areas in Jalan Suasas, Jalan Suasas 3, 4 and 5, Pekan Sungai Besi; and</li> <li>4. Building height outside the city center between 2 and 6 storeys (except Lot 25-29 and Lot 33-39)</li> </ol>
Local Commercial (LC)	<ol style="list-style-type: none"> <li>1. Building height up to 4 storeys.</li> </ol>

*Note: The height of the commercial terrace that has been approved is applicable to the particular lot only.*

### 3-3.2 Height Control for Residential

Residential height control involves Land Use Zone of Residential 1 (R1), Traditional Village 1 (TV1) and Established Residential (EH). It is intended to maintain the current typology taking into consideration in addition to consider the limited availability of infrastructure, utilities and public facilities.

Figure 3.3.2 and Table 3.3.2 show the residential height control for each land use zone.



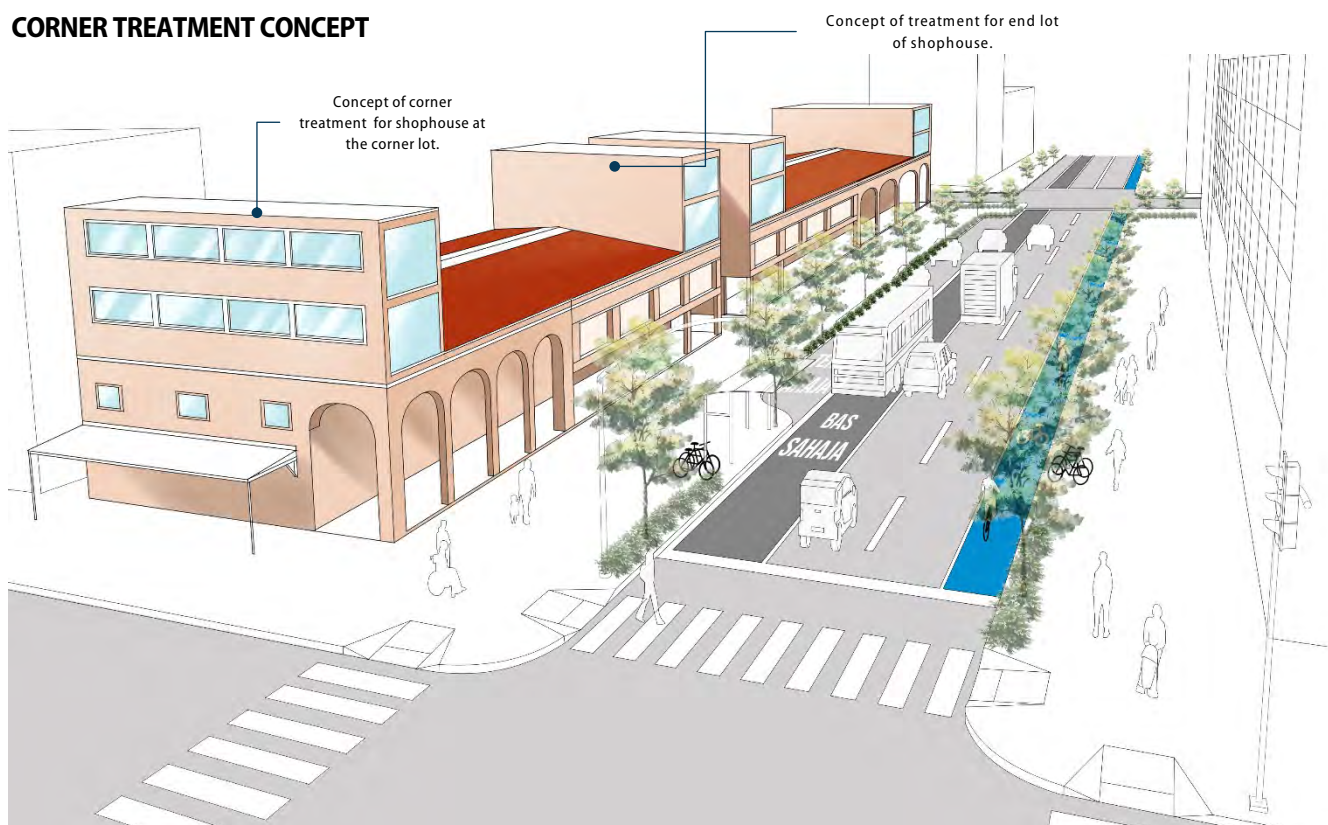
Residence in a residential area of Residence 1 (R1) with a height of 3 storeys.

Table 3.3.2: Residential Height Control

Land Use Zone	Building Height
Residential 1 (R1)	The height of the buildings is up to 3 storeys including landed strata with height control of 4 level and the typology of the building is maintained.
Traditional Village 1 (TV1)	The height of the buildings is up to 3 storeys with a design characteristic of traditional Malay houses.
Established Residential (EH)	The height of the buildings is up to 3 storeys with strict control and no significant change in terms of character, facade and is expected to remain until 2040. Redevelopment or new development needs to ensure harmony of character with adjoining lots.

#### BRIEF INFO

#### CORNER TREATMENT CONCEPT



Shops located in the corner lot or end lot of the commercial terrace are allowed under the corner treatment concept, that is the height of the building may exceed the height of the shops building in the middle lot, subject to the appropriateness of the building façade.



### 3-3.3 Determination of Household Size

The determination of occupancy rate enables the calculation of community facility needs with appropriate space that must be provided in line with the number of residents from the development of the site.

The current household size is 3.5 and it is estimated to be 3.0 by 2040. The use of only one occupancy rate limits the use of flexible unit sizes that can be offered at suitable prices according to population demographic needs in the future.

This determination of unit size and household size is applicable to all residing components referring to residential units, quarters, service apartments and commercial properties that are subject to the guidelines for the provision of community facilities and open spaces.

Occupancy rate based on the minimum residing unit is categorised as shown in Table 3.3.3.

**Table 3.3.3:**  
**Control of Residing Unit Size and Household Size**

Size of Residing Unit	Occupancy Rate/ Household Size
1. Unit size <450 square feet	1.0
2. 451 square feet up to 799 square feet	2.0
3. 800 square feet up to 1,599 square feet	3.0
4. Unit size over 1,600 square feet	4.0
5. Affordable homes, PPR/PA and landed residences	4.0

*Notes: The determination of percentage of size of small units, especially residences, is subject to guidelines determined at the planning permission stage.*

#### BRIEF INFO

##### DETERMINATION OF RESIDING UNIT SIZE

For planning control purposes, the minimum size of residential unit components is at 800 square feet except for special residential houses such as Subsidised Rental Public Housing (PASS), Public Housing (PA) and Program Perumahan Rakyat (PPR).

Minimum residential unit size of not less than 650 square feet is allowed for DINK, YUPPIES and senior citizens. Percentage of minimum residing unit is determined at the planning control level.

### 3-3.4 Large Residential Units

Demand for the provision of large residential units in Kuala Lumpur must be catered to balance the demand and in line with current trends for large residential units in certain areas of Kuala Lumpur by 2040.

A large residential unit is defined as a residence with a floor space of more than 1,600 square feet for a household of 4. The designation of this large residential unit is an additional control that aims to control the widespread of development of large units in Kuala Lumpur.

The planning control for these large size residential units is applicable for the period of the Draft KLLP2040. It involves the person per acre (PPA) intensity control and applies only to the development of large residential units. In the case of development of various sizes of residential units, the plot intensity ratio is applicable.

The planning of large residential units is determined by the development trend of large residential units and coordination with existing approvals. Five (5) locations that have the potential to offer large residential units are:

1. Mont Kiara;
2. Jalan Robson;
3. Jalan U-Thant;
4. Bukit Pantai; and
5. Bukit Bangsar.

This designation also applies to appropriate areas subject to the approval of the Mayor. The Draft KLLP2040 sets the comparison factor for changing the density to plot ratio based on the average size of residential developments in Kuala Lumpur. The calculation for setting person per acre intensity for large residential units apply the comparison factor that has been set.

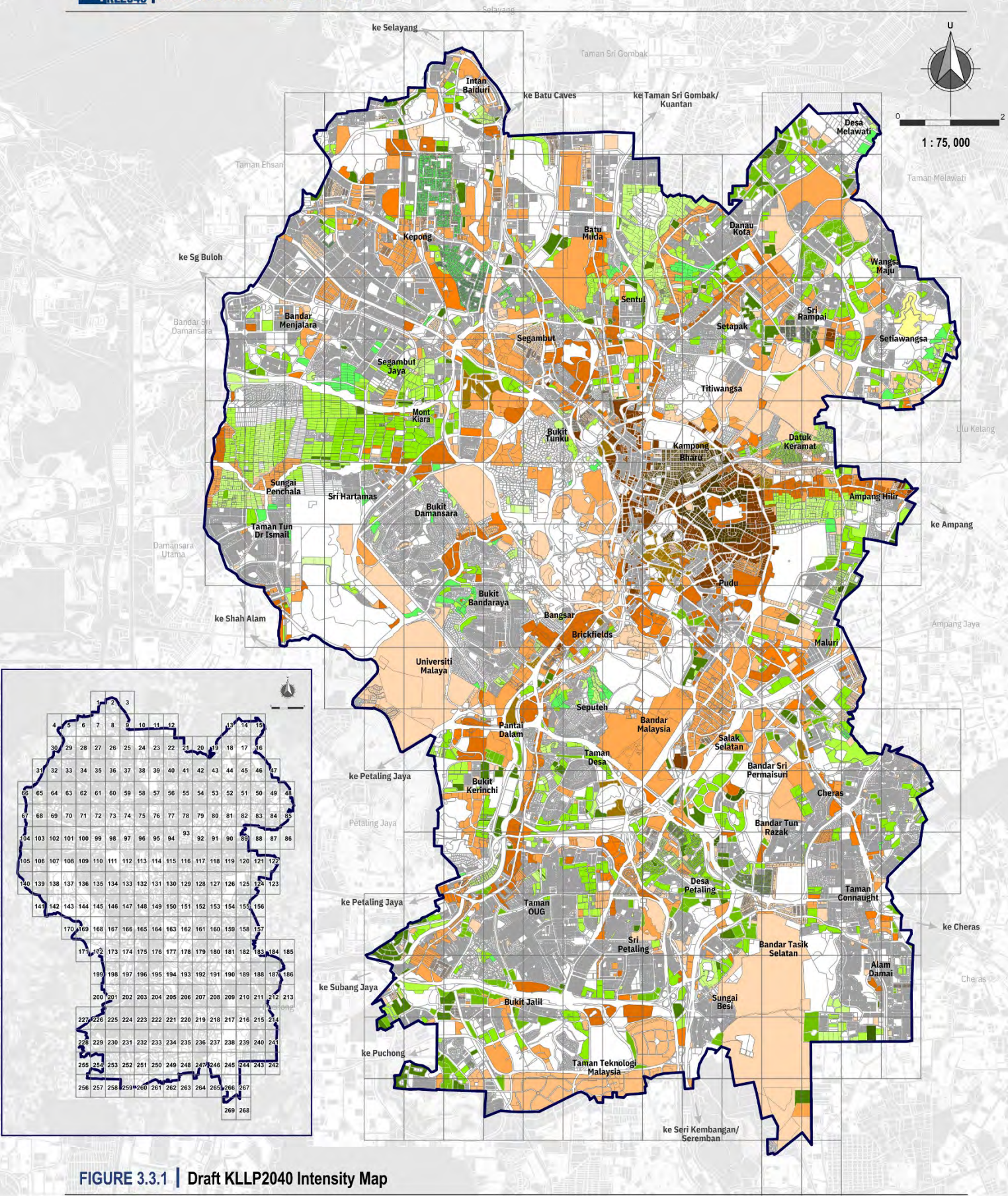
#### BRIEF INFO

##### PERSON PER ACRE (PPA) INTENSITY FOR LARGE RESIDENTIAL UNITS

Plot Ratio	PPA
1:2	Maximum 222
1:2.5	Maximum 277
1:3	Maximum 333
1:3.5	Maximum 388
1:4	Maximum 444









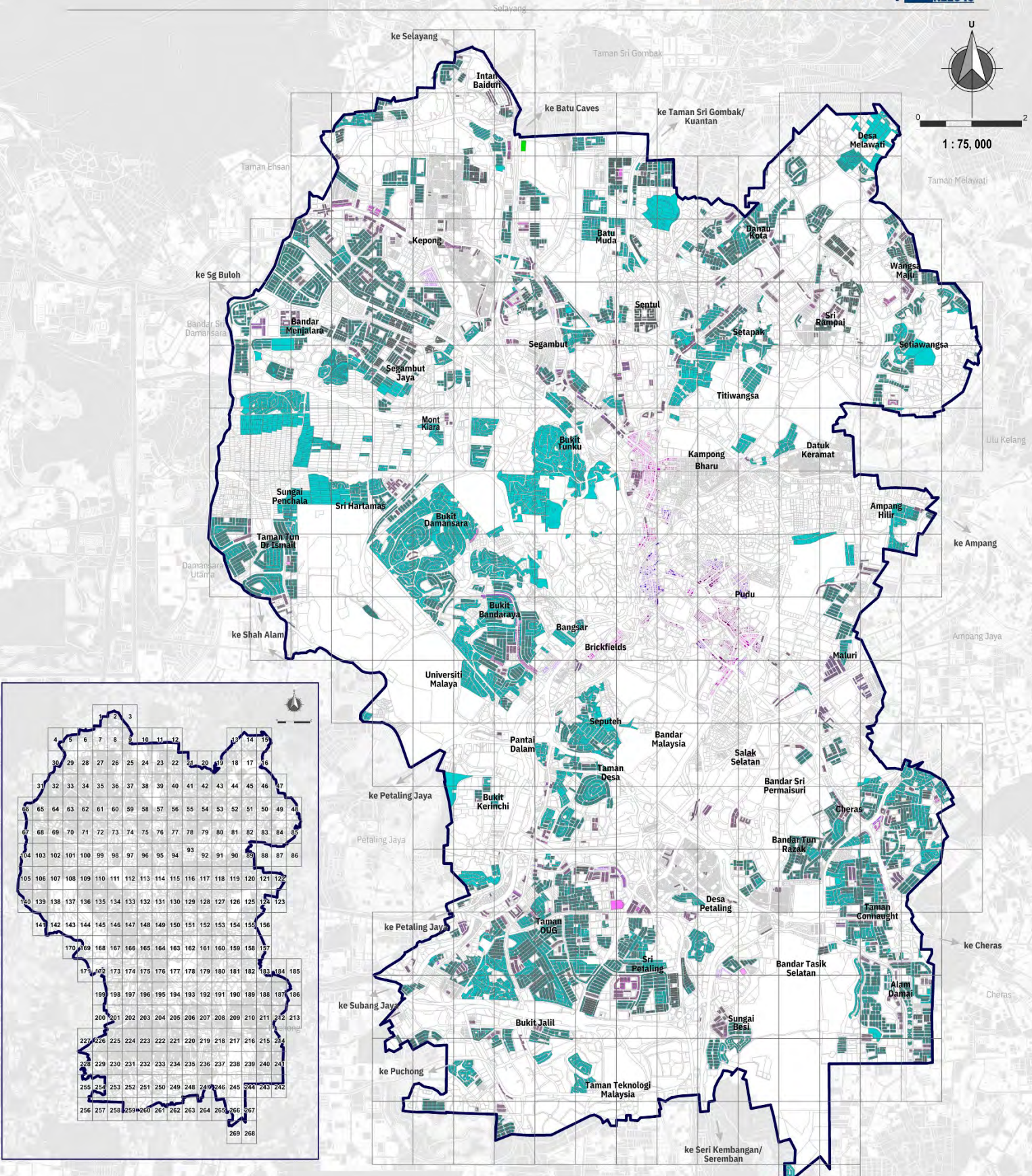


FIGURE 3.3.2 | Draft KLLP2040 Height Control Map of Residential and Commercial Terraces

Legend:

Height Control of Residential

- 2 Storeys
- 3 Storeys

Height Control of Commercial Terraces

- 2 Storeys
- 3 Storeys
- 4 Storeys
- 5 Storeys
- 6 Storeys
- 7 Storeys
- 10 Storeys
- 14 Storeys

Others

- No Intensity
- Road
- Kuala Lumpur Boundary



# 4.0

## PLANNING CONTROL LAYER





# 4.0 PLANNING CONTROL LAYER

DRAFT KUALA LUMPUR LOCAL PLAN 2040

The planning control layer is an additional control imposed on DCP1 - Land Use Zoning Map and DCP2 - Intensity Map KLDCP2024. These additional controls include land planning, intensity and building design.

The Draft KLLP2040 identified seven (7) layers of planning control for the areas involved in Kuala Lumpur namely:

1. Planning Control Layer of Layout Plan Plot Ratio;
2. Planning Control Layer for Side Corridors of River, Lake and Pond;
3. Planning Control Layer for Heritage Areas or Buildings;
4. Planning Control Layer for the Height Control Zone Surrounding The Istana Negara;
5. Planning Control Layer for Line of Sight Towards Landmarks;
6. Planning Control Layer for Public Open Spaces In Private Schemes (OS4); and
7. Planning Control Layer for Affordable Housing (RMM).

The planning and use of land as well as buildings involved in the seven (7) planning control layers must refer to and comply with the controls related to each of the designated planning control layers.



The development of Kuala Lumpur City Centre (KLCC) is involved in the Planning Control Layer of Layout Plan Plot Ratio.

## BRIEF INFO

### 7 PLANNING CONTROL LAYERS

#### Planning Control Layer of Layout Plan Plot Ratio

Control over an area that has been granted plot ratio approval based on the development scheme.



#### Planning Control Layer for Side Corridors of River, Lake and Pond

Control of all new development and redevelopment located adjacent to rivers, lakes and ponds.



#### Planning Control Layer for Heritage Areas or Buildings

Controls on sites and buildings to conserve and maintain the special character in the Kuala Lumpur Heritage Zone.



#### Planning Control Layer for the Height Control Zone Surrounding The Istana Negara

Controls to preserve vistas from and leading to the Istana Negara.



#### Planning Control Layer for Line of Sight Towards Landmarks

Additional control over the height, orientation and design of new buildings located within visual control zone of the lines of sight to identified landmarks in Kuala Lumpur.



#### Planning Control Layer for Public Open Space In Private Schemes (OS4)

Controls for public open spaces of multiple uses on ground or podium level accessible to the public and not specific to the local community.



#### Planning Control Layer for Affordable Housing (RMM)

Control for sites that have been committed to or have received the approval of a residential development order and that have free and affordable prices



## PLANNING CONTROL LAYER OF LAYOUT PLAN PLOT RATIO

Planning Control Layer of Layout Plan Plot Ratio is a control over an area that has been granted plot ratio approval based on the development scheme. The Layout Plan Plot Ratio is shown as a single development plot marked with only one plot ratio. Plot ratio according to the development plot should refer to the approved layout plan.

### 4-1.1 Objectives of Planning Control Layer of Layout Plan Plot Ratio

The use of Layout Plan Plot Ratio aims to control the maximum floor space approved within the distribution of development plots subject to the approved layout plan. In addition, it facilitates the implementation of a long period development due to certain changes based on market needs, demand and government policies without involving changes to the Draft KLLP2040.

### 4-1.2 Planning Control Aspects of Layout Plan Plot Ratio

This planning control considers four (4) main aspects which are as follows:

1. Concept of planning control of Layout Plan Plot Ratio;
2. Site of planning control layer of Layout Plan Plot Ratio;
3. Planning control of Layout Plan Plot Ratio; and
4. Calculation method of Layout Plan Plot Ratio.

These aspects are meant to ensure comprehensive and sustainable planning control can be implemented by 2040.

The details of Planning Control Aspects of Layout Plan Plot Ratio are as follows:

#### 1. Planning Control Concepts of Layout Plan Plot Ratio

This planning control concept applies to new proposed and existing development in areas involving layout plan plot ratio. The determination of the plot ratio according to the development plot is subject to the approved plot ratio for the overall layout plan.

The development plots can have different plot ratio on condition that the overall gross floor area is subject to the approval of the planning permission application.

The designated plot ratio is related to the land use strategy identified in the Draft KLLP2040. The land use strategy promotes sustainable land use, where optimal land development will be encouraged to support the vision of a City For All.

### 2. Planning Control Layer Sites of Layout Plan Plot Ratio

The Draft KLLP2040 has identified 25 control layer area for planning control layer of layout plan plot ratio as shown in Table 4.1.1.

### 3. Planning Controls of Layout Plan Plot Ratio

In line with plot ratio planning controls, all identified sites are encouraged to be developed based on the following controls:

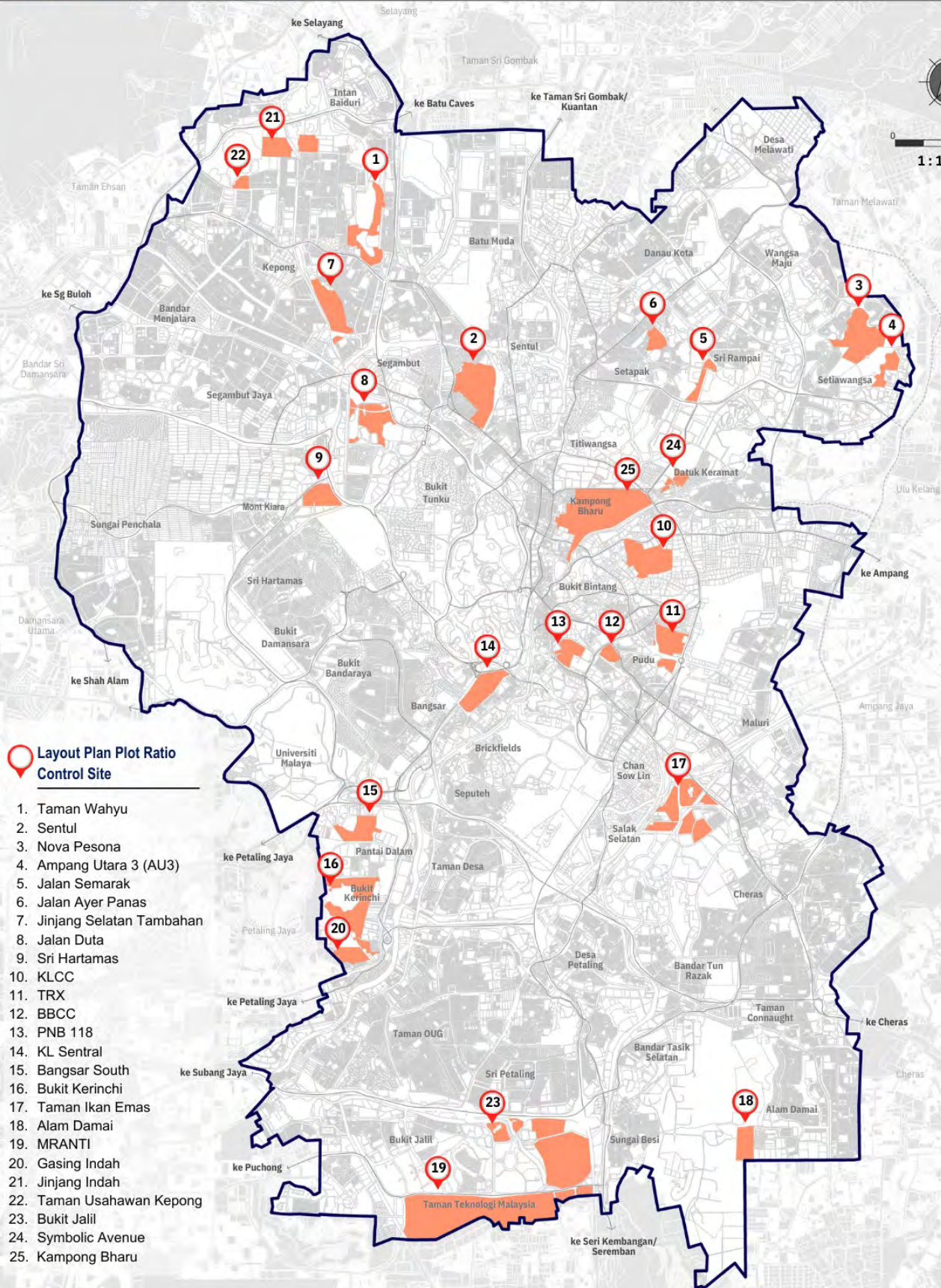
- a. Plots that have been approved with the allowable plot ratio, development is subject to the prescribed distribution;
- b. Amendments to distribution by plot can still be allowed subject to approval as long as the plot ratio distribution does not exceed the maximum approved plot ratio;
- c. A plot that has been sold to a new owner or a third party, any increase of intensity beyond the designated plot ratio must have the permission from the owner of the overall layout plan and is subject to excess intensity from undeveloped plots;
- d. Plot ratio that has been set according to development plots cannot be transferred to other plots that have not been developed if the development plot has been sold, transferred or completed to a third party unless the new owner agrees to the intensity that has not yet been used;
- e. The layout plan plot ratio set out in the DCP2 – Intensity Map is the basic plot ratio given and is exclusive of planning incentives. However, planning incentives are only applicable to particular lots; and
- f. Incentives will be considered and applied when the layout plan is approved subject to the planning evaluation during the planning application stage.

Table 4.1.1: List of Layout Plan Plot Ratio Control Site

Project / Location	Proposed Land Use Zone	Base Plot Ratio	Area (Hectares)
1. Cadangan Pelan Susun Atur Pembangunan Bercampur di Atas Lot PT26934, Jalan Kepong/Jalan Sibu Off Jalan Kuching	MX	1:4	32.60
2. Pelan Susun Atur Perancangan Kawasan Sentul Raya, Seksyen 83, Mukim Kuala Lumpur	MX	1:6	60.54
3. Pelan Susun Atur Pembangunan Perumahan Berstrata Berserta Rumah Kelab di atas Lot 26413, Mukim Setapak dan Lot 3538, Mukim Ulu Kelang	R2	1:0.5	54.26
4. Pelan Susun Atur Pangsapuri, Rumah Mampu Milik dan Lot Perniagaan, Mukim Ulu Kelang	R3	1:2.5	13.69
5. Pelan Susun Atur Pembangunan Bercampur di Jalan Ayer Keroh, Mukim Setapak	MX	1:4	10.63
6. Pelan Susun Atur Pembangunan Bercampur di Atas Lot 17899, Jalan Ayer Jerneh, Mukim Setapak	MX	1:4	11.24
7. Cadangan Pembangunan Bercampur Perdagangan dan Kediaman di Atas Lot 9714 di Sebahagian Atas Tanah Kerajaan, Mukim Batu	MX	1:5	35.13
8. Pelan Susun Atur Pembangunan Perdagangan, Mukim Batu, Bandaraya Kuala Lumpur, Wilayah Persekutuan Kuala Lumpur	MX	1:7	30.32
9. Pelan Susun Atur Pembangunan Perdagangan, Jalan Sri Hartamas 1, Mukim Batu	MC	1:6	21.60
10. Pelan Susun Atur Pembangunan Perdagangan Kuala Lumpur City Centre (KLCC), Bandar Kuala Lumpur	CCC	1:6	43.58
11. Pelan Susun Atur Pembangunan Bercampur "Tun Razak Exchange" di Jalan Tun Razak/Jalan Davis	CCC	1:6	28.61
12. Pelan Susun Atur Pembangunan Bercampur di Atas Lot PT 143, Seksyen 56, Jalan Hang Tuah/Jalan Pudu, Kuala Lumpur	MX	1:6	7.87
13. Pelan Susun Atur Pembangunan Bercampur, Mukim Bandar (PNB)	MC	1:5	16.32
14. KL Sentral	CCC	1:6	31.00
15. Pelan Susun Atur Pembangunan Bercampur, Bangsar South, Jalan Kerinchi, Kuala Lumpur	MX	1:6	24.67
16. Pelan Susun Atur Pembangunan Bercampur, Jalan Pantai Sentral, Mukim Kuala Lumpur	MX	1:4	54.84
17. Pelan Susun Atur Pembangunan Bercampur, Mukim dan Daerah Kuala Lumpur	MX	1:4	57.41
18. Pelan Susun Atur Pembangunan Kediaman, Persiaran Damai, Mukim Petaling	R3	1:4	19.29
19. Taman Teknologi Malaysia (Malaysian Research Accelerator for Technology & Innovation - MRANTI)	TP	1:3	259.20
20. Pelan Susun Atur Pembangunan Kediaman Villa Berstrata dan Rumah Sesebuah, Mukim Kuala Lumpur	R2	3 storeys	15.20
21. Pelan Susun Atur Pembangunan Bercampur, Mukim Batu	MX	1:6	29.41
22. Pelan Susun Atur Pembangunan Bercampur di Atas Lot 80782, Mukim Batu	MX	1:6	5.90
23. Pelan Susun Atur Pembangunan Bercampur di Jalan Merah Cagar, Bukit Jalil, Mukim Petaling	MX	1:6	31.78
24. Pelan Susun Atur Pembangunan Perdagangan dan Kediaman, Seksyen 87A, Daerah Kuala Lumpur	MX	1:5	8.05
25. Kampong Bharu	CCC	1:10	106.0

Note: Site boundaries and area are based on the approved layout plan. Calculations are rounded to two decimal points.





**FIGURE 4.1.1 | Location Plan of Areas Control Layout Plan Plot Ratio**

**Legend :**

**Control of Layout Plan Plot Ratio**

● Site of Layout Plan Plot Ratio

**Others**

— Main Road

— Kuala Lumpur Boundary



## BRIEF INFO

## DETERMINATION OF LAYOUT PLAN PLOT RATIO

Determination of layout plan plot ratio is as follows:

- The floor area of the proposed plot must not exceed the total floor area of the approved plot ratio for the entire development;
- An increase in the intensity of a plot that has been sold to a new owner or a third party beyond the designated plot ratio must have the permission of the owner of the overall layout plan and is subject to excess intensity from undeveloped plots;
- Plot ratio that have been set according to development plots cannot be transferred to other plots that have not yet been developed if the development plot has been sold, transferred or completed to a third party unless the new owner agrees to the intensity that has not yet been used; and
- Any changes to the redistribution of intensity by plot are subject to consideration at the planning permission stage.

## BRIEF INFO

## CALCULATION OF LAYOUT PLAN PLOT RATIO

## Step 1:

## Distribution of Plot Ratio according to Draft KLDCP2024

KLDCP2024 has determine the land use zone, designated boundary (area) and plot ratio for Lot 31118. Plot details are as shown in Table 4.1.2:

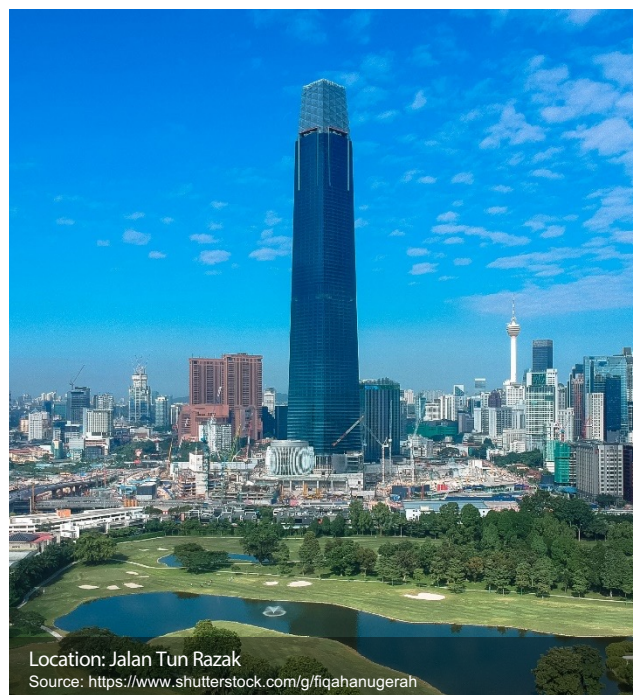
Table 4.1.2:

Example of Layout Plan Plot Ratio Calculation

Component	KLDCP2024	Permitted Layout
PLOT	Lot 31118	Lot 31118
LAND USE ZONING	Mixed Development	Mixed Development
LOT AREA	24.72 acres	24.72 acres
PLOT RATIO	1:4	1:4
GFA	400,153.12 square metre	400,153.12 square metre

## 4. Calculation Method of Layout Plan Plot Ratio

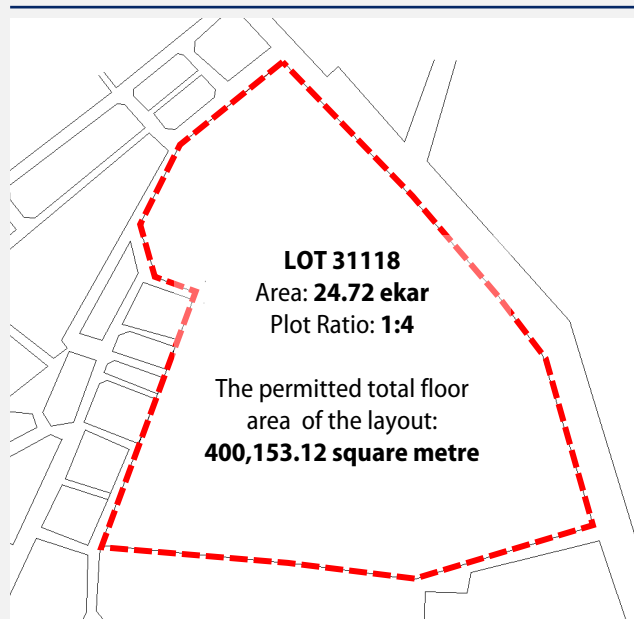
Calculation of layout plan plot ratio determine the maximum total floor area for the entire development plot and the maximum floor area distribution according to the development plot. The example of Layout Plan Plot Ratio calculation method for Land Use Zon of Mixed Development (MX) is detailed out in the brief info below.



Development's view around Tun Razak Exchange.

Figure 4.1.2:

Determination of The Permitted Total Floor Area of The Layout for Lot 31118



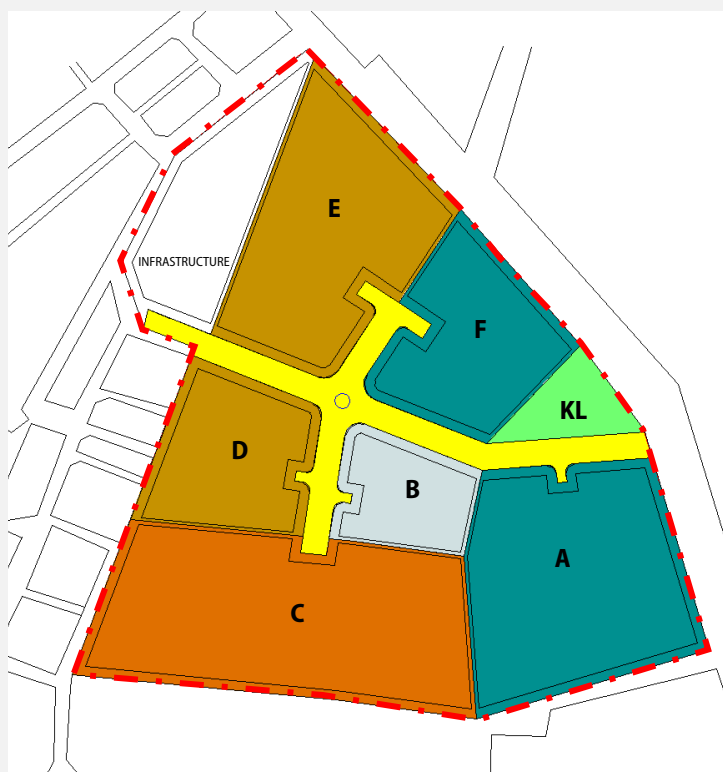
## BRIEF INFO

## SCENARIOS OF LAYOUT PLAN PLOT RATIO DETERMINATION

## SCENARIO 1:

## Calculation Steps for Approval of Layout Plan Plot Ratio

1. Based on KLDCP2024, Lot 31118 has an area of 24.72 acres with a permitted GFA of 400,153.12 square metre.
2. The proposed development must ensure that the proposed GFA does not exceed the permitted GFA.
3. Lot 31118 is subdivided into detailed layout plan as summarised in Table 4.1.3.
4. If the development proposal meets the necessary conditions and criteria, this layout plan proposal will be approved.



Legend:

Plot Boundary

Total plot area = 24.72 acres (**100,038.29 square metres**)

Base plot ratio = 1:4

Gross Floor Area (GFA) = Plot Area x Plot Ratio

= 100,038.29 square metres x 4

= **400,153.12 square metres**

Table 4.1.3:

Example of Calculation for Approval of Layout Plan Plot Ratio

Plot	Land Area (Acres)	Land Area (Square Metre)	Gross Floor Area (GFA) (Square Metre)	Plot Ratio (each plot)
A	4.33	17,522.89	60,022.97	3.43
B	1.31	5,301.38	40,015.31	7.55
C	5.39	21,812.56	160,061.25	7.34
D	2.15	8,700.74	52,019.91	5.98
E	3.78	15,297.12	48,018.37	3.14
F	2.43	9,833.86	40,015.31	4.07
Open Space	1.21	4,896.70	0.00	-
Infrastructure	2.14	8,660.27	0.00	-
Road	1.98	8,012.78	0.00	-
<b>Total</b>	<b>24.72</b>	<b>100,038.29</b>	<b>400,153.12</b>	<b>4.00</b>

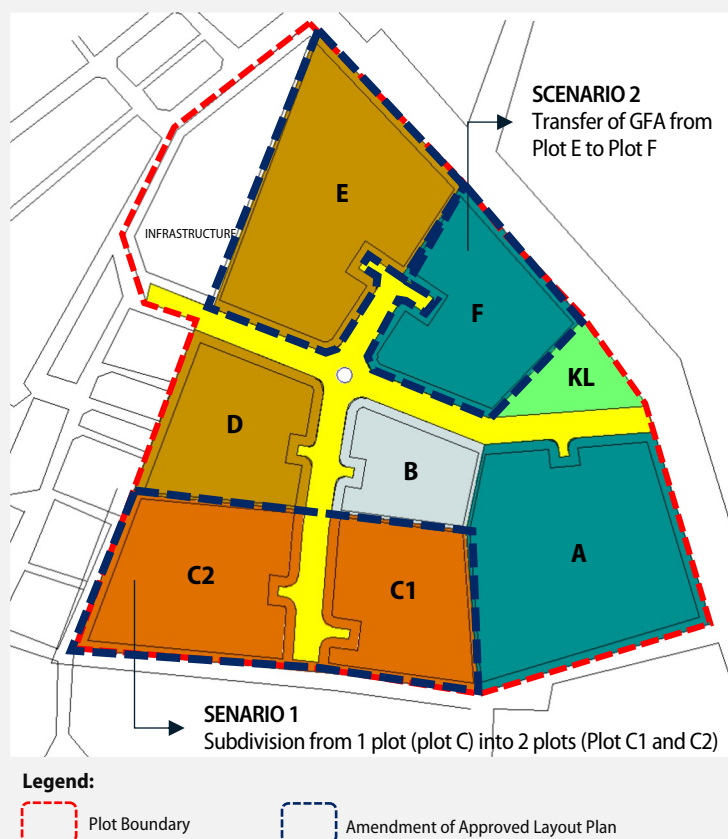
## BRIEF INFO

## SCENARIOS OF LAYOUT PLAN PLOT RATIO DETERMINATION

## SCENARIO 2:

## Amendment of Approved Layout Plan Plot Ratio

1. If the development proposal that has been approved needs to be amended, a proposed layout plan amendment must be submitted.
2. Amendments must ensure that the area, plot ratio and overall GFA do not exceed those set by the KLDCP2024.
3. Layout plan amendments should be submitted with details such as comparison between the approved layout plans and proposed amended layout plan.
4. Table 4.1.4 is an example of the calculation of the approved layout plan amendment.



**Table 4.1.4:**  
Example of Amendment Approved Layout Plan Plot Ratio

Plot	Land Area (Acres)	Land Area (Square Metre)	Gross Floor Area (GFA) (Square Metre)	Plot Ratio (each plot)	Note
A	4.33	17,522.89	60,022.97	3.43	
B	1.31	5,301.38	40,015.31	7.55	
C1	2.17	8,781.68	60,022.97	6.84	Plot subdivision
C2	2.85	11,533.54	100,038.28	8.67	Plot subdivision
D	2.15	8,700.74	52,019.91	5.98	
E	3.78	15,297.12	48,018.37	3.14	GFA increase
F	2.43	9,833.86	40,015.31	4.07	GFA decrease
Open Space	1.21	4,896.70	0.00	-	
Infrastructure	2.14	8,660.27	0.00	-	
Road	2.35	9,510.11	0.00	-	Area increase
<b>Total</b>	<b>24.72</b>	<b>100,038.29</b>	<b>400,153.12</b>	<b>4.00</b>	

## BRIEF INFO

## COMPARISON BETWEEN THE APPROVED LAYOUT PLANS AND PROPOSED AMENDED LAYOUT PLAN PLOT RATIO

Any amendment to the approved layout plan must show the comparison between the approved layout plans and amended layout plan as shown in Table 4.1.5.

**Table 4.1.5:**  
Example of Comparison Between The Approved Layout Plans and Proposed Amended Layout Plan Plot Ratio

Approved Layout Plan				Amended Layout Plan			
Lot	Area (Acres)	Gross Floor Area (GFA) (Square Metre)	Plot Ratio	Lot	Area (Acres)	Gross Floor Area (GFA) (Square Metre)	Plot Ratio
A	4.33	60,022.97	3.43	A	4.33	60,022.97	3.43
B	1.31	40,015.31	7.55	B	1.31	40,015.31	7.55
C	5.39	160,061.25	7.34	C1	2.17	60,022.97	6.84
				C2	2.85	100,038.28	8.67
D	2.15	52,019.91	5.98	D	2.15	52,019.91	5.98
E	3.78	48,018.37	3.14	E	3.78	48,018.37	3.14
F	2.43	40,015.31	4.07	F	2.43	40,015.31	4.07
Open Space	1.21	0.00	-	Open Space	1.21	0.00	-
Infra	2.14	0.00	-	Infra	2.14	0.00	-
Road	1.98	0.00	-	Road	2.35	0.00	-
<b>Total</b>	<b>24.72</b>	<b>400,153.12</b>	<b>4.00</b>	<b>Total</b>	<b>24.72</b>	<b>400,153.12</b>	<b>4.00</b>

Based on Table 4.1.5, the GFA of Lot 31118 remains as per the approved plan. Amendment involves:

1. Subdivision of Plot C into Plot C1 and Plot C2;
2. GFA amendment of Plots E and F; and
3. Increase in road layout following the subdivision of Plot C.



The Planning Control Layer for Side Corridors Of River, Lake And Pond as well as planning control for all new development and redevelopment located adjacent to rivers, lakes and ponds. This control layer is categorised into two (2) components as follows:

1. Riverside corridor; and
2. Lakeside and pondside corridors.

#### 4-2.1 Definition of Planning Control Layers for Side Corridors of River, Lake, and Pond

The definition of the Planning Control Layer For Side Corridor of River is as follows:

1. **River:** A large stream that is the natural drainage of a river basin in its natural form excluding tributaries or monsoon drain;
2. **River reserve:** A strip of land adjacent to both banks of a river that is usually gazetted; and
3. **River corridor:** The area outside the river reserve that is within an identified distance.

The definition of Planning Control Layer For Side Corridors of Lake and Pond is as follows:

1. **Lake:** A large water body (area of one (1) hectare or more) demarcated from the water's edge that can either function as a flood catchment area or a recreational area;
2. **Pond:** A medium sized or small water body (area less than one (1) hectare) that is demarcated from water tide and can function as either a flood catchment area or a recreational area; and
3. **Lake and pond corridor:** The area within an identified distance between the edge of water tide of lake and pond.

#### 4-2.2 Objectives of Planning Control Layer for Side Corridors of River, Lake and Pond

The provision and implementation of the riverside corridor as a planning control layer are intended to:

1. Preserve and conserve river corridors as flood control areas;
2. Create connected ecological corridors to enhance natural habitats of urban biodiversity assets;
3. Improve public access to the river through the provision of continuous access along the river;
4. Promote appropriate development that can generate the economy;
5. Contribute to the provision of green areas for Kuala Lumpur;
6. Develop river areas as public recreation centres and nodes to attract tourists and improve the image of Kuala Lumpur as a Green, Healthy and Vibrant City; and
7. Preserve the riverside corridor as an access route and space for river operation and maintenance work.

The provision and implementation of lakeside and pondside corridors as a planning control layer are intended to:

1. Ensure all forms of new development and redevelopment around the lake and pond area are harmonious, attractive, comfortable, safe and environmentally friendly;
2. Preserve and conserve the ecological corridor of the lake to enhance the natural habitat of the city's biodiversity assets;
3. Make the lakeside and pondside corridors into recreational areas that are accessible to the public; and
4. Maintain lakeside and pondside corridors as access routes and spaces for lake and pond operation and maintenance work.

### 4-2.3 Location of Planning Control Layers for Side Corridors of River, Lake and Pond

The implementation of the Planning Control Layers for Side Corridors of River involves 16 rivers as shown in Table 4.2.1 which are in three (3) categories, as follows:

1. **Category I:** The **main rivers** that are the backbone of Kuala Lumpur's drainage system;
2. **Category II:** The **large rivers** in the Kuala Lumpur drainage system; and
3. **Category III:** The **small rivers** in the drainage system of Kuala Lumpur.

**Table 4.2.1:**  
List of Rivers in Planning Control Layers for Riverside Corridor

River	Length (kilometre)
<b>Category I: Main Rivers</b>	
1. Sungai Klang	16.50
2. Sungai Gombak	8.20
3. Sungai Batu	8.10
<b>Category II: Large Rivers</b>	
4. Sungai Kerayong	9.37
5. Sungai Kuyoh	6.82
6. Sungai Bohol	0.80
7. Sungai Midah	1.74
8. Sungai Penchala	3.00
9. Sungai Jinjang	5.37
10. Sungai Keroh	6.90
11. Sungai Kemuning	4.30
12. Sungai Belankong	3.50
<b>Category III: Small Rivers</b>	
13. Sungai Bunus	9.60
14. Sungai Toba	4.20
15. Sungai Kayu Ara	3.60
16. Sungai Untut	3.70

Meanwhile, the implementation of the Planning Control Layers for Lakeside Corridor and Pond involves lakes and ponds that will be identified by KLCH.

The selection of lakes and ponds will take into consideration according to the function and planning, especially related to the aspects of flood retention and recreational needs.

### 4-2.4 Aspects of Planning Control for Side Corridors of River, Lake and Pond

Planning controls for side corridors of river, lake and pond apply to all plots of public and private land located on both sides of rivers, lakes and ponds.

Determination of control boundaries for this side corridor are as follows:

1. **Riverside corridor:** Starting from the boundary of the river reserve or the highest water tide as shown in Figure 4.2.1; and
2. **Lakeside and pondside corridor:** Starting from the highest water tide of lakes and ponds as shown in Figure 4.2.2.

Table 4.2.2 and Table 4.2.3 detail out the planning control for side corridors of river, lake and pond which involve following aspects:

1. Setback requirements;
2. Activities and structures;
3. Building orientation; and
4. Roads and public access.

Additional corridor space for the purpose of increasing connectivity and green network in Kuala Lumpur should be provided if there are future demand and need.



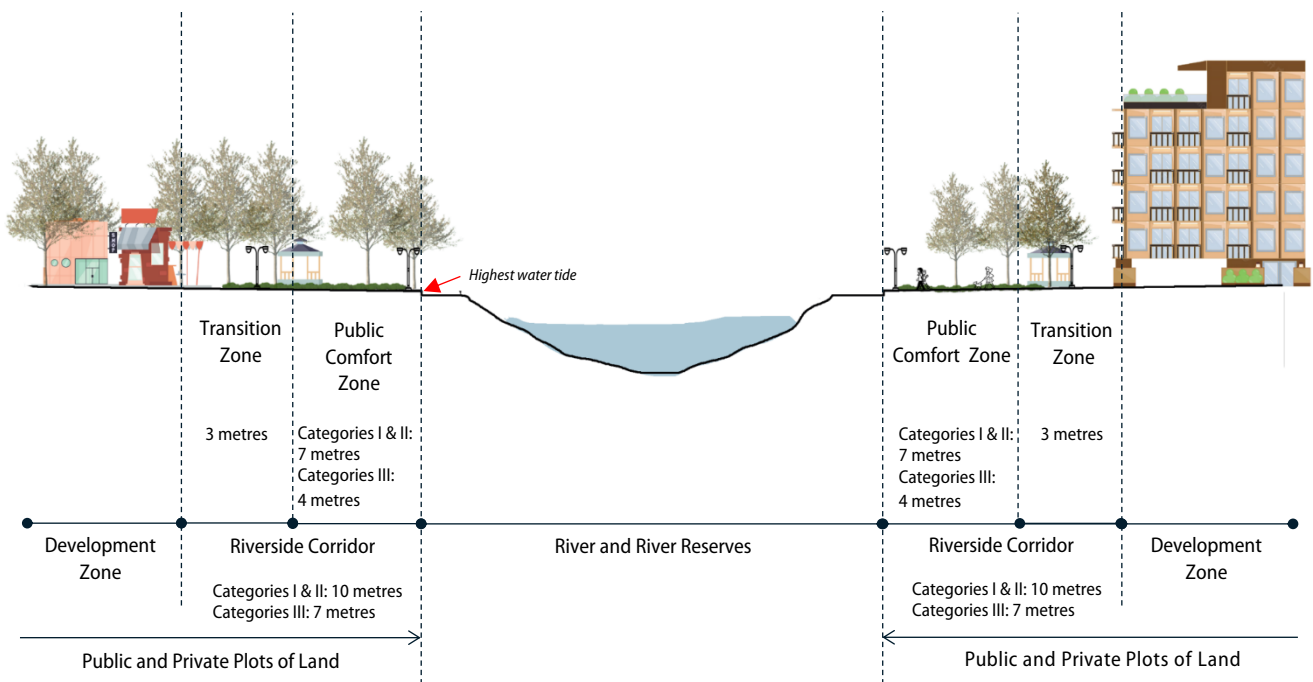
Location: Sungai Kuyoh

Rivers are an important natural ecological asset and habitat of various local species, contributing to the social and economic well-being in Kuala Lumpur.

## BRIEF INFO

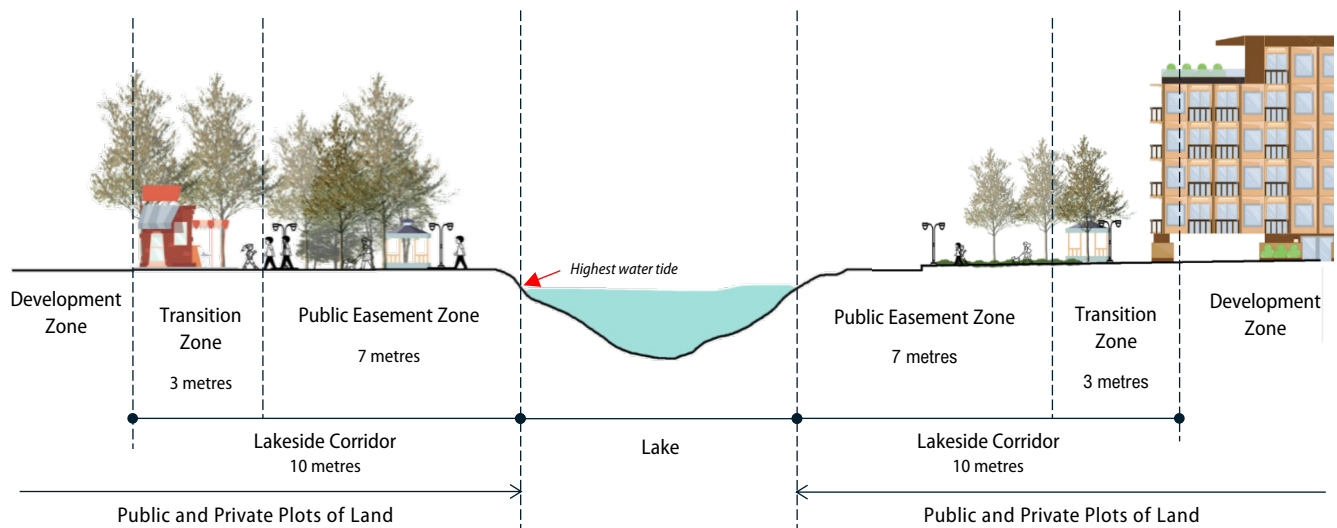
## ZONE OF SIDE CORRIDORS OF RIVER, LAKE, AND POND

Figure 4.2.1: Riverside Corridor Zone



The setback of the Riverside Corridor Zone is from the boundary of the river reserve or the highest water tide level of the river.

Figure 4.2.2: Lakeside and Pondsides Corridor Zones



The setback of the Lakeside and Pondsides Corridor Zone is from the highest water tide level of the lake and pond.

**Table 4.2.2:**  
**Type of Planning Control for Side Corridors of River**

<b>1. Setback Requirements</b>	
<p>The need of setback for side corridors of river means the minimum distance requirements between new development areas or redevelopment and river reserves or highest water tide. The required setback width is as follows:</p> <ol style="list-style-type: none"> <li>Minimum <b>ten (10) metres</b> for river of <b>category I and II</b>, and</li> <li>Minimum <b>seven (7) metres</b> for river of <b>category III</b></li> </ol>	
<b>2. Activity and Structure</b>	
<p>Permissible activities for plots of land along the river and designated boundary of the river reserve need to comply with the Land Use Zone and CULB2024.</p>	
<p><b>Public Easement Zone</b></p> <ol style="list-style-type: none"> <li>Plots located in this zone must be surrendered and are not considered as part of the open space requirement for development; and</li> <li>The minimum width is seven (7) metres for rivers of Category I and II and four (4) meters for Category III.</li> </ol>	<p><b>Permissible:</b></p> <ol style="list-style-type: none"> <li>Recreational activities;</li> <li>Elements of safe city (crime prevention through environmental design) such as the provision of CCTVs, security booths or lighting elements;</li> <li>Hardscape elements such as pedestrian and micromobility vehicle routes, gazebos, benches and platforms for riverside recreation purposes;</li> <li>Softscape elements such as shade trees, palms, shrubs and ground cover plants; and</li> <li>Urban farming is permissible with conditions for the purpose of meeting the needs of local community, and provision is subject to the related agencies and not obstructing public paths/access.</li> </ol> <p><b>Not Permissible:</b></p> <ol style="list-style-type: none"> <li>Permanent structures or buildings;</li> <li>Area for vehicle use (parking lot or access road);</li> <li>Private compound; and</li> <li>Poles for infrastructure, transport and utilities (highway or rail-based).</li> </ol>
<p><b>Transition Zone</b></p> <ol style="list-style-type: none"> <li>Plots located in this zone can be used as public space, semi-public or private space depending on the characteristics of the riverside corridor and the type of development proposed; and</li> <li>The minimum width is three (3) metres for all river categories.</li> </ol>	<p><b>Permissible:</b></p> <ol style="list-style-type: none"> <li>Recreational and socio-economic activities such as open dining (Al-Fresco);</li> <li>Elements of safe city (crime prevention through environmental design) such as the provision of CCTVs, security booths or lighting elements;</li> <li>Hardscape elements such as pedestrian and micromobility vehicle routes, gazebos, benches and platforms for riverside recreation purposes;</li> <li>Softscape elements such as shade trees, palms, shrubs and ground cover plants;</li> <li>Private compound (porches, terraces or decks) and building cantilevers from private areas (balconies or eaves); and</li> <li>The provision of a minimum requirement of two (2) metres of perimeter planting for all development schemes can be considered in this area (refer to the Planning Guidelines for Perimeter Planting Preparation, KLCH).</li> </ol> <p><b>Not Permissible:</b></p> <ol style="list-style-type: none"> <li>Structures that do not contribute to the overall character of public spaces located adjacent to public easement zones and river reserves such as rubbish collection areas;</li> <li>Direct discharge of sewage and pollutants; and</li> <li>Buildings or utility structures (unless decorated with softscape elements).</li> </ol>
<b>3. Building Orientation</b>	
<p>The following criteria apply to all new development and redevelopment of building orientation adjacent to the riverside corridor:</p> <ol style="list-style-type: none"> <li>The design of buildings and open spaces facing the river;</li> <li>Installation of boundary fences that do not obstruct views and provide access to riverside corridors; and</li> <li>Compliance with the requirements of the building setback line.</li> </ol>	
<b>4. Roads and Public Access</b>	
<ol style="list-style-type: none"> <li>Provision of public access to the Side Corridor of River; and</li> <li>Roads and public access located adjacent to the Side Corridor of River should be friendly and give priority to pedestrians with appropriate vehicle speed limits.</li> </ol>	

**Table 4.2.3:**  
**Type of Planning Control for Side Corridors of Lake and Pond**

1. Setback Requirements	
<ul style="list-style-type: none"> <li>a. The need of setback for lake and the pond means the minimum distance requirements between new development areas or redevelopment and lake and the pond or the highest water tide</li> <li>b. A minimum requirement of ten (10) metres of setback space for all lakes and ponds needs to be identified.</li> </ul>	
2. Activities and Structure	
<ul style="list-style-type: none"> <li>a. Permissible activities for plots of land along lakes and ponds and designated boundary of the lakes and ponds need to comply with the Land Use Zone and CULB2024.</li> </ul>	
<b>Public Easement Zone</b> <ul style="list-style-type: none"> <li>a. Plots located in this zone must be surrendered and are not considered as part of the open space requirement for development; and</li> <li>b. The minimum width is seven (7) metres.</li> </ul>	<b>Permissible:</b> <ul style="list-style-type: none"> <li>a. Recreational activities;</li> <li>b. Elements of safe city (Crime Prevention Through Environmental Design) such as the provision of CCTVs, security booths or lighting elements;</li> <li>c. Hardscape elements such as pedestrian and micromobility vehicle routes, gazebos, benches and platforms for riverside recreation purposes; and</li> <li>d. Softscape elements such as shade trees, palms, shrubs and ground cover plant.</li> </ul> <b>Not Permissible:</b> <ul style="list-style-type: none"> <li>a. Permanent structures or buildings;</li> <li>b. Area for vehicle use (parking lot or access road);</li> <li>c. Private compound; and</li> <li>d. Poles for infrastructure, transport and utilities (highway or rail-based).</li> </ul>
<b>Transition Zone</b> <ul style="list-style-type: none"> <li>a. Plots located in the zone can be used as public space, semi-public or private space depending on the characteristics of the lake and pondside corridor and the type of development proposed; and</li> <li>b. The minimum width is three (3) meters.</li> </ul>	<b>Permissible :</b> <ul style="list-style-type: none"> <li>a. Recreational and socio-economic activities such as open dining (Al-Fresco);</li> <li>b. Elements of Safe City (Crime Prevention Through Environmental Design) such as the provision of CCTVs, security booths or lighting elements;</li> <li>c. Hardscape elements such as pedestrian and micromobility vehicle routes, gazebos, benches and platforms for lake and pond recreation purposes;</li> <li>d. Softscape elements such as shade trees, palms, shrubs and ground cover plants;</li> <li>e. Private compound (porches, terraces or decks) and building cantilevers from private areas (balconies or eaves); and</li> <li>f. The provision of a minimum requirement of two (2) metres of perimeter planting for all development schemes can be considered in this area (refer to the Planning Guidelines for Perimeter Planting Preparation, KLCH).</li> </ul> <b>Not Permissible:</b> <ul style="list-style-type: none"> <li>a. Structures that do not contribute to the overall character of public spaces located adjacent to public easement zones and lake and pond reserves such as rubbish collection areas;</li> <li>b. Direct discharge of sewage and pollutants; and</li> <li>c. Buildings or utility structures (unless decorated with softscape elements).</li> </ul>
3. Building Orientation	
<p>The following criteria apply to all new development and redevelopment of building orientation adjacent to the Side Corridor of Lake and Pond:</p> <ul style="list-style-type: none"> <li>a. The design of buildings and open spaces facing lakes and ponds;</li> <li>b. Installation of boundary fences that do not obstruct views and provide access to lakes and ponds; and</li> <li>c. Compliance with the requirements of the building setback line.</li> </ul>	
4. Roads and Public Access	
<ul style="list-style-type: none"> <li>a. Provision of public paths to Side Corridor of Lake and Pond; and</li> <li>b. Roads and public access located adjacent to the Side Corridor of Lake and Pond should be friendly and give priority to pedestrians with appropriate vehicle speed limits.</li> </ul>	



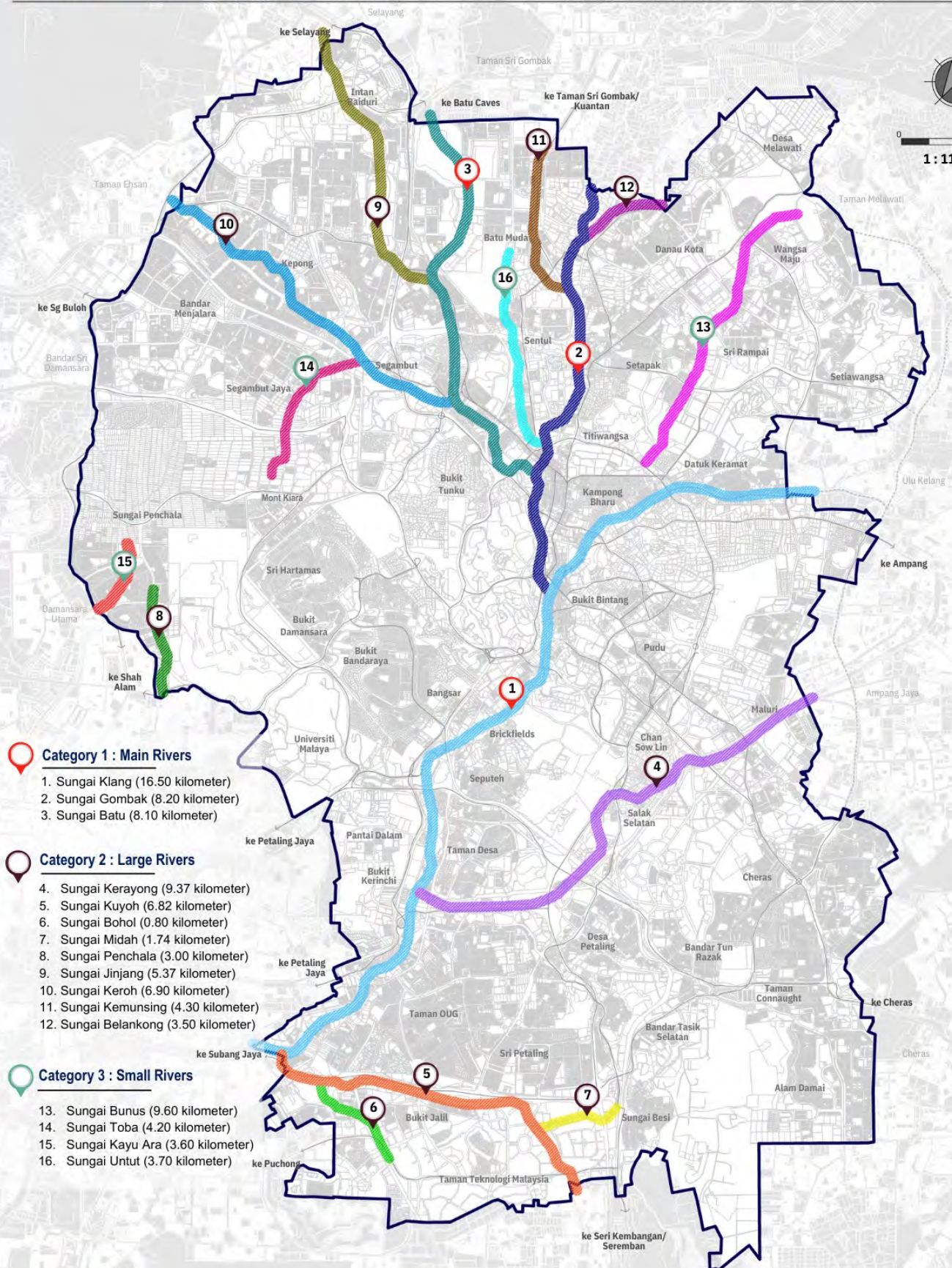


FIGURE 4.2.3 | Plan of Planning Control Layer For Side Corridors of River

Legend :

Side Corridors of River

- Main Rivers
- Large Rivers
- Small Rivers

Other

- Main Road
- Kuala Lumpur Boundary



## PLANNING CONTROL LAYER FOR HERITAGE AREAS AND BUILDINGS

The Planning Control Layer for Heritage Areas and Buildings is to ensure all buildings in the area comply with the requirements and guidelines. Compliance to this development guidelines is to conserve and maintain the special character of Kuala Lumpur Heritage Zone.

### 4-3.1 Objectives of Planning Control Layer for Heritage Areas and Buildings

The objectives of designating heritage areas and buildings are as follows:

1. To maintain and preserve buildings or sites that have historical significance for a specific area;
2. To highlight the uniqueness of Kuala Lumpur by conserving and improving the character, image and identity of cultural heritage of specific areas;
3. To ensure that changes and new developments comply with prescribe conditions based on heritage area development control mechanism; and
4. To provide long term protection on properties of heritage significance to ensure conservation through resource management on a larger scale.

### 4-3.2 Components of Planning Control Layer for Heritage Areas and Buildings

The heritage areas and buildings planning control layer is categorised into three (3) components which are as follows:

1. Heritage Zone;
2. Heritage Building; and
3. Heritage Site.

The details of each components are as shown in Table 4.3.1, 4.3.2 and 4.3.3.

**Table 4.3.1:**  
**Components of Planning Control Layer for Heritage Areas and Buildings**

A) Heritage Zone	
Refers to specific areas that have heritage significance. It requires different levels of control according to the overall character of the area, heritage buildings and new developments in the area.	
The Heritage Zone is categorised into three (3) zones:	
<ol style="list-style-type: none"> <li>1. Primary Heritage Zone;</li> <li>2. Secondary Heritage Zone; and</li> <li>3. Special Character Zone.</li> </ol>	
B) Heritage Building	
A heritage building with historical, architectural, cultural or aesthetic value that requires conservation either in whole or in part depending on the importance of the heritage category.	
It refer to a designated heritage building under Section 24 of the National Heritage Act 2005 (Act 645) or a building in the process of gazetting by the National Heritage Department (JWN). In consideration of the need for conservation and preservation of suitable shophouses, Draft KLLP2040 also determines the control on the particular buildings.	
Heritage buildings are classified into three (3) categories:	
<ol style="list-style-type: none"> <li>1. Category 1 Heritage Building;</li> <li>2. Category 2 Heritage Building; and</li> <li>3. Buildings of Heritage Significance.</li> </ol>	
C) Heritage Site	
A heritage site is identified as having important historical value to Kuala Lumpur.	
It can be designated as a heritage site under Section 24 of the National Heritage Act 2005 (Act 645), a heritage site in the gazetting process by the National Heritage Department (JWN) and other sites with heritage significance.	
Heritage sites refer to Taman Botani Perdana, Dataran Merdeka, Kompleks Sukan Negara Bukit Jalil, Taman Botani Rimba Ilmu Universiti Malaya and Taman Tasik Titiwangsa.	

## 1. Heritage Zone

The Heritage Zone consists of old buildings with a special architectural character which ought to be preserved. It is mostly concentrated in the Kuala Lumpur City Centre.

The level of importance and conservation approach in each Heritage Zone is varied depending on the character of the area, heritage buildings and new developments in the zone.



Location: Stesen Keretapi Kuala Lumpur  
Source : <https://www.shutterstock.com/g/RichieChan>

The Kuala Lumpur Railway Station is known for its Eclectic Mughal architectural style was built in 1910 as an important rail transport hub at the time. It was gazetted as a heritage building in 1983.

**Table 4.3.2: Heritage Zone Categories**

Heritage Zone Categories		Area/Site/Heritage Buildings
<b>PRIMARY HERITAGE ZONE</b>	<p>This zone consists of groups of buildings that have been gazetted under the Antiquities Act 1976 and, presently, the National Heritage Act 2005 (Act 645).</p> <ol style="list-style-type: none"> <li>1. Primary Heritage Zone of Bangunan Parlimen, Carcosa Seri Negara and Taman Botani Perdana; and</li> <li>2. Primary Heritage Zone of Dataran Merdeka, Stesen Keretapi Kuala Lumpur, Masjid Negara and Stadium Merdeka.</li> </ol>	<ol style="list-style-type: none"> <li>1. Bangunan Parlimen;</li> <li>2. Bangunan Carcosa and Istana Tetamu;</li> <li>3. Muzium Negara;</li> <li>4. Dataran Merdeka;</li> <li>5. Stesen Keretapi Kuala Lumpur and Masjid Negara; and</li> <li>6. Stadium Merdeka and Stadium Negara.</li> </ol>
<b>SECONDARY HERITAGE ZONE</b>	<p>This zone consists of buildings of historical interest and architectural value located close to each other and sited between new and old buildings.</p> <ol style="list-style-type: none"> <li>1. Secondary Heritage Zone of Jalan Petaling and Bukit Nanas; and</li> <li>2. Secondary Heritage Zone of Pekan Sungai Besi.</li> </ol>	<ol style="list-style-type: none"> <li>1. Bukit Nanas/St. John Cathedral;</li> <li>2. Jalan Tun HS Lee;</li> <li>3. Medan Pasar/Pasar Seni;</li> <li>4. Jalan Petaling/Jalan Panggung; and</li> <li>5. Jalan Suasana, Pekan Sungai Besi.</li> </ol>
<b>SPECIAL CHARACTER ZONE</b>	<p>This zone consists of old and new shophouses with some historical significance. The focus is on maintaining the character of the shophouses and urban continuity in terms of activities and functions.</p> <ol style="list-style-type: none"> <li>1. Special Character Zone of Jalan Raja Laut and Jalan Sultan Azlan Shah;</li> <li>2. Special Character Zone of Jalan Tuanku Abdul Rahman, Jalan Doraisamy and Jalan Dang Wangi;</li> <li>3. Special Character Zone of Jalan Pudu; and</li> <li>4. Special Character Zone of Brickfields.</li> </ol>	<ol style="list-style-type: none"> <li>1. Kawasan Jalan Chow Kit and Jalan Sultan Azlan Shah;</li> <li>2. Jalan Tuanku Abdul Rahman;</li> <li>3. Jalan Raja Laut;</li> <li>4. Jalan Masjid India;</li> <li>5. Jalan Melayu;</li> <li>6. Jalan Doraisamy;</li> <li>7. Jalan Kamunting;</li> <li>8. Jalan Pudu; and</li> <li>9. Brickfields area.</li> </ol>



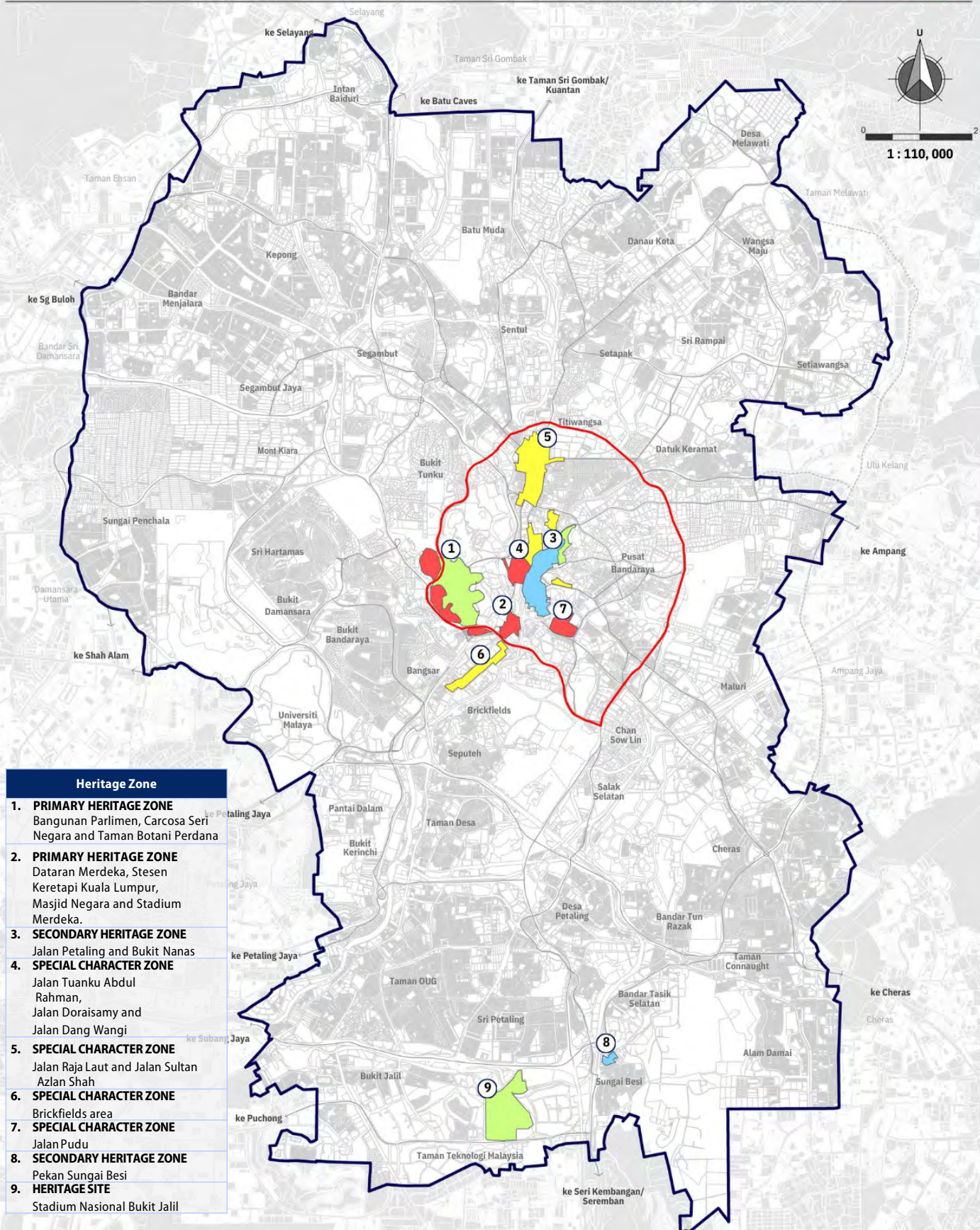
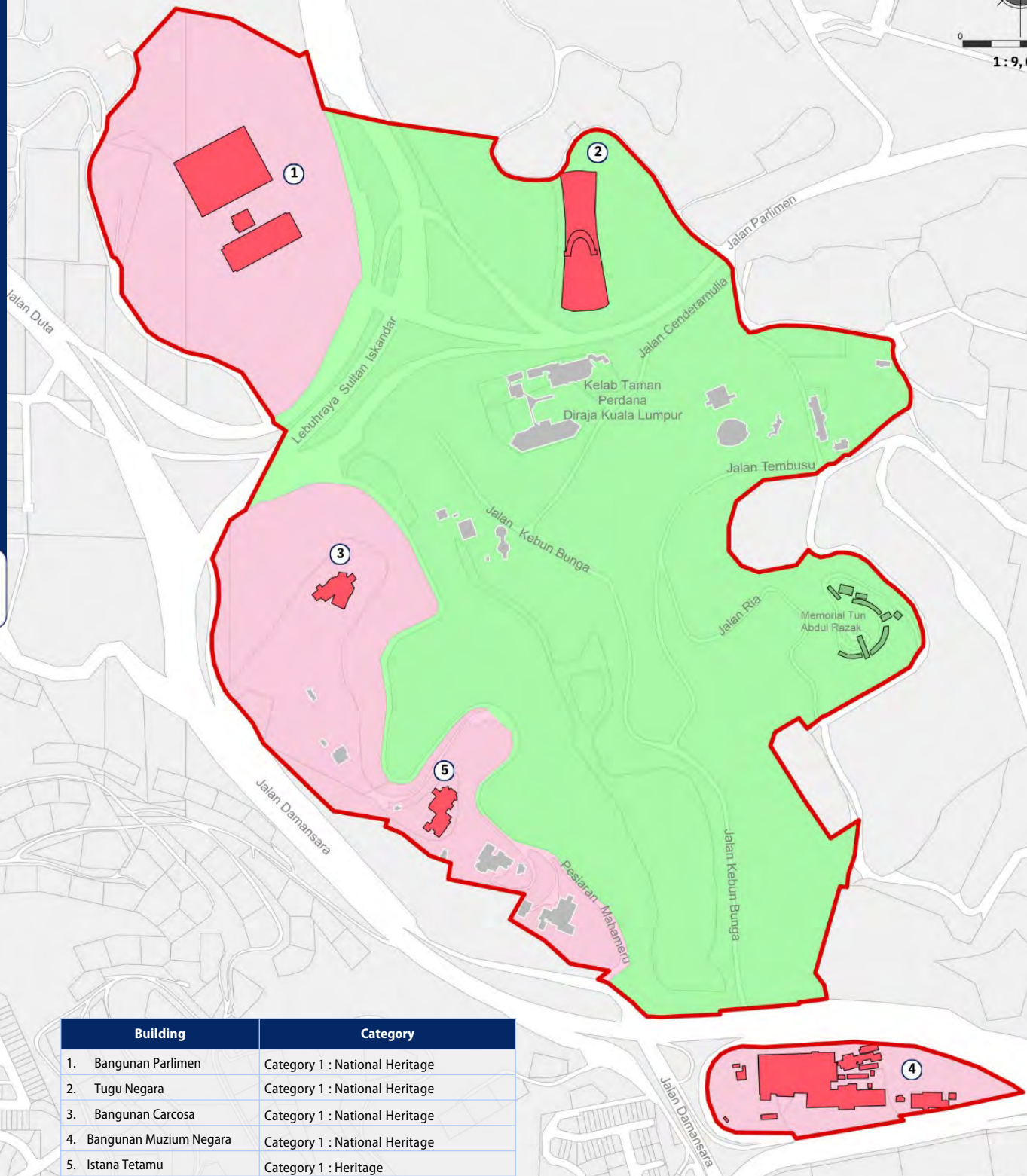


FIGURE 4.3.1 | Kuala Lumpur Heritage Zone Plan





**FIGURE 4.3.2 | Primary Heritage Zone Plan - Bangunan Parlimen, Carcosa Seri Negara and Taman Botani Perdana**

**Legend :**

**Heritage Zone**

- Primary Heritage Zone
- Heritage/Landscape

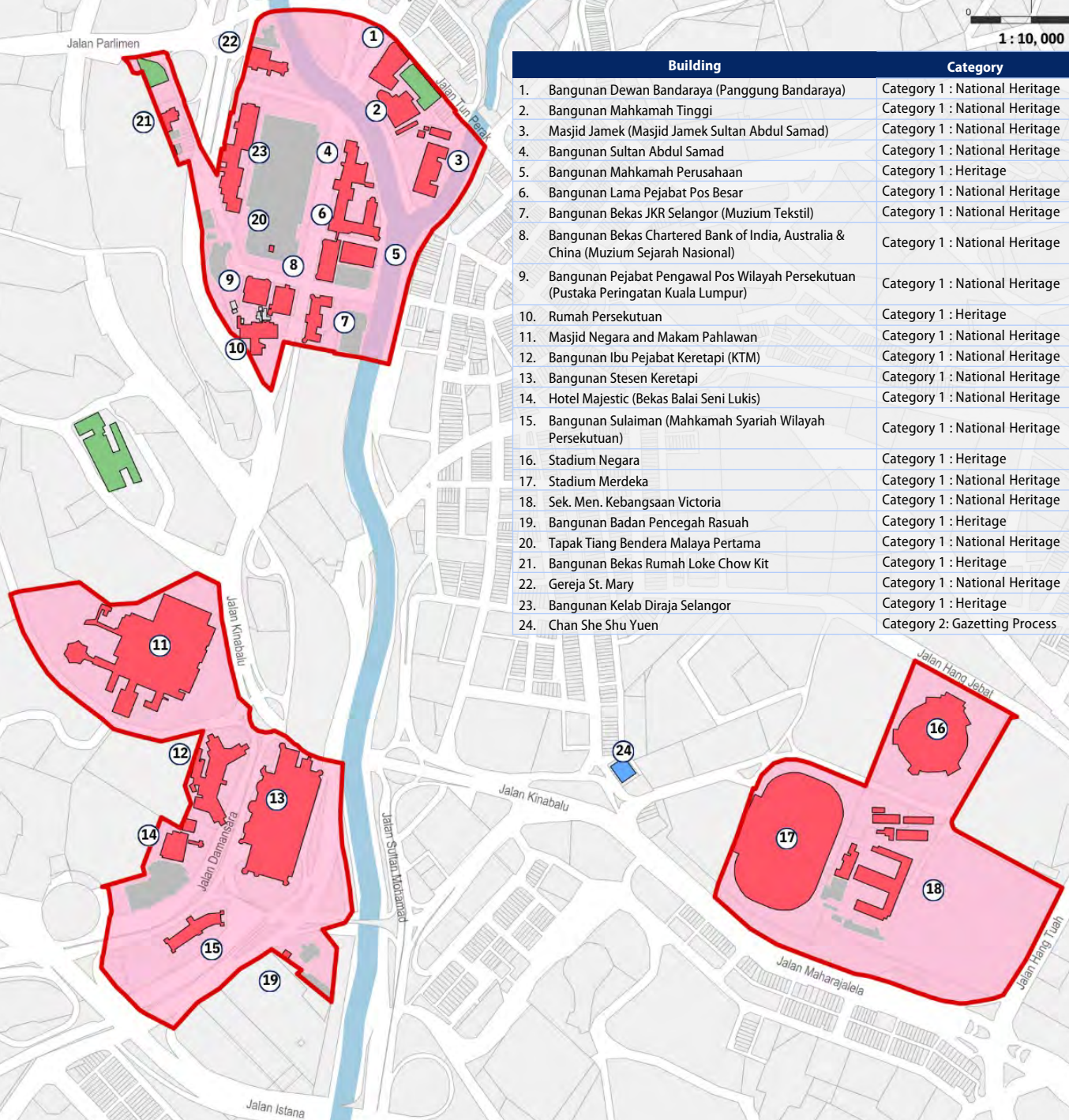
**Heritage Building**

- Category 1 Heritage Building
- Old Buildings of Heritage Interest

**Others**

- Heritage Zone Boundary





**FIGURE 4.3.3 | Primary Heritage Zone Plan - Dataran Merdeka, Stesen Keretapi Kuala Lumpur, Masjid Negara and Stadium Merdeka**

**Legend :**

**Heritage Zone**

Primary Heritage Zone

**Heritage Building**

Category 1 Heritage Building

Category 2 Heritage Building

Old Buildings of Heritage Significance

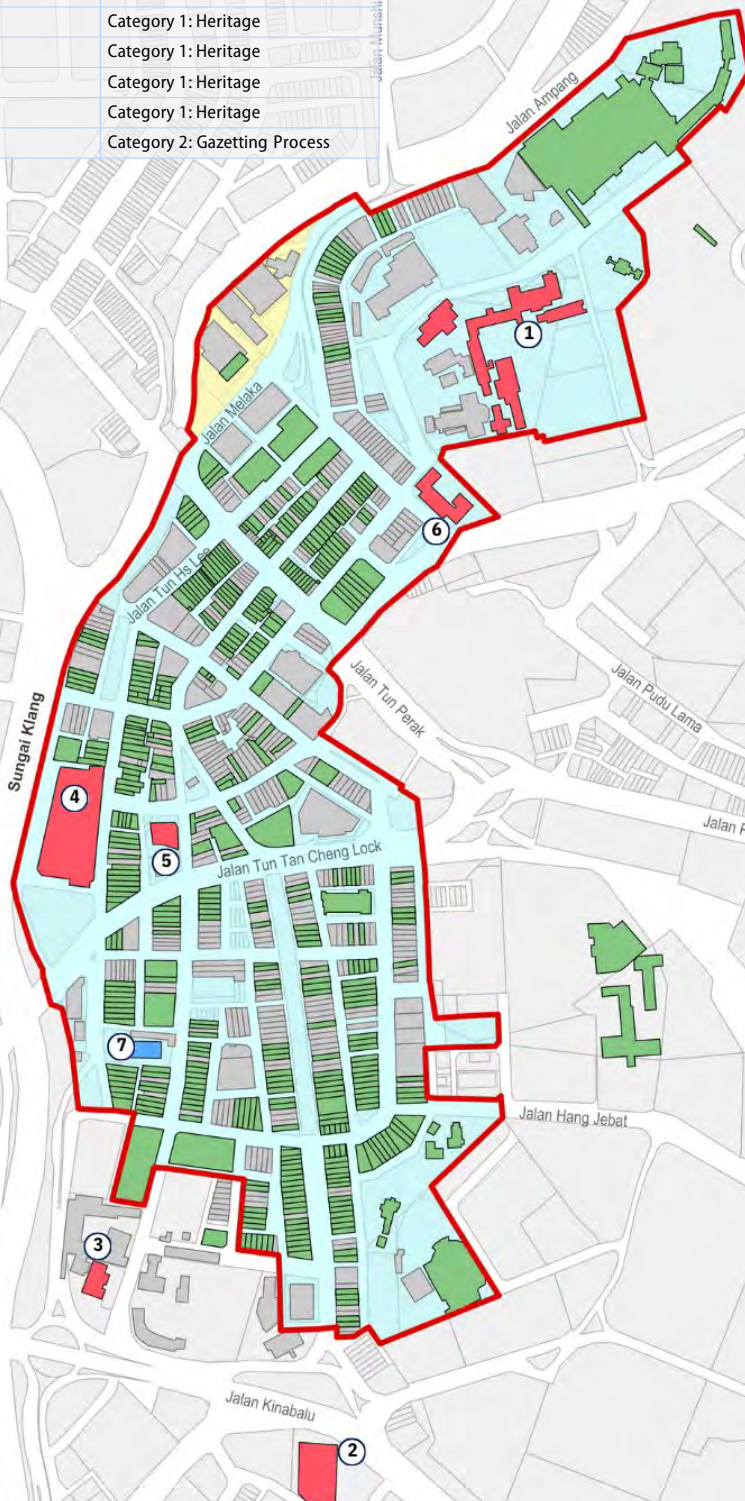
Non-Heritage Building

**Others**

Heritage Zone Boundary



Building	Category
1. Sekolah Menengah St. John	Category 1: National Heritage
2. Dewan Perhimpunan Cina Kuala Lumpur and Selangor	Category 1: National Heritage
3. Bangunan Panggung Drama	Category 1: Heritage
4. Bangunan Pasar Seni	Category 1: Heritage
5. Tokong Sin Sze Si Ya	Category 1: Heritage
6. Bangunan Muzium Telekom	Category 1: Heritage
7. Kuil Sri Maha Mariaman	Category 2: Gazetting Process



**FIGURE 4.3.4 | Secondary Heritage Zone Plan - Petaling Street and Bukit Nanas**

**Legend :**

**Heritage Zone**

Secondary Heritage Zone

**Heritage Building**

Category 1 Heritage Building

Category 2 Heritage Building

Old Buildings of Heritage Significance

Non-Heritage Building

**Others**

Heritage Zone Boundary





**FIGURE 4.3.5 | Secondary Heritage Zone Plan - Pekan Sungai Besi**

**Legend :**

**Heritage Zone**

Secondary Heritage Zone

**Heritage Building**

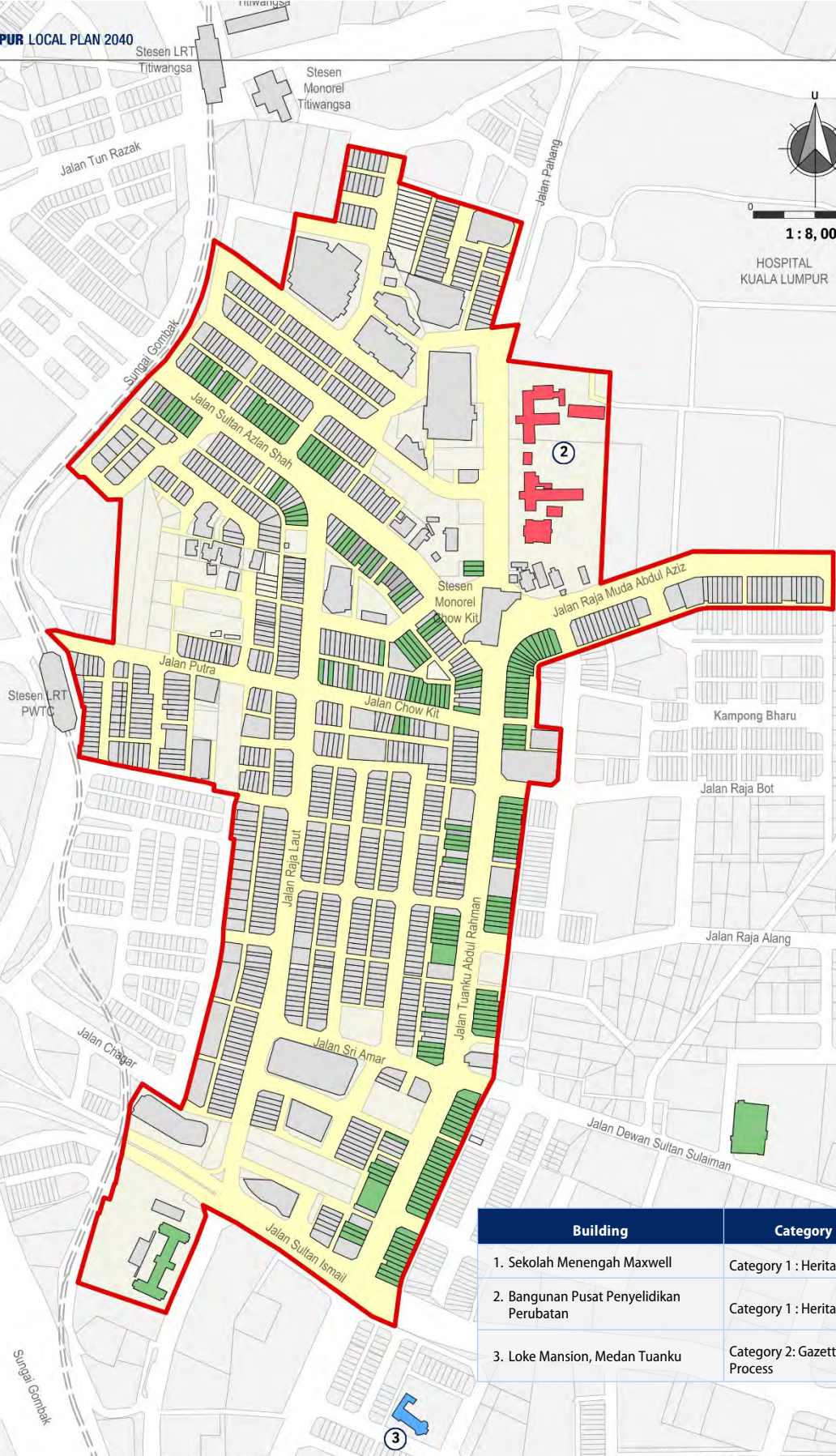
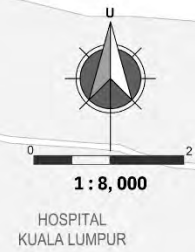
Old Buildings of Heritage Significance

Non-Heritage Building

**Others**

Heritage Zone Boundary





Building	Category
1. Sekolah Menengah Maxwell	Category 1 : Heritage
2. Bangunan Pusat Penyelidikan Perubatan	Category 1 : Heritage
3. Loke Mansion, Medan Tuanku	Category 2: Gazetting Process

**FIGURE 4.3.6 | Special Character Zone Plan - Jalan Raja Laut and Jalan Sultan Azlan Shah**

Legend :

Heritage Zone

Special Character Zone

Heritage Building

Category 1 Heritage Building

Category 2 Heritage Building

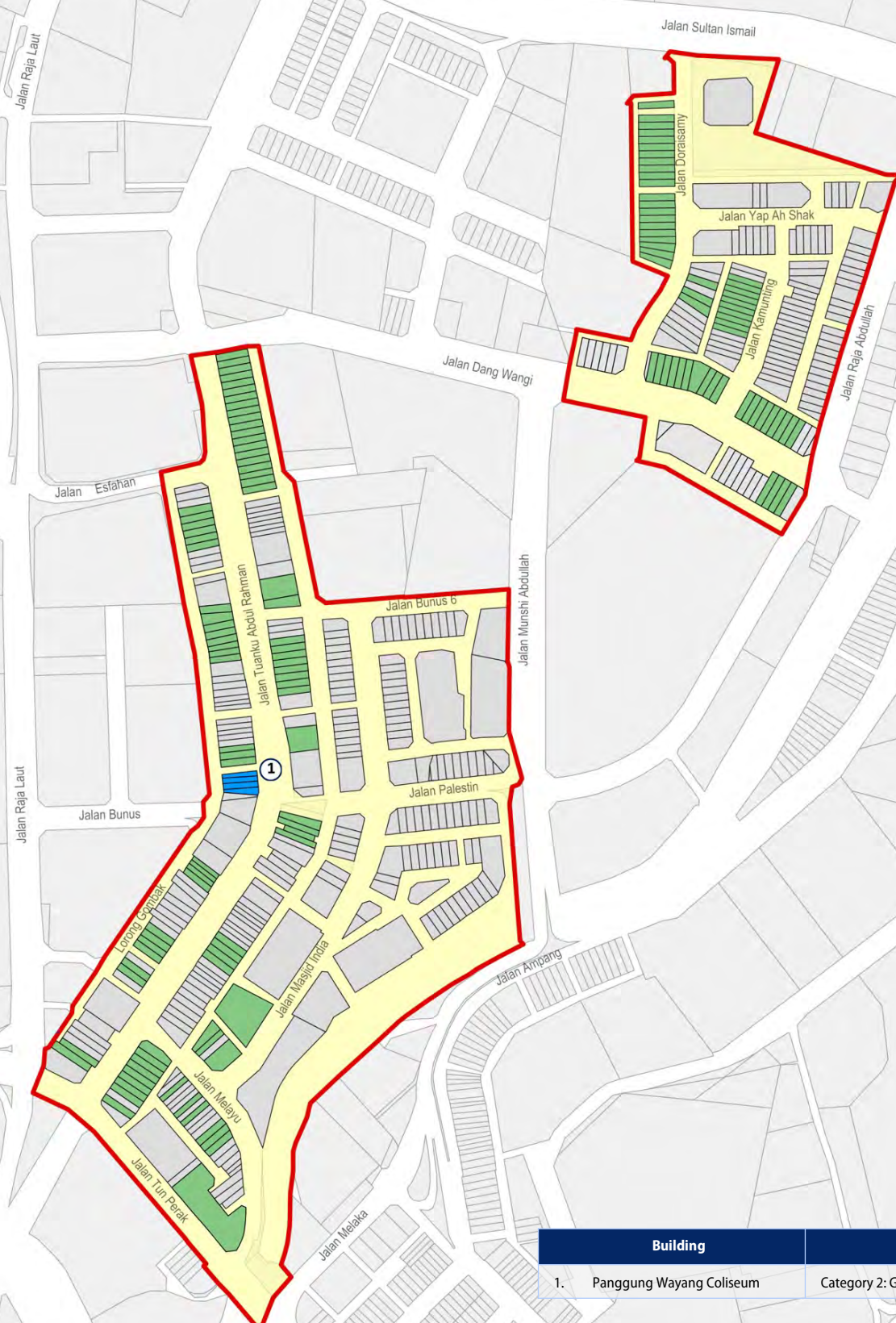
Old Buildings of Heritage Significance

Non-Heritage Building

Others

Heritage Zone Boundary



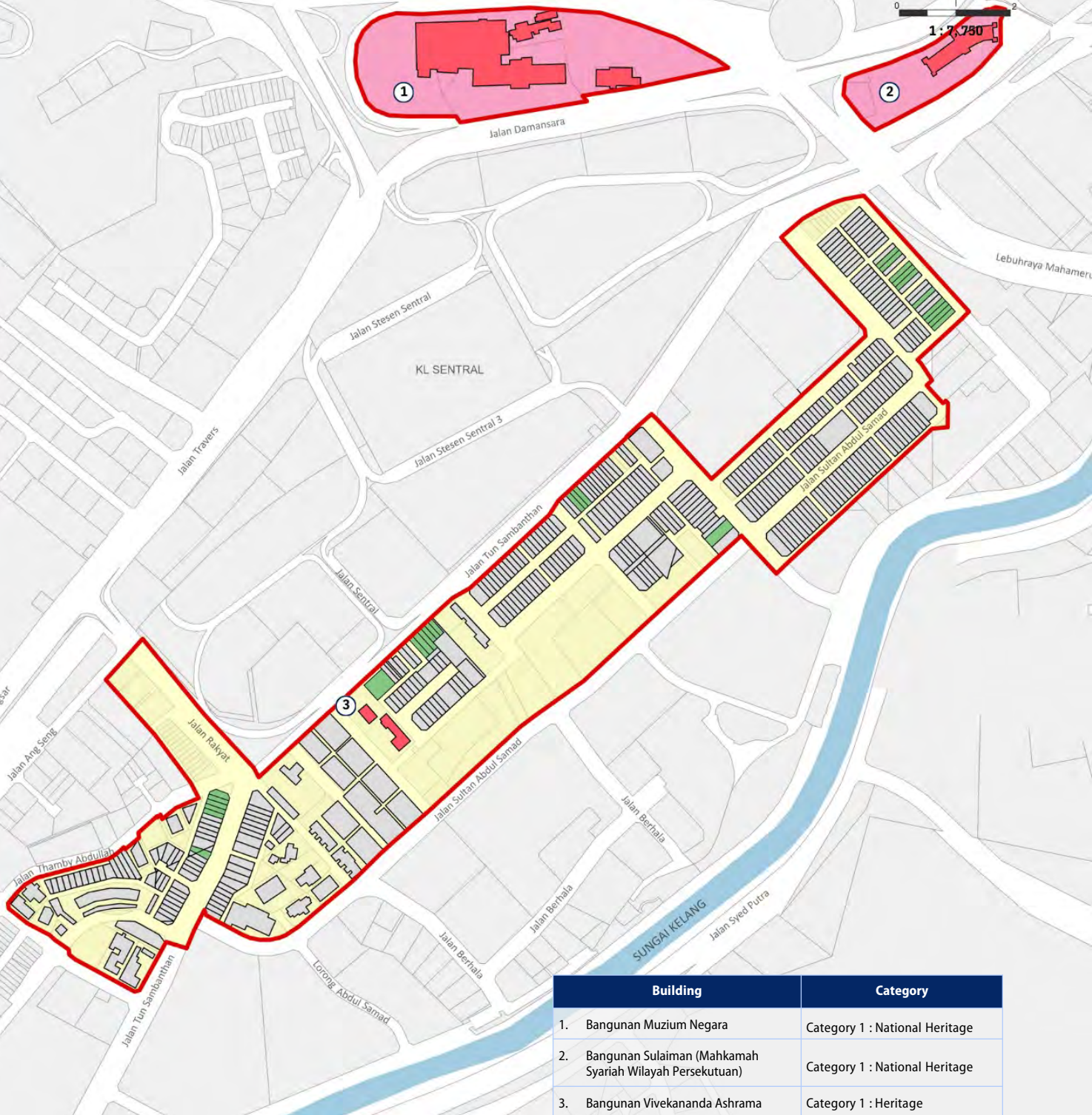


Building	Category
1. Panggung Wayang Coliseum	Category 2: Gazetting Process

**FIGURE 4.3.7 | Special Character Zone Plan - Jalan Tunku Abdul Rahman, Jalan Doraisamy and Jalan Dang Wangi**

- Legend :**
- Heritage Zone**
    - Special Character Zone
  - Heritage Building**
    - Category 2 Heritage Building
    - Old Buildings of Heritage Significance
    - Non-Heritage Building
  - Others**
    - Heritage Zone Boundary





**FIGURE 4.3.8 | Special Character Zone Plan - Brickfields Area**

**Legend :**

**Heritage Zone**

- Primary Heritage Zone
- Special Character Zone

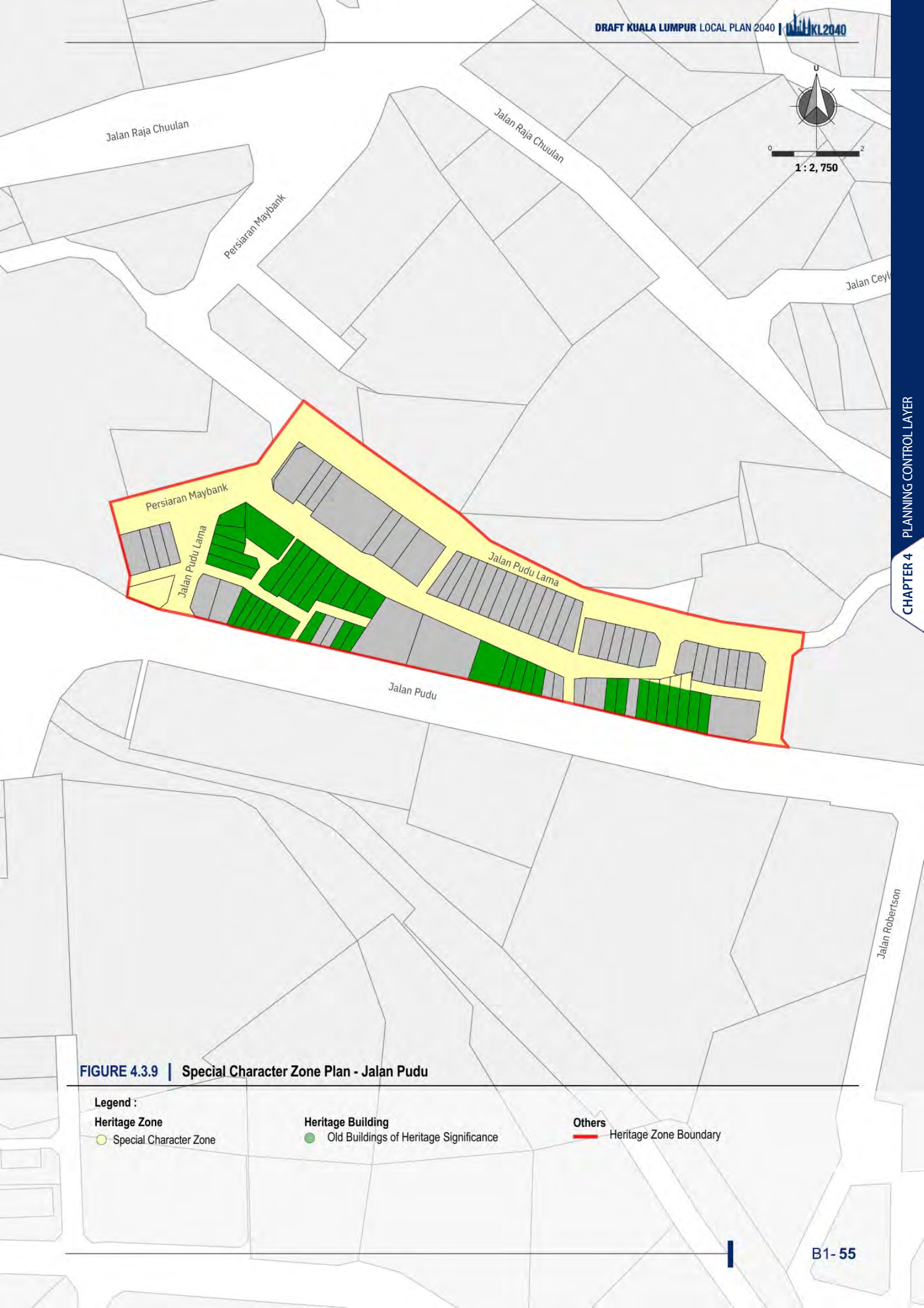
**Heritage Building**

- Category 1 Heritage Building
- Old Buildings of Heritage Significance

**Others**

- Heritage Zone Boundary







**FIGURE 4.3.9 | Special Character Zone Plan - Jalan Pudu**

**Legend :**

**Heritage Zone**

 Special Character Zone

**Heritage Building**

 Old Buildings of Heritage Significance

**Others**

 Heritage Zone Boundary

## 2. Heritage Building

Heritage buildings have historical, architectural, cultural or aesthetic values that require conservation either in whole or in part depending on the significance of the heritage category.

Old or historical buildings that have been identified will be declared heritage buildings by the Heritage Commissioner under the National Heritage Act 2005 (Act 645) or buildings with the potential to be retained under the jurisdiction of the relevant local planning authority.

The Draft KLLP2040 classifies heritage buildings into three (3) categories which are as follows:

1. Category 1 Heritage Building;
2. Category 2 Heritage Building; and
3. Buildings of Heritage Significance.

### BRIEF INFO

#### ARCHITECTURAL ERAS OF HERITAGE BUILDINGS, KUALA LUMPUR



Pre-colonial Era  
**Before 1870**



Colonial Era  
**1870 - 1957**



Independence Era  
**1945 - 1957**



Post-independence Era  
**1957 - present**

### a. Category 1 Heritage Building

Buildings and monuments that have significance in terms of design and construction.

1. Gazetted buildings under the National Heritage Act 2005 (Act 645). Encompass buildings of National Heritage and Heritage;
2. Buildings listed as National Heritage or Heritage by National Heritage Department (JWN) as shown in the Table 4.3.4;
3. Buildings with architectural, social (historical and cultural) and economic value to the community, state, country and internationally; and
4. Only conservation works is allowed on Category 1 Heritage Buildings and is fully controlled by the National Heritage Department (JWN).

**Table 4.3.3:**  
**Category 1 Heritage Building in Kuala Lumpur, 2022**

Site	National Heritage	Heritage
1. Building	30	42
2. Monument	2	1
<b>Total</b>	<b>32</b>	<b>43</b>
<b>Overall</b>	<b>75</b>	

Source : National Heritage Department, 2022



Location: Jalan Parlimen, Kuala Lumpur  
Source: <https://www.shutterstock.com/g/Laboo+Studio>

Bangunan Parlimen was gazetted as a National Heritage in 1983.

**Table 4.3.4: Category 1 Heritage Buildings and Sites**

Heritage Site	Site Category	Era - Architectural Style	Purpose	Heritage Category
1. Bangunan Bekas JKR Selangor (Muzium Tekstil), Jalan Sultan Hishamuddin	Building	Colonial-Mughal Eclectic	Gallery/Museum	National Heritage
2. Bangunan Canseleri, Dewan Tunku Canselor and Panggung Eksperimen, Universiti Malaya	Building	Post Independence-Modern	University	National Heritage
3. Bangunan Carcosa, Taman Tasik Perdana	Building	Colonial-Neo Gothic and Tudor	Empty/Idle	National Heritage
4. Bangunan Dewan Bandaraya (Panggung Bandaraya) Jalan Raja	Building	Colonial-Mughal Eclectic	Empty/Idle	National Heritage
5. Bangunan Ibu Pejabat Keretapi Tanah Melayu (KTM), Jalan Sultan Hishamuddin	Building	Colonial-Mughal Eclectic	Train Station	National Heritage
6. Bangunan Bekas Jabatan Penerangan (Blok D), Jalan Tun Perak	Building	Colonial-Mughal Eclectic	Empty/Idle	National Heritage
7. Bangunan Mahkamah Tinggi, Jalan Raja	Building	Colonial-Mughal Eclectic	Empty/Idle	National Heritage
8. Masjid Jamek Sultan Abdul Samad, (Bangunan Masjid Jamek), Jalan Tun Perak	Building	Colonial-Mughal Eclectic	Mosque/House of Worship	National Heritage
9. Bangunan Parlimen, Jalan Parlimen	Building	Post Independence-Modern	Parliament	National Heritage
10. Bangunan Pejabat Pengawal Pos Wilayah Persekutuan (Pustaka Peringatan Kuala Lumpur), Jalan Raja	Building	Colonial-Mughal Eclectic	Cafe	National Heritage
11. Bangunan Pejabat Pos Besar, Jalan Raja	Building	Colonial-Mughal Eclectic	Government Office	National Heritage
12. Bangunan Pejabat Ugama Islam W.Persekutuan (Muzium Sejarah Nasional), Jalan Raja	Building	Colonial-Mughal Eclectic	Empty/Idle	National Heritage
13. Bangunan Residen, Jalan Dato' Onn	Building	Colonial-No information	Gallery/Museum	National Heritage
14. Sekolah Menengah St. John, Jalan Bukit Nanas	Building	Colonial-Early Romanesque and European Borosque	School	National Heritage
15. Stesen Keretapi Kuala Lumpur, Jalan Sultan Hishamuddin	Building	Colonial-Mughal Eclectic	Train Station	National Heritage
16. Bangunan Sultan Abdul Samad, Blok A, Jalan Raja	Building	Colonial-Mughal Eclectic	Government Office	National Heritage
17. Dewan Perhimpunan Cina Kuala Lumpur dan Selangor, Jalan Maharajalela	Building	Colonial-Neo Classical	Hall	National Heritage
18. Gereja St. Mary, Jalan Raja	Building	Colonial-Early English Gothic	Church/House of Worship	National Heritage
19. Institut Penyelidikan Getah Malaysia, Jalan Ampang	Building	Colonial-Early Modern	Research Institute	National Heritage
20. Istana Negara (Muzium Diraja), Jalan Istana	Building	Colonial-Neo Classical	Gallery/Museum	National Heritage
21. Masjid Negara and Makam Pahlawan, Jalan Perdana	Building	Post Independence-Modern Vernacular	Mosque/House of Worship	National Heritage
22. Sekolah Menengah Kebangsaan Victoria, Jalan Hang Tuah	Building	Colonial-British Colonial	School	National Heritage
23. Stadium Merdeka, Jalan Stadium	Building	Independence era-Modern	Stadium	National Heritage
24. Tugu Negara, Jalan Parlimen	Monument	Post Independence-Not applicable	Monument	National Heritage
25. Bangunan Muzium Negara, Jalan Damansara	Building	Post Independence-Modern Vernacular	Museum	National Heritage
26. Dewan Tunku Abdul Rahman (MATIC), Jalan Ampang	Building	Colonial-Neo Classical	Hall	National Heritage
27. Bangunan lama Dewan Bahasa and Pustaka, Jalan Dewan Bahasa	Building	Post Independence-Modern	DBP Gallery	National Heritage

Source: 1. National Heritage Department, 2022  
2. Kuala Lumpur Heritage Trail Master Plan



Table 4.3.4: Category 1 Heritage Buildings and Sites (continued)

Heritage Site	Site Category	Era - Architectural Style	Purpose	Heritage Category
28. Bangunan Sulaiman (Mahkamah Syariah Wilayah Persekutuan) Jalan Sultan Hishamuddin	Building	Colonial-Art Deco	Hotel	National Heritage
29. Hotel Majestic, Jalan Sultan Hishamuddin	Building	Colonial Mughal Eclectic	Hotel	National Heritage
30. Bangunan Istana Budaya, Jalan Tun Razak	Building	Post Independence-Modern	Stage	National Heritage
31. Bangunan Perpustakaan Negara Malaysia, Jalan Tun Razak	Building	Post Independence-Modern	Library	National Heritage
32. Tapak Tiang Bendera Malaya Pertama, Jalan Raja	Monument	Post Independence-Not applicable	-	National Heritage
33. Bangunan Badan Pencegah Rasuah, Jalan Sultan Sulaiman	Building	Colonial-No information	Heritage Advancement Centre	Heritage
34. Bangunan Bekas Rumah Loke Chow Kit, Jalan Tangsi	Building	Colonial-European Baroque	Office building	Heritage
35. Bangunan Mahkamah Perusahaan, Jalan Belanda	Building	Colonial-Mughal Eclectic	Empty/Idle	Heritage
36. Bangunan Muzium Telekom, Jalan Raja Chulan	Building	Colonial-Neo Classical Greek	Museum/Gallery	Heritage
37. Bangunan Panggung Drama, Jalan Bandar	Building	Colonial-English Cottage	Empty/Idle	Heritage
38. Bangunan Pusat Penyelidikan Perubatan, Jalan Pahang	Building	Colonial-Art Deco	Research Institute	Heritage
39. Istana Tetamu, Taman Tasik Perdana	Building	Colonial-Neo Classical	Empty/Idle	Heritage
40. Bangunan Pasar Seni, Jalan Hang Kasturi	Building	Colonial-Art Deco	Market/Complex	Heritage
41. Pustaka Peringatan P.Ramlee, Jalan Dedap	Building	Post Independence-Modern	Gallery	Heritage
42. Bangunan Kelab Diraja Selangor, Jalan Raja	Building	Colonial-Mock Tudor	Club Building	Heritage
43. Asrama Anak-anak Polis Lelaki PULAPOL (JKR 2003), Jalan Semarak	Building	Colonial-No information	Institution	Heritage
44. Asrama Anak-anak Polis Wanita PULAPOL (JKR 1744-1755), Jalan Semarak	Building	Colonial-No information	Institution	Heritage
45. Bangunan Cawangan Pancaragam PULAPOL (JKR 1878), Jalan Semarak	Building	Colonial-No information	Institution	Heritage
46. Bangunan Cyberpool PULAPOL (JKR 1876), Jalan Semarak	Building	Colonial-No information	Institution	Heritage
47. Bangunan Kantin PULAPOL (JKR 1879), Jalan Semarak	Building	Colonial-No information	Institution	Heritage
48. Pejabat Kem Komandan, (Balai Pengawal) PULAPOL JKR 1786, Jalan Semarak	Building	Colonial-No information	Institution	Heritage
49. Pejabat Kewangan PULAPOL (JKR 2004), Jalan Semarak	Building	Colonial-No information	Institution	Heritage
50. Pejabat Komandan PULAPOL (JKR 2019), Jalan Semarak	Building	Colonial-No information	Institution	Heritage
51. Pejabat Seni Mempertahankan Diri PULAPOL (SMD) JKR 1874, Jalan Semarak	Building	Colonial-No information	Institution	Heritage
52. Sekolah Batu Lama PULAPOL (JKR 2006), Jalan Semarak	Building	Colonial-No information	Institution	Heritage
53. Bangunan Sukan PULAPOL (JKR 1877), Jalan Semarak	Building	Colonial-No information	Institution	Heritage
54. Bangunan Tadika Kemas PULAPOL (JKR 1869), Jalan Semarak	Building	Colonial-No information	Institution	Heritage
55. Bangunan Ujian Kecerdasan PULAPOL (UKF)(JKR 1875), Jalan Semarak	Building	Colonial-No information	Institution	Heritage
56. Bangunan Wisma Sukan PULAPOL (JKR 2076), Jalan Semarak	Building	Colonial-No information	Institution	Heritage

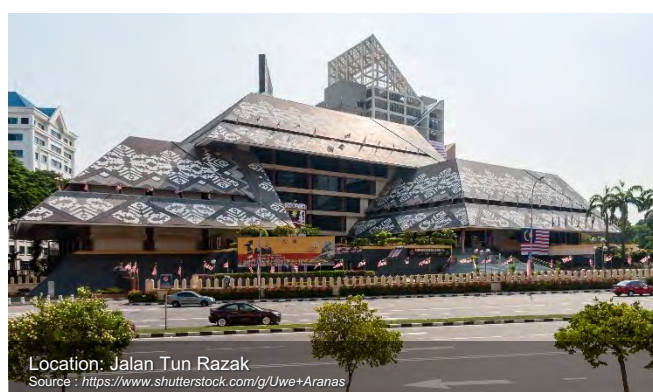
Source: 1. National Heritage Department, 2022  
 2. Kuala Lumpur Heritage Trail Master Plan



**Table 4.3.4: Category 1 Heritage Buildings and Sites (continued)**

Heritage Site	Site Category	Era - Architectural Style	Purpose	Heritage Category
57. Bengkel Senjata Pusat PULAPOL (JKR2005), Jalan Semarak	Building	Colonial- no information	Institution	Heritage
58. Berek Bujang Lelaki PULAPOL (JKR 341), Jalan Semarak	Building	Colonial- no information	Institution	Heritage
59. Berek Bujang Lelaki PULAPOL (JKR 342), Jalan Semarak	Building	Colonial- no information	Institution	Heritage
60. Berek Bujang Lelaki PULAPOL (JKR 343), Jalan Semarak	Building	Colonial- no information	Institution	Heritage
61. Berek Bujang Wanita PULAPOL (JKR 386), Jalan Semarak	Building	Colonial- no information	Institution	Heritage
62. Dewan Satu PULAPOL (JKR 2006), Jalan Semarak	Building	Colonial- no information	Institution	Heritage
63. Gurdwara Sahib PULAPOL (JKR 2078), Jalan Semarak	Building	Colonial- no information	Institution	Heritage
64. Kediaman Ajutan PULAPOL (JKR 1331), Jalan Semarak	Building	Colonial- no information	Institution	Heritage
65. Kediaman Komandan (PULAPOL) (JKR 817), Jalan Semarak	Building	Colonial- no information	Institution	Heritage
66. Kediaman Timbalan Komandan PULAPOL (Latihan) (JKR 1332), Jalan Semarak	Building	Colonial- no information	Institution	Heritage
67. Kediaman Timbalan Komandan PULAPOL (JKR 1333), Jalan Semarak	Building	Colonial- no information	Institution	Heritage
68. Rumah Kelamin Kelas F PULAPOL (JKR 1541-1544), Jalan Semarak	Building	Colonial- no information	Institution	Heritage
69. Surau PULAPOL (JKR 2085), Jalan Semarak	Building	Colonial- no information	Institution	Heritage
70. Tugu Polis Diraja Malaysia PULAPOL, Jalan Semarak	Monument	Colonial- no information	Institution	Heritage
71. Bangunan Rumah Persekutuan, Jalan Sultan Hishamuddin	Building	colonial- Utilitarian style	Government Office	Heritage
72. Stadium Negara, Jalan Stadium	Building	Post Independence- Modern	Stadium	Heritage
73. Bangunan Vivekananda Ashrama, Jalan Tun Sambanthan	Building	Colonial- Indian Renaissance	School	Heritage
74. Tokong Sin Sze Si Ya, Jalan Tun HS Lee	Building	Colonial- Traditional Chinese Temple Architecture	Temple/House of worship	Heritage
75. Sekolah Menengah Maxwell, Jalan Tun Ismail	Building	Colonial- Moorish and Gothic	School	Heritage

Source: 1. National Heritage Department, 2022  
2. Kuala Lumpur Heritage Trail Master Plan



Perpustakaan Negara Malaysia building at Jalan Tun Razak is a national heritage



Tugu Negara is a National Heritage.

## b. Category 2 Heritage Building

This category refers to buildings with historical significance or heritage architectural value that are in the process of gazettement under the National Heritage Act 2005 (Act 645). Generally, prior commitment has been agreed between the building owner and National Heritage Department (JWN).

The listed buildings also comprise those in an inventory list of the JWN that is expected to be gazetted during the planning period of the Draft KLLP2040.

Renovations and alteration of building in Category 2 Heritage Buildings will be referred to the JWN and the KLCH Design Review Panel. There are 10 buildings listed as Category 2 Heritage Buildings as shown in Table 4.3.5.



Location: Jalan Tuanku Abdul Rahman

The Coliseum Theatre building built in 1920 with a Neo-Classical architectural style is currently in the process of being gazetted by the National Heritage Department (JWN).



Location: Jalan Pahang, Taman Setapak Indah

Jamiul Ehsan Mosque (Old Setapak Mosque) is one of the oldest mosques in Kuala Lumpur. This mosque was originally made of wood with a pyramidal roof built in Kampung Puah, Setapak in 1879.

**Table 4.3.5:**  
**Category 2 Heritage Buildings in Kuala Lumpur, 2022**

Heritage Site	Site Category	Era - Architectural Style	Usage	Status
1. Terminal Udara dan Menara Kawalan Trafik, Lapangan Terbang Sungai Besi	Building	Colonial - no information	Institution	In gazettement process
2. Loke Mansion, Medan Tunku	Building	Colonial-Neo Classical	Private Institution	In gazettement process
3. Bangunan Arkib Negara Malaysia, Jalan Tuanku Abdul Halim	Building	Post Independence – no information	Institution	In gazettement process
4. Bangunan Institut Kefahaman Islam Malaysia (IKIM), Jalan Tuanku Abdul Halim	Building	Post Independence – no information	Institution	In gazettement process
5. Masjid Jamiul Ehsan (Masjid Lama Setapak), Jalan Pahang	Building	Kolonial-Sarasin	Mosque/House of Worship	In gazettement process
6. Kuil Sri Maha Mariaman, Jalan Bandar	Building	Kolonial-Dravidian	Temple/House of Worship	In gazettement process
7. Panggung Wayang Coliseum, Jalan Tuanku Abdul Rahman	Building	Colonial-Neo Classical	Restaurant/Cafe	In gazettement process
8. Galeri Pembentukan IWK, Loji Rawatan Kumbahan Pantai 1, Pantai	Building	Pre-Independence- no information	Gallery	In gazettement process
9. Bangunan No. 2, Jalan Stonor (Badan Warisan Malaysia)	Building	Colonial-British Colonial	Association Building	In gazettement process
10. Chan She Shu Yuen	Building	Classic Chinese	Association Building	In gazettement process

Source: National Heritage Department (JWN), 2022



### c. Heritage Significance Building

Buildings of heritage significance have some elements or characteristics of architectural or historical importance that have been recommended for preservation to form the heritage character of the city of Kuala Lumpur. The importance of conserving such buildings is under the jurisdiction of the KLCH Design Review Panel. Most of the buildings in this category are old shophouses with distinct heritage design and character and are recommended for preservation.

## 3. Heritage Site

This is a site or location identified as having historical value that is important to the culture of the community. It refers to any site that remains in a specific location and cannot be moved to another location such as a historical park, nature reserve and old burial ground.

A heritage site refers to the site itself with or without buildings to be preserved for its significance.

Cultural heritage sites should be seen as entities that are:

1. Permanent and non-renewable;
2. Valuable; and
3. Very sensitive.

Whenever a heritage site and its surroundings are threatened by the effects of development, priority should be given to the preservation of the physical form of the site. Five (5) heritage sites are listed and identified in Kuala Lumpur, as shown in Table 4.3.6.

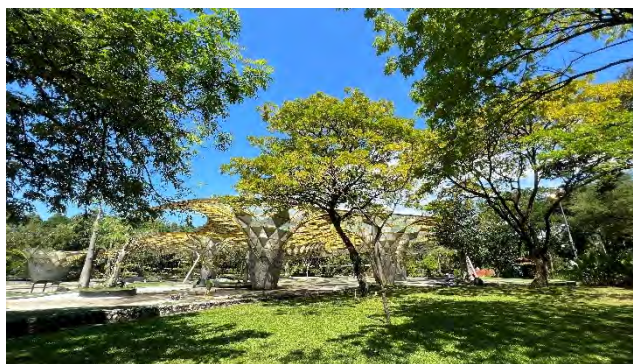
**Table 4.3.6:**  
Heritage Sites in Kuala Lumpur, 2022

Heritage Site	Category
1. Taman Botani Perdana (Taman Tasik Perdana)	National Heritage
2. Dataran Merdeka/Flagpole Base	National Heritage
3. Taman Botani Rimba Ilmu, Universiti Malaya	In the gazettement process by JWN
4. Taman Tasik Titiwangsa	In the gazettement process by JWN
5. Stadium Nasional Bukit Jalil	Historical Site



Location: Jalan Tuanku Abdul Rahman

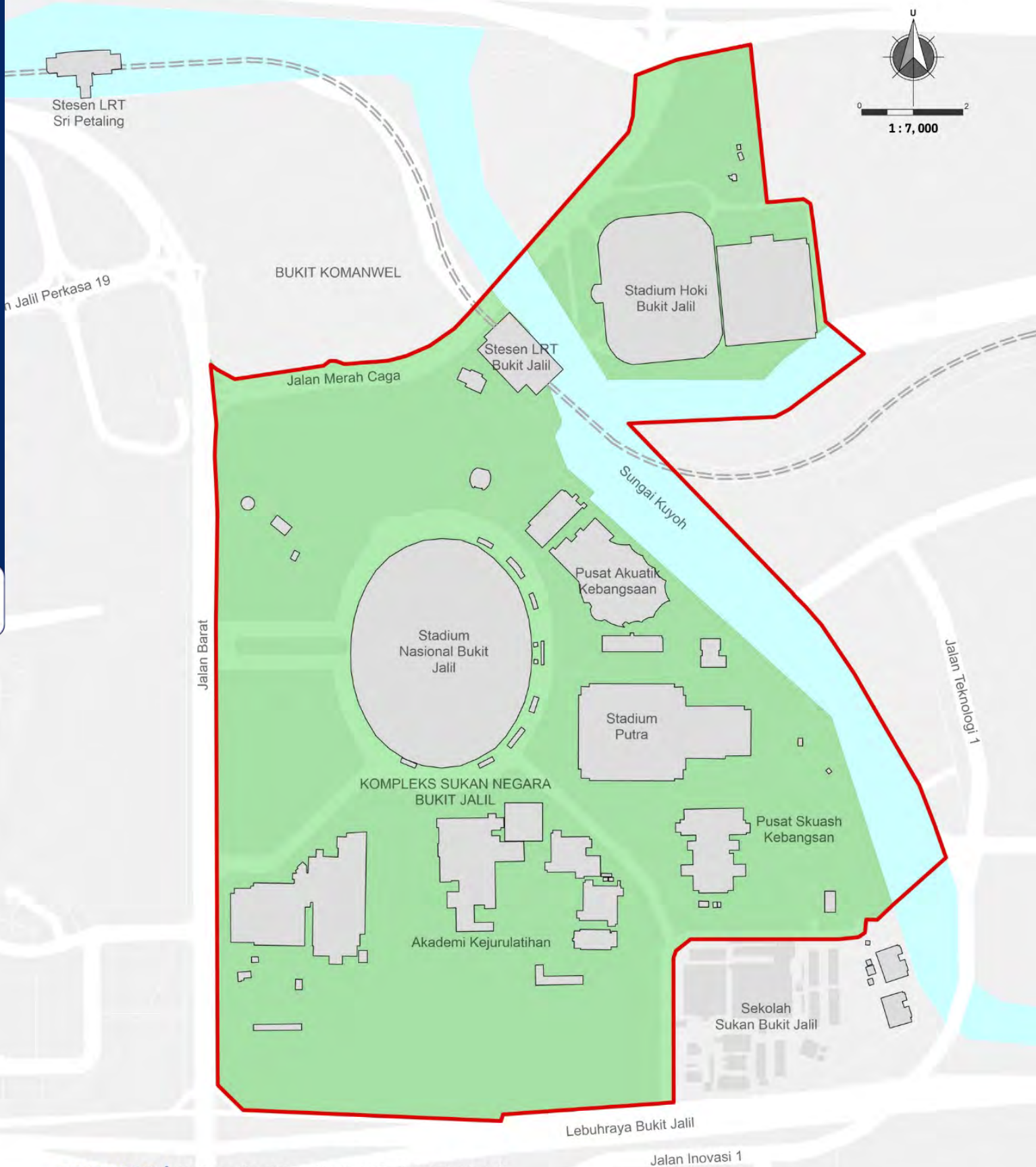
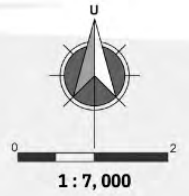
A row of old shophouses at Jalan Tuanku Abdul Rahman (near SOGO Complex) with heritage significance that is suitable to be maintained.



Location: Taman Botani Perdana Kuala Lumpur

Taman Botani Perdana Kuala Lumpur with an area of 91.6 hectares, formerly known as Taman Tasik Perdana Kuala Lumpur was established in 1888 and gazetted as a National Heritage in 2018.





**FIGURE 4.3.10 | Heritage Site Plan - Stadium Nasional Bukit Jalil**

**Legend :**

**Heritage Zone**  
 ● Heritage Site

**Heritage Building**  
 ○ Non-Heritage Building

**Others**  
 — Heritage Zone Boundary

### 4-3.3 Heritage Zone Planning Control

Heritage zone planning control refers to the conditions and regulations used to determine the permitted limits for development in the Heritage Zone. The focus components are buffer area, intensity control (number of storeys), approval of Commissioner of Heritage, Heritage Impact Assessments (HIA) report and engineering report, approval as well as general conditions. The details of the control are presented in Table 4.3.7.

**Table 4.3.7: Component Heritage Zone Planning Control**

Component	General Guideline	Primary Heritage Zone			Secondary Heritage Zone			Special Character Zone		
		Category			Category			Category		
		1	2	3	1	2	3	1	2	3
<b>Buffer Area</b>	Comply with the provision of a buffer zone within 200 meters of Category 1 Heritage Buildings.		✓	✓		✓	✓		✓	✓
	Comply with buffer zone requirements. The specific conditions will be decided by the Commissioner of Heritage in accordance to the physical assessment of the site and adjacent existing developments.	✓			✓			✓		
<b>Intensity Control (Number of Storeys )</b>  (Refer to Chapter 3-3: Height Control of Commercial Terrace)	Maintain the building scale, height and shape of the building based on the original features.	✓			✓			✓		
	Height limit of ten (10) storeys for shophouses building, infill sites and redevelopment in Special Character Zone at Jalan Tuanku Abdul Rahman, Jalan Masjid India, Lorong Haji Taib, Jalan Chow Kit and Lorong Bonus. The original façade of the renovated shop house should be maintained and subject to specific conditions which allowed.								✓	
	Height limit of seven (7) storeys for shophouse buildings, infill sites, and redevelopment in the heritage zone in Jalan Tun Sambanthan, Jalan Sultan Abdul Samad, Lorong Ayer Kuning, Jalan Tambypillai, Jalan Vivekananda, Jalan Tun Tan Cheng Lok, Jalan Hang Kasturi, Jalan Petaling and Jalan Hang Lekiu. The original façade of the renovated shop house must be maintained and subject to the specific conditions allowed.					✓				
	Height limit of up to seven (7) storeys is limited to building areas on Jalan Doraisamy (next to the Sheraton hotel) and Jalan Kamunting. The original façade of the renovated shop house must be maintained and subject to the specific conditions allowed.								✓	
	Height limit of up to three (3) storeys is limited to the front building area of SOGO Complex, Jalan Tuanku Abdul Rahman (Lot 22-36, Lot 190-191, Lot 20000 -20002).								✓	✓
	Height limit of up to three (3) storeys is limited to shop house building in Jalan Suasa, Jalan Suasa 3, 4 and 5, Pekan Sg Besi.					✓				
	Maintain a height limit of three (3) storeys is limited to the Old Gian Singh building area on Jalan Tun Perak (from Lebu Ampang junction to Masjid Jamek LRT Station)					✓			✓	✓
	The determination of height for new development on the individual lot should be according to the allowable plot ratio.			✓			✓			✓

Table 4.3.7: Component Heritage Zone Planning Control (continued)

Component	General Guideline	Primary Heritage Zone			Secondary Heritage Zone			Special Character Zone		
		Category			Category			Category		
		1	2	3	1	2	3	1	2	3
<b>Commissioner of Heritage's Approval</b>	Any changes and additions to Category 1 Heritage Buildings must be approved by the Commissioner of Heritage.	✓			✓			✓		
	Any application for a Development Order (DO) within 200 meters of Category 1 and 2 Heritage Buildings must be referred to the Heritage Commissioner under Section 40, National Heritage Act, 2005.	✓	✓		✓	✓		✓	✓	
<b>HIA Report and Engineering Report</b>	The requirement for a Heritage Impact Report (HIA) for any renovation and alterations to the Heritage Buildings is subject to the Commissioner of Heritage under Section 40, National Heritage Act, 2005 (Act 645).	✓	✓		✓	✓		✓	✓	
	An Engineering Report is required for alterations of building structures and demolition.	✓	✓	✓	✓	✓	✓	✓	✓	✓
<b>Approval</b>	Application for approval of any building demolition within the Heritage Zone.	✓	✓	✓	✓	✓	✓	✓	✓	✓
	Application for approval of the rehabilitation of damaged or collapsed structures in the Heritage Zone.	✓	✓		✓	✓		✓	✓	
	Application for approval of all new developments within the Heritage Zone.	✓	✓	✓	✓	✓	✓	✓	✓	✓
	Application for approval of adaptive reuse of Heritage Building.	✓	✓	✓	✓	✓	✓	✓	✓	✓
<b>General Conditions</b>	Comply with the conditions under Section 40, National Heritage Act 2005 (Act 645).	✓			✓			✓		
	Comply with the Urban Design Guidelines Kuala Lumpur City Centre (UDGKL).	✓	✓	✓	✓	✓	✓	✓	✓	✓
	Ensure that any developmental activity does not diminish the value of the Heritage Zone.	✓	✓	✓	✓	✓	✓	✓	✓	✓
	Requires regular maintenance and monitoring of heritage buildings.	✓			✓			✓		
	The need for the conservation of heritage buildings is decided by the KLCH Design Review Panel.		✓	✓		✓	✓		✓	✓
	New development must comply with design requirements to maintain its significance as a Heritage Zone.	✓	✓	✓	✓	✓	✓	✓	✓	✓
	New development in this zone must be referred to the DBKL Design Review Panel and related agencies for approval.	✓	✓	✓	✓	✓	✓	✓	✓	✓

Legend:

**CATEGORY 1**  
Building Category 1**CATEGORY 2**  
1. Building Category 2  
2. Infill Site Development (commercial terrace)  
3. Redevelopment**CATEGORY 3**  
1. New Development of Individual Lot



#### 4-3.4 Heritage Area Character Design Guide

The heritage area character design guide encompasses the Primary Heritage Zone, Secondary Heritage Zone and Special Character Zone. The details components that require attention for development activities within the area are shown in Table 4.3.8.

Most of the old buildings in Kuala Lumpur are about 100 years old or more and have architectural value and historical heritage that must be preserved according to specific guidelines.

In addition to the Urban Design Guidelines Kuala Lumpur (UDGKL), a general guideline for the character design of heritage areas must be prepared. This is to control the continuity of historical buildings in the Kuala Lumpur Heritage Zone so that the 'sense of place' does not disappear and following the designated Heritage Trail theme.

The design of the building and its surroundings must reflect the heritage identity and image based on the suitability of the development whether it is a commercial building, institution, public transport terminal and others.

Visual quality is very important to heritage preservation. The design and placement should not detract from the view and architectural character of an old building. Aspects that must be considered in the preparation of the character design guidelines of the heritage area are as follows:

1. Ensure that outstanding universal values are not disturbed;
2. Consider the heritage significance of the building or area;
3. Comply with relevant guidelines that preserve the heritage and historical value of the building;
4. Basic principles for conservation of heritage buildings:
  - a. Retain the elements of authenticity and integrity that are the identity of heritage buildings;
  - b. Retain all Category 1 Heritage Buildings;
  - c. Retain the original internal structure;
  - d. Retain the original surface of the building by not painting on previously unpainted surfaces; and
  - e. Retain any part of the building with new building materials.

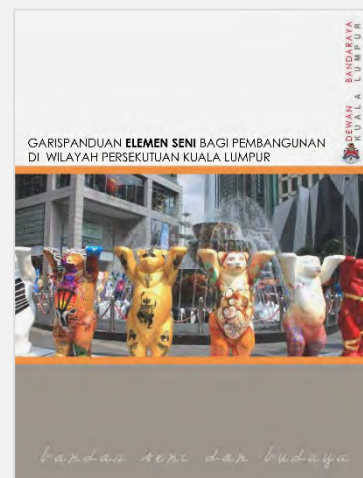
5. Retain the original pattern on any part of the heritage building; and
6. Consider the visual impact of a newly constructed building through:
  - a. Comply with the height requirements set for a particular area;
  - b. Create harmony among nearby heritage buildings in terms of image or setting as a Heritage Zone or Heritage Trail Zone;
  - c. Avoid excessive prominence that can diminish existing heritage buildings or monuments; and
  - d. Consider views from various angles including from the main road, ground or pedestrian level and focal areas.

#### BRIEF INFO

##### GUIDELINES RELATED TO HERITAGE AREAS



The Urban Design Guidelines Kuala Lumpur (UDGKL) is an example of a specific guideline that can be used as a reference to control the development of heritage areas in Kuala Lumpur City Centre.



Guidelines on art elements for the development of Kuala Lumpur as a city of art and culture.

Table 4.3.8 lists the main components for building character control guidelines in Primary Heritage Zones, Secondary Heritage Zones, and Special Character Zones. Meanwhile, Table 4.3.9 details the Heritage Zone character control guide.

Components that give a unique character to heritage areas include architectural style and building design, building materials, space configuration, building placement and setback, building height and scale, landscape elements, as well as environmental aesthetics.



Aerial view of Masjid Negara.



Aerial view of Muzium Negara.

## BRIEF INFO

### HERITAGE ZONE CHARACTER CONTROL



Heritage Zone Character Control involve

**14** MAIN COMPONENTS

**Table 4.3.8:**  
**Main Components of Heritage Zone Character Control Guide**

<b>1</b>	Regeneration and Infill Development in Heritage Zones														
<b>2</b>	Orientation of The Building Facing The Local Road of The Heritage Area														
<b>3</b>	Adaptive Re-use of Building														
<b>4</b>	Building Design Elements <table> <tr> <td><b>a</b></td><td>Building Facade Control</td></tr> <tr> <td><b>b</b></td><td>Envelope Control</td></tr> <tr> <td><b>c</b></td><td>Building Scale</td></tr> <tr> <td><b>d</b></td><td>5-Foot Walkways and Verandah Ways</td></tr> <tr> <td><b>e</b></td><td>Roof</td></tr> <tr> <td><b>f</b></td><td>Heritage Building Colour Scheme</td></tr> <tr> <td><b>g</b></td><td>Floor Finish</td></tr> </table>	<b>a</b>	Building Facade Control	<b>b</b>	Envelope Control	<b>c</b>	Building Scale	<b>d</b>	5-Foot Walkways and Verandah Ways	<b>e</b>	Roof	<b>f</b>	Heritage Building Colour Scheme	<b>g</b>	Floor Finish
<b>a</b>	Building Facade Control														
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<b>c</b>	Building Scale														
<b>d</b>	5-Foot Walkways and Verandah Ways														
<b>e</b>	Roof														
<b>f</b>	Heritage Building Colour Scheme														
<b>g</b>	Floor Finish														
<b>5</b>	Signboard or Advertisement Signboard														
<b>6</b>	Heritage Building Lighting Elements														
<b>7</b>	Utilities, Electrical Cables, and Plumbing														
<b>8</b>	Needs of Persons with Disabilities (OKU) and Age-Friendly														
<b>9</b>	Security Surveillance														
<b>10</b>	Integration of Heritage and New Buildings														
<b>11</b>	Entrance Gate To The Heritage Zone														
<b>12</b>	Landscape Elements														
<b>13</b>	Street Furniture														
<b>14</b>	Encouraged Type of Activities <table> <tr> <td><b>a</b></td><td>AI-Fresco Activity Area</td></tr> </table>	<b>a</b>	AI-Fresco Activity Area												
<b>a</b>	AI-Fresco Activity Area														

Table 4.3.9: Heritage Zone Character Control

Component	Control Actions
<b>Regeneration and Infill Development In Heritage Zone</b>	Regeneration must consider the background and history of the site.
	Buildings or sites that do not have heritage architectural significance can be demolished and replaced with a harmonious new development or upgraded with the provision of a 'false façade' adapted to the image and character of the area.
	Old buildings in the Heritage Zone that are to be demolished must have a verification report or building structure control approval from a certified engineer.
	Redevelopment in the Heritage Zone must follow the setting or scale and maintain the original landscape of the area.
	The composition of the new building façade must consider the design aspects, materials used, and textures of the adjacent buildings.
	New development needs to match the scale of adjacent buildings and overall scale of buildings within the Heritage Zone.
	For old buildings that are to be demolished, a measured drawing must be provided for KLCH reference and records.
<b>Orientation of building facing the local road in the heritage area</b>	The orientation of a new building must not obstruct the view towards the heritage building in order to highlight the 'visual exposure' to an open area, main road or facing the river.
	New buildings must consider public accessibility to the river reserve/back lane/plaza that have been upgrade for public walkways within the heritage zone.
	A new free-standing building facing and adjacent to a shophouse building in the heritage area must ensure harmony with the adjacent building in terms of height and activities following the heritage building in front and adjacent to it.
	Developments that aimed at visual monopoly, access and privacy of specific individuals in the heritage area should be avoided.
<b>Adaptive Re-use of Buildings</b>  Adaptive reuse is the process of reusing an existing building for a new function rather than its original purpose. It is a strategy to lengthen the life of heritage buildings to prevent damage with new, more competitive functions.	Must be sensitive and consider the historical value, architectural significance, and character of the heritage area.
	Maintain the profile of the shophouse building with the original design (horizontal rhythm) including balconies, ventilation, roof design, stairs, doors, and windows.
	An increase of potential through maximum use of interior space that adds value to heritage or old buildings is encouraged.
	All activities must comply with the permitted CULB2024.
	Ensure that changes in building fabric do not change the original characteristics of heritage elements, structures nor have a negative impact on the significance of heritage buildings and their original position.



Table 4.3.9: Heritage Zone Character Control (continued)

Component	Control Actions
<b>Building Design Elements</b> The design of buildings in the heritage zone must blend with the climatic conditions and ethnic influences appropriate to the era of construction.	The design of new buildings should emphasis on the visual and functional aspects of the Heritage Zone and surrounding buildings. The design of buildings at intersections or important buildings must have a strong character and identity through an emphasis on corner treatment. The design of new buildings must be harmonised with adjacent development, local physical and socio-cultural characteristics. Planning of layout, design of buildings, and surroundings within the Heritage Zone must create harmonious social interaction and have a local image. Heritage architecture such as roofs, windows and doors are designed to suit with the climate and harmony with ethnic influences in line with the era of construction.
<b>a. Building Facade Control</b> Façade refers to the exterior or surface that stands out the most on either the front or side of the building facing the pedestrian path. Façade conservation components include conservation from the ground floor, upper floor openings and consist of the control of horizontal and vertical lines such as walls, parapet lines, fascia lines or party wall pilasters.	Preserve and conserve the decorative elements of the original façade of old buildings in the Heritage Zone. The use of appropriate materials and methods in the process of restoration or improvement of heritage building façades is highly encouraged. The installation of roller shutters on the façade of heritage buildings is not recommended. The installation of air conditioning pipes and ducts on heritage building façades should be avoided while installation at the back of the building must be covered neatly.
<b>b. Envelope Control</b> Three (3)-dimensional envelope control involves height control, building lines, building setback, parapet lines, line of sight, and main roof.	Envelope control should be applied in the area of old shophouses with heritage significance. Envelope control in the heritage zone should observe the height control permitted that encompasses the number of floors and ground floor. Coordination with Urban Design Guidelines Kuala Lumpur City Centre (UDGKL).
<b>c. Building Scale</b> The original scale of heritage buildings should be maintained. New buildings should be balanced and sensitive to the surrounding heritage buildings in terms of height, size and design.	Block massing buildings must be distributed harmoniously so as not to form large blocks that will change the urban fabric in the Heritage Zone. The scale of a new building should consider the existing development to create a harmonious view of the surroundings within the Heritage Zone.
<b>d. 5-Foot Walkways and Veranda Ways</b> 5-foot walkways are an important element in building design and serve as paths and shade areas. Verandah ways are covered walkways on the side of the road.	The design, width and level of the walkways should be seamless, uniform and free of obstacles for the comfort of pedestrians. Maintain the originality or restore the design and decorative features on the 5-foot walkway with the same or suitable building materials. The 5-foot walkways of old shophouses must be maintained and integrated with the adjacent new development. Comply to the Universal Design Guidelines.

Table 4.3.9: Heritage Zone Character Control (continued)

Component	Control Action
<b>e. Roof</b> A roof is a structure that covers the top of a building, includes all materials and constructions that provide protection against weathers, temperatures, and winds.	Retain or restore the main structure of the original roof and traditional roofing materials on heritage buildings. The replacement of pitch roofs with flat concrete roofs on heritage buildings is not recommended. The replacement for original roof tiles shall be of the same material or material that is as similar as possible to the profile and slope of the original roof. Repainting of roof tiles is not recommended.
<b>f. Heritage Building Colour Scheme</b> The use and selection of colours must be appropriate to the character, activities, and surroundings of the heritage zone.	The colour scheme should harmonise and be in line with the character of the building, surroundings and streetscape of the heritage area. The use of pastel colours is allowed subject to an analysis of paint, character of the building and other buildings in the vicinity. Use of the same type of paint (water-based paint or alkyd-based paint) and the original colour is highly recommended. Repaint Category 1 Heritage Buildings in accordance with the Conservation Guidelines of the National Heritage Department (JWN).
<b>g. Floor Finish</b>	Retain or restore the original floor level and structure including primary timber beams, timber floor tie beams, and existing timber flooring. Maintain or restore the original floor finish. Repainting the original floor or replacing it with a concrete floor is not recommended.
<b>Signboard or Advertisement Signboard</b> Signboards on business premises introduce the activities carried out to attract the public.	Retaining or restoring the original traditional signs on heritage buildings is strongly encouraged. Control size, placement, and number of signboards to avoid obstruction on the view of the building façade in the Heritage Zone. The installation of large-scale advertising signboards and covering the façade of the building or ventilation spaces on the shop building is not allowed. The signboard must comply with established guidelines and harmony with the character of the Heritage Zone.
<b>Heritage Building Lighting Elements</b> Proper lighting that focuses on heritage buildings with unique architectural elements will reinforce the character of the building at night.	LED floodlights are permitted to highlight the heritage building façade. The installation of lights on the façade of a heritage building must not disturb the fabric of the building. The lighting can be installed at an angle and the lamp placed in an obscure area. The use of colourful light bulbs, chasing lights or shape lights is not recommended on the façade of buildings in the heritage zone. The lighting design must match the era and character of the Heritage Zone. Lighting must heed aesthetics and safety aspects at night. Lighting elements must following the Kuala Lumpur Lighting Master Plan and the Detailed Lighting Guidelines of Kuala Lumpur City Centre.

Table 4.3.9: Heritage Zone Character Control (continued)

Component	Control Action
<b>Electrical Cables, Plumbing and Utilities</b>	The installation of electrical cables and air-cooling compressor units on the building façade is not recommended. If necessary, treatment such as finishing louvres can be provided.
	Electrical cables and pipes installed on the back wall should be neatly covered.
	Utilities such as feeder pillars or substation buildings that obstruct the view of heritage buildings are not recommended.
<b>Needs of Persons With Disabilities and Age-Friendly</b> Building design must take into account the needs and comfort of various ages and people with disabilities.	The provision of support facilities such as parking, lifts, toilet facilities, stairs and directions should be practical, not burdening physical ability, as well as user-friendly.
<b>Security Surveillance</b> Surveillance measures and security protection in open areas should be provided in locations that do not disturb the view and are able to overcome the vandalism problem of heritage buildings.	Consider security elements by providing open, unobstructed spaces and clear visibility levels to create a safe and crime-free environment.
	Provision of security control and CCTV facilities in strategic areas as well as public focal points.
<b>Integration of Heritage and New Buildings</b> Integration between heritage buildings and new buildings will lead to architectural diversity. A harmonious combination can further increase the attractiveness of Kuala Lumpur.	The design of new buildings must consider the adjacent buildings and surroundings to create continuity as well as harmony between buildings.
	The design should be harmonious with the existing development in the adjacent lot and consider the local physical as well as socio-cultural characteristics.
<b>Entrance Gate to Heritage Zone</b> An entrance gate serves as a gateway or access road. Aspects of beautification and character of the gateway to the heritage area must be highlighted.	The entrance gate to the heritage zone must have a sense of welcome and a distinctive character.
	The theme and character of the entrance gate must be appropriately highlighted through specific street furniture design, heritage building lighting elements, walkways with special materials, harmonious landscape elements and activities that attract people to the Heritage Zone.
	Any form of business promotion or advertisement at the main entrance gate to the Heritage Zone is not recommended and must be referred to KLCH.
<b>Landscape Elements</b> Landscape elements should be the main aspect and be planted regularly to create a consistent visual sight line within the Heritage Zone.	The planting of shrubs and trees with minimalist concepts in front of heritage buildings is encouraged.
	Landscape maintenance and height control are important to avoid obstruction of the view towards heritage buildings.
	The selection of tree species must be approved by the KLCH Technical Department and related agencies.



Table 4.3.9: Heritage Zone Character Control (continued)

Component	Control Actions
<b>Street Furniture</b> Elements and components are provided along the route for various purposes. Design and placement should consider aesthetics, visual identity, functionality, pedestrian mobility, and public safety.	<p>The design of street furniture, pedestrian walkways, and materials used must have an impact on the heritage building environment.</p> <p>Street furniture elements must be appropriate to the character and architectural era of heritage buildings (examples: lamp posts, wayfinding, information plaques, litter bins, bollards, benches, and others).</p> <p>Street furniture must be placed in safe and user-friendly locations.</p>
<b>Types of Activities Encouraged</b> The competitiveness of a place depends on how it is exploited and activities that are encouraged to ensure it has its own advantages over other places.	<p>The activities encouraged must not affect the preservation of character and identity of the Heritage Zone.</p> <p>Activities that reflect heritage identity such as traditional skills, local food, sale of crafts, culture, and art are encouraged.</p> <p>All activities must comply with the permissible CULB2024.</p>
<b>a. Al-Fresco Activity Area</b> To encourage vibrant and active environment, Al-Fresco activity is allowed within the Heritage Zone and needs to comply with the prescribed guidelines.	<p>Double frontage shophouses are encouraged to activate service lanes for activities that support the Heritage Zone and strategic areas such as River of Life (RoL) and important nodes.</p> <p>Comply with Al-Fresco Dining Guidelines.</p>



Compliance with guidelines for Al-fresco dining activity in public areas



Activities in the form of culture and art as well as business with an open bazaar concept are encouraged in the Kuala Lumpur Heritage Zone with compliance with the guidelines.

## PLANNING CONTROL LAYER FOR HEIGHT CONTROL ZONE SURROUNDING THE ISTANA NEGARA

The planning control layer for height control zone surrounding the Istana Negara refers to the additional control mechanism to the height or intensity control guidelines of the building. Any building located within 800 metres and 1 kilometre from Balai Ketibaan Istana Negara boundary as shown in Figure 4.4.1 is included in this height control. This control is to preserve open vistas from and towards the Istana Negara itself.

### 4-4.1 Objectives of Planning Control Layer for Height Zone Surrounding the Istana Negara

The objective of this height planning control layer is to enhance the superiority and dignity of the Istana Negara as a symbol of the national government. In addition, it aims to ensure the tranquility and privacy of the palace are controlled against high-intensity development in the surrounding area.

### 4-4.2 Aspects of Planning Control for Height Zone Surrounding the Istana Negara

This planning control involves an area within a radius of 800 metres and 1 kilometre from the boundary of the Istana Negara and from Balai Ketibaan Istana Negara facing five (5) landmarks of Kuala Lumpur as follows:

1. Petronas Twin Tower (Menara KLCC);
2. Kuala Lumpur Tower (Menara Kuala Lumpur);
3. The Exchange 106;
4. Merdeka 118 Tower (Menara Merdeka 118); and
5. Bangunan Parlimen.

However, this height control is subjected to government regulations from time to time. The details of height control surrounding the Istana Negara are as follows:

#### Planning Control Area

**Area 1 – Distance 800 metres (1/2 mile) from the Istana Negara building. The building height should not exceed 400 feet ASML.**

- Retention of existing height development that is not exceeding 400 feet ASML based on the Garis Panduan Kawalan Had Ketinggian bagi Pembangunan di Sekitar Rizab Istana Negara (Pindaan 2).
- The proposed development within a radius of 800 metres is an existing development (low rise) and a committed development with a height exceeding 400 feet ASML.
- This area is fully controlled by not allowing approvals above 400 feet ASML.

**Area 2 – Distance 1 kilometre from the Istana Negara building. The height of the building does not exceed 500 feet ASML.**

- The development that will be developed in the areas of Jalan Lembah Ledang, Jalan Semantan 1, Jalan Semantan 2 and Jalan Bukit Ledang must not exceed 500 feet ASML and is subject to compliance with technical requirements of planning, safety aspects and certification by the agencies involved and the development design does not affect the superiority of the Istana Negara.
- This area consists of existing and committed development that exceeds 400 feet of ASML.

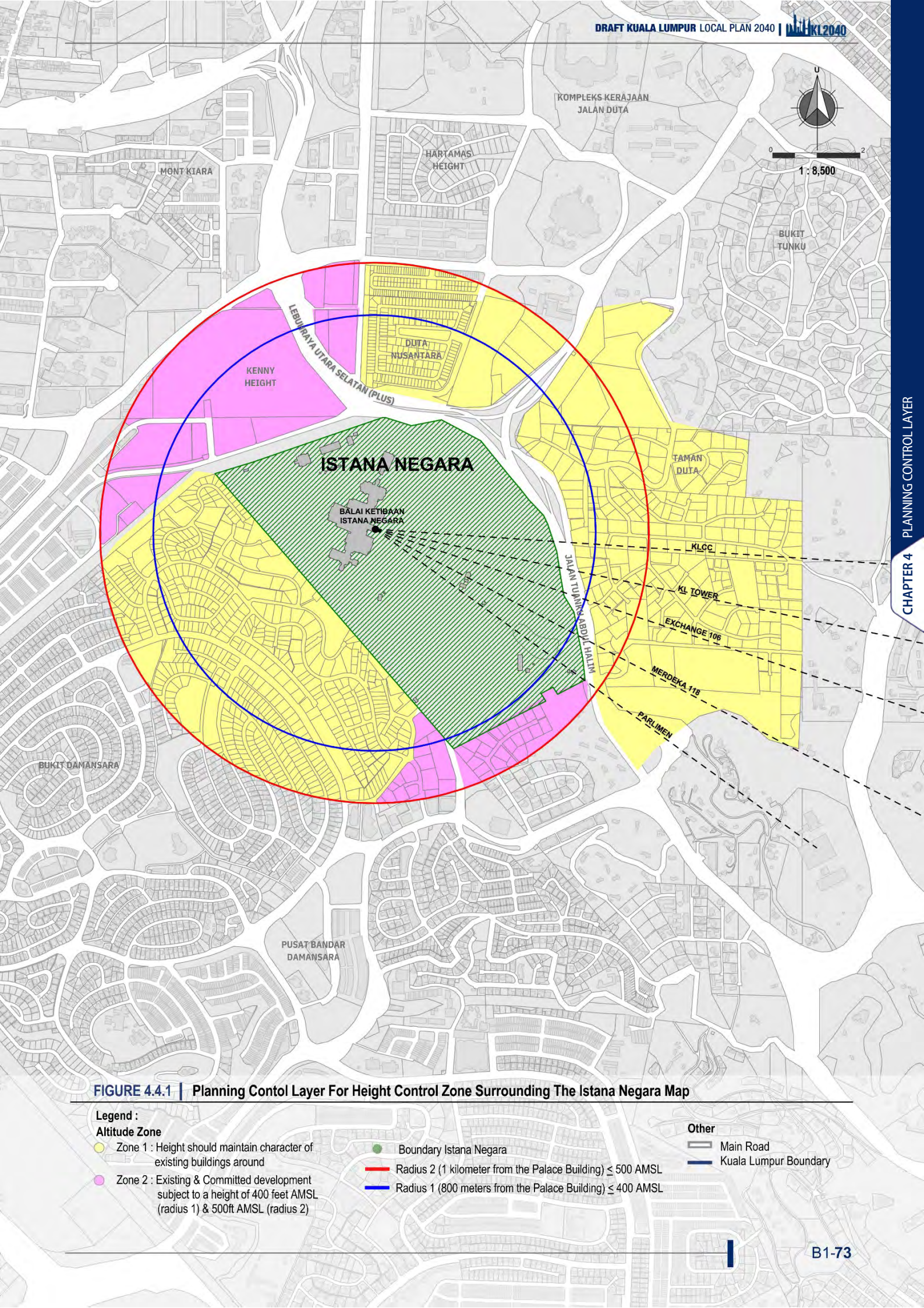
#### Area 3 – Additional Areas of Height Control.

- The area of Taman Duta, Bukit Tunku and the control of the Istana Negara view will be maintained and controlled according to the existing height and character of this area.



Aerial view of Istana Negara with Kuala Lumpur skyline as the background.





**FIGURE 4.4.1 | Planning Control Layer For Height Control Zone Surrounding The Istana Negara Map**

**Legend :**

**Altitude Zone**

- Zone 1 : Height should maintain character of existing buildings around
- Zone 2 : Existing & Committed development subject to a height of 400 feet AMSL (radius 1) & 500ft AMSL (radius 2)

● Boundary Istana Negara

— Radius 2 (1 kilometer from the Palace Building)  $\leq 500$  AMSL

— Radius 1 (800 meters from the Palace Building)  $\leq 400$  AMSL

**Other**

— Main Road

— Kuala Lumpur Boundary





Kuala Lumpur will become a developed city by 2040 where the construction of high-rise buildings is expected to become a trend for various parties to form their respective corporate identities. The new high-rise buildings will be the main element of urban design in the formation of Kuala Lumpur's image as an international city.

Line of Sight refers to additional control over the height, design and orientation of new buildings located in the identified line towards landmarks in Kuala Lumpur. KLCH plays a significant role in ensuring that new constructions within the identified line of sight maintain unobstructed views of Kuala Lumpur's landmarks.

#### 4-5.1 Objectives of Planning Control Layer for Line of Sight Towards Landmarks

The rapid growth of the city with the construction of high-rise buildings reflects the positive development of the city. While the construction of new buildings is essential in Kuala Lumpur city centre, it is important to implement view control measures for landmarks. This ensures the preservation of the significance of existing landmark buildings, which serve as iconic symbols of Kuala Lumpur.

#### 4-5.2 Location of Planning Control Layers for Line of Sight Towards Landmarks

Iconic landmarks that have been identified are the Kuala Lumpur Tower (Menara Kuala Lumpur), Petronas Twin Towers (Menara Berkembar Petronas) and Merdeka 118 Tower (Menara Merdeka 118). Draft KLLP2040 identifies six (6) points of view on Kuala Lumpur's iconic landmarks that have the potential to be preserved. These six (6) points of view are from the locations as shown Table 4.5.1 and Table 4.5.2.

**Table 4.5.1:**  
**Location Point of View Towards Landmarks**

Point of View Location
a. From Taman Tasik Titiwangsa (Point 1)
b. From Taman Tasik Titiwangsa (Point 2)
c. From Dataran Merdeka
d. From Istana Negara
e. From Mayor's Hill
f. From Taman Tasik Ampang Hilir



Aerial view of Kuala Lumpur Tower (Menara Kuala Lumpur).



Aerial view of Petronas Twin Towers (Menara Berkembar Petronas).



Aerial view of Merdeka 118 Tower (Menara Merdeka 118).



Table 4.5.2:  
Line of Sight Towards Landmarks from Points of View Taman Tasik Titiwangsa and Dataran Merdeka

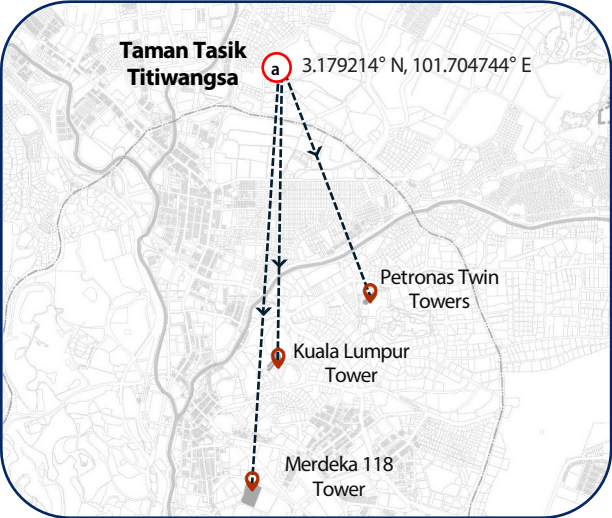

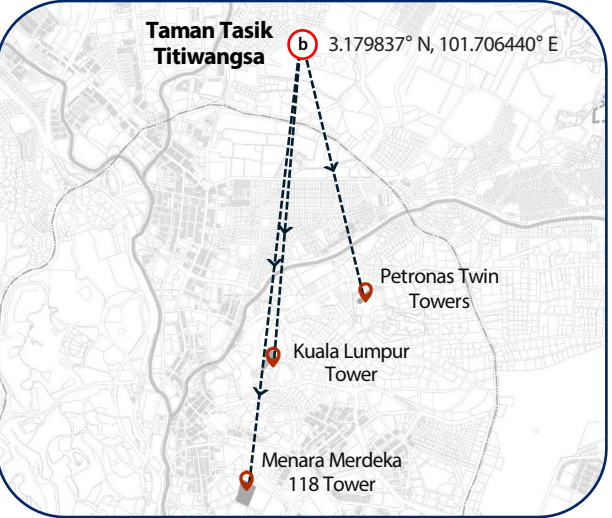

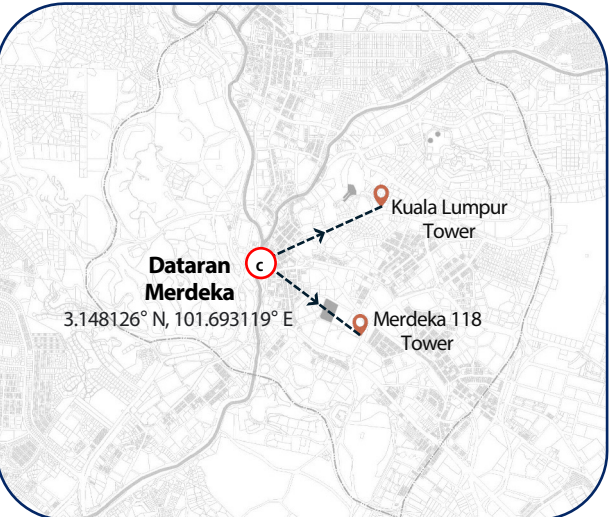
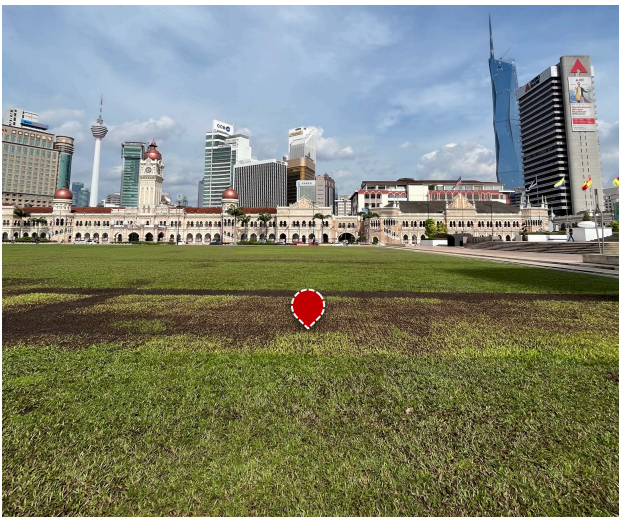
Line of Sight	Point of View Location
<div>a. TAMAN TASIK TITIWANGSA (POINT 1)</div> <div></div>	
<div>b. TAMAN TASIK TITIWANGSA (POINT 2)</div> <div></div>	
<div>c. DATARAN MERDEKA</div> <div></div>	



Table 4.5.3:  
Visual Towards Landmarks from Points of View Taman Tasik Titiwangsa and Dataran Merdeka

View Toward Landmark
 <p>Location: Taman Tasik Titiwangsa (Point 1)</p>
 <p>Location: Taman Tasik Titiwangsa (Point 2)</p>
 <p>Location: Dataran Merdeka</p>



Table 4.5.4:  
Line of Sight Towards Landmarks from Points of View Istana Negara, Mayor’s Hill and Taman Tasik Ampang Hilir

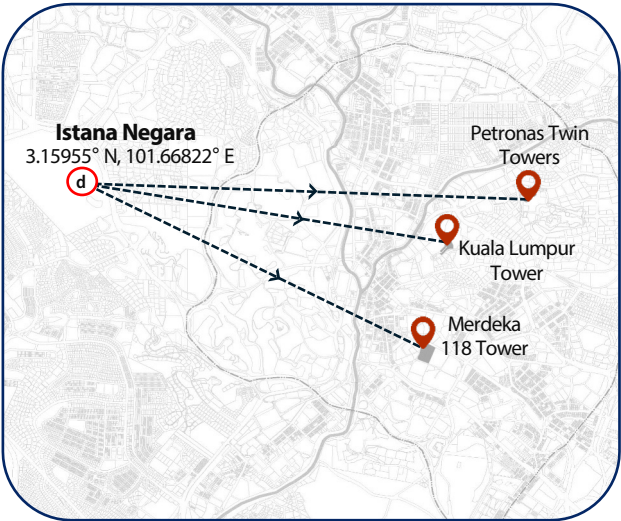

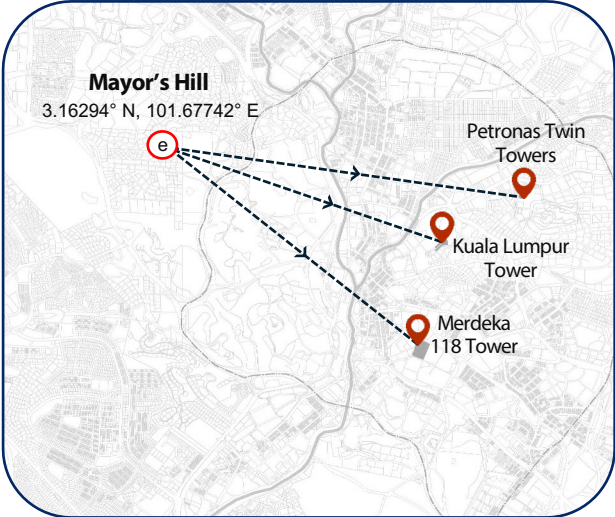

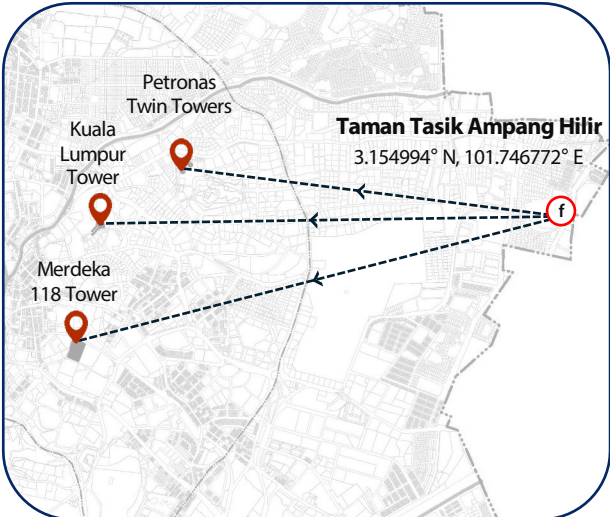

Line of Sight	Point of View Location
<div><p><b>d. ISTANA NEGARA</b></p></div>	
<div><p><b>e. MAYOR’S HILL</b></p></div>	
<div><p><b>f. TAMAN TASIK AMPANG HILIR</b></p></div>	



Table 4.5.5:  
Visual Towards Landmarks from Points of View Istana Negara, Mayor’s Hill and Taman Tasik Ampang Hilir

View Towards Landmark
 <p>Location: Istana Negara</p>
 <p>Location: Mayor's Hill</p>
 <p>Location: Taman Tasik Ampang Hilir</p>



#### 4-5.3 Aspects of Planning Control for Line of Sight Towards Landmarks

The mechanism to regulate the construction of new buildings involves controlling the minimum visibility limit of iconic landmark buildings from the identified location point. The recommended level of general visibility for identified iconic landmark buildings is 30 percent or equivalent to 1/3 of the building height. The planning control aspect can be implemented through three (3) forms namely height, design, or building orientation.

Planning control for areas identified within the line of sight involves additional requirements during the planning permission application approval stage. Among the aspects that need consideration are as follows:

1. The construction of a proposed building with horizontal orientation in the area of the line of sight that obstructs the view towards the iconic landmark should be avoided;
2. Planning approval applications for new developments in plots within the line of sight areas must be accompanied by additional document requirements for approval purposes:
  - i. A simulation analysis of the structural view of the proposed building to the visual of the iconic landmark; and
  - ii. Justification of the impact of the development on the visual towards the iconic landmark.
3. Only applications with minimal visual impact on iconic landmarks will be considered.

#### 4-5.4 Planning Control Guide for Line of Sight Towards Landmarks

The Draft KLLP2040 outlines the requirements for the construction of non-building structures such as infrastructure and street furniture at ground level that do not obstruct the view towards landmarks especially around the Kuala Lumpur City Centre. The planning guide to support this planning control proposal are as follows:

1. Advertising signs should be constructed or installed on existing buildings;
2. The construction of LRT/MRT lines must be carried out underground, especially for locations in the city centre;
3. Utility network systems such as electrical cables must be implemented underground; and
4. Construction of a closed-circuit camera system, streetlight, traffic light or other related minimal structure.

A follow-up study must be carried out to analyze and identify the actual height as well as the need for development control from the aspect of height, orientation and building design.

#### BRIEF INFO

##### HEIGHT OF KUALA LUMPUR'S ICONIC LANDMARKS

###### Kuala Lumpur Tower (Menara Kuala Lumpur)

Year Completed	Height
<b>1995</b>	<b>421.0 METER</b>

###### Petronas Twin Towers (Menara Berkembar Petronas)

Year Completed	Height
<b>1996</b>	<b>451.9 METER</b>

###### Merdeka 118 Tower (Menara Merdeka 118)

Year Completed	Height
<b>2022</b>	<b>678.9 METER</b>



As of 2023, the Merdeka 118 Tower (Menara 118 Merdeka) is the tallest building in Malaysia with a total height of 678.9 metres.

## PLANNING CONTROL LAYER FOR PUBLIC OPEN SPACES IN PRIVATE SCHEMES (OS4)

Public Open Spaces in Private Schemes (OS4) functions as multi-purpose public open spaces located at ground level or at the podium that are accessible to the public. It is an area of public use and not only limited to the local community.

OS4 is a planning control layer and is calculated in the provision of open space requirements and not in the calculation of land use zones. The basis of OS4 determination is as follows:

1. Surrender to KLCH through strata parcels or accessory parcels; or
2. Stated as the condition in the land title deed; or
3. Determined through an undertaking agreement with KLCH or related regulatory agencies.

### 4-6.1 Objectives of Planning Control Layer for Public Open Spaces in Private Schemes (OS4)

The designation of OS4 as a planning control layer for public open spaces with requirements and intents are as follows:

1. A new initiative to address the issue of limited land availability through innovative methods to ensure the use of multifunctional open spaces;
2. Ensuring the availability of public open spaces innovatively in large-scale private development schemes;
3. Optimising land use by allowing other activities underneath the open spaces;
4. Enabling collaboration with building owners for the high-quality management and maintenance of open spaces;
5. Addressing the issue of limited land availability for development purposes in urban areas in line with the need to achieve the target of 20 square meters of open space per person by 2040; and
6. Diversifying the types of public open spaces from concentrating on the ground level only.

### 4-6.2 Aspects of Planning Control Layer for Public Open Spaces in Private Schemes (OS4)

This planning control focuses on five (5) forms as follows:

1. The OS4 layer is shown as an overlay layer above other land use zones in land use zone map;
2. Any changes to the approval plan must ensure that the total area of OS4 is maintained even if the shape or design is changed;
3. The location of the OS4 area can be changed according to the level and must be accessible to the public from a public place or area;
4. The OS4 area must follow the open space guidelines and other requirements set by KLCH; and
5. OS4 open space can be managed by the developer or related agency by complying with the conditions set by KLCH.



Location: Lalaport, Bukit Bintang City Centre

Lalaport is a component of the Bukit Bintang City Centre development that offers the newest Public Open Space in a Private Scheme (OS4) in Kuala Lumpur.



#### 4-6.3 Location of Planning Control Layers for Public Open Spaces in Private Schemes (OS4)

OS4 location refers to development schemes that have been granted planning permission approval during the preparation of Draft KLLP2040. Generally, it is set in high-intensity development areas such as the city centre, new development, redevelopment and regeneration areas.

There are seven (7) identified development project sites, namely:

1. Cadangan Pembangunan Bercampur, Seksyen 56, Mukim Bandar Kuala Lumpur (PNB 118 - Menara Merdeka 118);
2. Cadangan Pembangunan Bercampur di Atas Lot PT143 Seksyen 56, Jalan Hang Tuah/Jalan Pudu (Bukit Bintang City Centre - BBCC);
3. Cadangan Pembangunan Bercampur 'Tun Razak Exchange' Seksyen 62, Bandar Kuala Lumpur, di Jalan Tun Razak/Jalan Davis (Tun Razak Exchange - TRX);

4. Cadangan Pembangunan Perdagangan di Atas Lot 20001, Seksyen 96a, Jalan Maarof (Bangsar 61, Jalan Riong, Bangsar);
5. Cadangan Pembangunan Perdagangan, off Lebuhraya Bukit Jalil, Bandar Bukit Jalil, Mukim Petaling (Pavilion Bukit Jalil);
6. Cadangan Pembangunan Bercampur di Atas PT26 dan PT31, Jalan Bangsar, Mukim Bandar Kuala Lumpur (Setia Federal Hill); and
7. Cadangan Pembangunan Pangsapuri Suites di Atas PT 9554 (Lot 58601) Batu 2 1/2, Jalan Cheras (Cordova Land).

In addition, KLCH will ensure that large-scale redevelopment schemes in the future will provide wider public open spaces to meet the needs of the community and highlight the image of Kuala Lumpur as a Green, Healthy and Vibrant City.

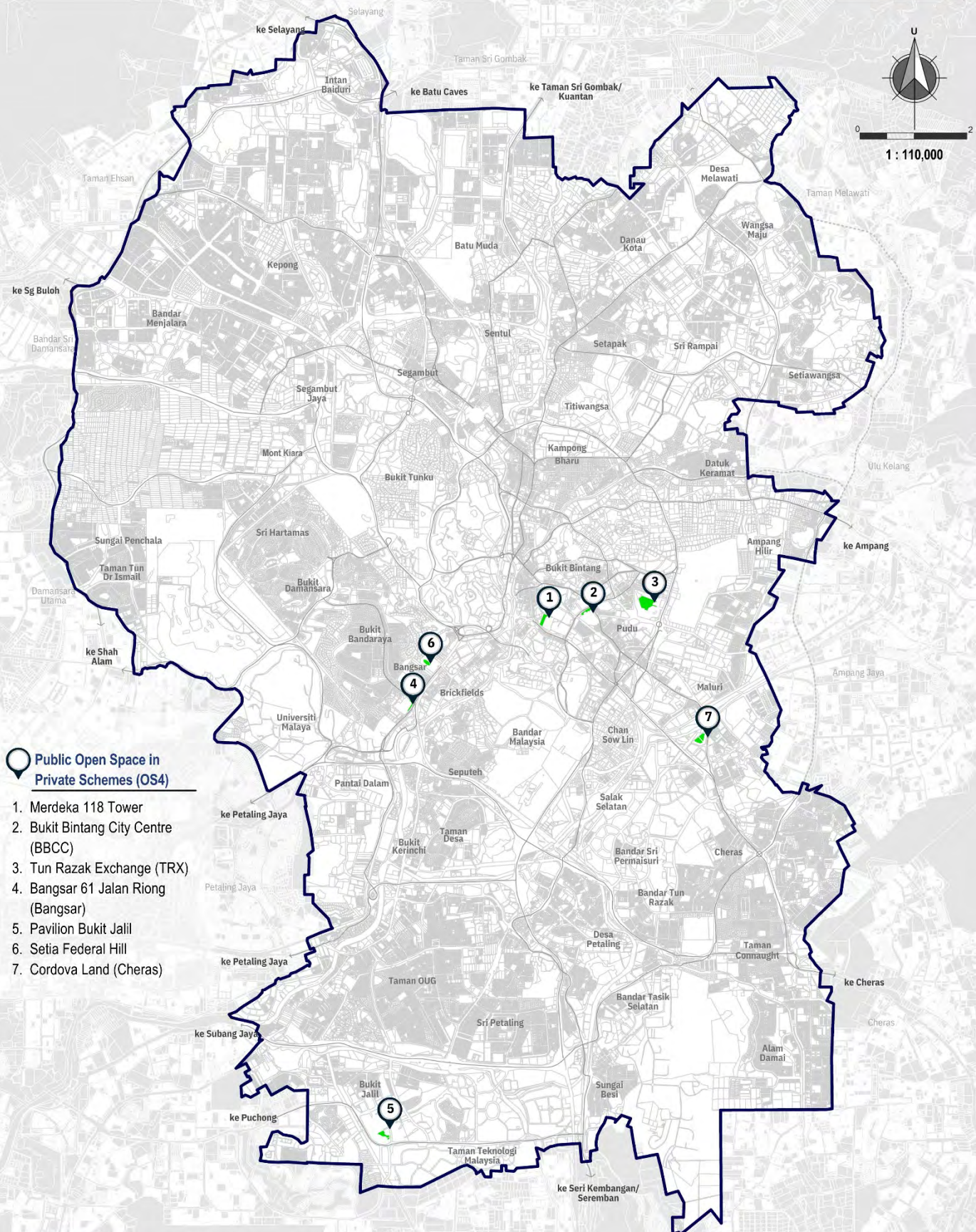
KLCH will diversify efforts and creative initiatives to ensure the provision of open space with improved quality and quantity based on an innovative approach towards the realisation of Kuala Lumpur's vision to be a City For All.



Location: Tun Razak Exchange (TRX)

The provision of open space in the podium section of the TRX development scheme that can be accessed by the public is an example of OS4 found in Kuala Lumpur.



**FIGURE 4.6.1 | Plan of Public Open Space in Private Schemes (OS4)**



The Planning Control Layer for Affordable Housing (RMM) refers to existing and approved affordable houses. These houses are affordable and livable to meet the needs of B40 and part of M40 income groups.

Affordable housing (RMM) in Kuala Lumpur is designated specifically in Land Use Zone of Residential 4 (R4). In addition, RMM components have also been built in other land use zones which are Land Use Zone of Residential 3 (R3), Land Use Zone of Mixed Development (MX) and Land Use Zone of Major Commercial (MC).

The planning provision of RMM components with open market housing or commercial components in designated zones should be marked as RMM control layers. This stipulation is to ensure that the existing RMM units can be retained and increased in line with the needs of the people of Kuala Lumpur in the future.

The RMM's unit in Kuala Lumpur must be maintained to ensure that the target group is able to continue living in Kuala Lumpur in line with the increase in house prices in the future. This is also in line with the desire to ensure that workers, especially those in the support sector, are close to the employment centre in Kuala Lumpur.

#### 4-7.1 Objectives of Planning Control Layers for Affordable Housing (RMM)

The determination of RMM as a planning control layer is intended as follows:

1. Ensure the retaining of existing RMM in line with the needs of the future population;
2. Coordinate existing RMM redevelopment to ensure that the redeveloped site retains the number of existing units and encourages the addition of units in the same development scheme; and
3. Balance RMM demand needs for B40 and part of M40 groups which is expected to experience pressure in demand compared to the overall supply of residential units in Kuala Lumpur by 2040.

#### 4-7.2 Aspects of Planning Control Layers for Affordable Housing (RMM)

The RMM planning control layer is determined for sites that are not zoned as Residential 4 (R4). It focuses on the designating of residential, commercial and mixed development land use zones that have been set throughout the preparation of the Draft KLLP2040 to provide RMM components in line with the high-intensity approval allowed for the relevant site or lot. Nevertheless, RMM development proposals in zones not marked with a planning control layer may be permissible subject to the designation of permitted land use zones.

#### 4-7.3 Location of Planning Control Layers for Affordable Housing (RMM)

The RMM layer is shown as a display symbol above the land use zones in the land use zone map. RMM designation criteria are based on redevelopment or new areas as follows:

1. Suitable government lands and privately owned lands;
2. Redevelopment areas zoned as Land Use Zone of Mixed Development (MX);
3. High-intensity areas with priority for Land Use Zone of Residential 3 (R3), Residential 4 (R4), Major Commercial (MC) and Mixed Development (MX) that can be integrated with other development components such as open market properties pricing; and
4. Suitable TPZ areas.

#### 4-7.4 Guide to Planning Controls for Affordable Housing (RMM)

Planning for the provision of high-intensity RMM development in an area must emphasise the level of infrastructure capacity and accessibility as well as recommendations for necessary improvements to avoid any conflict or congestion. RMM development must refer to the following criteria as follows:

1. The development of RMM must harmonise with the surrounding developments in terms of typology, height and facade design;
2. The development of RMM must ensure that an age and user-friendly design is applied in the building and supporting facility components; and
3. RMM development must consider the provision of comprehensive community facilities to the local residents.







# 5.0

## PLOT RATIO INCENTIVE







# 5.0 PLOT RATIO INCENTIVE

## DRAFT KUALA LUMPUR LOCAL PLAN 2040

The plot ratio incentive is an additional floor space allowable if an approved development contributes towards improving the quality of life, workspace environment, accessibility, community facilities, infrastructure and utilities in Kuala Lumpur including meeting the current policy of the government.

The plot ratio incentives encourage private developers to participate in developments that focus on:

1. Improvement of existing dilapidated residential areas and buildings towards more habitable developments;
2. Development of affordable homes for B40 and part of M40 groups to achieve the targets outlined in the affordable housing policy;
3. Provide cross-subsidy through additional floor area to cater for the additional cost such as resettlement of residents or redevelopment of infrastructure or utility in a redevelopment area; and
4. Provide and upgrade community facilities, infrastructure as well as utilities exceeding standard requirements to facilitate residents' access and accessibility to the facilities provided.

Plot ratio incentives through the redevelopment of brownfield or greyfield sites will contribute towards sustainable urban growth in line with Kuala Lumpur's development initiative as a Low-Carbon City, Smart City and Resilient City.

### BRIEF INFO

#### BENEFITS OF PLOT RATIO INCENTIVE

Plot ratio incentive provides benefits in the following aspects:

1. Encourage implementation of redevelopment programme of an area;
2. Encourage high-intensity development at transit stations to support public transportation usage;
3. Encourage amalgamation of lots for more comprehensive and economic developments; and
4. Provide housing for B40 and part of M40 group.



Redevelopment of the Bukit Bintang City Centre area above the Penara Pudu site.



Redevelopment of the KL Gateway area.

The Draft KLLP2040 determines the provision of an additional plot ratio to the base plot ratio as designated in the DCP2 - Intensity Map. The allocation of additional plot ratio is subject to compliance with the criteria that focus on township development with value-added towards quality of life and urban environment.

The provision of plot ratio incentive is subject to the following:

1. Plot ratio incentive is allowable if the development provides value-added, contributes towards improving the quality of the built environment and urban sustainability;
2. Approved additional plot ratio must be in accordance with the percentage of base plot ratio designated in the Draft KLLP2040 except incentives for lot amalgamation and affordable housing that are subject to the current government policy;
3. Redevelopment is entitled to additional incentives of maximum plot ratio not exceeding 100 percent including TPZ incentives as stated in the Panduan Pelaksanaan Pembaharuan Semula Bandar Kuala Lumpur prepared by the Department of Federal Territories (JWP);
4. Developments with lot amalgamation incentives are entitled only to TPZ incentives and affordable housing components and are not entitled to redevelopment incentives;
5. The additional incentives should also be subjected to the factors of site suitability (location, physical condition of the site, accessibility, availability of public facilities, infrastructure and compatibility with the surrounding area) and compliance with current planning guidelines as well as standards by KLCH, especially for affordable housing;
6. The determination of this incentive must consider additional development charges exceeding standard requirements for infrastructure and utilities upgrading, additional community facilities and other related physical aspects in line with additional intensity provided subject to the Mayor's approval;
7. The use of incentives also cannot cause a change in the land use zone;
8. The determination of all incentives does not apply to intensity based on height control; and

9. Any development or existing plot of land that has been approved more than the base plot ratio allowed is calculated as the maximum plot ratio applicable to that land. However, this approved plot ratio cannot be considered a basic plot ratio of granting incentives in the Draft KLLP2040. If the basic plot ratio, along with incentives, exceeds the approved plot ratio, the plot can be considered with the maximum plot ratio set.

The determination of the allowable intensity is subject to compliance with the overall technical planning requirements, current policies and guidelines. For sites which are not able to meet the stipulations, the approval of lower intensities should be considered at the planning control level.

Incentives of additional plot ratio are subjected to identified sites or areas as follows:

1. Transit Planning Zone (TPZ);
2. Lot Amalgamation Area;
3. Redevelopment Area; and
4. Development with Affordable Housing Components.

The planning incentives set by the government in the future can also be considered for use in the context of implementation for Draft KLLP2040, subject to the acceptance of their use at the KLCH level.

The overall application matrix for plot ratio incentive determined by the Draft KLLP2040 is shown in Table 5.1.1.

**Table 5.1.1:**  
**Matrix of Plot Ratio Incentive Application**

Incentive	TPZ	Lot Amalgamation Area	Redevelopment Area	Affordable Housing Component
TPZ		✓	* ✗	✗
Lot Amalgamation Area	✓		✗	✓
Redevelopment Area	* ✗	✗		✓
Affordable Housing Component	✗	✓	✓	

*Note: The TPZ incentive for redevelopment is allocated and subjected to the Panduan Pelaksanaan Pembaharuan Semula Kuala Lumpur.*

## PLOT RATIO INCENTIVE FOR TRANSIT PLANNING ZONE (TPZ)

The draft KLLP2040 sets the Plot Ratio Incentive for Transit Planning Zones (TPZ) referring to incentives given to areas in Transit Planning Zones (TPZ) and Transit Influenced Zones (TIZ) to encourage development in transit areas. Plot ratio incentives in TPZ and TIZ are additional plot ratios that are entitled to be considered by any development located in TPZ and TIZ.

Transit Planning Zone (TPZ) refers to an area located within 400 metres of a transit station that pertaining to existing transit stations, under construction and areas that have been acquired for future stations. A TPZ area is measured from the edge or end of the Station Box (not the building) and is moved 400 metres from the edge of the Station Box. Transit Influenced Zone (TIZ) refers to an area located within 200 metres from the radius boundary of the TPZ, as shown in Figure 5.1.1.

A total of 103 TPZs have been identified which include transit stations for Komuter, Monorail, Mass Rapid Transit (MRT), Light Rail Transit (LRT), High Speed Rail (HSR) and Express Rail Link (ERL) services as shown in the Table 5.1.2.

In addition, stations located outside the boundary of Kuala Lumpur are also entitled to TPZ incentives for the catchment areas located within Kuala Lumpur, subject to evaluation at the planning permission application stage.

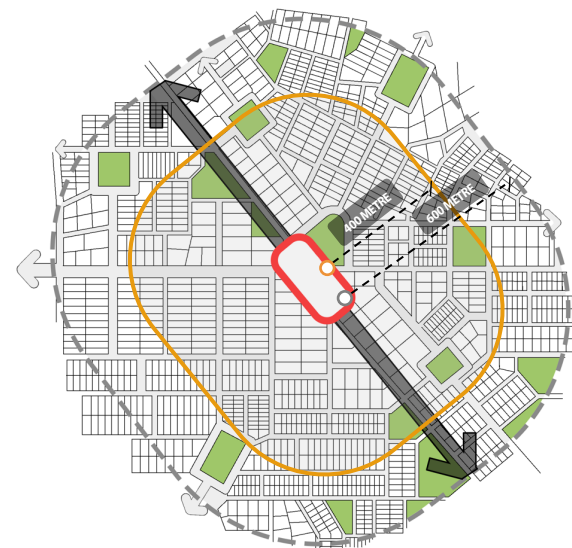
The determination of the number of TPZ may change if new rail lines or stations are built in the future. The TPZ incentive is also applicable to these new stations whether they are located within or outside the boundary of Kuala Lumpur.






Location: Stesen Monorel Medan Tuanku

Developments around a transit stations such as Monorail Station is entitled for Transit Planning Zone (TPZ).

**Figure 5.1.1:**  
Transit Planning Zone (TPZ) and Transit Influenced Zone (TIZ)



### LEGEND:

-  Station box
-  Transit Planning Zone (TPZ)  
(400 metres from the edge of station box)
-  Transit Influenced Zone (TIZ)  
(600 metres from the edge radius boundary from TPZ)

Note: Actual coverage of 400 metres from station box will be determined during application of planning permission by the applicant.

## BRIEF INFO

### CHARACTERISTICS OF LAND USE ACTIVITIES ENCOURAGED AROUND TRANSIT STATIONS

The type of land use activities that are encouraged around transit stations are as follows:

1. Activities that generate a high number of residents and jobs;
2. Activities that generate a high number of pedestrians;
3. Activities that have two-way movement;
4. Activities that promote active mobility such as walking and the use of micromobility vehicles; and
5. Activities that promote active function of the station surroundings and generate passengers at all times.



**Table 5.1.2:**  
**List of Transit Planning Zone (TPZ) and Transit Influenced Zone (TIZ) Stations**

	MRT 1	MRT 2	INTEGRATED TERMINAL	LRT - Kelana Jaya Line	LRT - Sri Petaling & Ampang Line	KOMUTER	MONORAIL	ERL
1	TTDI	Metro Prima	KL Sentral involves stations such as LRT, ERL, Komuter, ETS and Skypark Link	Taman Melati	Sentul Timur	Kepong	Titivangsa	Bandar Tasik Selatan
2	Phileo Damansara	Kepong Baru		Wangsa Maju	Sentul	Segambut	Chow Kit	
3	Pusat Bandar Damansara	Jinjang		Sri Rampai	Titivangsa	Taman Wahyu	Medan Tuanku	
4	Semantan	Sri Delima		Dato' Keramat	PWTC	Kampung Batu	Bukit Nanas	
5	Muzium Negara	Kampung Batu		Damai	Sultan Ismail	Batu Kentonmen	Raja Chulan	
6	Pasar Seni	Kentomen		Ampang Park	Bandaraya	Sentul	Bukit Bintang	
7	Merdeka	Jalan Ipoh		KLCC	Masjid Jamek	Putra	Imbi	
8	Bukit Bintang	Sentul Barat		Kampung Baru	Plaza Rakyat	Bank Negara	Hang Tuah	
9	Tun Razak Exchange (TRX)	Titivangsa		Dang Wangi	Hang Tuah	Kuala Lumpur	Maharajalela	
10	Cochrane	Hospital Kuala Lumpur		Masjid Jamek	Pudu	Mid Valley	Tun Sambanthan	
11	Maluri	Raja Uda		Pasar Seni	Chan Sow Lin	Seputeh	KL Sentral	
12	Taman Pertama	Ampang Park		Bangsar	Miharja	Salak Selatan		
13	Taman Midah	Persiaran KLCC		Abdullah Hukum	Maluri	Bandar Tasik Selatan		
14	Taman Mutiara	Conlay		Kerinci	Cheras	Abdullah Hukum		
15	Taman Connaught	Tun Razak Exchange (TRX)		Universiti	Salak Selatan	Angkasapuri		
16		Chan Sow Lin			Bandar Tun Razak	Pantai Dalam		
17		Bandar Malaysia North			Bandar Tasik Selatan	Petaling		
18		Bandar Malaysia South			Sungai Besi			
19		Taman Naga Emas			Bukit Jalil			
20		Sungai Besi			Sri Petaling			
21		Kuchai			Awan Besar			
22					Muhibbah			
	<b>15</b>	<b>21</b>	<b>1</b>	<b>15</b>	<b>22</b>	<b>17</b>	<b>11</b>	<b>1</b>
<b>103 TPZ and TIZ stations</b>								

### 5-1.1 Use of Plot Ratio Incentive for Transit Planning Zone (TPZ) and Transit Influenced Zone (TIZ)

The use of plot ratio incentive for TPZ and TIZ are classified into four (4) categories as follows:

1. Category TPZ 1 and TIZ 1;
2. Category TPZ 2 and TIZ 2;
3. Category TPZ 3 and TIZ 3, and
4. Category TPZ 4 and TIZ 4.

The classification and assessment of stations according to the TPZ and TIZ category is based on the following criteria:

1. Type of station;
2. Transit services provided;
3. Station location;
4. Patterns of urban activity around the station;
5. Facilities provided; and
6. Development intensity.

Based on designated criteria, the classification and assessment of station according to TPZ and TIZ category are shown in Table 5.1.3. Meanwhile, the distribution of stations according to TPZ and TIZ category are shown in Figure 5.1.2.

**Table 5.1.3:**  
Criteria and List of Transit Planning Zones (TPZ) as well as Transit Influenced Zone (TIZ) by Category

Criteria	Category TPZ 1 and TIZ 1	Category TPZ 2 and TIZ 2	Category TPZ 3 and TIZ 3	Category TPZ 4 and TIZ 4
<b>Type of Stations</b>	<ul style="list-style-type: none"> <li>• Transportation Hub</li> <li>• Integrated interchange station</li> <li>• Interchange station that connects directly with other rail modes</li> <li>• Stations connected with dedicated pedestrian walkway facilities</li> </ul>	<ul style="list-style-type: none"> <li>• Interchange station that connects with other modes of rail</li> <li>• Transit station</li> </ul>	<ul style="list-style-type: none"> <li>• Transit station</li> </ul>	<ul style="list-style-type: none"> <li>• Transit station</li> </ul>
<b>Transit Service</b>	<ul style="list-style-type: none"> <li>• More than 2 types of transit services (MRT, LRT, Monorail, KTM)</li> </ul>	<ul style="list-style-type: none"> <li>• 1 or 2 types of transit services (MRT, LRT, Monorail, KTM)</li> </ul>	<ul style="list-style-type: none"> <li>• 1 type of transit service (Monorail, KTM)</li> </ul>	<ul style="list-style-type: none"> <li>• 1 type of transit service (KTM)</li> </ul>
<b>Station Location</b>	<ul style="list-style-type: none"> <li>• Inside city center</li> </ul>	<ul style="list-style-type: none"> <li>• Inside city center</li> <li>• Outside city center</li> </ul>	<ul style="list-style-type: none"> <li>• Outside city center</li> </ul>	<ul style="list-style-type: none"> <li>• Outside city center</li> </ul>
<b>Development Intensity</b>	<ul style="list-style-type: none"> <li>• High intensity</li> </ul>	<ul style="list-style-type: none"> <li>• Medium-high intensity</li> <li>• Medium intensity</li> </ul>	<ul style="list-style-type: none"> <li>• Medium intensity</li> </ul>	<ul style="list-style-type: none"> <li>• Medium intensity</li> </ul>
<b>Municipal Activity Patterns</b>	<ul style="list-style-type: none"> <li>• Main business centre</li> <li>• High intensity mixed development</li> <li>• High intensity housing</li> <li>• Main office area</li> </ul>	<ul style="list-style-type: none"> <li>• Business centre</li> <li>• Medium-high mixed development</li> <li>• High and medium-high-intensity housing</li> <li>• Office area</li> </ul>	<ul style="list-style-type: none"> <li>• University</li> <li>• Major sport, cultural or civic center</li> <li>• Administrative office area</li> <li>• Housing</li> <li>• Industry</li> <li>• Institution</li> <li>• Specific employment activities</li> </ul>	<ul style="list-style-type: none"> <li>• Limited mixed development character</li> <li>• Vacant land and limited redevelopment potential</li> </ul>

*Note: The designated criteria are general criteria in determining the TPZ and TIZ category.*

Table 5.1.3:

Criteria and List of Transit Planning Zones (TPZ) as well as Transit Influenced Zone (TIZ) by Category (continued)

Criteria	Category TPZ 1 and TIZ 1	Category TPZ 2 and TIZ 2	Category TPZ 3 and TIZ 3	Category TPZ 4 and TIZ 4
<b>Facilities provided</b>	<ul style="list-style-type: none"> <li>• Park and ride facilities</li> <li>• Feeder bus</li> <li>• Pedestrian connectivity network</li> <li>• Micromobility vehicle connectivity network</li> <li>• Micromobility vehicle parking</li> <li>• Taxi, e-hailing and other services</li> </ul>	<ul style="list-style-type: none"> <li>• Park and ride facilities</li> <li>• Feeder bus</li> <li>• Pedestrian connectivity network</li> <li>• Micromobility vehicle connectivity network</li> <li>• Micromobility vehicle parking</li> <li>• Taxi, e-hailing and other services</li> </ul>	<ul style="list-style-type: none"> <li>• Park and ride facilities</li> <li>• Feeder bus</li> <li>• Pedestrian connectivity network</li> <li>• Taxi, e-hailing and other services</li> </ul>	<ul style="list-style-type: none"> <li>• Park and ride facilities</li> <li>• Feeder bus</li> <li>• Pedestrian connectivity network</li> <li>• Taxi, e-hailing and other services</li> </ul>
<b>Transit station</b>	<p><b>Integrated Terminal</b></p> <ol style="list-style-type: none"> <li>1. KL Sentral</li> </ol> <p><b>MRT 1</b></p> <ol style="list-style-type: none"> <li>2. Muzium Negara</li> <li>3. Pasar Seni</li> <li>4. Merdeka</li> <li>5. Bukit Bintang</li> <li>6. Tun Razak Exchange (TRX)</li> </ol> <p><b>MRT 2</b></p> <ol style="list-style-type: none"> <li>7. Titiwangsa</li> <li>8. Ampang Park</li> <li>9. Persiaran KLCC</li> <li>10. Tun Razak Exchange (TRX)</li> <li>11. Bandar Malaysia North</li> <li>12. Bandar Malaysia South</li> </ol> <p><b>LRT Kelana Jaya</b></p> <ol style="list-style-type: none"> <li>13. Pasar Seni</li> <li>14. Masjid Jamek</li> <li>15. KLCC</li> <li>16. Ampang Park</li> </ol> <p><b>LRT Sri Petaling and Ampang</b></p> <ol style="list-style-type: none"> <li>17. Pudu</li> <li>18. Hang Tuah</li> <li>19. Plaza Rakyat</li> <li>20. Masjid Jamek</li> <li>21. Titiwangsa</li> </ol> <p><b>Monorel</b></p> <ol style="list-style-type: none"> <li>22. KL Sentral</li> <li>23. Hang Tuah</li> <li>24. Bukit Bintang</li> <li>25. Titiwangsa</li> </ol> <p><b>HSR*</b></p> <ol style="list-style-type: none"> <li>26. Bandar Malaysia</li> </ol>	<p><b>MRT 1</b></p> <ol style="list-style-type: none"> <li>1. Cochrane</li> <li>2. Maluri</li> </ol> <p><b>MRT 2</b></p> <ol style="list-style-type: none"> <li>3. Kampung Batu</li> <li>4. Sentul Barat</li> <li>5. Hospital Kuala Lumpur</li> <li>6. Conlay</li> <li>7. Chan Sow Lin</li> <li>8. Kuchai</li> <li>9. Sungai Besi</li> <li>10. Raja Uda</li> </ol> <p><b>LRT Kelana Jaya</b></p> <ol style="list-style-type: none"> <li>11. Kerinchi</li> <li>12. Dang Wangi</li> <li>13. Kampung Baru</li> <li>14. Damai</li> <li>15. Dato' Keramat</li> </ol> <p><b>LRT Sri Petaling</b></p> <ol style="list-style-type: none"> <li>16. Bandar Tasik Selatan</li> <li>17. Salak Selatan</li> <li>18. Sungai Besi</li> <li>19. Bukit Jalil</li> <li>20. Awan Besar</li> </ol> <p><b>LRT Ampang</b></p> <ol style="list-style-type: none"> <li>21. Miharja</li> <li>22. Maluri</li> </ol> <p><b>LRT Sri Petaling and Ampang</b></p> <ol style="list-style-type: none"> <li>23. Sentul Timur</li> <li>24. Sentul</li> <li>25. Sultan Ismail</li> <li>26. Bandaraya</li> <li>27. PWTC</li> <li>28. Chan Sow Lin</li> </ol>	<p><b>MRT 1</b></p> <ol style="list-style-type: none"> <li>1. Phileo Damansara</li> <li>2. Taman Pertama</li> <li>3. Semantan</li> <li>4. Taman Midah</li> <li>5. Taman Connaught</li> </ol> <p><b>MRT 2</b></p> <ol style="list-style-type: none"> <li>6. Metro Prima</li> <li>7. Kepong Baru</li> <li>8. Jinjang</li> <li>9. Sri Delima</li> <li>10. Kentonmen</li> <li>11. Jalan Ipoh</li> <li>12. Taman Naga Emas</li> </ol> <p><b>LRT Kelana Jaya</b></p> <ol style="list-style-type: none"> <li>13. Universiti</li> <li>14. Abdullah Hukum</li> <li>15. Bangsar</li> <li>16. Sri Rampai</li> <li>17. Wangsa Maju</li> <li>18. Taman Melati</li> </ol> <p><b>LRT Sri Petaling</b></p> <ol style="list-style-type: none"> <li>19. Cheras</li> <li>20. Bandar Tun Razak</li> <li>21. Sri Petaling</li> </ol> <p><b>KTM</b></p> <ol style="list-style-type: none"> <li>22. Abdullah Hukum</li> <li>23. Salak Selatan</li> </ol>	<p><b>MRT 1</b></p> <ol style="list-style-type: none"> <li>1. Taman Tun Dr. Ismail</li> <li>2. Pusat Bandar Damansara</li> <li>3. Taman Mutiara</li> </ol> <p><b>LRT Sri Petaling</b></p> <ol style="list-style-type: none"> <li>4. Muhibbah</li> </ol> <p><b>KTM</b></p> <ol style="list-style-type: none"> <li>5. Taman Wahyu</li> <li>6. Segambut</li> <li>7. Angkasapuri</li> <li>8. Seputeh</li> <li>9. Pantai Dalam</li> <li>10. Petaling</li> <li>11. Kepong</li> </ol>

Note: The designated criteria are general criteria in determining the TPZ and TIZ category.



**Table 5.1.3:**  
**Criteria and List of Transit Planning Zones (TPZ) as well as Transit Influenced Zone (TIZ) by Category (continued)**

Criteria	Category TPZ 1 and TIZ 1	Category TPZ 2 and TIZ 2	Category TPZ 3 and TIZ 3	Category TPZ 4 and TIZ 4
<b>Type of station</b>		<b>Monorail</b> 29. Tun Sambanthan 30. Maharajalela 31. Imbi Plaza 32. Raja Chulan 33. Bukit Nanas 34. Medan Tuanku 35. Chow Kit  <b>KTM</b> 36. Kampung Batu 37. Batu Kentonmen 38. Putra 39. Bank Negara 40. Kuala Lumpur 41. Mid Valley 42. Bandar Tasik Selatan 43. Sentul  <b>ERL</b> 44. Bandar Tasik Selatan		

Note:

- \*The HSR and MRT3 networks will be considered if implemented in the future after the land acquisition process;
- Stations located outside the boundary of Kuala Lumpur will be considered if the catchment of TPZ and TIZ cover areas within Kuala Lumpur;
- The determination of the TPZ and TIZ station category is also subject to the potential of the station and the surroundings; and
- The designated criteria are general criteria in determining the TPZ and TIZ category.



Location: Kuala Lumpur Sentral

Development patterns that have taken place around KL Sentral.



TPZ 4 & TIZ 4

MRT 1

1. Taman Tun Dr. Ismail
2. Pusat Bandar Damansara
3. Taman Mutiara

LRT Sri Petaling

4. Muhibbah

KTM

5. Taman Wahyu
6. Segambut
7. Angkasapuri
8. Seputeh
9. Pantai Dalam
10. Petaling
11. Kepong

TPZ 2 & TIZ 2

MRT 1

1. Cochrane
2. Maluri

MRT 2

3. Kampung Batu
4. Sentul Barat
5. Hospital Kuala Lumpur
6. Conlay
7. Chan Sow Lin
8. Kuchai
9. Sungai Besi
10. Raja Uda

LRT Kelana Jaya

11. Kerinchi
12. Dang Wangi
13. Kampung Baru
14. Damai
15. Dato' Keramat

LRT Sri Petaling

16. Bandar Tasik Selatan
17. Salak Selatan
18. Sungai Besi
19. Bukit Jalil
20. Awan Besar

LRT Ampang

21. Miharja
22. Maluri

LRT Sri Petaling and Ampang

23. Sentul Timur
24. Sentul
25. Sultan Ismail
26. Bandaraya
27. PWTC
28. Chan Sow Lin

Monorel

29. Tun Sambanthan
30. Maharajalela
31. Imbi
32. Raja Chulan
33. Bukit Nanas
34. Medan Tuanku
35. Chow Kit

KTM

36. Kampung Batu
37. Batu Kentonmen
38. Putra
39. Bank Negara
40. Kuala Lumpur
41. Mid Valley
42. Bandar Tasik Selatan
43. Sentul

ERL

44. Bandar Tasik Selatan



TPZ 3 & TIZ 3

MRT 1

1. Phileo Damansara
2. Taman Pertama
3. Semantan
4. Taman Midah
5. Taman Connaught

MRT 2

6. Metro Prima
7. Kepong Baru
8. Jinjang
9. Sri Delima
10. Kentomen
11. Jalan Ipoh
12. Taman Naga Emas

LRT Kelana Jaya

13. Universiti
14. Abdullah Hukum
15. Bangsar
16. Sri Rampai
17. Wangsa Maju
18. Taman Melati

LRT Sri Petaling

19. Cheras
20. Bandar Tun Razak
21. Sri Petaling

KTM

22. Abdullah Hukum
23. Salak Selatan

TPZ 1 & TIZ 1

Terminal Bersepadu

1. KL Sentral

MRT 1

2. Muzium Negara
3. Pasar Seni
4. Merdeka
5. Bukit Bintang
6. Tun Razak Exchange (TRX)

MRT 2

7. Titiwangsa
8. Ampang Park
9. Persiaran KLCC
10. Tun Razak Exchange (TRX)
11. Bandar Malaysia North
12. Bandar Malaysia South

LRT Kelana Jaya

13. Pasar Seni
14. Masjid Jamek
15. KLCC
16. Ampang Park

LRT Sri Petaling and Ampang

17. Pudu
18. Hang Tuah
19. Plaza Rakyat
20. Masjid Jamek
21. Titiwangsa

Monorel

22. KL Sentral
23. Hang Tuah
24. Bukit Bintang
25. Titiwangsa

HSR\*

26. Bandar Malaysia

FIGURE 5.1.2 | Plan of Transit Planning Zone (TPZ) and Transit Influenced Zone (TIZ)

Legend :

Proposal

- TPZ & TIZ Category 1
- TPZ & TIZ Category 2
- TPZ & TIZ Category 3
- TPZ & TIZ Category 4
- TPZ & TIZ Outside Kuala Lumpur

Public Transport Network

- KTM Line and stations
- LRT Line and stations
- MRT Line and stations
- Monorail Line and stations
- ERL Line and stations
- MRT Line 3 Corridor and Station (In Planning)

Other

- Main Road
- Kuala Lumpur Boundary

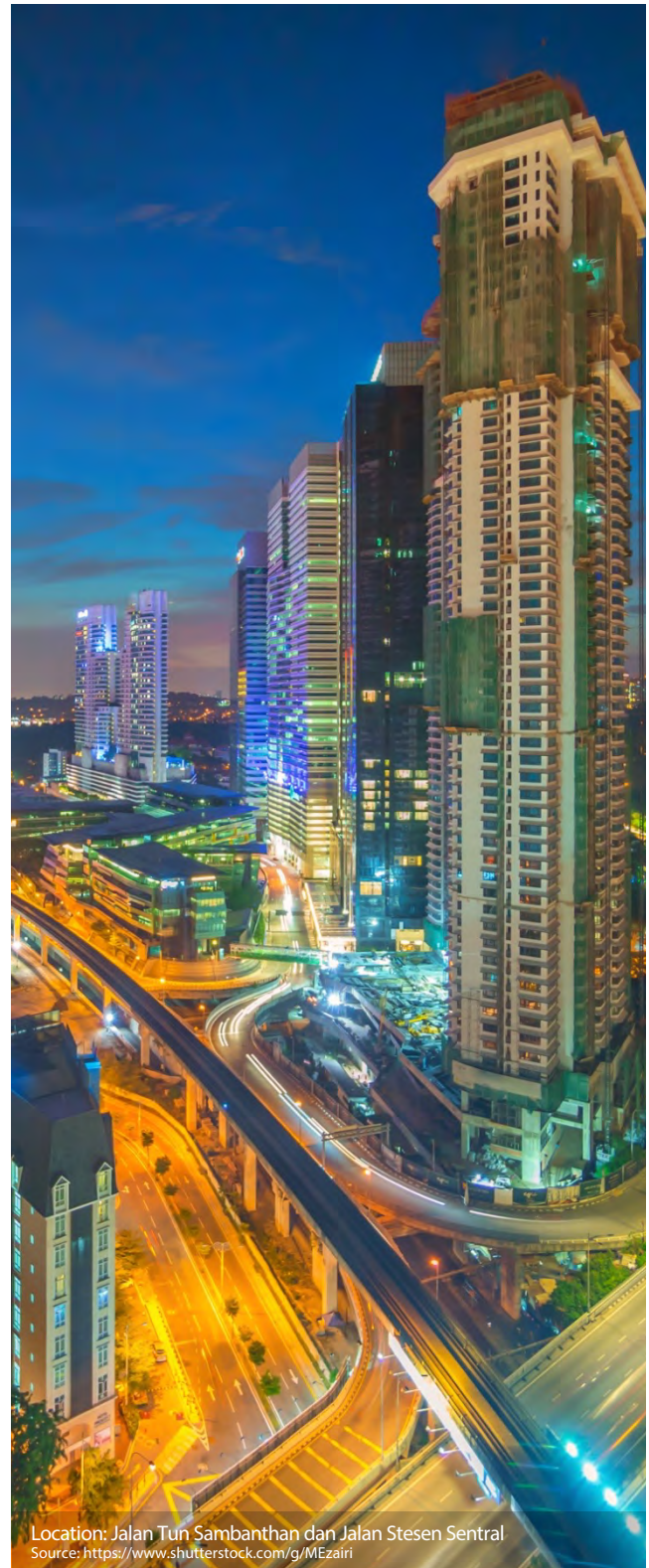


### 5-1.2 Determination of Plot Ratio Incentive for Transit Planning Zones (TPZ) and Transit Influenced Zone (TIZ)

The Draft KLLP2040 determine additional plot ratio to the base plot ratio as stipulated in the DCP2 - Intensity Map for plots located inside the TPZ area. The allocation of additional plot ratio is subject to compliance with the criteria that focus on township development with value-added towards the quality of life and urban environment as well as to ensure connectivity within 400 metres and 600 metres.

The allocation of additional plot ratio is according to the percentage as shown in Table 5.1.4. The actual additional plot ratio is subject to the percentage of the plot located inside the TPZ and TIZ. This incentive does not apply to the following developments or land use zones:

1. Land use zones that are not relevant to the plot ratio;
2. Land use zones related to height control;
3. Parking space or area;
4. Any type of resident facility provided and handed over to the management or related agency; and
5. Existing or new developments that have been approved with plot ratios higher than the permitted plot ratio including incentive in KLDCP2024.



Location: Jalan Tun Sambanthan dan Jalan Stesen Sentral  
Source: <https://www.shutterstock.com/g/MEzairi>

High intensity development pattern around KL Sentral



**Table 5.1.4:**  
**Provision of Additional Plot Ratio According to the Percentage**

Criteria	Compliance Criteria	Maximum Incentive Percentage by Category			
	400 metres	TPZ 1	TPZ 2	TPZ 3	TPZ 4
		30%	20%	10%	5%
	600 metres	TIZ 1	TIZ 2	TIZ 3	TIZ 4
		20%	15%	10%	5%
<b>MAIN CRITERIA:</b> 1. Provision of development with direct connectivity to the station (station development). Direct connectivity components can be provided in the form of connecting routes such as bridges or tunnels to stations or transit vehicles such as sky train, cable car or Autonomous Rail Rapid Transit (ART).	Incentives are granted if the main criteria are complied.	Up to 20%	Up to 15%	Up to 8%	Up to 3.5%
		Up to 15%	Up to 10%	Up to 7.5%	Up to 3%
<b>SUPPORTING CRITERIA:</b> 2. Provision of affordable housing or affordable commercial space; 3. Provision of functional open space exceeding the minimum requirements that can be used by the public with the concept of a plaza or public space that allows the public to use; 4. Implementation of green city initiatives such as green walls, vertical green, water elements and shades; 5. Provision of community facilities on the development site or building space; 6. Green Building Certification (certification recognised by KLCH); 7. Provision of Low Carbon City elements such as energy efficiency, solid waste disposal, water harvesting, anaerobic digester, mini- incinerator, electric vehicle charging stations and other related elements; 8. Provision of Safe City elements such as CCTVs, bollards, safety mirrors and other related elements; 9. Provision of Smart City elements such as smart vehicle facilities, smart bins and other related elements; and 10. Provision of Resilient City elements such as sustainable drainage systems, green infrastructure, water-sensitive urban design and the sponge city concept.	Incentives are granted if the applicant <b>can provide or comply 5 of the 9 supporting criteria</b> designated.	Up to 10%	Up to 5%	Up to 2%	Up to 1.5%
		Up to 5%	Up to 5%	Up to 2.5%	Up to 2%
	Incentives are granted if the applicant <b>can provide or comply 3 of the 9 supporting criteria</b> designated.	Up to 5%	Up to 4%	Up to 1.5%	Up to 1%
		Up to 3%	Up to 1.7%	Up to 1.2%	Up to 1%
	Incentives are granted if the applicant <b>can provide or comply at least 1 of the 9 supporting criteria</b> designated.	Up to 2.5%	Up to 3%	Up to 1%	0%
		Up to 1.5%	Up to 1.3%	Up to 1%	0%

### 5-1.3 Calculation to Determine Plot Ratio Incentive of Transit Planning Zones (TPZ) and Transit Influenced Zone (TIZ)

The steps to calculate plot ratio incentive for TPZ and TIZ are as follows:

1. Refer to the TPZ or TIZ if the plot or part of the plot is within TPZ area identified;
2. Refer to DCP 2 – Intensity Map for the designated base plot ratio for that particular plot; and
3. Determine total area of plot that are within TPZ or TIZ either overall or part of the plot to allocate the additional plot ratio according to the compliance of criteria:
  - a. If entire plot is within the TPZ or TIZ, then additional plot ratio is based on overall area; or
  - b. If only a part of the plot is within the TPZ or TIZ area, then the additional plot ratio incentive will be applied only to the part included in the TPZ or TIZ area.

#### BRIEF INFO

#### Calculation Step 1:

##### Calculation of Plot Ratio Incentive According to Plot Size

Total area of the plot (A and B) = 4 acres

Base plot ratio = 1:6

Total area Site A	Total area Site B
2.8 acres (70% of plot outside TPZ)	1.2 acres (30% of plot inside TPZ)

#### Calculation Step 2:

##### Calculation Step of TPZ Area Incentive

1. Plots within the TPZ area are entitled to receive plot ratio incentives after complying with the prescribed criteria.
2. Incentive entitlement is based on compliance with the set percentage.
3. For example, plot (B) is located in the Category TPZ 1 and has complied with the **main criteria** and five (5) of the nine (9) **supporting criteria** designated, so it is entitled to 30% incentives.

Items	Site A	Site B
Plot Ratio Calculation	Site A uses the base plot ratio as it is located outside the TPZ	Site B entitle additional plot ratio incentives (30% x base NP) + base NP = (0.3 x 6) + 6 = 7.8
Plot ratio	1:6	1:7.8

#### Calculation Step 3:

##### Total GFA earned by the entire plot

Site A	Site B
Site (A) GFA outside TPZ Area = 2.8 acres x 43,560 square feet x 6 = <b>731,808 square feet</b> = <b>67,987.19 square metre</b>	Site (B) GFA in TPZ Area = 1.2 acres x 43,560 square feet x 7.8 = <b>407,721.6 square feet</b> = <b>37,878.58 square metre</b>

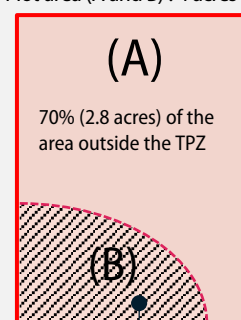
The GFA obtained is applicable to the entire plot = Site A + Site B

= 731,808 square feet + 407,721.6 square feet

= **1,139,529.6 square feet or 105,865.77 square metre**

\* Note : The calculation steps above are also applicable to the calculation of the Incentive TIZ Area

Plot area (A and B) : 4 acres



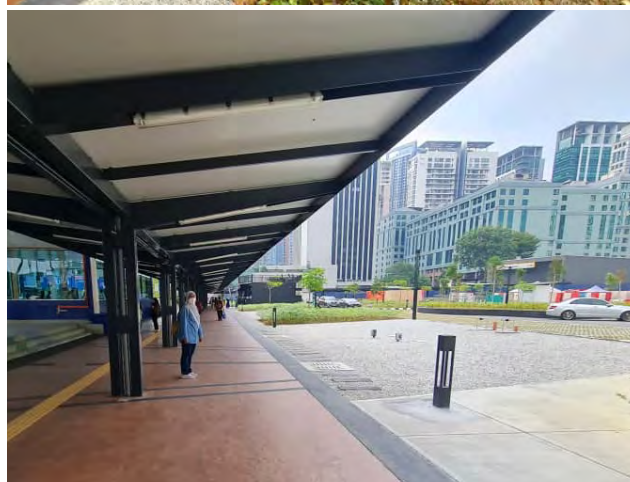
30% (1.2 acres) of the area is within the 400m radius of the TPZ

### 5-1.4 Planning Guide for Transit Planning Zones (TPZ) and Transit Influenced Zone (TIZ)

Development within TPZ and TIZ must focus on pedestrian and micromobility vehicles development oriented to increase user access to transit services and create a vibrant environment. Based on the provision of incentives, all developments within TPZ and TIZ should be planned based on the outlined as follows:

1. Development components must contain compact city elements that will encourage pedestrians and micromobility vehicles as well as efficient public transport to reduce dependency on the use of private vehicles. It also focuses on mixed development with organised urban infrastructure and facilities;
2. Characteristics of mixed land use activities around transit stations are residing, working and leisure activities as well as optimising the use of public transport;
3. Road and street design should provide space to improve the comfort and safety of pedestrians and micromobility vehicle users as well as facilitate pedestrian routes to transit stations;
4. Pedestrian and micromobility vehicle route planning must be comprehensive, continuous as well as age-friendly and disable-friendly with design features and facilities as follows:
  - a. Pedestrian walkways in the city centre should not be roofed to reduce blocking and encourage refreshing wind flow through the concept of a wind tunnel. However, consideration must be given to provide shade for the convenience of pedestrians;
  - b. Covered pedestrian walkways or a shaded landscape can be provided outside the city centre to encourage pedestrian activity;
  - c. Pedestrian crossings should be user-friendly, and integrated with smart elements for security purposes such as CCTV and panic buttons;
  - d. Facilities and walkway design that can support the movement of all groups, including the provision of signage for people with hearing and sighting disabilities; and
  - e. Separated or dedicated pedestrian and micromobility vehicle routes.
5. High-quality designs of public space designs and the potential as a placemaking location to attract numbers of transit users;

6. Building design that is based on active street activity to encourage pedestrians that enliven the environment in TPZ and TIZ;
7. Provision of facilities for micromobility vehicles within the TPZ and TIZ area to ensure improvement of first and last mile connectivity; and
8. Provision of public parking for areas outside the City Center.



Location: Laluan LRT Ampang Park  
Source: <https://www.mrt.com.my>

Covered walkway from LRT Ampang Park.



Lot amalgamation involves two (2) or more land lots that share the same boundary and are located in the same land use zone with an appropriate lot size to qualify for the intensity increase.

The plot ratio incentive for the lot amalgamation area is intended to encourage viable and innovative land development. It is also to ensure the comprehensive development or replanning of focus areas within the implementation period of the Draft KLLP2040.

### 5-2.1 Use of Plot Ratio Incentive for Lot Amalgamation Areas

Incentive for lot amalgamation areas is applicable at 17 areas that have been identified:

1. Jalan Inai;
2. Lorong Abdullah/Jalan Abdullah/Lengkok Abdullah;
3. Jalan Bukit Ceylon;
4. Jalan Kolam Air;
5. Jalan Damai;
6. Jalan U-Thant/Jalan Desa Pandan;
7. Jalan Gurney;
8. Jalan Syed Putra /Jalan Robson;
9. Jalan Ang Seng/Jalan Thamby Abdullah;
10. Lorong Palas off Jalan Ampang;
11. Lorong Setapak, Jalan Pahang;
12. Tiong Nam Area;
13. Jalan Merpati;
14. Jalan Ampang Kiri/Jalan Ampang Ulu 3;
15. Jalan Walter Granier/Lorong Walter Granier;
16. Jalan Nipah; and
17. Persiaran Klang.

The designation of 17 lot amalgamation area considers current needs, surrounding development pressure as well as a pioneer implementation of lot amalgamation initiative in Kuala Lumpur.

The location and planning control details of areas for lot amalgamation are as shown in Figure 5.2.1 and Table 5.2.1.

### 5-2.2 Determination of Plot Ratio Incentive for Lot Amalgamation Areas

The plot ratio incentive for lot amalgamation areas is intended to encourage development by providing better infrastructure and environmental quality as well as to meet the needs of a more conducive and sustainable community.

Incentive for the plot ratio of a lot amalgamation area is based on seven (7) criteria as follows:

1. Small size lot or form that is not economical to develop on its own;
2. Lots are with limited access, infrastructure and utilities;
3. Lots are located in areas with potential to be developed with more economic activities and intensity;
4. Lots are located in areas that focus on improving the environment or with a character of renewal area;
5. The amalgamation of lots must be towards creating suitable shape and size for development;
6. Developments that receive lot amalgamation incentives are entitled to TPZ incentives and affordable housing components incentives but not entitled to redevelopment incentives;
7. The determination of the maximum plot ratio for the lot amalgamation area is subject to provision or upgrading the capacity of infrastructure and utility, community facilities, connectivity with rail-based transport harmony with the surrounding developments; and
8. Incentives are also encouraged to get the views of residents, subject to suitability according to guidelines.



The environment of Lorong Abdullah/Jalan Abdullah/Lengkok Abdullah is one of the areas identified for the provision of lot amalgamation incentives.

### 5-2.3 Calculation to Determine Plot Ratio Incentive of Lot Amalgamation Area

The steps to calculate plot ratio incentive for lot amalgamation areas are as follows:

1. Refer to the selected location. If the land is included in the plot ratio incentive area of the identified lot amalgamation, then it is entitled to the plot ratio incentive;
2. Refer to DCP1 – Land Use Zone Map and DCP2 – Intensity Map for the designated land use zone and base plot ratio of that particular lot; and
3. If a lot amalgamation area is included within a radius of 400 metres of a TPZ and 600 metres of a TIZ area, then the additional plot ratio incentive will be applied only to the part included in the TPZ or TIZ particular area.

#### BRIEF INFO

##### Calculation Step 1:

##### Calculation for Plot Ratio Incentive According To The Lot Size

Area: Jalan Damai

Base Plot Ratio: **1:2**

Entitlement of amalgamation lot incentive according to the size:

- a. 20,000 square feet - 29,999 square feet = Plot Ratio 1:3
- b. >30,000 square feet = Plot Ratio 1:4

##### Calculation:

- A. Total area of Lot A = 0.5 Acre (21,780 square feet)
- B. Total area of Lot B = 0.2 Acre (8,712 square feet)
- C. Total lot amalgamation area Lot = A + B  
= 21,780 square feet + 8,712 square feet  
= **30,492 square feet (Lot C)**

Therefore, the Site C site is entitled for lot amalgamation incentive with a Plot Ratio of 1:4.



##### Calculation Step 2:

##### Calculation for Plot Ratio Incentive of Lot Amalgamation Area Within TPZ or TIZ Area

If the site is located within a 400 metre radius of the TPZ or 600 metres radius of the TIZ priority area, it is eligible for the TPZ plot ratio incentive. An example of the eligibility calculation of the lot amalgamation incentive for a site within a 400 meter radius of the TPZ area is as follows:

- A. Plot Ratio for Site C = 1:4 (30,492 square feet )
- B. Eligible percentage criteria of TPZ for Site C = 20%
- C. Eligible plot ratio incentive = 20% X Base Plot Ratio (1:2)  
= 20% X 2 = 0.4
- D. Eligible additional plot ratio  
= Plot Ratio 1:4 + Plot Ratio Incentive 0.4  
= **Plot Ratio 1:4.4**

Therefore, the additional plot ratio for Site C is 1:4.4.

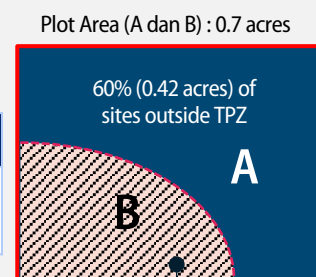
If only part of the site is located within a 400 metres radius of the TPZ or 600 metres radius of the TIZ area, only the area involved is eligible to receive TPZ incentives.

**Calculation Step 3:****Calculation of Incentive Eligibility for Part of the Lot Amalgamation Area Involved in the TPZ or TIZ Area**

Total area of the plot (Lot C) = 0.7 acres

Base Plot Ratio = 1:4

Site A	Site B
0.42 acres (60% of sites outside TPZ)	0.28 ekar (40% of sites inside TPZ)



40% (0.28 acres) of sites  
inside radius 400m TPZ

**Calculation Step 4:****Calculation Step for the TPZ Area Incentive Eligibility**

1. Plots within the TPZ area are entitled to receive plot ratio incentive after complying with the prescribed criteria.
2. Incentive entitlement is based on compliance with the set percentage.
3. For example, plot (B) is located in the category TPZ 1 and has complied with the **main criteria** and five (5) of the nine (9) **supporting criteria** designated, so it is entitled for 30% incentives.

Item	Site A	Site B
Plot Ratio Calculation	Site A uses the base plot ratio as it is located outside the TPZ	$(30\% \times \text{base NP}) + \text{base NP} = (0.3 \times 4) + 4$ $= (0.3 \times 4) + 4$ $= 5.2$
Plot ratio	1:4	1:5.2

**Calculation Step 5:****Total GFA earned by the entire plot**

Site A	Site B
Site (A) GFA outside TPZ Area $= 0.42 \text{ ekar} \times 43,560 \text{ square feet} \times 4$ <b><math>= 73,180.80 \text{ square feet or}</math></b> <b><math>= 6,798.72 \text{ square metres}</math></b>	Tapak (B) GFA dalam Kawasan TPZ $= 0.28 \text{ ekar} \times 43,560 \text{ square feet} \times 5.2$ <b><math>= 63,423.36 \text{ square feet or}</math></b> <b><math>= 5,892.22 \text{ square metres}</math></b>

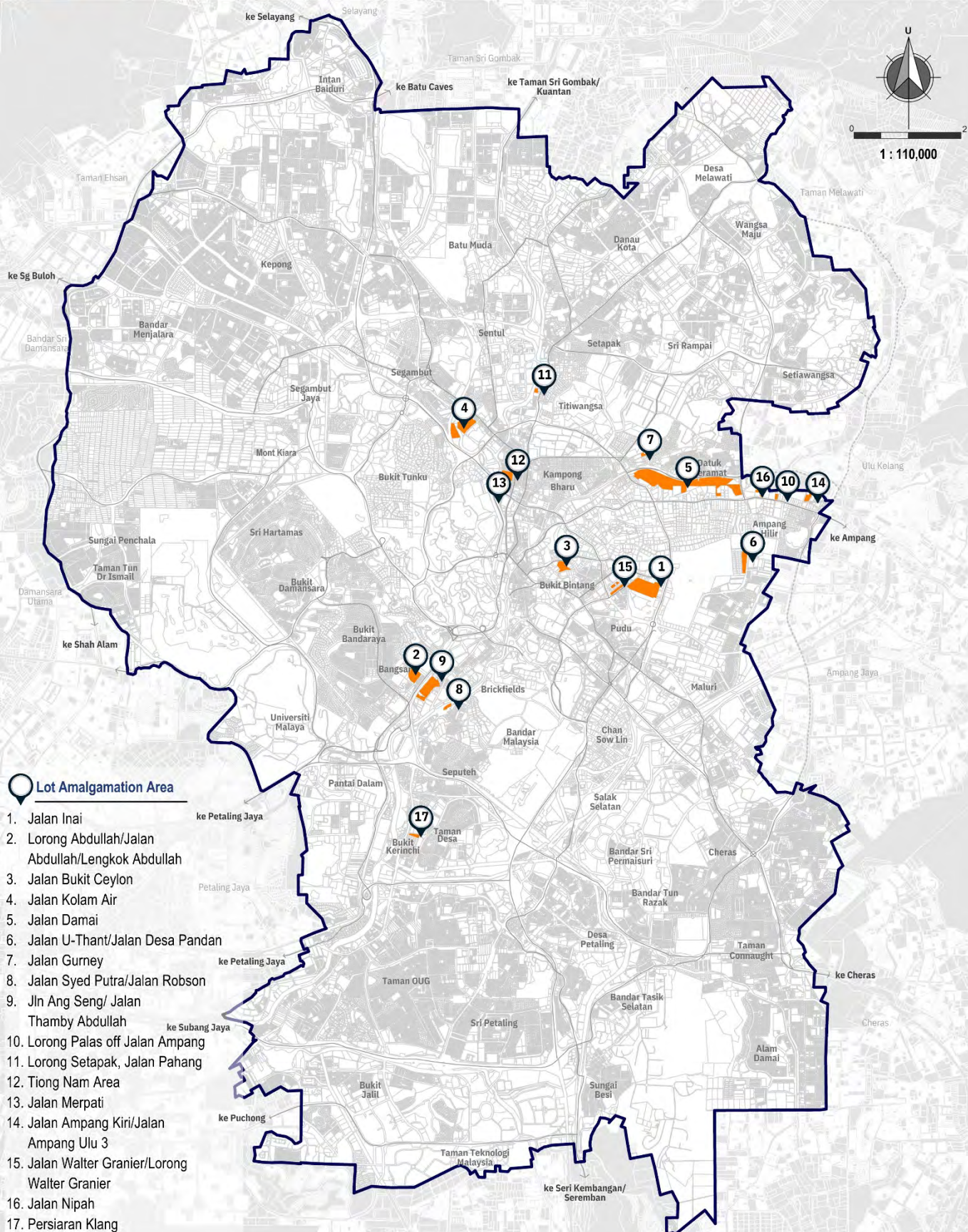
The GFA obtained is applicable to the entire plot = Site A + Site B

$= 73,180.80 \text{ square feet} + 63,423.36 \text{ square feet}$

**$= 136,604.16 \text{ square metres or } 12,690.94 \text{ square metres}$**

\* Note : The calculation steps above are also applicable to the calculation of the Incentive TIZ Area


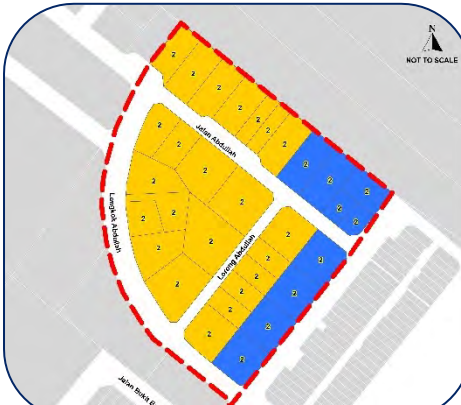







**FIGURE 5.2.1 | Distribution Plan for Lot Amalgamation Incentive Site**



**Table 5.2.1:**  
**List of Lot Amalgamation Incentive Area**




Location/Area	Development Control
<p><b>1. Jalan Inai</b></p> 	<p>Land Use Zone: <b>City Centre Commercial (CCC)</b></p> <p>Base Plot Ratio: <b>1:4</b></p> <p>1. Lot Amalgamation Area:</p> <ol style="list-style-type: none"> <li>20,000 square feet - 29,999 square feet = <b>Plot Ratio 1:6</b></li> <li>30,000 square feet - 49,999 square feet = <b>Plot Ratio 1:8</b></li> <li>50,000 square feet and above = <b>Plot Ratio 1:10</b></li> </ol>
<p><b>2. Lorong Abdullah/Jalan Abdullah/Lengkok Abdullah</b></p> 	<p>Land Use Zone: <b>Major Commercial (MC)</b></p> <p>Base Plot Ratio: <b>1:2</b></p> <p>1. Lot amalgamation with an area of 30,000 square feet and above are entitled for a <b>Plot Ratio of 1:4</b> provided that the lot amalgamation is of an appropriate and continuous form.</p> <p>Land Use Zone: <b>Residential 2 (R2)</b></p> <p>Base Plot Ratio: <b>1:2</b></p> <ol style="list-style-type: none"> <li>Entitlement for Plot Ratio 1:2 only allowed for lot with minimum area of 30,000 square feet; and</li> <li>Development that does not comply with designated minimum area will be controlled through height control of 3 storeys.</li> </ol>
<p><b>3. Jalan Bukit Ceylon</b></p> 	<p>Land Use Zone: <b>City Centre Commercial (CCC)</b></p> <p>Base Plot Ratio: <b>1:2</b></p> <p>1. Lot Amalgamation Area:</p> <ol style="list-style-type: none"> <li>20,000 square feet - 29,999 square feet = <b>Plot Ratio 1:5</b></li> <li>30,000 square feet - 39,999 square feet = <b>Plot Ratio 1:6</b></li> <li>40,000 square feet and above = <b>Plot Ratio 1:8</b></li> </ol>

**Table 5.2.1:**  
**List of Lot Amalgamation Incentive Area (continued)**

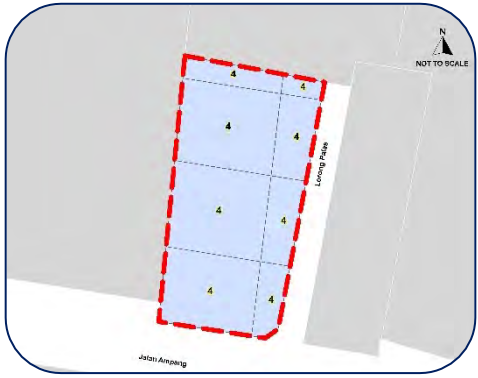


Location/Area	Development Control
<p><b>4. Jalan Kolam Air</b></p> 	<p>Land Use Zone: <b>Mixed Development (MX)</b></p> <p>Base Plot Ratio: <b>1:2</b></p> <p>1. Lot Amalgamation Area:</p> <ol style="list-style-type: none"> <li>20,000 square feet - 29,999 square feet = <b>Plot Ratio 1:4</b></li> <li>30,000 square feet - 39,999 square feet = <b>Plot Ratio 1:5</b></li> <li>40,000 square feet and above = <b>Plot Ratio 1:6</b></li> </ol>
<p><b>5. Jalan Damai</b></p> 	<p>Land Use Zone: <b>Mixed Development (MX)</b></p> <p>Base Plot Ratio: <b>1:2</b></p> <p>1. Lot Amalgamation Area:</p> <ol style="list-style-type: none"> <li>20,000 square feet - 29,999 square feet = <b>Plot Ratio 1:3</b></li> <li>30,000 square feet and above = <b>Plot Ratio 1:4</b></li> </ol>
<p><b>6. Jalan U-Thant/Jalan Desa Pandan</b></p> 	<p>Land Use Zone: <b>Residential 2 (R2)</b></p> <p>Base Plot Ratio: <b>1:2</b></p> <ol style="list-style-type: none"> <li>Entitlement for <b>Plot Ratio of 1:2</b> is allowed only for sites with a minimum area of 20,000 square feet and access from Jalan Desa Pandan only; and</li> <li>Development with access from Jalan U-Thant, will be controlled through height control of 3 storeys.</li> </ol>



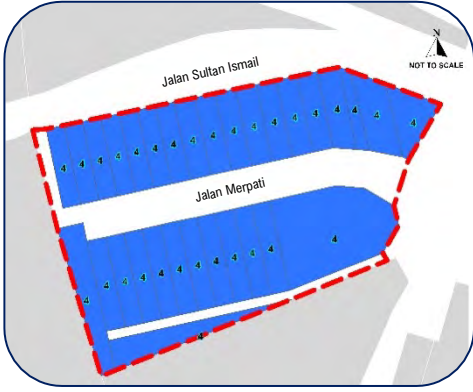


**Table 5.2.1:**  
**List of Lot Amalgamation Incentive Area (continued)**

Location/Area	Development Control
<p><b>7. Jalan Gurney</b></p> 	<p>Land Use Zone: <b>Commercial (C)</b></p> <p>Base Plot Ratio: <b>1:2</b></p> <ol style="list-style-type: none"> <li>1. Lot amalgamation with an area of 30,000 square feet and above are entitled for a <b>Plot Ratio of 1:4</b> provided that the lot amalgamation is of an appropriate and continuous form.</li> </ol>
<p><b>8. Jalan Syed Putra/Jalan Robson</b></p> 	<p>Land Use Zone: <b>Commercial (C)</b></p> <p>Base Plot Ratio: <b>1:4</b></p> <ol style="list-style-type: none"> <li>1. Lot Amalgamation Area: <ol style="list-style-type: none"> <li>a. 30,000 square feet - 39,999 square feet = <b>Plot Ratio 1:5</b></li> <li>b. 40,000 square feet and above = <b>Plot Ratio 1:6</b></li> </ol> </li> </ol>
<p><b>9. Jalan Ang Seng/Jalan Thamby Abdullah</b></p> 	<p>Land Use Zone: <b>Mixed Development (MX)</b></p> <p>Base Plot Ratio: <b>1:4</b></p> <ol style="list-style-type: none"> <li>1. Lot Amalgamation Area: <ol style="list-style-type: none"> <li>a. 20,000 square feet - 39,999 square feet = <b>Plot Ratio 1:5</b></li> <li>b. 40,000 square feet - 59,999 square feet = <b>Plot Ratio 1:6</b></li> <li>c. 60,000 square feet - 79,999 square feet = <b>Plot Ratio 1:7</b></li> <li>d. 80,000 square feet and above = <b>Plot Ratio 1:8</b></li> </ol> </li> </ol>

**Table 5.2.1:**  
List of Lot Amalgamation Incentive Area (continued)

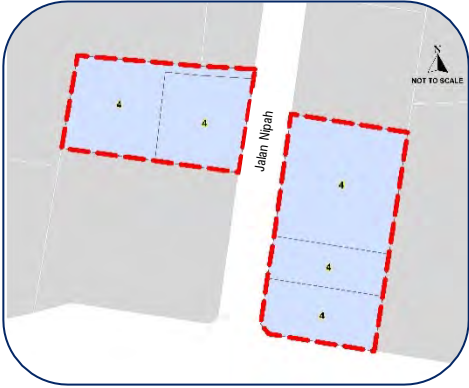

Location/Area	Development Control
<p><b>10. Lorong Palas off Jalan Ampang</b></p> 	<p>Land Use Zone: <b>Commercial (C)</b></p> <p>Base Plot Ratio: <b>1:4</b></p> <p>1. Lot amalgamation with an area of 30,000 square feet and above are entitled for a <b>Plot Ratio of 1:6</b> provided that the lot amalgamation is of an appropriate and continuous form.</p>
<p><b>11. Lorong Setapak, Jalan Pahang</b></p> 	<p>Land Use Zone: <b>Commercial (C)</b></p> <p>Base Plot Ratio: <b>1:2</b></p> <p>1. Lot amalgamation with an area of 30,000 square feet and above are entitled for a <b>Plot Ratio of 1:4</b> provided that the lot amalgamation is of an appropriate and continuous form.</p>
<p><b>12. Tiong Nam Area</b></p> 	<p>Land Use Zone: <b>Main Commercial (MC)</b></p> <p>Height Control: <b>4 Storeys</b></p> <p>1. Lot Amalgamation Area:</p> <ul style="list-style-type: none"> <li>a. 20,000 square feet - 29,999 square feet = <b>Plot Ratio 1:5</b></li> <li>b. 30,000 square feet - 39,999 square feet = <b>Plot Ratio 1:6</b></li> <li>c. 40,000 square feet and above = <b>Plot Ratio 1:8</b></li> </ul> <p>However, priority is given to comprehensive block development; and</p> <p>2. The requirement of minimum lot amalgamation as stipulated above (item 1) for Lot 1583 to Lot 1589 (7 lots) is <b>exempted</b> and need to be developed as a comprehensive development to entitle for a <b>Plot Ratio of 1:6</b>.</p>

**Table 5.2.1:**  
**List of Lot Amalgamation Incentive Area (continued)**

Location/Area	Development Control
<p><b>13. Jalan Merpati</b></p> 	<p>Land Use Zone: <b>Major Commercial (MC)</b></p> <p>Height Control: <b>4 Storeys</b></p> <p>1. Comprehensive amalgamation for all lots is entitle for a <b>Plot Ratio of 1:6</b> with consideration on accessibility and limited access of the site.</p>
<p><b>14. Jalan Ampang Kiri/Jalan Ampang Ulu 3</b></p> 	<p>Land Use Zone: <b>Mixed Development (MX)</b></p> <p>Base Plot Ratio: <b>1:2</b></p> <p>1. Lot Amalgamation Area:</p> <ol style="list-style-type: none"> <li>20,000 square feet - 29,999 square feet = <b>Plot Ratio 1:4</b></li> <li>30,000 square feet - 49,999 square feet = <b>Plot Ratio 1:6</b></li> <li>50,000 square feet and above = <b>Plot Ratio 1:8</b></li> </ol>
<p><b>15. Jalan Walter Granier/Lorong Walter Granier</b></p> 	<p>Land Use Zone: <b>City Centre Commercial (CCC)</b></p> <p>Height Control: <b>7 Storeys and 10 Storeys</b></p> <p>1. Lot amalgamation with an area of 30,000 square feet and above are entitled for a <b>Plot Ratio of 1:7</b> provided that the lot amalgamation is of an appropriate and continuous form.</p>



**Table 5.2.1:**  
**List of Lot Amalgamation Incentive Area (continued)**

Location/Area	Development Control
<p><b>16. Jalan Nipah</b></p> 	<p>Land Use Zone: <b>Commercial (C)</b></p> <p>Base Plot Ratio: <b>1:4</b></p> <ol style="list-style-type: none"> <li>1. Comprehensive amalgamation for all lots is entitled for a <b>Plot Ratio of 1:6</b>; and</li> <li>2. Individual lot development is entitled for a <b>Plot Ratio of 1:4</b>.</li> </ol>
<p><b>17. Persiaran Klang</b></p> 	<p>Land Use Zone: <b>Residential 3 (R3)</b></p> <p>Base Plot Ratio: <b>1:3</b></p> <ol style="list-style-type: none"> <li>1. Comprehensive amalgamation for all lots is entitled for a <b>Plot Ratio of 1:3</b>; and</li> <li>2. Individual lot development is entitled for 3 storeys.</li> </ol>

### 5-2.4 Planning Guide for Lot Amalgamation Area

Development planning in the lot amalgamation area must be focused on comprehensive development to create a good quality environment. In line with the provision of incentives, all lot merger areas should be developed based on the outlines as follows:

1. Provision or upgrading of infrastructure, community facilities and other facilities that are compatible with high-intensity development;
2. Planning of pedestrian walkways that are seamless age-friendly and disable-friendly;
3. Provision of appropriate access for the lot amalgamation area to facilitate movement in the surrounding area;
4. Planning of permissible activities according to Volume 1: Part 3 of the Classes of Use of Land and Buildings 2024 (CULB2024); and

5. Development planning that meets the conditions and criteria set by KLCH.



Location: Lorong Abdullah/Jalan Abdullah/Lengkok Abdullah

The environment of Lorong Abdullah/Jalan Abdullah/Lengkok Abdullah as one of the areas identified for the provision of lot amalgamation incentives.

## PLOT RATIO INCENTIVE FOR REDEVELOPMENT AREA

Redevelopment refers to new development on an existing site or area that involves renewal of the whole or most of the area including layout, structure, function and activity of the building or area. Redevelopment enables replanning the area in terms of design, infrastructure, utilities and community facilities. It is an alternative to accommodate the limited land needs in Kuala Lumpur. While ensuring Kuala Lumpur will always have a refreshing image.

This incentive aims to encourage the redevelopment of dilapidated areas, uninhabitable buildings and areas with deteriorating infrastructure and surroundings.

### 5-3.1 Use of Plot Ratio Incentive for Redevelopment Areas

The use of redevelopment area incentives involves two (2) forms of development as follows:

1. Redevelopment that involves compliance with redevelopment criteria as per Table 5.3.1. In general, 139 sites have been identified as priority redevelopment areas in the Draft KLLP2040 such as dilapidated areas, Malay Reserve areas, villages, informal settlements and low or medium-cost apartments as well as buildings over 30 years old; and
2. Redevelopment consist of eight (8) Special Redevelopment Incentive sites from 139 identified sites that have been zoned as Residential 2 (R2) and Residential 3 (R3) which are usually less than 10,000 square feet to initiate the redevelopment of the area. However, these sites must comply with certain criteria as per Table 5.3.3 to be entitled to the use of designated land use zone and are also permitted to be considered for redevelopment incentives. If the development is carried out without complying with the listed criteria, these sites are subject to the condition of the current land use zone, intensity, and character.

## BRIEF INFO

### REDEVELOPMENT INCENTIVE

#### Redevelopment Area

**139** sites

#### Special Redevelopment Incentive

**8** sites

from 139 identified sites



Location: Kampung Datuk Keramat

The surroundings of Kampung Datuk Keramat, as a redevelopment area.

### 5-3.2 Determination of Plot Ratio Incentive For Redevelopment Areas

The Draft KLLP2040 determines the incentive for redevelopment areas of three (3) main land use classifications which are residential, commercial and industrial as well as other land uses with criteria as per Table 5.3.1. This incentive is an additional plot ratio to the base plot ratio that set in the KLDCP2024.

Consideration for incentive is according to Table 5.3.2 and three (3) categories as follows:

1. Development of the whole area with permanent structures and stratified ownership;
2. Replanning of old, dilapidated or abandoned areas, buildings or facilities; and
3. Regeneration planning of an area or building.

The determination percentage for additional incentive of plot ratio is subject to evaluation at the planning control level considering criteria and categories of redevelopment. Redevelopment is also entitled to other incentives subject to the maximum percentage of 100 percent from base plot ratio in the KLDCP2024. However, if the redevelopment of an area uses incentives as specified by the Pelan Panduan Pelaksanaan Pembaharuan Semula Kuala Lumpur, the incentives specified in the KLDCP2024 is not applicable. Incentives may also vary according to policy changes from time to time.

If the building structure involves only part of the redevelopment site, then the granting of incentives is subject to the area of the structure plot or the area only subject to the determination at the planning control level.

**Table 5.3.1:**  
**Criteria of Redevelopment**

Land Use Classification	Criteria
Residential	<ol style="list-style-type: none"> <li>1. Old residential areas or buildings of more than 30 years, excluding land use zones R1, EH and TV1;</li> <li>2. Abandoned residential buildings for more than ten (10) years or development projects acknowledge as abandoned by the responsible agency or committee;</li> <li>3. Building certified unsafe for occupancy by responsible agency;</li> <li>4. Socially-not-fit housing areas; and</li> <li>5. Residential areas with multiple land ownership.</li> </ol>
Commercial and industrial	<ol style="list-style-type: none"> <li>1. Commercial and industrial areas or buildings of more than 30 years;</li> <li>2. Abandoned residential buildings for more than ten (10) years or development projects acknowledge as abandoned by the responsible agency or committee;</li> <li>3. Complex buildings or business centers that is non-viable; and</li> <li>4. Buildings certified unsafe for occupancy by responsible agencies.</li> </ol>
Others	<ol style="list-style-type: none"> <li>1. Solid waste disposal sites that are no longer operating and have comply with site rehabilitation period;</li> <li>2. Sewage treatment plant sites that are no longer operating and have comply with site rehabilitation period;</li> <li>3. Dilapidated or abandoned institutions areas;</li> <li>4. Former depots, public transport stations, infrastructure and utility facilities areas. However, infrastructure or utility sites that have been given ownership for development purposes still need to maintain the current functionality of the infrastructure or utility subject to the requirements; and</li> <li>5. Informal settlements.</li> </ol>

Source: Adapted from *Pelan Panduan Pelaksanaan Pembaharuan Semula Kuala Lumpur*



The Draft KLLP2040 designated the percentage of incentives given by considering the redevelopment category as shown in Table 5.3.2:

**Table 5.3.2: Categories for Designated Redevelopment Incentives**

<b>CATEGORY 1</b>	
<b>DEVELOPMENT OF WHOLE AREA WITH PERMANENT STRUCTURES AND STRATIFIED OWNERSHIP</b>	
<ol style="list-style-type: none"> <li>High-rise residential commercial or industrial development schemes with a strata concept. The forms of development refer to: <ol style="list-style-type: none"> <li>KLCH Perumahan Awam (PA) or Program Perumahan Rakyat (PPR) and private housing (landed or strata) that are not suitable for occupancy (socially not fit); and</li> <li>Old and dilapidated development schemes identified for redevelopment.</li> </ol> </li> <li>Residential or commercial or industrial development of more than 11 storeys with single ownership; and</li> <li>Development of informal settlement areas (residential, commercial, industrial) involve costs or labour borne by the developers, for example, costs of moving or resettlement of squatters.</li> </ol> <p>Note: This proposed development scheme involves transforming the whole area including the layout, structure, function and activity of the building or area. Infrastructure facilities, utilities and community facilities are also provided according to the needs of the new development.</p>	
<b>CATEGORY 2</b>	
<b>REPLANNING OF OLD, DILAPIDATED OR ABANDONED AREAS, BUILDINGS OR FACILITIES</b>	
<ol style="list-style-type: none"> <li>Replanning of land use for the Landed Development Scheme;</li> <li>Multi-storey buildings with single ownership of 4 - 10 storeys;</li> <li>Abandoned buildings or developments more than 10 years from the date of stop of work or development projects acknowledged as abandoned by the responsible agency or committee;</li> <li>Replanning of the area or site is as follows: <ol style="list-style-type: none"> <li>Former solid waste disposal sites or sewage treatment plant sites that have been optimized or are no longer operating such as oxidation ponds or pump houses;</li> <li>Institution sites such as higher education complexes or schools, government offices, military camps, health centres, higher education centres or airports;</li> <li>Former rail or aircraft or port (main transport) depots;</li> <li>Public transport terminals including park and ride sites that will be developed for station development; and</li> <li>Infrastructure and utility facilities.</li> </ol> </li> <li>Development of informal settlement areas (residential, commercial, industrial) does not involve resettlement costs (settlement involves only one-off payments) and placement efforts of residents or users of the area.</li> </ol> <p><i>*Note: This proposed development scheme involves transforming the whole area including the layout, structure, function and activity of the building or area. Infrastructure facilities, utilities and community facilities are also provided according to the needs of the new development.</i></p>	
<b>CATEGORY 3</b>	
<b>REGENERATION PLANNING OF AN AREA OR BUILDING</b>	
<p>Refers to a building or area with the following characters or structures with areas of more than 20,000 square feet* as follows:</p> <ol style="list-style-type: none"> <li>Replanning of land use for buildings or landed developments (single buildings) not exceeding three (3) storeys; and</li> <li>Uninhabitable and dilapidated areas with temporary development and insignificant land use activities such as sales exhibition centres, hawker sites, sports facilities, sports, or recreational facilities and offices.</li> </ol> <p><i>*Note:</i></p> <ol style="list-style-type: none"> <li>Area and size that can be redeveloped comprehensively.</li> <li>The incentive calculation must refer to the area of the site involved with the structure as specified by category.</li> </ol>	

### 5-3.3 Planning Guide for Redevelopment Areas

Following the provision of incentives, all redevelopment areas are encouraged to be developed based on the following outlines:

1. Mixed development with activities that complement each other such as residential, office or business;
2. Development that emphasises the public interest such as provision of community facilities for sites owned by the government and its agencies;
3. Adoption of development principles for inclusive, equitable and engagement with relevant stakeholders;
4. Provision at least 30 percent of the entire development for mixed-income housing including affordable housing with priority to the current residents of the site;
5. Provision of affordable space for retail and office that can be rented to encourage entrepreneurial activities;
6. Determine provision of parking incentives for redevelopment in TPZ or TIZ areas;
7. Provision of integrated, good quality and inclusive green space, open space and community facilities;
8. Application of Safe City, Smart City and Resilient City elements in redevelopment design; and
9. Preparation of impact studies such as social, environmental and traffic subject to suitability and needs imposed by related agencies; and
10. Incentives are also encouraged to get the views of the population subject to suitability according to the relevant agency's decision.

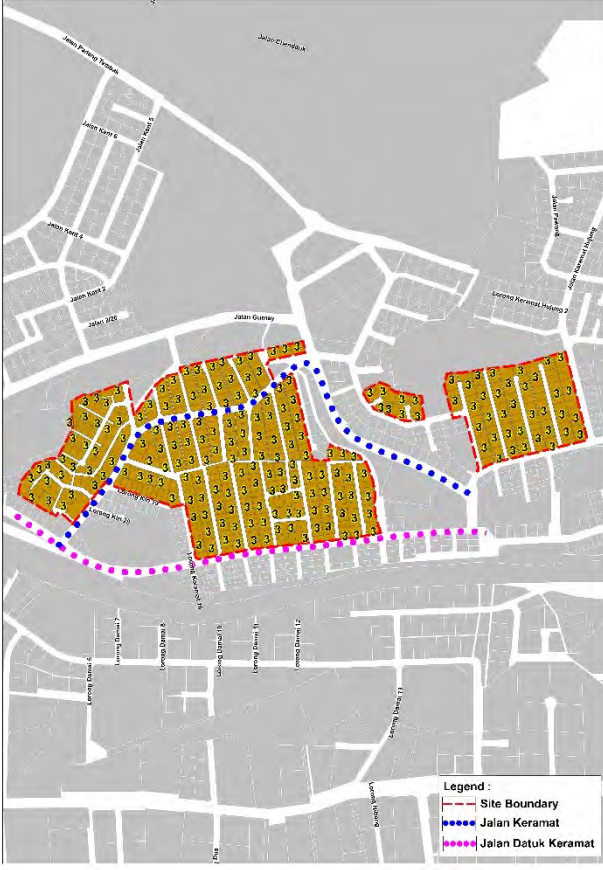



Location: Jalan Kampung Banda Dalam

Aerial view around the Kampung Banda Dalam area with Kuala Lumpur City Centre in the background.



**Table 5.3.3:**  
**Specific Sites for Redevelopment Incentives**

Location/Area	Development Control
	<p><b>Site 1:</b></p> <p><b>Kampung Datuk Keramat</b></p> <ol style="list-style-type: none"> <li>The designated Land Use Zone of Residential 3 (R3) and Plot Ratio 1:3 as stipulated in the KLDCP2024 are subject to:             <ol style="list-style-type: none"> <li>Comprehensive or cluster block development with a minimum area of 30,000 square feet; and</li> <li>Direct access to the site through Jalan Keramat or Jalan Datuk Keramat and the provision of a main access road with a minimum reserve of 15 metres.</li> </ol> </li> <li>The additional redevelopment incentive as stipulated in Table 5.3.2 (if applicable) is calculated from the base plot ratio set in the KLDCP2024 and subject to compliance with items 1a and 1b;</li> <li>Developments that do not comply with item 1 will be subject to a height control of three (3) storeys; and</li> <li>Redevelopment that creates an uneconomic lot is not encouraged.</li> </ol>
	<p><b>Site 2a:</b></p> <p><b>Kampung Baru Salak Selatan</b></p> <ol style="list-style-type: none"> <li>The designated Land Use Zone of Residential 3 (R3) and Plot Ratio 1:3.5 as stipulated in the KLDCP2024 are subject to:             <ol style="list-style-type: none"> <li>Comprehensive or cluster block development with a minimum area of 30,000 square feet; and</li> <li>Direct access to the site through Jalan 1/108 or Jalan 9 or Jalan 13 and the provision of a main access road with a minimum reserve of 15 metres.</li> </ol> </li> <li>The additional redevelopment incentive as stipulated in Table 5.3.2 (if applicable) is calculated from the base plot ratio set in the KLDCP2024 and subject to compliance with items 1a and 1b;</li> <li>Developments that do not comply with item 1 will be subject to a height control of three (3) storeys; and</li> <li>Redevelopment that creates an uneconomic lot is not encouraged.</li> </ol>



**Table 5.3.3:**  
**Specific Redevelopment Incentive Sites (continued)**


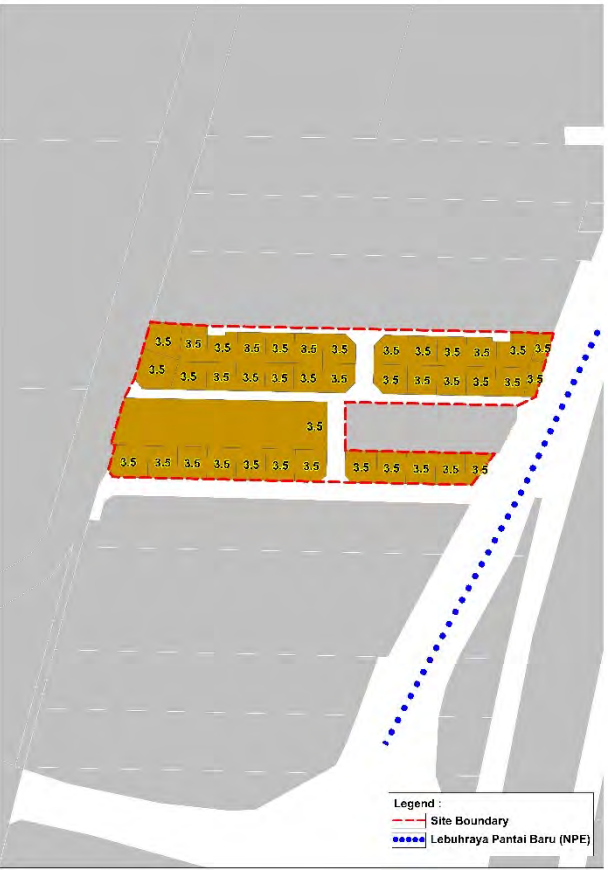


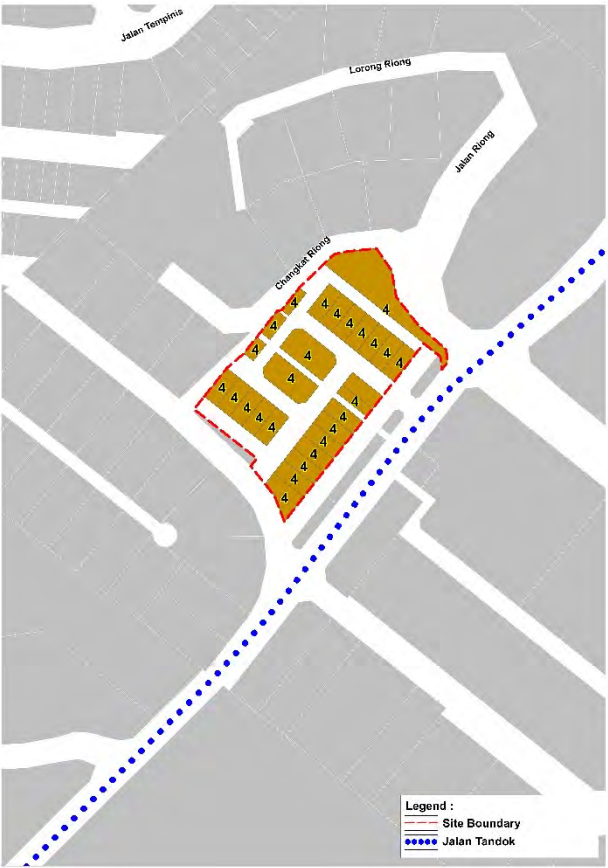
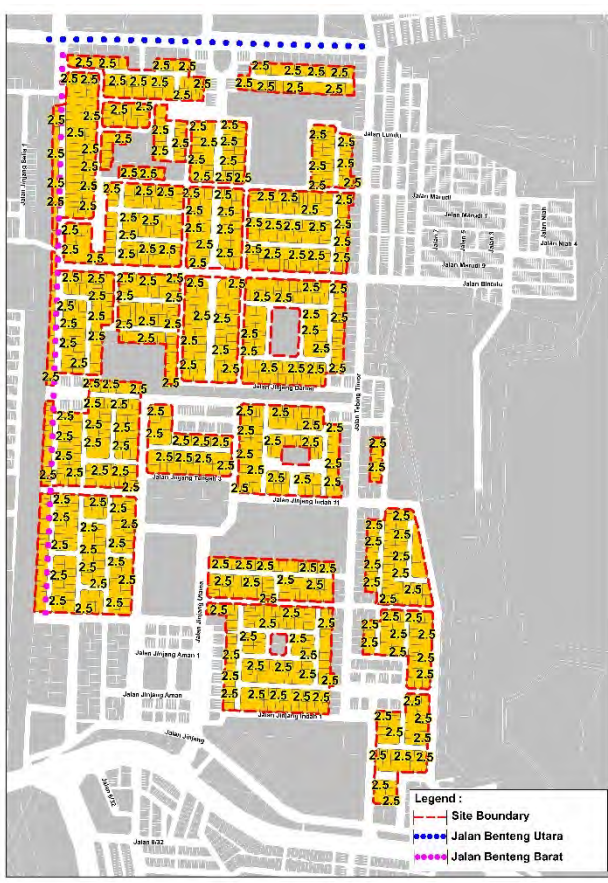
Location/Area	Development Control
	<p><b>Site 2b:</b> <b>Kampung Baru Salak Selatan (continued)</b></p> <ol style="list-style-type: none"> <li>The designated Land Use Zone of Residential 3 (R3) and Plot Ratio 1:3.5 as stipulated in the KLDCP2024 are subject to:             <ol style="list-style-type: none"> <li>Comprehensive or cluster block development with a minimum area of 30,000 square feet; and</li> <li>Provision of main access with a road reserve of at least 15 metres to the Sungai Besi Highway dedicated driveway (slip road) or to Jalan 28 or Jalan 1.</li> </ol> </li> <li>The additional redevelopment incentive as stipulated in Table 5.3.2 (if applicable) is calculated from the base plot ratio set in the KLDCP2024 and subject to compliance with items 1a and 1b;</li> <li>Developments that do not comply with item 1 will be subject to a height control of three (3) storeys; and</li> <li>Redevelopment that creates an uneconomic lot is not encouraged.</li> </ol>
	<p><b>Site 3:</b> <b>Kampung Pantai, Pantai Dalam (near Al-Khadijah Jamek Mosque)</b></p> <ol style="list-style-type: none"> <li>The designated Land Use Zone of Residential 3 (R3) and Plot Ratio 1:3.5 as stipulated in the KLDCP2024 are subject to:             <ol style="list-style-type: none"> <li>Comprehensive or cluster block development with a minimum area of 30,000 square feet; and</li> <li>Provision of main access with a minimum reserve of 15 metres to the New Pantai Expressway (NPE) dedicated driveway (slip road).</li> </ol> </li> <li>The additional redevelopment incentive as stipulated in Table 5.3.2 (if applicable) is calculated from the base plot ratio set in the KLDCP2024 and subject to compliance with items 1;</li> <li>Developments that do not comply with item 1 will be subject to a height control of three (3) storeys; and</li> <li>Redevelopment that creates an uneconomic lot is not encouraged.</li> </ol>

Table 5.3.3:  
Specific Redevelopment Incentive Sites (contiued)

Location/Area	Development Control
 <p>Legend :</p> <ul style="list-style-type: none"><li>Site Boundary</li><li>Jalan Rejang</li><li>Jalan Rejang 1</li><li>Jalan Rejang 10</li></ul>	<p><b>Site 4:</b></p> <p><b>Jalan Rejang Cluster Housing</b></p> <ol style="list-style-type: none"><li>The designated Land Use Zone of Residential 3 (R3) and plot ratio 1:3.5 as stipulated in the KLDCP2024 are subject to:<ol style="list-style-type: none"><li>Comprehensive or cluster block development; and</li><li>Direct access to the site through Jalan Rejang or Jalan Rejang 1 or Jalan Rejang 3 or Jalan Rejang 6 or Jalan Rejang 7 or Jalan Rejang 10 and the provision of the main access road with a minimum reserve of 15 metres.</li></ol></li><li>The additional redevelopment incentive as stipulated in Table 5.3.2 (if applicable) is calculated from the base plot ratio set in the KLDCP2024 and subject to compliance with items 1;</li><li>Developments that do not comply with item 1 will be subject to a height control of three (3) storeys; and</li><li>Redevelopment that creates an uneconomic lot is not encouraged.</li></ol>
 <p>Legend :</p> <ul style="list-style-type: none"><li>Site Boundary</li><li>Jalan Bangsawan</li><li>Jalan Ikhlas</li><li>Jalan Jujur</li></ul>	<p><b>Site 5:</b></p> <p><b>Jalan Jujur and Jalan Ikhlas Housing Area, Bandar Tun Razak</b></p> <ol style="list-style-type: none"><li>The designated Land Use Zone of Residential 3 (R3) and plot ratio 1:3.5 as stipulated in the KLDCP2024 is subject to:<ol style="list-style-type: none"><li>Comprehensive or cluster block development; and</li><li>Direct access to the site through Jalan Makmur or Jalan Bangsawan or Jalan Jujur or Jalan Ikhlas.</li></ol></li><li>The additional redevelopment incentive as stipulated in Table 5.3.2 (if applicable) is calculated from the base plot ratio set in the KLDCP2024 and subject to compliance with items 1;</li><li>Developments that do not comply with item 1 will be subject to a height control of three (3) storeys; and</li><li>Redevelopment that creates an uneconomic lot is not encouraged.</li></ol>



**Table 5.3.3:**  
**Specific Redevelopment Incentive Sites (contiued)**

Location/Area	Development Control
 <p>Legend :</p> <ul style="list-style-type: none"> <li>Site Boundary</li> <li>Jalan Tandok</li> </ul>	<p><b>Site 6:</b></p> <p><b>Taman Taiping Apartment (Jalan Tandok)</b></p> <ol style="list-style-type: none"> <li>The use of Land Use Zone of Residential 3 (R3) and Plot Ratio 1:4 as stipulated in the KLDCP2024 are subject to:             <ol style="list-style-type: none"> <li>Development that is comprehensive as a whole or in groups by block; and</li> <li>Access that is through Jalan Tandok.</li> </ol> </li> <li>The additional redevelopment incentive as stipulated in Table 5.3.2 (if applicable) is calculated from the base plot ratio set in the KLDCP2024 and subject to compliance with item 1;</li> <li>Development that does not comply with item 1, will be subject to the land use zone, intensity and current character of the site; and</li> <li>Redevelopment that creates an uneconomic lots is not encouraged.</li> </ol>
 <p>Legend :</p> <ul style="list-style-type: none"> <li>Site Boundary</li> <li>Jalan Benteng Utara</li> <li>Jalan Benteng Barat</li> </ul>	<p><b>Site 7:</b></p> <p><b>Jalan Jinjang Aman (Jinjang Utara)</b></p> <ol style="list-style-type: none"> <li>The use of Land Use Zone of Residential 2 (R2) and Plot Ratio 1:2.5 as stipulated in the KLDCP2024 are subject to:             <ol style="list-style-type: none"> <li>Development that is comprehensive or in clusters by block with a minimum site area of 30,000 square feet; and</li> <li>Access to the site that must be via Jalan Benteng Utara or Jalan Benteng Barat as well as the provision of a main access road with a minimum reserve of 15 meters.</li> </ol> </li> <li>The additional redevelopment incentive as stipulated in Table 5.3.2 (if applicable) is calculated from the base plot ratio set in the KLDCP2024 and subject to compliance with item 1;</li> <li>Developments that do not comply with item 1 will be subject to a height control of three (3) floors; and</li> <li>Redevelopment that creates an uneconomic lots is not encouraged.</li> </ol>



**Table 5.3.3:**  
**Specific Redevelopment Incentive Sites (continued)**

Location/Area	Development Control
	<p><b>Site 8:</b></p> <p><b>Jalan Jinjang Selatan</b></p> <ol style="list-style-type: none"> <li>The use of Land Use Zone of Residential 2 (R2) and Plot Ratio 1:2.5 as stipulated in the KLDCP2024 is subject to:             <ol style="list-style-type: none"> <li>Development that is comprehensive or in clusters with a minimum site area of 30,000 square feet; and</li> <li>Access to the site that must be via Jalan Jambu Jemba or Jalan Jambu as well as the provision of a main access road with a minimum reserve of 15 meters.</li> </ol> </li> <li>The additional redevelopment incentive as stipulated in Table 5.3.2 (if applicable) is calculated from the base plot ratio set in the KLDCP2024 and subject to compliance with item 1;</li> <li>Developments that do not comply with point 1, will be subject to a height control of three (3) floors; and</li> <li>Redevelopment that creates an uneconomic lots is not encouraged.</li> </ol>



The surroundings of Kampung Salak Selatan, Bandar Tun Razak and Kampung Datuk Keramat are among the identified redevelopment areas.



## PLOT RATIO INCENTIVE FOR DEVELOPMENT WITH AFFORDABLE HOUSING (RMM) COMPONENTS

Affordable housing (RMM) refers to housing that is affordable, habitable and safe to occupy for the needs of people in the B40 and some of the M40 income groups. Planning for the provision of RMM must consider housing supply towards 2040 and provided by government or private sector or through collaboration. RMM consist of Perumahan Rakyat 1 Malaysia (PR1MA), Residensi Wilayah, Perumahan Penjawat Awam Malaysia (PPAM), Residensi Prihatin and Perumahan MADANI.

RMM consists of at least three (3) bedrooms, two (2) bathrooms, a kitchen, living room, dining room, yard for laundry use and one (1) parking space. It must also be equipped with comprehensive public facilities and social needs to create a habitable and conducive environment. The provision of these facilities is subject to planning control considerations.

The public facilities that are required to be provided in RMM development are based on current policy and population catchment. Main facilities include surau or prayer rooms, multi-purpose halls or meeting rooms, management offices, nurseries, kindergartens, community shops or stalls and rooms for Muslims Mortuary Management.

### 5-4.1 Use of Plot Ratio Incentive for Development with Affordable Housing (RMM) Components

This incentive applies to sites that can be considered for affordable housing (RMM) development in Kuala Lumpur and are subject to compliance with the standards designated under the related affordable housing policy.

The Draft KLLP2040 determines the development of RMM as follows:

1. Development of affordable housing (RMM) in the City Centre Commercial (CCC), Major Commercial (MC), Commercial (C), Mixed Development (MX), Residential 3 (R3) and Residential 4 (R4) or in accordance with CULB2024 is permissible. RMM development in the Land Use Zone of Traditional Village 2 (TV2) must consider the aspects of accessibility, infrastructure, utilities, community facilities and surrounding environment;
2. Development of affordable housing in the Land Use Zone of Institution (INT) is subject to the needs of agencies as well as socioeconomic benefits in line with needs and planning of related agencies.

3. Development of affordable housing (RMM) in the Land Use Zone of Public Facility (PF1) is not permitted for residential units. However, the development of a dormitory is permissible with condition for the use of agencies and temporary support only; and
4. Development of affordable housing in Land Use Zone of Residential 1 (R1), Residential 2 (R2), Traditional Village 1 (TV1) and Establish Residential (EH) is not permissible and this incentive does not apply.

### 5-4.2 Determination of Plot Ratio Incentive for Developments with Affordable Housing (RMM) Components

This incentive is to encourage the development and provision of affordable housing (RMM) and the application is subject to the government's policy on affordable housing (RMM). This incentive includes items as follows:

1. The determination of this incentive should consider the minimum site area requirement of 30,000 square feet, the cost and technology of construction for RMM component development to ensure the provision of quality and sustainable housing; and
2. The determination of this incentive is applicable based on the guidelines adopted by KLCH periodically.



Location: PPAM Metropolitan Kepong  
Source: <https://www.shutterstock.com/g/cantik>

View of the affordable housing scheme from Taman Metropolitan Kepong.

### 5-4.3 Planning Guide for Developments with Affordable Housing (RMM) Component

Housing developers must refer to the RMM development criteria, conditions and facilities provision for incentives as follows:

1. The environment of RMM development must be harmonised and compatible with the surroundings;
2. Construction of RMM on lots bordering existing low-intensity residential areas or sharing the same access road is discouraged;
3. The development of comprehensive RMM should consider the provision of support facilities in line with current needs such as development of learning centres and provision of related facilities;
4. The provision of RMM should consider the elements of comprehensive public facilities for the local community and be supported by the provision of community facilities. This can be in the form of community centres and spaces for interactive communities such as open spaces, multi-purpose halls, public halls, religious sites and others;
5. The construction of RMM development should be implemented in stages through the use of Industrial Building System (IBS). The quality of affordable housing must meet the QCLASSIC assessment to guarantee a better construction quality score;
6. The application of Universal Design in the design of buildings and components of support facilities should consider the usage adults, children, the elderly and disabled;
7. The incentives are also encouraged to get the views of the population, subject to suitability according to the guidelines; and
8. Commercial development in affordable housing buildings for the purpose of title issuance may be considered subject to compliance with CULB2024 as well as the requirements of relevant agencies.



Location: PPAM Metropolitan Kepong



Location: PPAM Mercu Jalil  
Source: <https://property.locally.my/listing/ppam-mercuro-jalil>

Existing affordable housing programme in Kuala Lumpur for ownership of comfortable and quality homes.





# ATTACHMENT 1



# GAZETTED PLAN

BIL	LIST	PLAN NUMBER
1.	Kepong Baru	4001, 4001-1, 4023, 4023-1
2.	Taman Kepong	4002, 4002-1
3.	Taman Ibu Kota	4003-1
4.	Taman Setapak	4004-1
5.	Taman Sri Segambut	4005
6.	Taman City & Perumahan PKNS	4006
7.	Taman Kok Doh	4007
8.	Taman Overseas Union	4008, 4008-1, 4008-2
9.	Taman Mutiara	4009
10.	Taman Cheras	4010
11.	Taman Taynton	4011, 4011-1
12.	Taman Salak South	4012
13.	Taman Salak south timur	4012-1
14.	Taman Lian Hoe	4013
15.	Taman Midah	4014, 4014-1
16.	Taman Lee Yan Yian	4016
17.	Taman Kok Lian	4017, 4017-1
18.	Taman Bunga Raya	4018-1
19.	Taman Tun Dr Ismail	4022, 4022-1, 4022-2, 4022-3, 4022-4, 4022-5, 4022-6
20.	Taman Bukit Maluri (Peringkat 1B)	4024-1
21.	Taman Bukit Maluri (sebahagian dari Peringkat 1A dan kawasan perusahaan)	4024-2
22.	Bukit Bangsar	4025
23.	Taman Lucky	4025-1
24.	Bangsar Baru	4025-2
25.	Taman Bandaraya	4025-3
26.	Bukit Bandaraya	4025-5
27.	Taman Maluri	4027, 4027-1
28.	Taman Melewar	4028
29.	Sentul Jaya	4029
30.	Taman Yarl	4032, 4032-1
31.	Taman Goodwood, Taman Continental, Taman Skyline dan Taman Gembira	4034
32.	Taman Gembira	4034-1, 4034-2
33.	Taman Gembira Fasa 9, Taman Bukit Indah dan kawasan sekitar	4034-3
34.	Taman Rampai	4035
35.	Taman Sri Rampai	4035-1



# GAZETTED PLAN

BIL	LIST	PLAN NUMBER
36.	Taman Eastern	4036
37.	Taman Batu	4037
38.	Taman Cuepacs	4038
39.	Taman Setapak Jaya	4039
40.	Taman Batu View	4040
41.	Taman Wahyu	4042
42.	Taman Beringin	4043
43.	Taman Nanyang	4044-1
44.	Taman Petaling	4045
45.	Taman Rowther	4046
46.	Taman Pelangi	4047
47.	Syarikat Kerjasama Polis	4048



# GLOSSARY AND ABBREVIATION





# GLOSSARY

TERMS	INTERPRETATION
<b>Affordable Housing</b>	Affordable housing includes low-cost, medium-low-cost, and medium-cost housing with sales prices between RM42,000 and RM150,000 per unit to meet the needs of household income groups between RM1,500 to RM4,000 per month.
<b>Aged-friendly</b>	The inclusive layer of society covers all ages, including children, youth and the elderly.
<b>Amalgamation Lot</b>	A minimum amalgamation of two (2) or more land lots that are bordered and located in the same land use zone with the suitable lot size of area to qualify the lot to obtain an increase in intensity
<b>Buffer Zone</b>	A buffer zone is usually in the form of a green area, which separates two (2) land use activities that do not complement each other such as residential and industrial areas.
<b>Built-up Area</b>	Areas where the main land use is municipal land use consisting of housing, commerce, industry, institutions and other infrastructure facilities.
<b>Conducive</b>	Suitable and can help achieve or encourage to produce something good.
<b>Disabled People</b>	According to the Persons with Disabilities Act 2008, PWDs include those who have long-term physical, mental, intellectual or sensory disabilities that, when interacting with various barriers, can restrict their full and effective participation in society.
<b>e-hailing</b>	The process of ordering a car, taxi, limousine, or any other form of transportation through a virtual, computer or mobile cellular device.
<b>Elderly</b>	Individuals aged 60 years and above.
<b>Inclusive</b>	Inclusive means covering all groups and layers of society among citizens, whether in terms of gender, ethnicity, socio-economic status, age, education, religion, and space.
<b>Micromobility vehicle</b>	Micromobility Vehicles are a type of transportation mode of light vehicles such as electric scooters and bicycles such as shared bicycles, Electric Pedal-Assisted Bikes and the like.
<b>Mikromobility</b>	Refers to transportation solutions for short distance travel usually in the first or last distance of the trip.
<b>Mixed Development</b>	Development that contains various types of development, such as residential, commercial and industrial.
<b>Mobility Active</b>	Pedestrian paths, bicycle paths and micromobility vehicles.
<b>Transit Oriented Development, TOD</b>	TOD is a development concept based on rail or bus public transport stations with a surrounding environment that has high connectivity, is user-friendly, and reduces dependence on motor vehicles. It encourages mixed and high-intensity development within walking distance of surrounding development components, such as residential and commercial, as well as promoting optimal and inclusive land and space development.
<b>Plot Ratio</b>	Plot ratio means the ratio between the area of the floor space of the building and the area of the building plot as defined in Act 267 (Part 1, Section 2).
<b>Public Space</b>	Public space is a social space that is generally open and can be used by the public.
<b>Redevelopment</b>	A new development on an existing site or area involves the renewal of the whole or a large part of the area, covering the layout, structure, function and activities of the building or area.
<b>Spatial</b>	Spatial is related to space.
<b>Universal Design</b>	Defined as the design of environments and products that can be used freely by all groups of individuals without the need for any adaptation or special design

# ABBREVIATION

## ENGLISH

<b>KL</b>	Kuala Lumpur
<b>KLCC</b>	Kuala Lumpur City Centre
<b>KLDCP 2015</b>	Kuala Lumpur Development control Plan 2015
<b>KLDCP2022</b>	Kuala Lumpur Development control Plan 2022
<b>KLIA</b>	Kuala Lumpur International Airport
<b>KLUDG</b>	Kuala Lumpur Urban Design Guidelines
<b>KRM</b>	Gombak Malay Reserve Area
<b>KTM</b>	Malayan Railway
<b>JWP</b>	Department of Federal Territories
<b>LC</b>	Local Commercial
<b>LED</b>	Light Emitting Diode
<b>LRT</b>	Light Rail Transit
<b>M40</b>	Middle 40%
<b>MAS</b>	Malay Agricultural Settlement
<b>MC</b>	Management Corporation
<b>MC</b>	Major Commercial
<b>MICE</b>	Meeting, Incentives, Conferences and Exhibitions
<b>MOTAC</b>	Ministry of Tourism, Arts and Culture
<b>MRA</b>	Gombak Malay Reserve Area
<b>MRT</b>	Mass Rapid Transit)
<b>MX</b>	Mix Development
<b>MX</b>	Mixed Development
<b>MXI</b>	Mixed Industry
<b>NPE</b>	New Pantai Expressway
<b>OKU</b>	Persons with disabilities
<b>OS1</b>	Public Open Space 1
<b>OS2</b>	Public Open Space 2
<b>OS3</b>	Private Open Space
<b>OSE</b>	
<b>PA</b>	Public Housing
<b>PASS</b>	Subsidised Rental Public Housing
<b>PBRKL2020</b>	Kuala Lumpur City Plan 2020
<b>PBT</b>	Local Authority
<b>PF1</b>	Public Facilities
<b>PF2</b>	Private Facilities
<b>PKPR</b>	Small Office Home Office

## BAHASA MELAYU

Kuala Lumpur
Pusat Bandaraya Kuala Lumpur
Pelan Kawalan Pembangunan Kuala Lumpur 2015
Pelan Kawalan Pembangunan Kuala Lumpur 2022
Lapangan Terbang Antarabangsa Kuala Lumpur
Garis Panduan Reka Bentuk Bandar Kuala Lumpur
Kawasan Rizab Melayu
Keretapi Tanah Melayu
Jabatan Wilayah Persekutuan
Perdagangan Tempatan
Diod pemancar cahaya
Transit Aliran Ringan
Kumpulan Pertengahan 40%
Malay Agricultural Settlement
Perbadanan Pengurusan
Perdagangan Utama
Mesyuarat, Insentif, Persidangan dan Pameran
Kementerian Pelancongan, Seni dan Budaya
Kawasan Rizab Melayu
Transit Aliran Massa
Pembangunan bercampur
Pembangunan Bercampur
Industri Bercampur
Lebuhraya Pantai Baru
Orang Kelainan Upaya
Kawasan Lapang Awam 1
Kawasan Lapang Awam 2
Kawasan Lapang Swasta
Orang Seekar
Perumahan Awam
Perumahan Awam Sewa Bersubsidi
Pelan Bandar Raya Kuala Lumpur 2020
Pihak Berkuasa Tempatan
Kemudahan Masyarakat Awam
Kemudahan Masyarakat Swasta
Pejabat Kecil Pejabat Rumah

# ABBREVIATION

## ENGLISH

<b>PPA</b>	person per acre
<b>PPAM</b>	
<b>PPK</b>	Special Area Management
<b>PPR</b>	
<b>PR1MA</b>	
<b>PSKL2040</b>	Kuala Lumpur Structure Plan 2040
<b>PTKL2040</b>	Kuala Lumpur Local Plan 2040
<b>R&amp;D</b>	Research and Development
<b>R1</b>	Residential 1
<b>R2</b>	Residential 2
<b>R3</b>	Residential 3
<b>R4</b>	Residential 4
<b>RMM</b>	Affordable Housing
<b>RoL</b>	River of Life
<b>TIZ</b>	Transit Influenced Zone
<b>TLK</b>	Parking lot
<b>TLM</b>	Motorcycle Parking
<b>TOD</b>	Transit- oriented development
<b>TP</b>	Technology Park
<b>TPZ</b>	Transit Planning Zone
<b>TR</b>	Transportation
<b>TRX</b>	Tun Razak Exchange
<b>TTDI</b>	Taman Tun Dr. Ismail
<b>TV1</b>	Traditional Village 1
<b>TV2</b>	Traditional Village 2
<b>TWG</b>	Technical Working Group
<b>UDGKL</b>	Urban Design Guidelines Kuala Lumpur City Centre
<b>WPKL</b>	Kuala Lumpur City Centre
<b>YUPPIES</b>	Young Urban Professionals

## BAHASA MELAYU

Perumahan Penjawat Awam Malaysia
Pengurusan Kawasan Khas
Program Perumahan Rakyat
Perumahan Rakyat 1 Malaysia
Pelan Struktur Kuala Lumpur 2040
Pelan Tempatan Kuala Lumpur 2040
Pendidikan dan Penyelidikan
Kediaman 1
Kediaman 2
Kediaman 3
Kediaman 4
Rumah Mampu Milik
Zon Pengaruh Transit
Tempat Letak Kereta
Tempat Letak Motosikal
Pembangunan Berorientasikan Transit
Taman Teknologi
Zon Perancangan Transit
Pengangkutan
Tun Razak Exchange
Taman Tun Dr. Ismail
Perkampungan Tradisi 1
Perkampungan Tradisi 2
Kumpulan Kerja Teknikal
Garis Panduan Reka Bentuk Bandar Pusat Bandaraya Kuala Lumpur
Wilayah Persekutuan Kuala Lumpur







