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Wala Lumpur 2020

DRAFT KUALA LUMPUR CITY PLAN 2020



Towards a World Class City





Volume

# Towards a World Class City









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### Kuala Lumpur's vision is to be A World Class City

Kuala Lumpur promotes the concept of growth with distribution, physical and social sustainability, economic justification as well as a fair and efficient government, provisions of public amenities and good quality of life in the pursuit of a knowledgeable society.



### **Vision for A World Class City**

The vision for 'Kuala Lumpur - A World Class City', encapsulates the ambition to make Kuala Lumpur a city that will assume a major global and sub-global role for the benefits of all its communities, workers, visitors and investors. The Kuala Lumpur Structure Plan 2020 (KLSP 2020) emphasizes that the vision and goals of Kuala Lumpur have been formulated with the aims of creating a sustainable city with Kuala Lumpur City Hall (KLCH) ensuring that the planning for Kuala Lumpur will strike a balance between physical, economic, social and environmental development.

Draft Kuala Lumpur City Plan 2020 (Draft KL City Plan 2020) is KLCH's primary planning and development strategy. It is the draft local plan for Kuala Lumpur prepared under provision of Section 13 of Federal Territory (Planning) Act 1982. The Plan sets out a twelve year plan on what must happen to achieve the vision for the City of Kuala Lumpur

in 2020. This will guide decision makers, city planners, designers and builders the direction of growth for Kuala Lumpur and lead the City into A World Class City status.

### 'Kuala Lumpur - A World Class City'

This vision is a commitment that by the year 2020, the City of Kuala Lumpur will have made many positive changes to the physical environment without compromising the local environment and its ecology. It also commits to create a liveable and attractive environment for residents, businesses and visitors.

The Draft KL City Plan 2020 has adopted a set of strategic directions to realise this vision and they are centred on the five main goals of the KLSP 2020.

#### Goal 1

To enhance the role of Kuala Lumpur as an international commercial and financial centre.

### Goal 2

To create an efficient and equitable city structure.

### Goal 3

*To enhance the city living environment.* 

### Goal 4

To create a distinctive city identity and image.

#### Goal 5

To have an efficient and effective governance.

These goals are the framework for Draft KL City Plan 2020, in which its greatest challenge is the implementation of the goals and policies to achieve environmental, social and economic development simultaneously; where the improvement of one shall not be to the detriment of the other.

# Holistic Planning and Development

The essence of world class Kuala Lumpur is a place where all citizens of Kuala Lumpur and visitors feel the greatest possible sense of physical, emotional, intellectual and spiritual well-being. The process undertaken will be towards a long-term goal of "meeting the needs of the present without compromising the ability of future generations to meet their own needs". This means ensuring that the ways in which we live, work and play will not interfere with nature's inherent ability to sustain life. In essence, Kuala Lumpur aims to be a liveable city that promotes health, safety, a vibrant economy and a just society that respects the natural environment and values of its cultural heritage. By engaging the community in the process of building a strategy for the future, Kuala Lumpur will be a city for all to share and enjoy, for today and the future.



Kuala Lumpur's approach towards holistic development embrace the universal principles of Islam Hadhari, in which sustainability is embedded within these principles.

- 1. Faith and piety towards God;
- 2. A just and trustworthy government;
- 3. Free and liberated people;
- A rigorous pursuit and mastery of knowledge;
- Balance and comprehensive economic development;
- 6. A good quality of life for the people;
- Protection of the rights of minority groups and women;
- 8. Cultural and moral integrity;
- Safeguarding of the environment;
- 10. Strong defence capabilities.

Therefore it must be emphasized that the foundation for world class Kuala Lumpur shall be its commitment towards a holistic planning and development where Kuala Lumpur must commit itself to sustainability as its central vision. The holistic planning approach refers to the universal development framework principles outlined in Islam Hadhari (Civilisation Islam). The universal set of values emphasises development that focus on among others faith in God, equity and enhancing quality of life through the mastery of knowledge and the development of the city and its nation.

Emphasis is also given to the implementation of a dynamic and balanced economic development while ensuring our environment is protected and our culture and moral integrity is upheld.

### 'Kuala Lumpur - A World Class City'

**Key Principles:** 

A world-class working environment A world-class living environment A world-class business environment A world-class governance

The world class working environment, of Kuala Lumpur will provide a wide range of job and career opportunities. Good communication, efficient transportation and infrastructure services will enable its citizen, workers and investors to efficiently work. The Draft KL City Plan 2020 will ensure a pleasant working environment that also provides for conducive and well supported enabling facilities.

The world class living environment of Kuala Lumpur will possess good quality housing with adequate, accessible and high quality community facilities. The Draft KL City Plan 2020 will strive to make Kuala Lumpur healthy, safe and comfortable with vibrant living environment for all walks of people in the City.

Draft KL City Plan 2020 promotes a world class business environment for the City. Development of high quality business premises with excellent and efficient communication and information infrastructures that links Kuala Lumpur regionally and internationally will be of major concern.

KLCH commits to provide world class city governance. It will enable and facilitate development and investment in Kuala Lumpur through efficient and equitable use of available financial, organizational and human resources. Good governance also calls for innovation, creativity and integrity that ensure the provision and maintenance of the best possible working, living and business environments.

### **NPP'S OBJECTIVES:**

- To rationalise national spatial planning for economic efficiency and global competitiveness;
- To optimise utilisation of land and natural resources for sustainable development;
- · To promote balanced regional development for national unity; and
- To secure spatial and environmental quality and diversity for a high quality of life.

### National Development Context

### **National Physical Plan**

The National Physical Plan (NPP) is a long term national level plan for the period until 2020. NPP spells out policies that determine the direction of physical development and conservation areas within Peninsular Malaysia. The main goal of the NPP is to create an efficient, equitable and sustainable national spatial framework to guide the overall development of the country towards achieving a competitive developed nation status by the year 2020.

NPP defines Kuala Lumpur – Klang Valley – Seremban areas as the National Growth Conurbation which effectively covers an area of approximately 504,000 hectares and is to provide for a potential population of 8.5 million or 32% of the Peninsular Malaysia population by 2020. This means that the regional population of Kuala Lumpur Conurbation is more than five times Kuala Lumpur's population of 1.6 million (2005).

The strong regional population base will enhance Kuala Lumpur's role as an international regional commercial and financial centre. Thus the Draft KL City Plan 2020 will be a flexible document; one that is attractive to investments, able to accommodate new economic activities as well as catering for the increasing job opportunities in the City.

Kuala Lumpur will be the key beneficiary to the NPP's regional strategy. Regional linkages via rail to Johor Bharu and George Town Conurbation will further strengthen Kuala Lumpur's position and the vision for World Class City becomes more significant in the context of the NPP.

### Ninth Malaysia Plan

It is Kuala Lumpur's national mission to take upon the Ninth Malaysia Plan's call for cities to be more competitive to meet global challenges. In doing so, it aims to obtain greater performance for the importance of a joint national effort to ensure the achievement of nation building.





## KUALA LUMPUR'S COMMITMENTS

- Provide equal opportunity to all its local population and stakeholders to share in its liveability.
- Provide quality of life for its people.
- Create quality built environment with priorities to people's safety and comfort.
- Manage its natural environment and protect natural resources that provides a foundation for liveability.
- Build on its strong cultural environment, which includes the history, arts and multi-cultural values.



The Draft KL City Plan 2020 development strategies and initiatives are in accordance to the five thrusts of the Ninth Malaysia Plan. The City acknowledges and encourages the growing needs of new economies, the need for innovation-driven small medium enterprises (SME's), development of industrial parks as well as research and development institutions. The City will also expand its ICT infrastructure and more cyber centres will be encouraged to strengthen Kuala Lumpur's position in the Multimedia Super Corridor (MSC).

The Draft KL City Plan 2020 emphasises on liveability and quality of life for its local communities. It will ensure the protection of public housing to reduce disparity of urban poor issues in Kuala Lumpur. The Plan shall also focus on urban transportation and quality urban services in order to promote investment and development.

Environmental sustainability is one of the fundamental principles emphasized for development in the Draft KL City Plan 2020. Here designated Environmental Protection Zones have been identified with controlling guidelines for development.

### NINTH MALAYSIA PLAN KEY THRUSTS

#### **Thrust 1**

To Move the Economy up the Value Chain.

#### **Thrust 2**

To Raise the Capacity for Knowledge and Innovation and Nurture 'First Class Mentality'.

#### **Thrust 3**

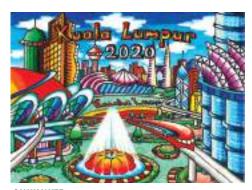
To Address Persistent Socio-Economic Inequalities Constructively and Productively.

### **Thrust 4**

To Improve the Standard and Sustainability of Quality Of Life.

#### Thrust 5

To Strengthen the Institutional and Implementation Capacity.



OH WAN YEE
SJK(C) Jinjang Selatan
1st Winner (Category A) of Drawing Competition in Conjunction with
Preparation of Draft KL City Plan 2020.



### THE NUP GOAL

To Create Visionary Cities with Peaceful Community and Living Environment through Sustainable Urban Development

### **National Urbanisation Policy**

The National Urbanisation Policy (NUP) is a fundamental framework for the Draft KL City Plan 2020. The policy calls for the creation of visionary cities, one that promotes liveable communities as well as sustainable urban development.

The six thrusts of the NUP outline thirty (30) policies that provide frame work for Draft KL City Plan 2020.

The KLSP 2020 vision and policies are in line with the National Urbanisation Policy. The Draft KL City Plan 2020 further strengthen its proposals to ensure these policies are translated into development strategies for the City.

The Draft KL City Plan 2020 emphasises on optimum and balanced land development. Infill development becomes a priority where uneconomic use of land, regeneration of urban blight areas are emphasised to ensure that the City is safe, healthy and liveable.

The Draft KL City Plan 2020 integrates land use with transportation to ensure transportation network are economicallyviableandphysically accessible. It also integrates land use with environmentally sensitive areas to ensure protection of sensitive areas as well as control the environmental quality of the City.

**DRAFT KUALA LUMPUR CITY PLAN 2020** 

### **Meeting the People's Needs**

More than 5000 numbers of people came to a forum, attended workshops, were respondents to surveys, or participated in focus-group discussions and technical working committees, or made suggestions via the web site. These were part of the public consultation process that the Draft KL City Plan 2020 undertook throughout the plan making period.

The consultations are carried out with two main groups:-

- Stakeholders Consultations i.e. with the people, residents and local communities; and
- Technical Consultations i.e. with the technical and decision making parties.

The public consultations were undertaken to empower the people in decision making especially in deciding how they should live and what they want in making living in Kuala Lumpur safe, healthy and prosperous. The consultations also provided opportunities for the people to raise specific or general issues and put forward views and ideas in the development and implementation of the Draft KL City Plan 2020.

The Draft KL City Plan 2020 must meet the needs of the people. The inputs provided are vital for the development of Kuala Lumpur as it reflects upon a strong social foundation that creates an inclusive society for the City.

### **National Urbanisation Policy**

THRUST	KEY THRUST STATEMENT	
Thrust 1	An efficient and sustainable urban development.	
Thrust 2	An urban economy that is resilient, dynamic and competitive	
	urban economy.	
Thrust 3	An integrated and efficient urban transportation system.	
Thrust 4	Provision of quality urban services, infrastructure and utilities.	
Thrust 5	Creation of a conducive liveable urban environment with	
	identity.	
Thrust 6	Effective urban governance	
Source : Na	tional Urbanisation Policy	

Towards a World Class City

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### THE PEOPLE'S NEEDS

- Good and reliable public transportation system;
- Safe and walkable city;
- Improve living standards and create a conducive physical environment;
- Employment and business opportunities in the City;
- Quality urban environment and inner city living;
- · ICT availability;
- Barrier-free environment;
- Accessible public and community facilities;
- Involved in decision-making process;
- Empower NGO's to manage community facilities;

The above are the main issues raised from consultations undertaken. One (1) forum, sixteen (16) focus group discussions (FGD), three (3) workshops and several technical working group discussions were carried out

Meeting these needs is a commitment from the KLCH that the economic, social and environmental performance is working towards one vision i.e. A World Class City that promotes quality living and a liveable city.

The Draft KL City Plan 2020 acknowledges that the needs consultation raised during the sessions revolve around quality built environment and the desire to be part of the decision making system i.e. to be consulted. Thus, this must be adopted as KLCH's practice to be transparent and providing feedbacks on any matters raised.

### **Realising the Vision**

Realising the vision of Draft KL City Plan 2020 will require great cooperation of all those that has a stake in the City of Kuala Lumpur's future. KLCH needs the cooperation and partnerships with various external agencies, ministries, the business and wider community.

The Draft KL City Plan 2020 has identified projects and programmes that are to be implemented throughout the Plan's period. To achieve the vision, the Plan must be well translated into four different forms of implementation mechanisms i.e.

### **Management Mechanism**

- Dynamic city governance;
- Delivery of key city services;
- Efficient and effective integrated regulatory environment; and
- Innovative and integrated strategic and corporate planning.

### **Planning Control Mechanism**

- Development Control documents;
- Development Guide Plans; and
- Planning and Design Guidelines.

### **Marketing Mechanism**

- Marketing the City's culture, characteristics, advantages and abilities;
- Marketing for investments and business; and
- Marketing for tourism attractions.

### **Partnership Mechanism**

- Partnership and alliances with stakeholders;
- Partnership with investors;
- Networks with business and community groups; and
- Network with non-government organisations, professional bodies etc.

Thus an integrated planning framework has been put in place to ensure that all of the policies, strategies and development programmes identified works towards achieving the vision. The integrated planning framework is made up of the Development Plans prepared under the Federal Territory (Planning) Act 1982 (Act 267) and KLCH's corporate plans.

The integrated framework includes:-

- Kuala Lumpur Structure Plan 2020 (KLSP 2020);
- Kuala Lumpur City Plan 2020 (the local plan for Kuala Lumpur);
- Kuala Lumpur City Hall Strategic Plan: and
- Kuala Lumpur City Hall Annual Budget.



### Integrated Planning Framework

This planning framework outlines an integrated planning approach to ensure that policies are translated into actions and implementation plans.



From KLSP 2020, the policies are translated into strategic directions, initiatives, actions, programmes and development control principles in the KL City Plan 2020. This in turn has to be translated into KLCH's Strategic Plan and subsequently onto KLCH's Annual Budgets for implementation.



Draft Kuala Lumpur City Plan 2020 promotes planning and development in a sustainable manner, where environmental quality, social equity and economic prosperity are essence in its long-term plan. New developments and opportunities are essential and encouraged, while at the same time protecting and enhancing Kuala Lumpur's valued assets.

### **Kuala Lumpur's Regional Role**

Kuala Lumpur - Malaysia's Capital and Premier City is the main location for business and trade in Malaysia, it accommodates regional headquarters of national and multinational companies, international and regional commercial and financial services, specialised highend retail services, high-technology manufacturing activities, education training services, and national cultural institutions.

Kuala Lumpur is one of the most affordable cities to live and conduct business. This positive factor contributes to why Kuala Lumpur is sought by international establishments to locate their regional headquarters in Kuala Lumpur.

The dynamic growth of the Asean and Asia-Pacific region also exerts a significant influence on Kuala

Lumpur and Malaysia's economy. However, Kuala Lumpur needs to remain competitive and the diversity of multi-cultural society makes Kuala Lumpur an ideal location to live and work.

Kuala Lumpur positions itself as an educational hub of international status. With more than 100 educational establishments in Kuala Lumpur that provides employment to more than 17,000 people, the educational activities in Kuala Lumpur has great impacts towards a foundation for a knowledge city. It has also made Kuala Lumpur an education destination for the Asean region as well as other parts of the globe, where students population come as far as from the Middle East countries, India, Korea, China, Mongolia, etc.



The City of Kuala Lumpur - A Snapshot

Land Area	242.2 sq.km
2005 Population	1.6 million
2020 Population	2.2 million
2020 National Conurbation Population	8.5 million
Population Density 2005	6,840 person/sq.km.
Employment 2005	729,300
Employment 2020	1,419,600
Total No of Dwellings 2005	464,996 units
Number of Distributive Trade 2001	25,228 nos.
Number of Manufacturing Establishments 2005	1,929

# **Kuala Lumpur and the Kuala Lumpur Conurbation**

Kuala Lumpur has great influence on the Kuala Lumpur Conurbation growth pattern. As a premier trade and financial city, Kuala Lumpur is at the centre of a large regional economy, thus its growth gives impact upon its surroundings and the City sets the pace of development for the region. This includes development of new centres that compliments the activities of Kuala Lumpur as well infrastructure development that links Kuala Lumpur to the conurbation area.

The future growth of the conurbation is expected to be even more challenging as cities in Selangor expands and the need to manage regional balance and environmental sensitivity must remain the main agenda for the conurbation.



The Kuala Lumpur conurbation is a metropolitan region that houses Putrajaya - the National Administration Centre, the Kuala Lumpur International Airport (KLIA) – international gateway to the country, Kuala Lumpur City – Capital City of Malaysia and centre for the trade and finance, Shah Alam – the capital city of Selangor, Seremban – the capital city of Negeri Sembilan and other major cities such as Petaling Jaya, Subang Jaya, Klang, Ampang, Selayang, Kajang and Bangi.



### KUALA LUMPUR AND THE KL CONURBATION

Kuala Lumpur's planning and development will take cognizance that the national conurbation is one single integrated unit.

Planning strategies and solutions should lead to a more sustainable conurbation, where inter-urban and intra-city movement will be enhanced, development will be based on the region's potential benefits and promotion of development will spearhead the conurbation as one economic region.

Kuala Lumpur will take a lead role in K-economy for the Kuala Lumpur Conurbation.

The national conurbation has a population of 4.8 million (2000) and has intense commercial and industrial activities. It is a vibrant metropolitan area with high inter-dependency for support services, thus requiring intergrated approach to development of the conurbation. Planning strategies and solutions need to ensure development of a sustainable conurbation, enhancement of interurban and intra-city movement. Development will be based on the region's potential benefits and promotion of development will enhance the conurbation as one economic region.

Kuala Lumpur's role as a national centre for job creation is still very relevant. It has a leading role in transforming the conurbation into a K-economy region as it capitalises on the City's human capital and thus it will enhance its regional and global competitiveness as a centre of knowledge-based economy. As

the nation's main city, Kuala Lumpur enjoys the highest per capita GDP (RM37,000 in 2005), rising productivity levels, highest household income (RM5,011 in 2004) and one of the lowest poverty rates in the country (level of poverty - 1.5% in 2004).

### **Surrounding Municipalities**

The City of Kuala Lumpur shares boundary with several districts that falls under different municipalities or local planning authorities within the state of Selangor. The urbanisation and dynamic growth of Kuala Lumpur has great impacts towards the surrounding areas and this lead to many issues that are common to the various municipalities as well as to the Kuala Lumpur City Hall.

Areas surrounding Kuala Lumpur are Petaling Jaya (under the Petaling Jaya City Council, Ampang (under the Ampang Jaya Municipal Council),

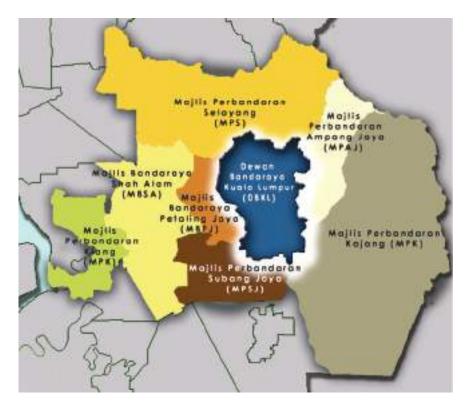


Selayang (under Selayang Municipal Council) and Kajang (under Kajang Municipal Council).

### **Regional Sustainability**

Regional sustainability is a common agenda for Kuala Lumpur City Hall as well as all surrounding municipalities and local planning authorities. Managing development pressure and undertaking a consultative process with surrounding authorities is part of the planning and development process. This includes matters related to road networks, public transportation networks as well as solid waste management issues. Development pressure also causes change to existing activities and the Draft KL City Plan 2020 responds to these anticipated changes and has reflected this in its proposal and its land use zoning structure.

This common agenda and collaborative effort will bring the Kuala Lumpur Conurbation to realise one of the National Physical Plan's main objectives i.e. to secure spatial and environmental quality and diversity for high quality of life. Collaborative work between Kuala Lumpur City Hall and these authorities must be undertaken continuously to address common issues that contributes towards the regional sustainability.





# A COMMON AGENDA - Collaborative Effort With Surrounding Municipalities

Improving regional sustainability;

Improving the quality of Sg Klang and the natural environment surrounding it;

Managing future road and rail networks;

Managing Bukit Sungai Putih Forest Reserve and Bukit Gasing;

Managing residential growth and development pressure;

Managing solid waste and drainage issues;

Responding to change in business and industry; and

Managing development and land use change within neighbouring areas.

### **River Quality**

One pressing issue that greatly affects the region is water quality of Sg Klang. This river flows through Kuala Lumpur and Selangor before it eventually flows into the Straits of Malacca. The river is 120km in length and drains a basin of 1288 sq. km. Cleaning the river will enhance the most populated area in the country. However, such effort is only possible through integrated river management procedures and practices that are adopted by Kuala Lumpur City Hall and all the municipalities. This also calls for controls in planning and development for various land uses and activities within the river catchment area as well as management of stormwater/runoff to prevent and manage pollutants from entering the river system.

### **Hill Land**

Bukit Sungai Puteh Forest Reserve and Bukit Gasing are two natural areas that share common boundaries with neighbouring districts. These Rank 1 Environmentally Sensitive Area are totally protected where no development is allowed except for those related to eco-tourism and research activities. Managing the areas surrounding these natural assets are important where development pressure has caused much concern for encroachment. These natural areas are important for Kuala Lumpur and the region's bio-diversity and they are part of an eco-system that is invaluable to the urban community. It sustains urban wildlife population, provides green lung which is an urban sanctuary for Kuala Lumpur and improves air quality of the City. Hence, Kuala Lumpur City Hall and Petaling Jaya City Council (for Bukit Gasing) and Kajang Municipal Council (for Bukit Sungai Puteh Forest Reserve) have to protect the natural areas and limit development at its immediate surrounding areas.





# **Kuala Lumpur and MSC Malaysia**

Kuala Lumpur is within the Malaysia Multimedia Super Corridor (MSC Malaysia) that is fast becoming a global centre for innovation and communication technologies (ICT). With an MSC hub in Cyberjaya, Kuala Lumpur has three of the seven Cybercities approved in the country i.e. Kuala Lumpur City Centre (KLCC), KL Tower and Technology Park Malaysia (TPM), whilst KL Sentral is one out of the three Cybercentres approved in Malaysia.

There are 67 numbers of World-Class ICT companies in MSC Malaysia undertaking research, developing new products and technologies and exporting them. These numbers are encouraging as it will create an industry cluster, where similar technology companies locate themselves within similar geographical areas where high speed

data networks and infrastructure as well as all other supportive services and resources are available.

Kuala Lumpur as the premier city of the nation will be a Cybercity by 2020. This means that Kuala Lumpur will be a self-contained intelligent city offering world-class business and living environment and all incentives under the MSC Bills of Guarantee (BoGs). The Draft KL City Plan 2020 development encourages new within Kuala Lumpur especially office buildings, research and development premises, public institution as well the technology parks to be Cybercentres with state of the art infrastructure. To ensure quality, the MSC Performance Guarantees with Financial Penalties (PGFPs) will regulate the quality for telecommunication and utilities in Kuala Lumpur.

### KUALA LUMPUR CYBERCITY BY 2020

A self-contained intelligent city offering world-class business and living environment and all incentives under the Bills of Guarantee (BoGs).

New Cybercentres in Kuala Lumpur - Public buildings, universities, research and development (R&D) premises and office buildings.

Kuala Lumpur Cybercity will enhance the quality of service by both public and private sectors to the business and living community. It will gear Kuala Lumpur into a performance culture that complies with the MSC Malaysia Standards which encompass utilities, telecommunications and urban services.

### **CURRENT STATUS**

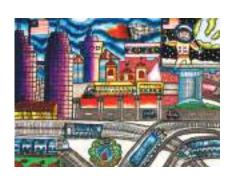
MSC Approved Companies **1677 nos** 

Foreign Companies

137 nos

MSC Companies in Kuala Lumpur **241 nos** 

World Class ICT Companies **67 nos** 



**LIM YANG WEI**SJK(C) Chong Hwa
2nd Prize Winner ( Category A ) of Drawing Competition in Conjunction with Preparation of Draft KL City Plan 2020.

### The People of Kuala Lumpur

Kuala Lumpur is a multi-cultural city that truly reflects an Asean city. The Malays (41%) and Chinese (39%) almost equally share the majority population in the City while the Indians comprise of 10%, followed closely by the foreign population at 7%. Such trend will continue until 2020 when Kuala Lumpur population grows to 2.2 million.

More than 30% of Kuala Lumpur's residents achieved tertiary education vis-à-vis the Malaysian average of 16.7%. This indicates a well-educated workforce that is able to respond to new economies that the City targets.

Kuala Lumpur is a city with high proportion of working-age residents, where more than 53% of its residents are of 20 to 55 years of age. This indicates strong human resources available in the City to participate actively in economic activities of the City.

It also has 42% of youth group (20 to 45 years of age). This reflects young adults population that is significantly high and thus creates demand for affordable accommodation, active recreation and social facilities in the City. Another estimated 25% of the City's resident population are students. This is a vital contributing factors to planning and development of Kuala Lumpur as it serves the needs of the young population of the city.



#### **VIBRANT AND 'YOUNG CITY'**

Kuala Lumpur is a multi-cultural city and has a 'young' population where more than 53% of its residents are of working age i.e. 20 to 55 years of age, while another 25% of the city's resident are students.

### **NEW NEEDS**

- Affordable Housing
- Recreation
- Social Opportunities
- Student Friendly City

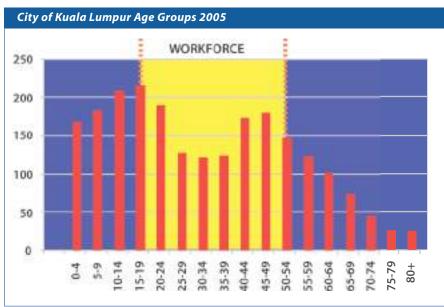
Kuala Lumpur will meet the needs of the minority i.e. the persons with disabilities and elderly residents of the City by ensuring comfortable accessibility and reachable public spaces.



Kuala Lumpur has a small proportion of elderly residents. Only 4% of the residents are 65 years and above whilst its persons with disabilities account for only 1.13% of the City's population. Though the number is small, Kuala Lumpur will ensure that facilities and accessibility for this small group of residents is comfortable and the City is reachable especially to public spaces and buildings.

The people of Kuala Lumpur are the social capital of the City. Healthy social development is the essence towards building the societal strength that is needed for economic growth of Kuala Lumpur and the nation. The need to build communities that live in harmony and comfort enhances capacity for people to recognise





shared values. Kuala Lumpur will enable all people to positively participate and contribute to the life of the City. 'Place Making' will be emphasised in the inner city and neighbourhood areas. This is important to nurture a sense of belonging to Kuala Lumpur.

Kuala Lumpur experiences population loss to areas surrounding the City. The establishment of Putrajaya as the national federal administrative decentralization centre, economic development to other more industrialized states and the availability of relatively cheaper housing development in other parts of Klang Valley, has caused Kuala Lumpur to experience net outmigration population, which together with a relatively slower rate of natural increase in population, has resulted in a lower than national population growth rate.

This is an important issue to address, as the City needs a strong population base to sustain its activities, its urban services and its transportation system.

### NURTURING THE SOCIAL CAPITAL

Kuala Lumpur will nurture the social capital in its city planning. 'Place Making' will be emphasised in the inner city and neighbourhood areas to nurture a sense of belonging to Kuala Lumpur.

Participatory planning process will be made part of the planning and development process.



### **Density Structure of Kuala Lumpur**

Strategic Zones	Area Sq.Kms	Population 2005	Density 2005 (person/ sq.km)	Population 2020	Density 2020 (person/ sq.km)
City Centre	17.7915	143,000	8,038	245,611	13,805
Wangsa Maju- Maluri	46.5858	380,300	8,163	443,712	9,525
Sentul-Menjalara	46.0998	344,500	7,473	445,025	9,654
Damansara- Penchala	47.4531	167,100	3,521	259,078	5,460
Bukit Jalil-Seputeh	43.2288	318,300	7,363	464,271	10,740
Bdr.Tun Razak- Sg.Besi	41.1694	266,900	6,483	340,669	8,275
KUALA LUMPUR	242.3284	1,620,100	6,840	2,198,366	9,577

### **CHANGING CITY**

# Future Population of Kuala Lumpur

To achieve the vision for A World Class City by 2020, Kuala Lumpur needs an optimum population that supports the City's global and economic role as a leading centre of the new economy. As such Kuala Lumpur will have a population of 2.2 million and employment of 1.4 million. With a sustainable growth rate, where its population density far exceeds other urbanised areas, Kuala Lumpur's regional role and being the epicentre of the nation's national growth conurbation provides it with the opportunity and advantage to achieve its targets.

This would be achieved by implementing measures to reverse the declining population growth rate in the past and speed up population growth over the next 12 years. If Kuala Lumpur is to remain as the premier city for national and international business and services, population growth needs to be equal to or higher than the national average growth rate of urban population in Peninsular Malaysia by 2020.

The current population of the City is unevenly distributed with large pockets of low densities residential, which will be reutilized and revitalized for potential development through redevelopment and infill development.

Thus, the strategy is to intensify development where resources are available, develop infill sites; redevelop brownfield sites or land of uneconomic use.

Most importantly, Kuala Lumpur must be liveable in order to encourage people to live in the City comfortably and though the plan allows for an additional 600,000 people in the next 12 years, it must do so by protecting the City's natural environment and enhancing the existing neighbourhoods.

All areas are expected to increase in population with the highest being in Bukit Jalil-Seputeh followed by City Centre and Sentul Menjalara Strategic Zone.

The population increase will require more than 150,000 homes in the next 12 years (based on an average household size of 4). However to date, more than 211,397 residential units have been committed (i.e. under construction, approved with Development Order and approved in principle). Assuming only 70 per cent of these homes will be owner-occupied; hence, the housing requirement up to 2020 has been met. Hence, the challenge is to continue protect the liveability of the established areas and to concentrate changes in undeveloped land and redevelopment of infill sites.



# Meeting Economic Challenges

## Kuala Lumpur as a Premier City

Kuala Lumpur's position as the national premier city must be sustained in the short to long term. The City of Kuala Lumpur will strive to remain competitive with other cities globally. The new conurbation within emerging Malaysia i.e. the Iskandar Malaysia, the Northern Corridor Economic Region and the East Coast Economic Region are competing for investments, whether foreign or domestic. In the short span of 12 years before reaching 2020, Kuala Lumpur must grow its economy to maintain its premier position at the national level while competing internationally to be a global investment centre.

Three main economic strategies are identified for Kuala Lumpur. They are:-

- positioning to be an attractive global investment centre with a critical population mass and a reasonable pool of skilled workers,
- positioning to be a vibrant job creation centre through marketdriven cluster development to sustain and expand its business base, and
- modernizing manufacturing cluster and integrating it with services sector to facilitate shift to the new K-economy.



## **Enhancing Competitiveness and Internationalization**

To meet the KLSP 2020 objective of establishing Kuala Lumpur as a world class working and business environment, the main economic policy states that "KLCH shall implement measures to develop Kuala Lumpur as the centre of knowledge-based economy", while information technology will be the fundamental enabling tool. The nucleus of KBE will be human capital – its capacity to create, innovate, generate and exploit new ideas as well as apply technology and exercise superior entrepreneurial skills.

Kuala Lumpur will adopt the cluster-base strategy to build competitiveness in the K-economy. Clusters, which are groups of interrelated industries, have two key elements. Firstly, firms in the cluster must focus on enhancement to prevent relocations of international businesses from Kuala Lumpur to alternative sites. Secondly, groups of inter-linked companies should locate in close proximity to one other to

create economies for scale that would further enhance the various clusters.

Clusters in Kuala Lumpur take various shapes. Some are fledglings requiring intensive support and incentives; some are growing in strength but would still need support and incentives; all are vital to the future growth of the urban economy of Kuala Lumpur.

Kuala Lumpur can play a pivotal and leading role in the development of ICT clusters as it is located in the Multi Media Super Corridor (MSC). The MSC, established in 1996, sets the platform to build cyber cities and cyber centres which would contain a competitive cluster of local ICT companies and a sustainable ICT industry.

The ICT clusters to be enhanced to provide new sources of growth for Kuala Lumpur include software development and support services outsourcing (SSO). Malaysia has been identified to have an edge in higher valued-added services and call centres. The Ninth Malaysia Plan is giving emphasis to attract more global players as well as encouraging

the participation of the local industry in potential markets. Towards this end, international promotion and marketing will be intensified, focusing on attracting large-scale centres from energy, finance, logistics, manufacturing and healthcare sectors.

## Provision of World Class Infrastructure

The provision of world-class infrastructure, especially in the designated international zones are vital to the internationalization process and to raise the City's competitive edge. Priority should be accorded to major physical improvements in the international zones in order to sustain and attract more investment into the City. Kuala Lumpur must have a worldclass physical environment in order to attract and retain the presence of regional headquarters of national and multinational companies. It needs to improve its physical environment to establish itself as an international and regional centre for financial and business enterprises.

Interconnectivity within Kuala Lumpur is critical to position Kuala Lumpur as a premier city in the country. Poor interconnectivity within Kuala Lumpur has encouraged firms, especially international firms, to consider locations outside of the City towards neighbouring cities.

### **Skilled Human Resources**

Trend in expected job growth for all sectors in the future will be based on high-tech, knowledge-based as well as high and medium skilled jobs. New jobs creation process in the City has to be based on the availability of skilled manpower by 2020. The thrust into the new economy would impact on the demand for skills structure that reflects on higher skilled workforce. The City needs to achieve a new work force structure that reflects one that is usually associated with most developed countries where the knowledge based economy (KBE) elements are high.

The components of such structure are as follows:-

- Increase highly skilled workers to 60% of workforce for Kuala Lumpur to be the centre of knowledgebased industries by 2020.
- Maintain the proportion of medium skilled at 35%.
- Maintain basic or elementary jobs at 10% to 25%.







# Positioning Kuala Lumpur As a Vibrant Centre for New Job Creation

Over the period 1991-2000, the employment-population ratio Kuala Lumpur was exceptionally high at 0.48. In the year 2000, it ranked the highest among all states in Malaysia in terms of job creation, indicating its economic strength in absorbing its own working population as well those from the neighbouring areas. This trend is also evident from the period 2000-2005 when the number of jobs in Kuala Lumpur increased from 640,400 in 2000 to 729,300 in 2005, resulting in a net job creation of around 73,000.

Slightly more than a third of the new jobs created are concentrated in the financial and business sub-sector. It is followed by transportation, storage



and communications sub-sector, especially in the communications sub-sector, which also includes ICT activities. Together, the two sub-sectors accounted for 65% of new jobs created in the City.

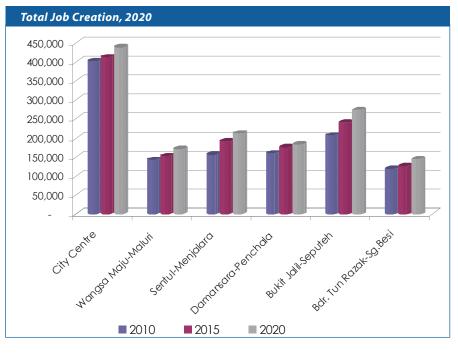
The dynamism and vibrancy of the services industry, which is a vital part of Kuala Lumpur's economy, is critical for sustaining population and economic growth in Kuala Lumpur. It is, therefore, important for Kuala Lumpur to continue to strengthen its regional economic position and its role as a centre for job creation.

Total jobs created in the City are expected to rise from around 729,000 in 2005 to 1.2 million in 2010, 1.3 million in 2015, and to 1.4 million by 2020. The City Centre is expected to maintain its position as the major source of jobs creation, followed by

Bukit Jalil-Seputeh Strategic Zone, and Sentul-Menjalara Strategic Zone.

trends indicate The projected manufacturing would decline as a major source of jobs creation in Kuala Lumpur. This pattern results from the restructuring of the manufacturing sector leading to a greater emphasis research and development activities which are oriented towards 'manufacturing-services'. Most of such activities are expected to be housed in office accommodations and in business parks, indicating a shift in demand for property types in the manufacturing sector.

To create more jobs in Kuala Lumpur entails strengthening the City's economic activities. Supporting the development of existing and emerging clusters within Kuala Lumpur enables the City to focus on a few







### **Draft Kuala Lumpur City Plan 2020**

To Achieve A World Class City through a 12 year plan to manage the City's growth.

Three Main Elements:

- Environmental Quality
- Social Equity
- Economic Prosperity

key economic activities that provide value-added services. These would serve as a source of growth in the local economic base, supporting businesses and creating employment for the targeted population of 2.2 million. Kuala Lumpur's two key clusters are that its business and financial sector, and its urban tourism, especially shopping and retailing.

There are emerging clusters such as medical, educational, and professional consulting services, which the City can promote further. These services, what are termed as producer services; are largely k-based, demanding highly skilled and knowledge-based workers. In supporting these economic industries, the City will enhance its economic base, enabling its economy to grow faster, be more productive, thus, enhancing its ability to be more competitive globally.

Due to its prime location as the national capital, with good connectivity at the national level in terms of the international airport at KLIA, railway, ports and highways, Kuala Lumpur is the main international tourist destination in the country. In 2004, Kuala Lumpur's share of foreign tourists' arrivals in the country is more than a third at 36%, making it a popular destination for foreign tourists to Malaysia. Growth has been significantly higher compared to the national average. Tourist arrivals to Kuala Lumpur have been rising over the past four to five years. Kuala Lumpur is positioning itself as a regional shopping hub, which it has been relatively successful. Shopping is now a major attraction that the City offers, especially with its variety of shopping complexes, and goods that are competitively priced compared to international shopping havens like Hong Kong and Singapore.





The task of making Kuala Lumpur a centre for job creation is not limited to the formal economy. More often than not, a vibrant city like Kuala Lumpur has a strong informal sector that exists along side its highly productive, high value-added services sector. The informal services play an important role in the economy of Kuala Lumpur. The informal economy is an important source of income to the urban poor, many of whom could be holding down a day job as well. In Kuala Lumpur, hawking forms a major part of the informal services. The urban poor work in some of the more essential services in the city as labourers, street cleaners, etc. They live in the City because of the convenience but many are struggling with the high costs of living in the city-rising fuel costs, high transport costs, and high cost of accommodations. But because of its social and economic contributions to the life of the city, the informal sector should not be neglected; instead it should be supported and nurtured to integrate with the formal sector, especially the urban tourism cluster. The intention is to provide a more sustainable form of livelihood for the urban poor, and to empower them with the means to improve their living conditions.

## Modernising the Industrial Clusters for Higher Economic Growth

The industrial sector in Kuala Lumpur must take a leading role in transforming the City's economic base towards KBE in tandem with its location within the MSC. The transition from the P-economy to the K-economy means that while new types of industries that are clean, requiring high skilled labour and employing new technologies will be the focus for the city, Kuala Lumpur will revitalise its existing industrial base and it will ensure that the existing industries are integrated with new developments that are now taking place.





## The Draft KL City Plan 2020

The Draft KL City Plan 2020 translates the vision for A World Class City in a 12-year plan to manage the city's growth. It promotes planning and development that is sustainable and places priority on three main elements of:

- Environmental Quality
- Social Equity; and
- Economic Prosperity

These three elements form the framework for the Draft KL City Plan

### **5 CORE PRINCIPLES**

**Planning For Wealth Creation** 

Planning For Safety and Comfort

Planning For Connectivity and Accessibility

**Planning For Greener Standards** 

Planning 'with and for' the People 2020 which encourage innovative solutions to land development and at the same time controls allowable activities and intensity of the said development.

As A World-Class City, Kuala Lumpur will plan and develop quality living environment as well as ensure opportunities for wealth creation. Quality of life is of utmost importance to the residents and the civil minimum that every resident is entitled to; ensure communities access to infrastructure, access to clean environment, affordable housing, green spaces and parks as well as having the opportunity to earn a living and prosper.

Thus, the Draft KL City Plan 2020 has 5 core guiding principles that will frame the Draft Kuala Lumpur City Plan 2020. These principles are:

### **Planning For Wealth Creation**

Kuala Lumpur core business and financial zone is strengthened by not limiting it to the once famous 'Golden Triangle'. The Draft KL City Plan 2020 identifies a Premier Business and Financial Zone for Kuala Lumpur that will enhance the financial and business cluster in Kuala Lumpur. It would induce the growth of supporting financial, professional and business activities in and around this core area. The

financial precinct or premier financial core area is bounded by Jalan Sultan Ismail – Jalan Raja Chulan – Jalan Tun Perak – Jalan Raja Laut.

Kuala Lumpur's cluster-based strategy will create distinct employment centres, distributed within Kuala Lumpur.

The Plan also creates opportunities for innovation and knowledge economy within existing and emerging industries and land use zoning for Technology Parks, SME Business Parks and Industrial Park.

### Planning for Safety and Comfort

Kuala Lumpur priority is to ensure that it is a safe and comfortable city, i.e. where everyone feels at ease and is able to do business, work, live and play in the city in peace, free from threat of crime, violence or intrusion.

The Plan focus upon protecting the natural environment, enhancing the built environment, conserving of culture and heritage to create an image that is uniquely Malaysia, providing quality parks and open spaces and facilities for all age groups of the community.

# Planning for Connectivity and Accessibility

Draft KL City Plan 2020 has planned for a comprehensive network of public transportation system for the city. The main aim is to make Kuala Lumpur a well-connected city, enhancing accessibility and improving reachability.

This will be done through integrated land use development with transportation planning and development, which will encourage development to grow in a smart manner i.e. in areas that is well served and accessible by public transportation.

Rail will be one of the major public transportation system planned for Kuala Lumpur. The Plan recognises the need for 11 new lines which will have a total length of 131.2 km. These lines once completed will increase the coverage for areas within transit corridors (i.e. 400 meters on each side of rail line) from the present 20 per cent to 52% of Kuala Lumpur.

Urban developments is promoted and intensified along transit corridors and transit nodes to make workplace and community services more accessible.

### Planning For Greener Standards

Draft KL City Plan 2020 will adopt greener standards, where environmental sustainability will be a priority.

The Plan calls for optimum growth where land use development integrates and co-exists with environment.

Water resource management is promoted in the city, where water recycling and rainwater harvesting will be encouraged.

Energy efficient city will be one key feature of this Plan, Kuala Lumpur will promote alternative use of energy and renewable energy in the City.

Encourage reduction in green house gas emission by planning for public transportation.

Reduce household waste generation and encourage reuse and recycling of waste materials.



### Planning 'For and With' the People

Kuala Lumpur is a city that houses 2.2 million populations and provides employment to 1.4 million people. The City will thus ensure that whatever it plans, builds or develop are what the people wants and needs. As a city that responds to the changing needs of the city, it will be 'Inclusive' i.e. where it enables the people to participate fully in Kuala Lumpur city life, where social inclusion and liveability are important elements of this City.

Kuala Lumpur will also strive to become an **'Engaging City'** – where it will engage its people to support the city's activities such as recreation, cultural and social event, festivals, sports activities, etc for the greater benefit of the community.



Kuala Lumpur shall strengthen its role as the premier business and retail hub of the nation, becoming a dynamic international centre that will promote national and regional growth.

Kuala Lumpur's vision to create a world class living and business environment coincides with the national goal on being competitive and knowledge-intensive. This entails a paradigm shift to the new economy where more knowledgeintensive industries are encouraged to locate and concentrate in Kuala Lumpur. This thrust is expected to permeate throughout the urban economy, creating more wealth for the people and business community, it shall leading to higher living standards, and a more conducive creative working environment that will encourage active interactions among skilled workers, technopreneurs, innovators, creative artistes and k-businesses.

This new urban economy in Kuala Lumpur shall be characterized by an abundance of skilled human resources. Wealth generated largely from the intense sharing of codified knowledge and information, and new knowledge gained from R&D and intellectual property, increased collaboration between firms and institutions, the extensive use of information and communications technologies (ICT) and the development of more effective learning mechanisms across the entire economy.

Spatial growth in Kuala Lumpur over the past decade has been closely associated with the growth of its urban economic activities. The new emerging economies envisioned for Kuala Lumpur shall strengthen it's central role as the Capital City of Malaysia and the nation's largest employment centre, where principal concentrations of business, finance, retail, tourist and cultural activities for the entire nation are located.

It is thus crucial for the Draft KL City Plan 2020 to develop an urban structure that will meet the demands of this new urban economy. The plan will identify how this economic growth and expansion will be accomodated in Kuala Lumpur's spatial and development built form. The Plan must also acknowledge the role of Kuala Lumpur not only as the Capital City of Malaysia but also its function as an important national growth node gateway towards developing a global economy.

# Strengthening Kuala Lumpur's role as a World Class Business City

Strategic Direction 3.1

**Enhancing the Premier Business and Financial Zone and International Zone** 

Strategic Direction 3.2

Developing A World Class Retail Business Cluster

Strategic Direction 3.3

Promoting Commercial Areas In International Urban Tourism

### Accommodating Economic Growth Spatially

Strategic Direction 3.4 **Developing Key Urban Centres** 

Strategic Direction 3.5

Enhancing Small to Medium Business and Managing Supply for Commercial

Strategic Direction 3.6

Modernising and integrating the Informal Sector into the Formal Economy

Strategic Direction 3.7

Enhance and Modernising Existing Industrial Areas

### **Creating an Innovative City**

Strategic Direction 3.8

Up-scaling all Existing Economic Activities for Higher Growth

Strategic Direction 3.9

Strengthening KBE Elements in the Urban Economy

Strategic Direction 3.10

Encouraging ICT Clusters to Raise Productivity

# Strengthening Kuala Lumpur's role as a World Class Business City

Kuala Lumpur's role as a World Class Business City will be strengthened by making it an attractive and safe place to conduct business, work and live. It shall encourage the increased presence of international organization to enhance networking partnerships and to create economies of scale through ICT and skilled human resources.

### Strategic Direction 3.1 – Enhancing the Premier Business and Financial Zone and International Zone

Enhancing the premier business and financial zone and international zone to attract and sustain international investment.

The Draft KL City Plan 2020 has designated a premier business and financial zone in the City Centre, which is also identified as an international zone. This premier zone shall spearhead Kuala Lumpur into a global city and shall provide for quality housing, grade 'A' office buildings, international 5-star hotels and a quality built environment that is safe and attractive for international business and local communities to conduct business, work and live in Kuala Lumpur.

The vision for Kuala Lumpur to be A World-Class City is encapsulated in this premier zone which is also a competitive zone in the city. It gives the city an edge in attracting new investments and in expanding existing investments. The premier zone shall have two main areas, i.e., the financial precinct and the business precinct.

### **Financial Precinct**

The financial precinct will provide for corporate international and national headquarters, agencies and embassy functions which are becoming increasingly important because of globalization. The presence of international financial and business activities in the city provides great opportunities for international and domestic organisations including financial intermediaries and institutions to enhance their networking partnerships, to create economies of scale in adopting innovative information technology and infrastructure and to attract skilled human resources who are fundamental to the continuous success of business. The thrust for ward therefore demands that Kuala Lumpur invests in its physical capital and assets (housing, environment, physical infrastructure, community facilities, roads, transportation), and also in its human capital. By doing this, it further enhances itself as an attractive location for knowledge-intensive businesses.

This would also assist in fulfilling the objectives of Malaysia's Capital Master Plan (CMP) and the Financial Sector Master Plan (FSMP). One strategic move is to make Malaysia a major international Islamic Financial hub which will further enhance Kuala Lumpur's position internationally. The increasing liberalization of the financial industry, accompanied by the expansion of the range and types of financial services, would attract international banks, financial institutions and other trading and broking firms to set up their regional international headquarters in Kuala Lumpur. The designated financial precinct shall provide the enabling environment for these entities.

### KUALA LUMPUR PREMIER ZONE AND INTERNATIONAL ZONE

#### **Financial Precinct**

Jalan Sultan Ismail-Jalan Raja Chulan-Jalan Tun Perak –Jalan Raja Laut

#### **Business Precinct**

Jalan Ampang, Jalan Tun Razak, KLCC, Bukit Bintang leading to Jalan Imbi, Jalan Tunku Abdul Rahman, Chow Kit, Jalan Dang Wangi, Jalan Pudu, Jalan Petaling, Jalan Sultan, Jalan Hang Jebat, Jalan Davis.

### **International Zone**

- · City Centre Kuala Lumpur
- · Bangsar-Damansara
- Bukit Jalil
- Jalan Ampang-Jalan Tun Razak-Jalan U - Thant - Jalan Damai - Jalan Semarak and Titiwangsa

### **Characteristics and Activities**

- Corporate international and national headquarters
- International islamic financial and banking hub
- Insurance, stock broking, financial services, investment banking etc
- · International Business
- Embassy enclaves
- Wide range of hotels
- Cultural and entertainment facilities
- World class professional services
- Access to ICT hardware and software
- Creative clusters
- Education and training centres

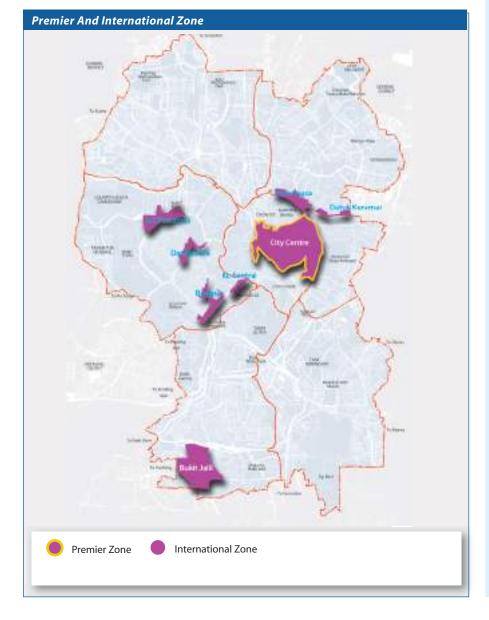
#### **Business Precinct**

The business precinct is the immediate area outside of the financial precinct within the area of Jalan Ampang-Jalan Tun Razak -KLCC-Bukit Bintang leading into Jalan Imbi: Jalan Tun Abdul Rahman-Chow Kit-Jalan Dang Wangi, Jalan Pudu-Jalan Petaling-Jalan Sultan -Jalan Hang Jebat. Here, all kinds of businesses are attracted by the spillover effects of Kuala Lumpur's dynamic economic activities in the financial precinct which will serve as the catalyst for the growth of other business and professional services.

It is here that the City will be able to focus on building up a creative class because creativity is the winning factor to increase a city's competitive edge. Creative and cultural activities are central to an advanced city's identity and a key sector in world cities. Creative and cultural activities and business tend to be attracted to international zones because of their ambience and cosmopolitanism.

#### **International Zone**

Kuala Lumpur acknowledges the expansion of its international community. Where traditionally, the international community in Kuala Lumpur is centred on and in areas where the



#### **CURRENT STATUS**

- Kuala Lumpur is currently the centre of the national financial industry.
- Over 400 financial institutions are here.
- Over 2,000 SME professional firms (legal, engineering, accounting, architectural, medical, and advertising) with 40,000 workers who are also here.
- These firms would need to be upgraded and strengthened financially and technologically to enable them to face the challenges in a rapidly internationalizing economy.

### **INITIATIVES**

The premier zone and international zone must demonstrate the intrinsic characteristics of quality living and working environment. Thus improvements and enhancement of the physical environment in these areas must be prioritised so that they serve as model developments in the City.

- Redevelop and regenerate dilapidated and blighted areas in these zones;
- Enhance city's safe environment, giving priority to enhancing personal and community safety;
- Provide excellent infrastructure and infostructure;
- Enhance interconnectivity within premier zone, and with other business zones in Kuala Lumpur.
- Encourage activities that would support the premier zone such as quality office space equipped with IT enabling environment, fine dining, quality hotels, shopping, quality housing including affordable housing, educational facilities, recreational and entertainment facilities.
- Provide quick access to competitive regulations through the newly proposed 'Kuala Lumpur Invest' housed in Kuala Lumpur City Hall;
- Develop a strong image that sustains the identity of "internationalism".

foreign embassies are located. The City has become more cosmopolitan, with the rise of multinational and transnational firms setting up offices and headquarters in Kuala Lumpur. Many of the members of its international community are workers and permanent residents attracted into the country by the abundance

of job opportunities, and by the offer of attractive terms under the 'Malaysia My Second Home' (MM2H) program.

Kuala Lumpur has designate four areas as International Zone and these areas include the City Centre, Bangsar-Damansara, Jalan AmpangJalan Tun Razak - Jalan U-Thant - Jalan Damai - Jalan Semarak, Titiwangsa and Bukit Jalil. These international enclaves do not merely serve as residential areas; they also function as key commercial areas, offering quality offices, entertainment, fine dining, shopping, and tourismbased activities such as medical and educational tourism.

### Strategic Direction 3.2 – Developing A World Class Retail Business Clusters

Developing a world class retail experience for Kuala Lumpur that is vibrant and internationally acclaimed.

In strengthening the role of Kuala Lumpur as a dynamic business city, the Draft KL City Plan 2020 identifies retail activities as one of its key commercial function. These retail activities will turn Kuala Lumpur into a world class shopping city.

### **Prime Shopping Precinct**

A prime shopping precinct centred at KLCC-Bukit Bintang area has been designated in Kuala Lumpur. The area will have international appeal and attracts visitors and domestic shoppers. Within it, there are prime shopping centres such as Starhill, Lot 10, Bukit Bintang Plaza, Sungai Wang Plaza, Berjaya Times Square and the Pavilion.

Supporting this prime shopping precinct are five and four-star hotels such as the Mandarin Oriental, Peninsula, JW Marriot, Park Royal, Westin, Impiana and the Regent; dining facilities offering a variety of cuisine, both local and international, and entertainment facilities that adds to the vibrant atmosphere of the shopping precinct.

### **Sub-Prime Shopping**

Away from the prime shopping, dining and entertainment zone of the City Centre, there are also the sub-prime shopping areas, some of which are in the City Centre like the traditional shopping areas of Jalan Tunku Abdul Rahman-Masjid India-

Chow Kit area, Jalan Petaling, and Jalan Pasar; and others are located mainly in the district centres in each strategic zone within Kuala Lumpur.

designation of sub-prime shopping, dining and entertainment areas across the City is also intended to provide an enabling environment that can support the role of district centres. The sub-prime shopping areas are newly created shopping precincts or existing centres in the various strategic zones. Currently, they may not have been able to perform their roles effectively, especially where there is proliferation of shop houses. The resultant effect is considerable vacant or empty commercial floor space in these places. Enhancing the sub prime areas is an important measure because it allows Kuala Lumpur City Hall to direct its resources to encourage redevelopment regeneration in these areas by attracting more investments into them.



Shopping Precinct	Initiatives
Prime Shopping Precinct KLCC-Bukit Bintang	<ul> <li>Continuous monitoring on improvements, enhancement, safety, and cleanliness.</li> <li>Permissible retail activities are: high-end retailing of apparels, small scale food and provision shops, dining and general dining - all of which are subject to excellent standards on hygiene and cleanliness; souvenirs, boutiques, beauty salons, and customer-centered services usually associated with retailing and entertainment.</li> <li>Not permissible are industrial activities, service industries that are generally polluting, wholesale, and warehousing.</li> <li>Introduce pedestrian malls to separate vehicular and pedestrian movements in order to intensify the shopping experiences in the premier shopping precinct.</li> <li>Provide access to public transportation network and enhance facilities for transit stops and pedestrian connectivity.</li> </ul>
Sub-Prime City Centre Jalan Tunku Abdul Rahman Masjd India-Chow Kit Jalan Petaling Jalan Pasar	<ul> <li>Regenerate and beautify Jalan Tunku Abdul Rahman- Masjid India-Chow Kit area for specialty shopping taking into consideration the uniqueness of the architectural characteristics of these areas.</li> <li>Embark on sustained maintenance program for Jalan Petaling area.</li> <li>Encourage regeneration and redevelopment in Jalan Pasar area.</li> </ul>
Sub-Prime Damansara Penchala  Brickfields- Bangsar-Mid Valley  Sri Hartamas	<ul> <li>Embark on sustained maintenance program for the Mid-Valley and Bangsar precinct that catered to high-end retailing and fine dining, attracting wealthy domestic shoppers and the international community.</li> <li>Embark on sustained maintenance program for the Sri-Hartamas-Mont Kiara precinct, allowing room for new development.</li> <li>Allow regeneration and redevelopment in the Brickfields area to target new retailers, and achieve a mix between traditional and modern retailing establishments.</li> </ul>
Sub-Prime Wangsa Maju-Maluri Datuk Keramat Jalan Ampang	<ul> <li>Pursue enhancement programs in Dato Keramat and Jalan Ampang sub-prime shopping precincts comprising the following:         <ol> <li>Improve existing parking layouts,</li> <li>Increase high connectivity,</li> <li>Undertake pocket landscaping with high permeability pedestrian walkways,</li> <li>Exclude industrial activities, polluting service industries, and warehouse.</li> </ol> </li> <li>Allow re-use/redevelopment of vacant shop houses (Taman Miharja &amp; Taman Maluri) into service apartments,</li> <li>Reposition Jalan Cochrane and Jalan Cheras in Dato Keramat for offices.</li> </ul>
Sub-Prime Sentul-Manjalara  • Sentul Raya  • Menjalara  • Bandar Sri Utara	<ul> <li>Embark on sustained maintenance program for Sentul Raya to ensure that its reputation and role as sub-prime shopping, dining and entertainment precinct is maintained.</li> <li>Pursue enhancement program for Bandar Menjalara and Bandar Sri Utara comprising the following:         <ol> <li>Improve existing parking layouts,</li> <li>Increase high connectivity,</li> <li>Increase high connectivity,</li> <li>Exclude industrial activities, polluting service industries, and warehouse.</li> </ol> </li> <li>Designate Upper part of Jalan Ipoh as a specialty retail and wholesale centre for building supplies and materials, and motor vehicle accessories. Allow redevelopment and regeneration to maximize potential land area here. Permit showrooms and warehousing to meet the needs of these industries.</li> </ul>
Sub-Prime Bukit Jalil-Seputeh Bukit Indah Bukit Jalil	<ul> <li>Embark on sustained maintenance program for Bukit Jalil and Bukit Indah to attract domestic shoppers and the international community. The sub-prime precincts are targeted to house shopping complexes, hypermarket, exhibition centre, and family entertainment theme park (indoor and outdoor).</li> <li>Encourage the development of specialty shopping areas in Bukit Indah for furniture and interior decor displays and showrooms</li> <li>Promote Bukit Jalil for ICT products and services.</li> </ul>
Sub-Prime Bandar Tun Razak-Sungei Besi Bandar Tun Razak Taman Segar	<ul> <li>Introduce an enhancement program for Bandar Tun Razak through:         <ul> <li>(i) Promoting the development of street mall/side walk retail outlets,</li> <li>(ii) Development of a specialty area for educational products, health –related goods like pharmaceutical and medical products, sports equipment and related services.</li> </ul> </li> <li>Embarking on sustained maintenance program in Taman Segar which has the Cheras Leisure Mall, hypermarkets like Giant and Tesco in order to strengthen its shopping function and attract domestic shoppers and diners.</li> </ul>

DRAFT KUALA LUMPUR CITY PLAN 2020

Towards a World Class City 3.5

### **KUALA LUMPUR RETAIL**

### **Shopping Centres**

- 75 numbers of shopping complexes (2005).

### **High End Shopping**

 Suria KLCC, Star Hill, Lot 10, Pavilion, Berjaya Times Square, Bukit Bintang and Sungai Wang Plaza, Mid Valley and Gardens.

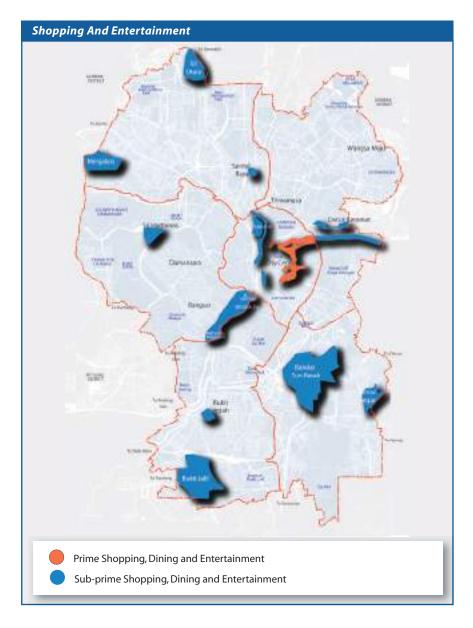
### **Traditional Shopping**

 Jalan Tunku Abdul Rahman, Jalan Masjid India, Jalan Pasar, Jalan Petaling, and Chow Kit.

### **Specialty Shopping**

Jalan Pasar (electronic components),
 Imbi Plaza (computer accessories),
 San Peng (wholesale garments),
 Jalan Inai, Jalan Delima, Jalan Utara
 Jalan Kemuning (Chocolate Boutiques) and Jalan Masjid India (sewing accessories).







### **KLSP 2020 POLICY**

**EC 3** KLCH shall develop and promote tourism as an important economic sector.

### Key Initiatives towards Enhancing Kuala Lumpur's International Urban Tourism

- Making Kuala Lumpur a tourist friendly city.
- Expanding MICE facilities.
- Promoting Kuala Lumpur as a regional centre for educational tourism.
- Enhancing urban heritage tourism
- Developing sports and recreational to strengthen local tourism.

International tourism is a major industry in Kuala Lumpur; it has a cluster of inter-linked economic activities and is one of the driving forces of the local economy.

In 2007, Kuala Lumpur receives 14 million tourist or 29% of total arrivals in the country. Tourism brings about an income multiplier effect of at least 0.66.



Enhancing Kuala Lumpur urban tourism and promote it as world class international tourist destination by focusing on its niche tourism products.

Kuala Lumpur will be a major international tourist destination offering a life-style destination in the international circuit. This shall be achieved by developing and sustaining a world class image for the city.

International tourism is a major industry in Kuala Lumpur; it has a cluster of economic activities and is one of the driving forces of the local economy. It is an integral part of the strong service sector in Kuala Lumpur. Tourism permeates the entire services sector, with linkages to wholesale and retail trade, restaurants and hotels (e.g. shopping, food, accommodations); transportation, and storage communications (e.g. car rentals, taxis, travel agencies, tour operators, airlines, cruise ships, bus transportation, etc); finance, insurance, real estate and business (e.g. banking, money changers, insurance, property

development), and community, social and personal services (e.g. tour guides, drivers, food operators, small service providers).

Kuala Lumpur will enhance and focus on its niche tourism products like Meetings, Incentives, Conferences and Exhibitions (MICE), medical tourism and educational tourism which has a great potential for growth on top of its already established diversified products such as shopping, culture, heritage, parks and garden tourism.

Increasing tourist arrivals have boosted investors' confidence in the City's hotel industry. The number of hotels in Kuala Lumpur has been rising since 2000 and by 2006, there are 196 hotels, offering a total of 28,668 rooms. The majority of available rooms are high-end quality accommodations in contrast to the available number of budget hotels, indicating a gap in the supply of rooms for backpackers and budget travellers. Thus, KLCH shall encourage the development of all classes of hotels to support the tourism industry.

### **Tourist Accommodations in Kuala Lumpur, 2006**

			Hotels	Rooms
		5-star	15	6,734
	High & Middle Class	4-star	15	6,443
		3-star	20	4,762
		2-star	15	1,455
Hotel	Budget	1-star	7	404
		Orchid	5	201
	Subtotal Rated		77	19,999
	Unrated Hotels	120	8,669	
	Total Hotels	196	28,668	
C	Rated Orchid	3	565	
Service Apartments	Unrated		12	2,510
Apartments	Subtotal	15	3,075	
Total Tourist Accomm		212	31,743	





#### **KLSP 2020 POLICY**

**EC 3** CHKL shall promote the development of MICE facilities and encourage the holding of International events in the City.

#### **Tourist Friendly City**

As a tourist friendly city, Kuala Lumpur shall undertake to improve the facilities and to upgrade the supporting amenities for tourists.

#### **Expanding MICE facilities**

MICE sector or business tourism is an important component of the tourism industry. MICE participants usually are high yield tourists, spending more on a daily basis than pure leisure tourists. Therefore there is a need for more large-scale, quality MICE facilities in Kuala Lumpur to accommodate large-scale international expositions, exhibitions, and conferences. These facilities, however, have to be located in areas that are easily accessible by mass public transportation as well as having complementary services such as dining, shopping, and accommodations.

The Draft KL City Plan 2020 proposes that a new MICE facility be developed at the Jalan Duta area where the present Government complex is located. The new MICE facility should be large enough to accommodate regional and international events with indoor exhibition halls. This would complement MATRADE, providing the mass required to support the construction of public mass transportation links.

Improvements and upgrading of existing sports facilities would also add to the supply of MICE facilities in Kuala Lumpur, namely:

- Bukit Jalil National Sport Complex; and
- Stadium Merdeka and Stadium Negara in the City Centre

### Developing medical tourism into a competitive sector for Kuala Lumpur

Areas surrounding key medical centres that contribute towards the growth of medical tourism are designated as medical tourism zones and are therefore encouraged to be enhanced with development that supports and promotes medical tourism in the City. These include the development of:

- Service apartments as alternative accommodations for foreign patients seeking treatment in the hospitals;
- Hostels and budget accommodations to meet both the needs of foreign patients, and workers in the medical centres;
- Urban facilities that provides for a clean, safe and comfortable built environment for the tourist; and
- Public transport facility and connectivity to surrounding areas.

#### **KLSP 2020 POLICY**

**EC 7** KLshall encourage, promote and facilitate the development of education and health as commercial services.

### INITIATIVES TOWARDS A FRIENDLY TOURIST CITY

- Creating strategically located visitors' centres in the City to provide information and guide tourists.
- Encourage private sectors to open souvenir outlets integrated with visitors centres and the introduction of user-friendly information kiosks should be encouraged.
- Encourage the location of in-bound travel agencies around visitors centres to assist foreign visitors, and to create greater synergy between the Tourist Information Office and the private tour agencies, leading to a centralization of inbound tour agencies for Kuala Lumpur in order to streamline and standardize costs.
- Upgrade all sign postings, and add critical tourist information on signboards as tourist guide notes eg. along Jalan Ipoh, Jalan Sentul, Jalan Kuching, MRR2, and Jalan Kepong.
- Enhance and beautify tourist areas in the City Center with street furniture and facilities that will create a safe and barrier free environment.
- Identify coach parking and dedicated car parking facilities in major tourist attraction areas.
- Create sufficient public transportation facilities such as buses and taxis.
- Create Kuala Lumpur's tourist police to assist tourists and to help keep the City safe. Tourist police posts can be stationed at popular tourist destinations such as Bukit Bintang, Jalan Sultan Ismail, Jalan Petaling, Pasar Seni and Jalan Alor in the City Centre.
- Sustain a maintenance program on all public toilets, including those in private buildings where pedestrian traffic is high, for example, shopping complexes, museum and galleries and all public/exhibit centres.
- Assume greater and more direct influence over the routes and quality of the hop-onhop-off bus services to ensure that they are effective and reasonably priced.
- Encourage the development of the backpackers' market by supporting the development of budget hotels, bed and breakfast facilities in the City Centre, Damansara Penchala and around the Brickfields area. Allow for regeneration and redevelopment of dilapidated areas in tourism zones for such developments.
- Allow the conversion of heritage and unutilized buildings in the city to be converted into boutique hotels and quality bed and breakfast accommodations, especially in the City Centre and in Damansara Penchala area.

#### **MEDICAL TOURISM ZONES**

- City Centre: areas around Tung Shin Hospital at Jalan Pudu, and HSC Medical Centre in Jalan Binjal.
- Wangsa Maju-Maluri: Areas around the National Heart Institute and the Kuala Lumpur General Hospital; Tawakal Hospital, Gleneagles-Intan Medical Center, and PUSRAWI,
- Damansara Penchala: area around Pantai Medical Center, and a wellness area centered in Brickfields to encourage the development of specialist services in 'Blind Massage and Ayurvedic Treatment.'
- Bandar Tun Razak-Sg. Besi: areas around Cheras Pantai Medical Center, and UKM Hospital.

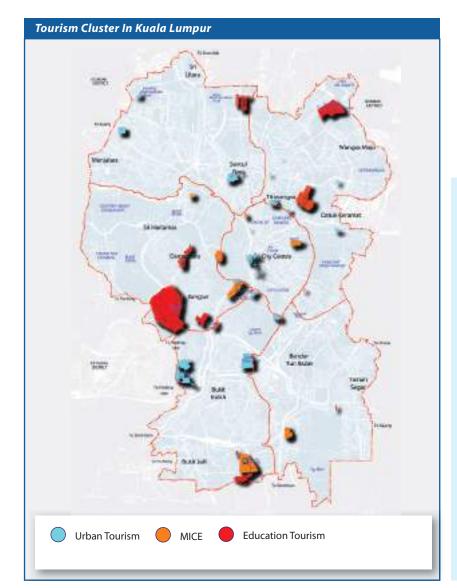
# Promoting Kuala Lumpur as a regional centre for educational tourism

Kuala Lumpur also serves as the centre of higher education in the country through the presence of key national universities in the city; the University of Malaya and University of Technology Malaysia in Jalan Gurney and private educational institutions that have also converged in Kuala Lumpur. Today, there are more than 100-odd private education institutions; found scattered all over the City. This is one of the fastest growing industries, with high productivity and high capital intensity, making it an ideal base to raise the k-elements in Kuala Lumpur's economy.

Draft KL City Plan 2020 thus designates educational zones in Kuala Lumpur. This would help Malaysia market itself as an attractive regional hub for educational tourism.

One measure to spearhead Kuala Lumpur as a regional tertiary educational hub is to make land available for the development and expansion of new educational facilities but this will be a challenge given that in the city, most of the land is privately owned, and are too expensive. Thus regeneration and redevelopment of old, unused or dilapidated buildings are encouraged for conversion into educational use, especially for post-secondary education and training in order to create 'city campuses.'

To further support the industry, a student friendly program should be initiated to cover the whole of Kuala Lumpur. This program requires collaborative efforts between Kuala Lumpur City Hall, educational institutions as well as the business community. Activities that support educational centres as well availability of public transportation should be encouraged and made available within educational zones



#### **EDUCATIONAL TOURISM ZONES**

- Damansara-Penchala: the areas around University College HELP, and University Malaya.
- Wangsa Maju-Maluri: the Wangsa Maju educational zone where University Tunku Abdul Rahman (UTAR) and Kolej TAR are located; and the Semarak Cluster where Universiti Teknologi Malaysia (UTM), Pusat Latihan Polis (PULAPOL), and Telekom College are located.
- Sentul-Menjalara: the area in Taman Koperasi Polis where the international Islamic College (ILLUC) and UNIKOP are located.
- Bukit Jalil-Seputeh: Taman Teknologi where ASTRO Broadcasting centre and the Asia Pacific Teknologi Institute are located, at Kuchai Entrepreneur Park along Jalan Klang Lama.
- Bandar Tun Razak-Sg Besi: around UCSI in Taman Connaught which is part of the Taman Segar district centre.

#### **Enhance Urban Heritage Tourism**

Kuala Lumpur is a cosmopolitan city with rich cultural heritage. There are many heritage and cultural landmarks in the city of Kuala Lumpur that draws upon its rich cultural heritage. In short, Kuala Lumpur has a varied range of urban cultural resources that include historical, industrial, social and artistic artefacts. These are manifested in the city's architecture, urban landscape, and landmarks. Complementing this, is the presence of a multi-ethnic and multi-cultural society which creates distinctive and unique city identity. Thus, Draft KL City Plan 2020 encourages urban heritage to be expanded as part of the City's urban tourism by:

 Enhancingurbanwalksalongheritage trails in Kuala Lumpur to strengthen their appeal to international tourists.



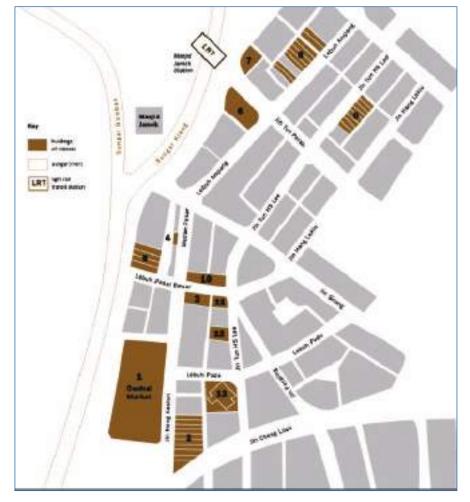
Urban walks are the concept of walkabouts which provide an alternative way for tourists to experience Kuala Lumpur at ground level. The array and mix of urban walks that combine heritage buildings, lifestyle, and parks allow international tourists the opportunities to appreciate the intrinsic attractions of Kuala Lumpur. There are numerous heritage trails in the City Center

located at Merdeka Square, Market Square, Chinatown and ending in Jalan Tunku Abdul Rahman.

 Encouraging improvements and adaptive reuse of heritage and cultural buildings to enhance their tourism appeal.

Heritage buildings can be reused for functional activities that are tourism-based. Adaptive reuse of

#### Heritage Trails 1



#### **Market Square**

- 1. Central Market
- 2. Jalan Hang Kasturi
- 3. OCBC Building
- 4. Market Square
- 5. Sin Seng Nam Restaurant
- 6. The Old Gian Singh Building
- 7. Bank Bumiputra Building
- 8. Lebuh Ampang
- 9. Jalan Tun H.S. Lee
- 10. The Old Federal Stores Building
- 11. MS Alley Company
- 12. Bank Simpanan Building
- 13. The Sze Ya Temple & Environs





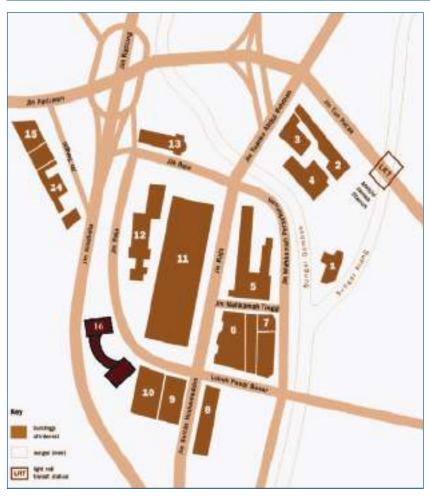
such buildings breathe life into them, allowing them to showcase their aesthetic features while continuing to serve their functional roles.

## Developing sport and recreational to strengthen local tourism

Among the visionary goals of Kuala Lumpur is to promote a healthy society in the City and the way to do this is to focus on its sports and recreational resources. The sports and recreational resources not only serve to

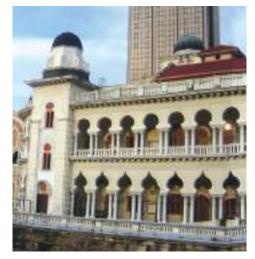
enhance the health of its residential population but they are also potential sources for domestic tourism, attracting visitors from other parts of the country to Kuala Lumpur.

#### **Heritage Trails 2**

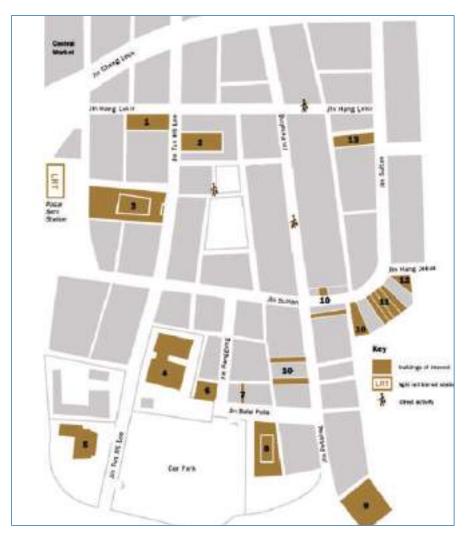


#### **Merdeka Square**

- 1. Jamek Mosque
- 2. The Old Survey Department
- 3. The Old City Hall
- 4. The Old High Court
- 5. Sultan Abdul Samad Building
- 6. The Old Post Office
- 7. Industrial Court
- 8. Textile Museum
- 9. Natural History Museum
- 10. Kuala Lumpur Memorial Library
- 11. Merdeka Square
- 12. Royal Selangor Club
- 13. Chathedral of St. Marry the Virgin
- 14. PAM Building
- 15. Ekran House
- 16. Kuala Lumpur City Library







#### Chinatown

- 1. Lee Rubber Building
- 2. Kwong Siew Association
- 3. Sri Maha Mariamman Temple
- 4. Old High Street Police Building
- 5. Old Victoria Institution
- 6. Old Post Office
- 7. Old China Cafe
- 8. Chee See Shu Yuen Association
- Chinese Ritual Object and Funerary Supplies
- 10. Colonial Hotel Building
- 11. Yan Keng Benevolent Dramatic Association
- 12. Selangor & Federal Territory Grocers Guild
- 13. Hawker Stall & Street









#### Accommodating Economic Growth Spatially

The economic growth of Kuala Lumpur has to be supported spatially where physical development provides for the enabling environment for economic activities to prosper and creates economies of scale that benefit the local community.

#### **Strategic Direction 3.4 – Developing Key Urban Centres**

Developing commercial areas in key urban centres for a balanced growth in Kuala Lumpur and an effective utilisation of local resources.

Draft KL City Plan 2020 has identified commercial areas in key urban centres that will support the economic growth of Kuala Lumpur. These key urban centres are in line with the call for the functional distribution of urban centres by the KLSP 2020. Thus, the plan has identified three categories of commercial areas, and they are:-

- City Centre Commercial;
- District Centre Commercial; and
- Neighbourhood Centre Commercial

The categories of commercial areas relate to the level of activities and intensity of development that the area supports to reflect its functions and to enhance economic growth in the area.

The City Centre Commercial (CCC) is a commercial zone which allows for the highest order of commercial activity i.e. a broad range of commercial acctivities. It also allows other

activities such as residential and institutions that are supportive of its major financial and business centres in the City. The city centre commercial area is a major employment zone, and allowing for higher intensity development will help maximise resources available in the inner city area. The encouragement of mixed uses/activities will also lead to an integrated form of development. Redevelopment and development of infill sites with uses that are more economic is encouraged. Developments must provide for high quality premises that would reflect Kuala Lumpur's role as the financial and capital city of the country. Two areas have been identified as the City Centre Commercial area:

- The Financial and Business District of the City Centre; and
- KL Sentral

### CATEGORIES OF COMMERCIAL ZONES IN KEY URBAN CENTRES

#### City Centre Commercial

Commercial areas that provide for the widest range of uses and most intensive in terms of commercial plot ratio and height to reflect its role as the centre for business, finance and employment in Kuala Lumpur.

#### • District Centre Commercial

Commercial areas within the identified boundary of District Centres. Certain activities that would normally be permissible within the City Centre Commercial areas can be alowed in this zone.

#### **Neighbourhood Centre Commercial**

Commercial areas within residential neighbourhood that serve local community with small scale retail and services that are needed on a daily basis.

The District Centre Commercial (DCC) is a commercial zone within the identified boundaries of District Centres. These areas have the opportunity to be developed as sub-prime commercial zones and become secondary employment centres in Kuala Lumpur. The DCCs are located city-wide and service the population catchment of the district centres. They also allow for flexibility of activities and uses that will attract investments into the urban centres, thus creating a more balanced growth in Kuala Lumpur. Fourteen DCCs have been created in the Draft KL City Plan 2020 and they are:-

- Bangsar
- Sri Hartamas
- Bukit Indah
- Damansara
- Bandar Manjalara
- Maluri
- Wangsa Maju
- Bandar Tun Razak
- Kepong
- Sri Utama
- Sentul Raya
- Bukit Jalil
- Datuk Keramat
- Taman Segar



A Neighbourhood Centre Commercial (NCC) is the commercial area within a residential neighbourhood that provides small scale retail and services. Here the intensity of development and type of activities permissible have to be consistent with the character of their respective neighbourhood areas in order to ensure that the activities allowed in such zones have no adverse impact onto the local community. These include the prohibition of activities such as entertainment and industries. Eighteen (18) zones have been classified as NCC and they are :-

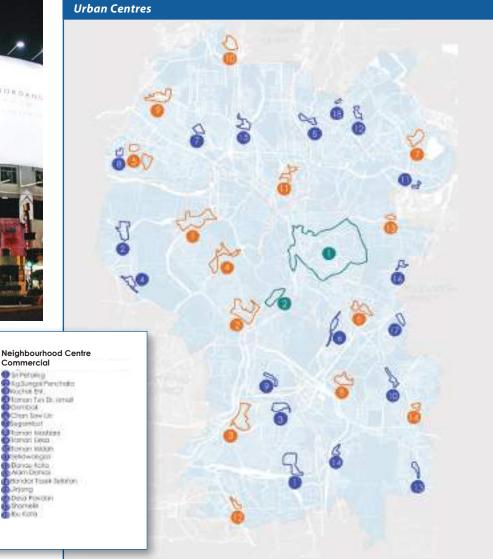
- Sri Petaling
- Kg Sungai Penchala
- Kuchai Enterprise
- Taman Tun Dr Ismail
- Gombak
- Chan Sow Lin
- Segambut

- Taman Mastiara
- Taman Desa
- Taman Midah
- Setiawangsa
- Danau Kota
- Alam Damai
- Bandar Tasek Selatan
- Jinjang
- Desa Pandan
- Shamelin
- Taman Ibu Kota



District Centre Commercial

Schorones Wat Tiretak Damenued : Bondor Venjalara Meluf Wargeavialu



Commercial

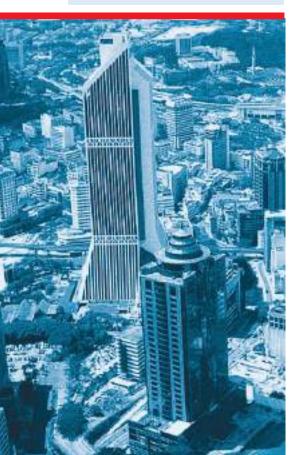
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#### Activities And Uses Allowed In Urban Centres Commercial Zones

City Centre Commercial Zone	District Centre Commercial Zone	Neighbourhood Centre Commercial Zone
Multi Dwelling Units	Multi Dwelling Units	General Retail and Office
Hostels	Residential Care Homes	Food and Drinks
Residential Care Homes	General Retail and Office	Civic Use
General Retail and office	Food and Drinks	Integrated Community and Cultural
Food and Drinks	Service Apartment	Complex
Entertaiment	Home Based Business	Emergency and Security
Leisure	Technology, Research	
Hotels	and Development	
Service Apartment	Civic Use	
Home Base Business	Integrated Community and Cultural	
Technology, Research	Complex	
and Development	Emergency and Security	
Civic Use		
Integrated Community and Cultural		
Complex		
Emergency and Security		

Note: The above uses and activities are permitted out rightly within the identified zones. There are several uses which are allowed with conditions in these zones – refer to the KL Development Control Plan 2008.

When these three commercial areas in the various hierarchies of urban centres are in place, the distribution of commercial activities in Kuala Lumpur will be well balanced and would support the objective of sustainable land use development and economic growth in Kuala Lumpur.



### Strategic Direction 3.5 – Enhancing Commercial Floor

Enhancing small to medium businesses and manage the demand for commercial floor area to ensure of the City's sustainable growth in commerce activities.

The continuous demand for commercial floor space in Kuala Lumpur must be managed to ensure sustainable growth in commerce activities. The commercial floor area relates to the whole range of commercial use and activities. The distribution of these commercial spaces shall support the various economic activities, accomodating their anticipated growth and expansion.

Thus, it important to acknowledge the business clusters available in Kuala Lumpur allow for their expansion spatially. This is especially so for the small to medium sized business as they form an important element of Kuala Lumpur's economy. These businesses are often involved in new creative ideas and services that could enrich the City's business environment.

Attractive and affordable premises for these businesses are important and they can be located in the City Centre or in the identified District Centres. Clustering of these businesses helps to assist local businesses to expand. However, ready access to markets and clientele is also important, hence importance must be given to the need for integration of land use planning with transportation.

The commercial floor space generated by the Draft KL City Plan 2020 relates capacity to the proposed initiatives that support the city's medium to long term growth. This huge amount of commercial floor space must be managed to ensure of the city's sustainability. Monitoring mechanism must be put into place by CHKL to ensure there is no oversupply or shortage of spaces.

#### **COMMERCIAL FLOOR SPACE, 2020**

The economic growth of Kuala Lumpur shall be reflected mainly in the commercial floor space made available by the Draft KL City Plan 2020. The plan has identified a total floor area of 65 million sq.m. The KLSP 2020 projects commercial floor area requirement of 41 million sq.m. by the year 2020.

Strategic Zone	Commercial GFA		KL Commercial GFA (sq.m.) 2005		
	KLSP 2020	Draft KL City Plan 2020	Existing GFA	Committed GFA	Total Supply
City Centre	14,383,332	30,665,958	10,775,960	4,767,212	15,543,172
Wangsa Maju Maluri	4,612,246	7,656,906	3,162,130	2,518,142	5,680,272
Sentul Menjalara	5,068,064	8,412,345	2,989,734	2,721,891	5,711,625
Damansara Penchala	6,039,804	9,733,124	3,898,419	2,067,896	5,966,315
Bukit Jalil Seputeh	7,361,979	4,064,180	2,729,771	1,419,585	4,149,356
Bandar Tun Razak Sg Besi	3,810,083	5,267,693	2,457,169	1,421,282	3,878,451
KUALA LUMPUR	41,275,508	65,800,206	26,013,183	14,916,008	40,929,191

Source: Draft Kuala Lumpur City Plan 2020

#### Strategic Direction 3.6 – Modernising and Integrating the Informal Sector

Modernizing and integrating the informal sector into Kuala Lumpur's formal sector will provide better income, earning opportunities and stable employment for low income earners.

The informal sector plays an important role in the local economy. It is an important source of income for the urban low-income group who tend to hold down at least two jobs to live in Kuala Lumpur where the costs of living tend to be higher than other cities in the country. Hawking and petty trading tend to form a large proportion of the secondary jobs among the urban poor. The additional income enables them to continue to reside in the City, especially within the City Centre area.

A large proportion of the informal sector in Kuala Lumpur is engaged in the preparation and sale of food. In fact, restaurants or food catering is one of the most dynamic sub-components of tourism. The proliferation of

hawkers, especially food hawkers, has raised some concerns over the traffic congestion. The KLSP 2020 has identified the concern over the shortage of purpose built hawker's facilities, and the inappropriate location of existing hawkers' centres. Hawkers in Kuala Lumpur are highly dependent on pedestrian and motor vehicle movements to ply their trade effectively.

Modernizing and integrating the informal sector into the formal sector is a move not only towards eradication of urban poverty, but it legitimizes petty trading as a stable source of employment and income-earning source. Modernizing and integrating them entails stepping

up on licensing and provision of appropriate hawkers' facilities. As many of them are engaged in food preparation where hygiene and cleanliness are deemed critical, licensing enables the authorities to monitor their impact on the urban environment as well as to check on possible disease outbreaks.

The provision of appropriate hawkers' facilities is crucial for keeping the city clean and to sustain cleanliness of the environment at all times. These hawkers' facilities must be integrated and placed near to the formal sector business premises. Placing them in shop lots, integrating them in complexes that are properly designed for hawking activities can further enhance this informal sector.

#### Strategic Direction 3.7 – Enhancing Industrial Area

Enchancing and modernising exisiting industrial areas to meet the dynamic growth of Kuala Lumpur

The industrial sector remains relevant in the Kuala Lumpur economy as a source of employment, wealth creation, and in establishing the knowledge-based economy for Kuala Lumpur. With an estimated 1,424 establishments (Annual Survey of Manufacturing Industries, 2004), it is a relatively small contributor to employment (providing only 5.8% of the total employment in 2005 - 42,000 jobs). This matured economic sector in Kuala Lumpur been declining over time and trends indicate a shift towards the new economy where there is increasing use of higher technology and R&D in this sector.

In meeting the needs of economic growth and rapid urbanization in Kuala Lumpur, the industrial sector have to be modernized and existing industrial areas will need to be enhanced; with SME's transforming themselves into the knowledge-

base economic activities. This is in line with Malaysia's effort to enhance its global position as a trading nation as well as its effort to raise competitiveness in its manufacturing sector.

The Draft KL City Plan 2020 has designated 398.9 ha (2020) of land for industrial activities (466.67 ha - 2005), with some industrial land converted into other uses that attracts innovation-based activities, such as:-

- Mixed Use Commercial and Industry
- Mixed Use Commercial
- Technology Park

Kuala Lumpur's initiatives will attract investment and new integrated industrial parks which promote greater integration between manufacturing and services.



#### **INITIATIVES**

#### Modernize and Upgrade Existing Industrial Areas for SME

 Modernise and upgrade existing industrial areas to enhance SME transition to k-based economy.

#### Promote Integrated Parks for Manufacturing and Services

• Promote integrated parks for manufacturing and services to encourage greater integration of the two key urban economic activities.



ATILYA PUTRI BINTI AHMAD HILMY

SK Wangsa Melawati 3rd Prize Winner ( Category A ) of Drawing Competition in Conjunction with Preparation of Draft KL City Plan 2020.



#### **Creating an Innovative City**

The dynamic growth of the city and its economy requires Kuala Lumpur to be creative and progressive so as to continuously develop and innovate to meet new challenges and demands of a growing City.

### Strategic Direction 3.8 – Encouraging Up-scaling of Existing Economies

Encouraging up-scaling of existing economies enables Kuala Lumpur to be a competitive global centre for trade and industries.

### MIXED USE COMMERCIAL INDUSTRY (MXCI)

An area intended to promote combination of commercial and industry on the same site. Such activities also relate to large big-box facilities, such as showroom and some amount of services



The strategy to upscale Kuala Lumpur into A World Class City that is capable of competing at both global and regional level for international businesses and investment is a maior, long-term exercise demands considerable commitment from both the public and private sectors. It is an all-encompassing move that requires investment in both structural and non-structural changes. The physical environment has to be enhanced. Connectivity within the city and between the city and other parts of the country has to be upgraded to a level that is comparable to any world class city. Inter-urban connectivity will provide the basis upon which all kinds of businesses thrive, enabling them to expand, to continue to grow, as well as to sustain their growth within Kuala Lumpur. In addition, human resources in the city have to be upgraded, their skills enhanced in order to continue to attract international investments. These, in turn, are expected to create more jobs and wealth in the city, further attracting more skilled workers into Kuala Lumpur to work and live.

Workers will find the living environment conducive for them and for their families if the city invests heavily in its social infrastructure such as housing, recreation, cultural,

dining and shopping facilities to meet international standards. Concerns for the environment, for sustainability in using the city's resources, and for the poor as well as for the rich groups are encapsulated into this strategy to make Kuala Lumpur into a major international investment and job centre that is globally competitive.

One key activity that has been progressively changing is industry. Much of these activities in Kuala Lumpur have up-scaled themselves and some have moved away from the city because of their inability to adapt to a rapidly growing city like Kuala Lumpur. The redevelopment and upgrading of existing industrial estates into Integrated Business Parks for mixed use activities such industrial, support services, commercial, training facilities and housing must be encouraged to cater to this need.

In encouraging these industries to upscale, business premises for them are encouraged to be located in a built environment that is clean and modern. Draft KL City Plan 2020 thus allows for such activities to integrate with office activities resulting in a Mixed Commercial Industrial (MCI) land use zone. A total of 138 acres of land (2020) has been identified for the zoning of MXCI which is spread city wide in Kuala Lumpur.



**Strategic Direction 3.9– Knowledge Based Economy (KBE)**Strengthening the KBE elements of the local economy through clustering and modernising.

A cluster-based strategy is currently a key strategy adopted by many cities in many parts of the world to build up its competitiveness in the k-economy and to revitalize its urban economic base. Clusters are groups of inter-related industries, specialized suppliers, service providers, firms in related industries, and associated institutions which may compete and co-operate with one another. Firms in a cluster are linked, and groups of inter-linked companies tend to locate in close proximity to one other. Co-location encourages the formation of contacts between firms and can enhance the value creating

benefits arising from networks. The geographic area covered by clusters can vary dramatically, and can exist in multiple operating scales, from local to regional, national and even international level.

Clustering draws upon economic diversity in the city which is seen in terms of a mix of inter-related services and manufacturing functions that give a city its relative strengths and skilled workforce upon which knowledge-based economies depend on.



### Kuala Lumpur Cybercity And Cybercentre

KLCC			
KLCC Tower Office Area	4.1 million sq. feet. 88 storey Petronas Twin Tower, 864 seats Philharmonic Concert, Retail mall and 50 acre park		
No of MSC Companies	94 (as of Oct 2006)		
Feature	Central Communication Office, District Cooling and Smart Office Applications and Design		
	KL Tower		
Height	421 metres		
MSC Targets	Premier Telecommunication and Broadcasting Centre		
User	Telekom Malaysia		

### Technology Park Malaysia 92.7 hectare

RTM

Area

No of MSC	136
Companies	(as of Oct 2006)
Office Area	1.51 million sq.ft.
MSC Targets	Techno entrepreneur – Information Tech Telecommunication and Electronics Engineering Bio-technology Commercial

	KL Sentral
Area	29 hectare
Office Area	1.63 million sq.ft.
No of MSC	11
Companies	(as of Oct 2006)
MSC Targets	Blue-chip investors and start- up companies, SME Enterprises Nurture creative content industry.

#### **Strategic Direction 3.10 – Encouraging ICT Clusters**

Encouraging the development of ICT clusters to attract KBE investments and encouraging participation of local industry players

The development of ICT clusters in Kuala Lumpur will attract global players and encourage the participation of the local industry in potential markets. Kuala Lumpur will need to enhance its support services and facilities to successfully attract KBE investments.

### Promote and Developing Cybercities and Cybercentres

Kuala Lumpur plays a pivotal and leading role in the Multi Media Super Corridor (MSC) in Malaysia. The MSC, established in 1996, sets the platform to build cybercities and cybercentres which would support a competitive

clusters of local ICT companies and thus a sustainable ICT industry in order to achieve a complete MSC-enabled environment by the year 2020.

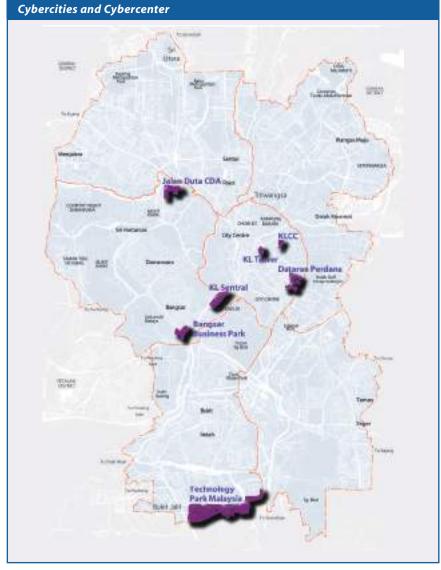
Kuala Lumpur now has 3 designated cybercity and 1 number cybercentre. These areas provide the enabling environment that supports the establishment of ICT clusters. Draft KL City Plan 2020 encourages more areas/buildings to be designated as Cybercity or Cybercentres. Such initiatives should be identified

at the early stage of planning to ensure that its implementation meets the requirements set by MDeC (Multimedia Development Corporation).

Areas identified as potential MSC cybercity or cybercentre, are:-

- Bangsar Business Park (Exisiting Lever Brothers site)
- Comprehensive Development Area of Jalan Duta (Matrade area)
- Jalan Davis development (also known as Dataran Perdana)







Aspiring to be A World Class City requires new integrated approach to development and land use management of the City. Attaining sustainability must become priority and focus for future development and growth of Kuala Lumpur.



The pattern of land uses; their location, mix, and intensity; is a critical component of the city's character and structure. It is intended to provide sufficient land for residential, commercial, industrial, and public uses; to locate these various uses appropriately in order to enhance community balance and character; to preserve and protect important natural resources; and to enable the city to provide adequate public services to city dwellers and visitors.

Today's land use pattern has been the result of past practices and development trends which were based on single land use zoning. Changes in market forces, trends and focus coupled with environmental and climatic concerns requires new and integrated approach to land use planning and spatial developments and the need to look at a more flexible approach to land use zoning and the manner in which developments



#### Meeting Future Land Requirements for Sustainable Growth

Strategic Direction 4.1

Allocating Land for Future Requirements

Strategic Direction 4.2

Facilitating Use of Land and Buildings

Strategic Direction 4.3
Regulating Incompatible Land Use and
Activities

### **Integrating Transport and Spatial Development**

Strategic Direction 4.4
Integrating Land Use and Transport

Strategic Direction 4.5

Designating Transit Planning Zones

Strategic Direction 4.6

Encouraging Mixed Use Developments

#### Transforming and Regenerating Brownfield Sites and Urban Villages

Strategic Direction 4.7

Promoting Redevelopments of

Promoting Redevelopments of Brownfield Sites and Development on Infill Sites

Strategic Direction 4.8

Enhancing and Facilitating Development of Urban Villages

### Meeting Future Land Requirements for Sustainable Growth

Land scarcity in the city requires coordinated effort in ensuring and supporting growth of the city in order to be able to cater for requirements of its population, maintain its competitiveness as the nation's capital city, and enhance its sustainability and environmental health.

### Strategic Direction 4.1 – Allocating Land for Future Requirements

Allocating land for future requirements in support of population growth for the year 2020 and to facilitate economic and social developments of the City.

Land use distribution and allocation in the Draft KL City Plan 2020 reflects the physical directions of development for the city towards meeting its vision and achieving various objectives set out in particular by the Kuala Lumpur Structure Plan 2020; which provides the basis and framework for structure and future growth of Kuala Lumpur until year 2020. This is reflected and translated into the development control plans through the Land Use Zoning Map and Development Intensity Maps (i.e. the Density and Plot Ratio Maps) which takes account of:-

- The need to allocate land for future development based on projected land requirements by the Kuala Lumpur Structure Plan 2020;
- The strategic economic directions towards achieving economic objectives for World Class Business Environment and responding to future economic challenges and trends;

- The aspirations to create a world class liveable city with high quality residential environment and improved quality of life for city dwellers;
- The requirements to provide comprehensive public amenities within a high quality urban environment;
- The need to improve and alleviate the effect of traffic congestion in the city by influencing land use and travelling pattern;
- The need to protect and enhance natural environment and public open spaces; and
- Vision for sustainable development by balancing the economic, social and environmental goals.







#### **FUTURE LAND REQUIREMENTS, 2020**

Population of the city is expected to grow to 2.2 million people and employment is projected to increase to 1.4 million by the year 2020. As such, land to cater for housing needs, employment places and for social amenities including community and recreational facilities need to be allocated and balanced accordingly. This has been one of the factors taken into consideration in land use distribution by the Draft KL City Plan 2020.

#### Projected Future Land Requirements by 2020

	Total Future Land	Breakdown of Future Land Requirement by Land Use Type (Hectares)			
Strategic Zone	Requirement, 2020 (Hectares)	Housing	Commercial	Industry	Community Facilities
City Centre	373.36	212.77	14.81	25.73	120.05
Damansara Penchala	789.73	457.70	133.7	25.84	172.49
Sentul Menjalara	338.43	208.77	43.27	-21.75	108.14
Wangsa Maju Maluri	468.75	358.57	6.44	19.79	83.94
Bandar Tun Razak Sg Besi	397.63	184.39	62.54	18.38	132.31
Bukit Jalil Seputeh	553.22	213.40	75.64	34.49	229.69
KUALA LUMPUR	2921.12	1635.60	336.4	102.48	846.62

Source: Kuala Lumpur Structure Plan, 2020

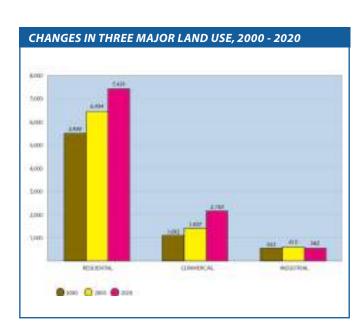
#### Land Use Zones, 2020

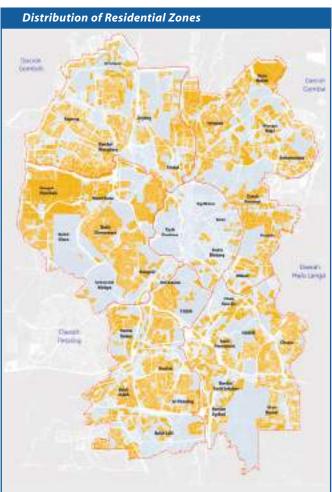
Land Use Zone 2020	Area (Hectares)	%
RESIDENTIAL		
Residential 1	2,033.41	
Residential 2	1,869.44	
Residential 3	1,826.84	
Established Housing	1,490.49	
Public Housing	203.66 <b>7,423.84</b>	30.65
COMMERCIAL	7,423.64	30.65
City Centre Commercial	351.16	
District Centre Commercial	264.25	
Neighbourhood Centre Commercial	154.59	
<del>-</del>		
Commercial	781.42	
Mixed Use Commercial  Mixed Use Residential	275.41 184.37	
Mixed Use Commercial & Industrial	137.59	
	2,148.79	8.87
INDUSTRIAL		
Technology Park	162.85	
Industrial	399.16	
	562.01	2.32
INSTITUTIONAL Public Institutional	1001.00	
Private Institutional	1881.22 129.73	
Trivare institutional	2010.95	8.30
OPEN SPACE	2010.70	0.00
Public Open Space	1,882.08	
Private Open Space	563.30	
	2,445.38	10.09
COMMUNITY FACILITIES		
Education	777.19	
Religions	169.19	
Community Centre	151.76	
Security & Emergency	77.67	
Cemetery	332.10	
Health	207.38	
Welfare Home	20.96	7 17
FOREST RESERVE	1,736.25	7.17
TOREST RESERVE	60.93	0.25
INFRASTRUCTURE	00.70	0.23
Road /Rail Reserve	6548.84	
River Reserve	740.23	
Terminal	49.99	
	6,961.62	28.74
UTILITIES		
Electricity	222.72	
Water	68.44	
Telecommunications	9.88	
Sewerage	182.25	
Solid Waste	7.66	
Gas	2.89	
	493.84	2.04
TOTAL	24,221.05	100.00

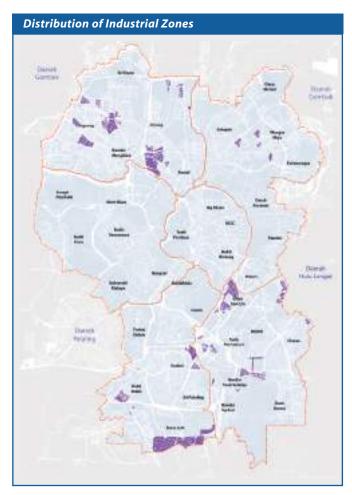


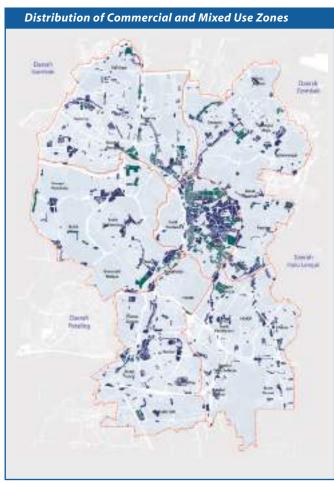
By the year 2020, area zoned for residential accounts for 30% of total Kuala Lumpur which reflects 15% increase from the year 2005. This is to provide for additional land requirements for 2020 projected population in the city. Commercial zone accounts for close to 10% of the total Kuala Lumpur area; doubles the area in 2005. Industrial zone is expected to reduce by 8% compared to year 2005 significantly due to the Draft KL City Plan 2020's initiatives to requlate indiscriminate locations of industrial activities into designated industrial zones.

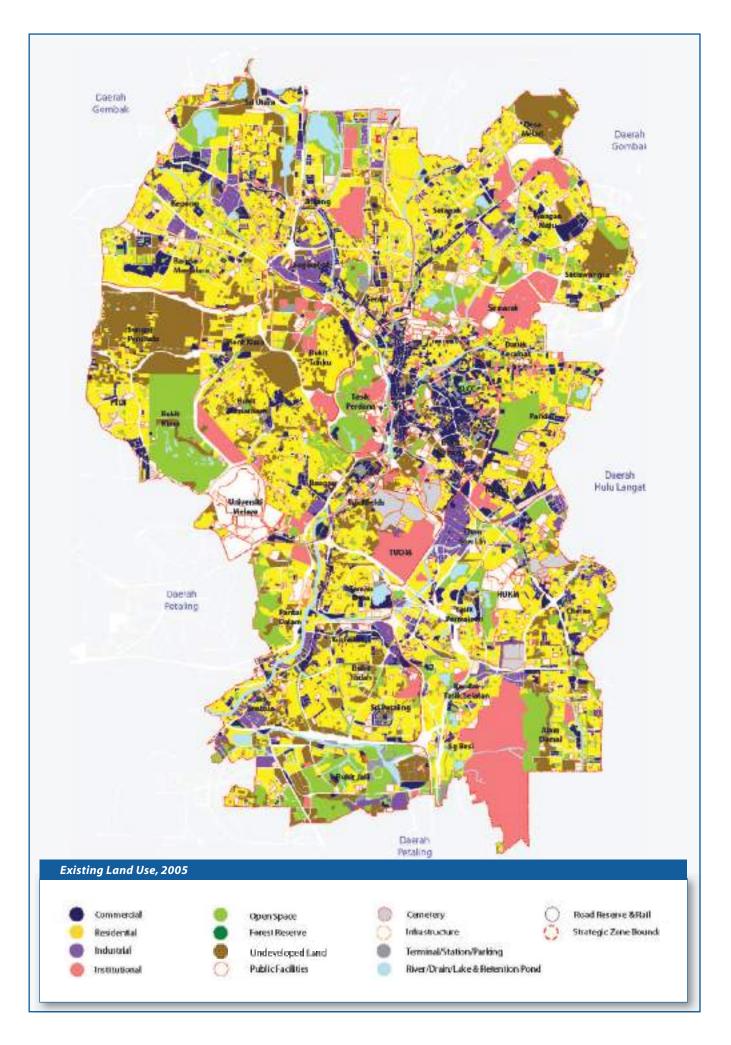


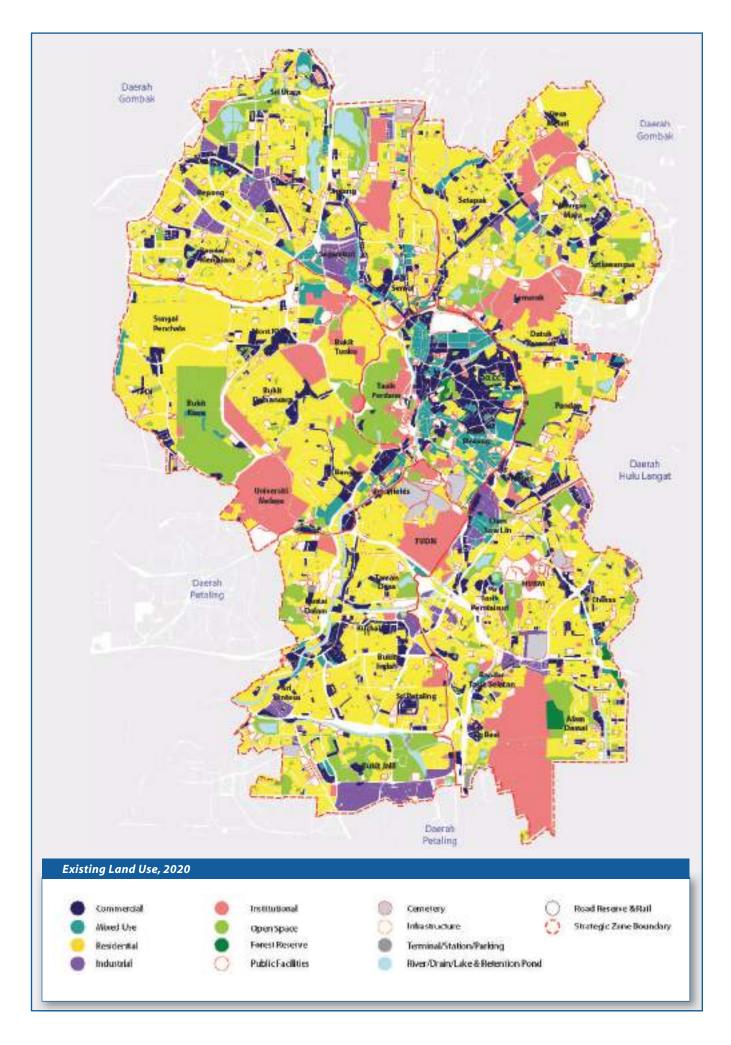












## Strategic Direction 4.2 – Facilitating Use of Land and Buildings

Facilitating use of land through a more inclusive and flexible zoning system to allow for various types of use within a land or building.

Current development trend and activities requires new approach to development control that are flexible and response positively to changes in market forces. For Kuala Lumpur to be a globally recognized city that is attractive for investments and businesses, and to be able to facilitate its cluster economic strategy, KLCH recognizes that changes to the currents

development control system needs to be implemented. With the formulation of the Draft KL City Plan 2020, new form of zoning system is to be adopted through introduction of a zoning district system, new land use zoning classification and a series zoning schedules which will replace the current Comprehensive Development Plans (CDPs) and



Use Classes Rule. The new zoning system as prescribed in the Kuala Lumpur Development Control Plan 2008 (KLDCP 2008) will allow for a more flexible and more inclusive use of land and buildings within a respective zone.

#### **LAND USE ZONES FOR DRAFT KL CITY PLAN 2020**

The Draft KL City Plan 2020 designates 20 classifications for land use zones in the Land Use Zoning Map (referred as DCP1-Land Use Zoning Map in the KLDCP 2008, see Volume 2 Part 1-KLDCP 2008). This includes introduction of mixed use zones (Mixed Use Commercial, Mixed Use Residential and Mixed Use Commercial and Industry), new classification for commercial zones to reflect function and hierarchy of commercial centres, and further breakdown of residential zones to reflect intensity of residential developments.

Primary Land Use	Land Use Zones
Commercial	City Centre Commercial District Centre Commercial Neighbourhood Centre Commercial Commercial
Mixed Use	Mixed Use Commercial Mixed Use Residential Mixed Use Commercial and Industry
Residential	Residential 1 Residential 2 Residential 3 Established Housing Area Public Housing
Industrial and Special Industries	Industrial Technology Park
Institution	Public Institution Private Institution
Open Space	Public Open Space Private Open Space
Special Use	Forest Reserve White Zone (Community Facilities and Infrastructure and Utilities)

### EFFECT OF DRAFT KL CITY PLAN 2020 AND KLDCP 2008

The existing statutory plans used in Development Control are the CDP 1039 (Central Commercial Area), CDP 1040 (Density Zoning) and CDP 1041 (Land use Zoning). The CDP 1039 is applicable to the Central Commercial Area within the City Centre. The CDP1040 is a density zoning plan covering 36sq miles of the city and is applied to residential development. The CDP 1041 is a land use zoning plan which identifies 10 categories of land use in the City.

The Draft KL City Plan 2020 and its supporting KLDCP 2008 will supersede the three CDPs and all other gazette plan currently being enforced.



### Strategic Direction 4.3 – Regulating Incompatible Land Use and Activities

Managing incompatible land use and activities to achieve a healthy and clean built environment.

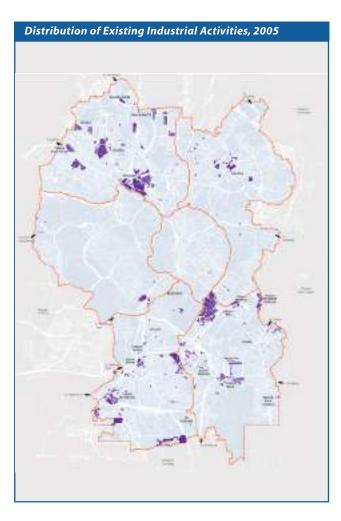
One of the major concerns for Kuala Lumpur is the presence of large numbers of incompatible activities and industries, which are located in unplanned industrial sites, residential areas, commercial precincts and on government land. These have created nuisance to adjoining land and residential neighbourhoods, unsightly physical environment of commercial centres and uncontrolled discharges effluents into drainage system.

In order to realize the city's vision for a world class urban environment, it is important therefore that the activities be regulated properly. Measures within the Draft KL City Plan 2020 includes designating industrial parks where clean, modern and compatible industrial activities are encouraged, and regulating industrial and other polluting activities through Zoning Schedules and licensing. Activities such as motor cycle repair and services, recycling operations, noise generating activities such as entertainment will be controlled and be permitted in certain zones.

Activities Most Likely Will Create Nuisance or Generate Pollution	Permitted Zone	Permitted with Conditions
Entertainment	City Centre Commercial	District Centre Commercial
Recycling Operations	-	Mixed Use Commercial and Industry Industrial Park
Motorcycle and Motorcar Repairs, Service Centres and Workshops	Industrial Park	Commercial Mixed Use Commercial and Industry
Manufacturing Industry	Industrial Park	Mixed Use Commercial and Industry
Printing and Publication	Industrial Park	Mixed Use Commercial and Industry
Service Industry	Industrial Park	Mixed Use Commercial and Industry



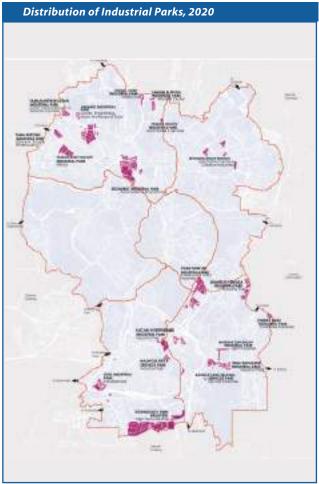
CHAN KAR LEONG SMK Danau Kota 2nd Winner ( Category B ) of Drawing Competition in Conjunction with Preparation of Draft KL City Plan 2020.











## Integrating Transport and Spatial Development

A close co-ordination of transport and land use development will support Kuala Lumpur's development and growth and help to ensure that people of Kuala Lumpur have convenient access to the services and facilities they need. Good public transport access will not in itself guarantee development but is a necessary condition for sustainable development.

### Strategic Direction 4.4 – Integrating Land Use and Transport

Integrating planning and development of public transport with land use framework and encourage land use that supports public transport investments.

Effective land use and transport planning aims to minimise the need to travel or influence the mode of travel through coordinated arrangement or siting of locations of traffic generating uses in relation to existing transport, particularly the more sustainable transport mode and influencing proposals for new route in line with land use planning.

It is the objective of the Draft KL City 2020 to provide for a comprehensive integrated transportation system that caters for the needs of inter and intra city travel and to integrate land use development with public transportation and road network. To achieve the above, the Draft KL City Plan 2020 seeks to facilitate and ensure that future proposals for expansion of the rail network in Kuala Lumpur be integrated with land use and physical development of the city. Plan for future expansion of Kuala Lumpur's rail network will be encouraged to serve new areas not yet served by rail network and to link district centres, employment areas, density residential neighbourhoods, comprehensive development areas, and large redevelopment of brownfield sites in the city centre.

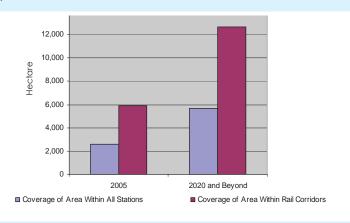
In achieving this objective, KLCH shall seek to establish close coordination

with public transport implementing agencies in ensuring that public transport investment is directed and supports sustainable land use objective of the Draft KL City Plan 2020.



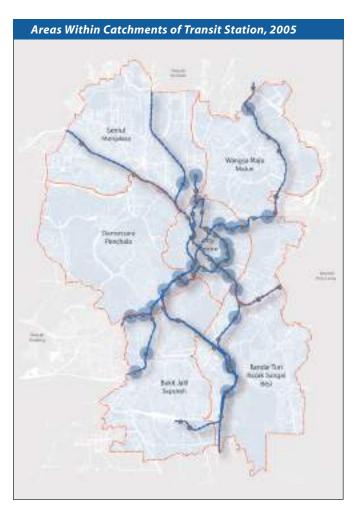
### LONG TERM TARGET FOR KUALA LUMPUR'S INTEGRATED LAND USE AND TRANSPORT DEVELOPMENT

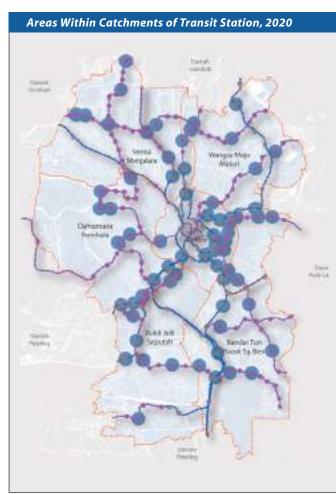
To increase coverage of areas within catchments of transit stations from 10% to 24%.

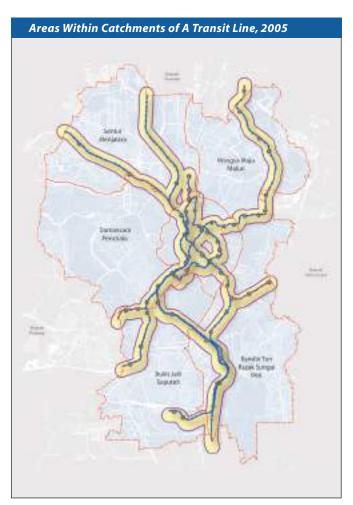


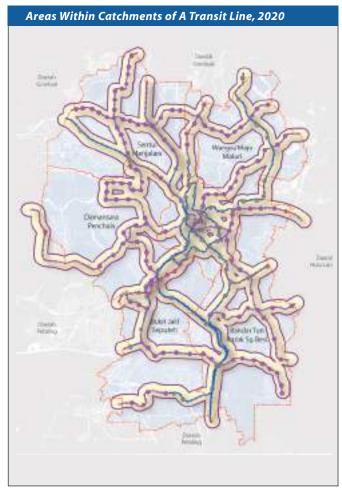
Currently, only approximately 10% of Kuala Lumpur is directly served or within the transit catchments of existing transit stations (i.e. within radius of 400 meters around transit station) and 24% of Kuala Lumpur are within transit corridor (i.e. areas within 400m on each side of rail line). The proposed rail extension shall increase the total coverage of areas within transit corridor to be 52% (12,646 hectares) of Kuala Lumpur. Areas identified to be within the transit catchments are areas that are located within 400m radiuses of transit stations.

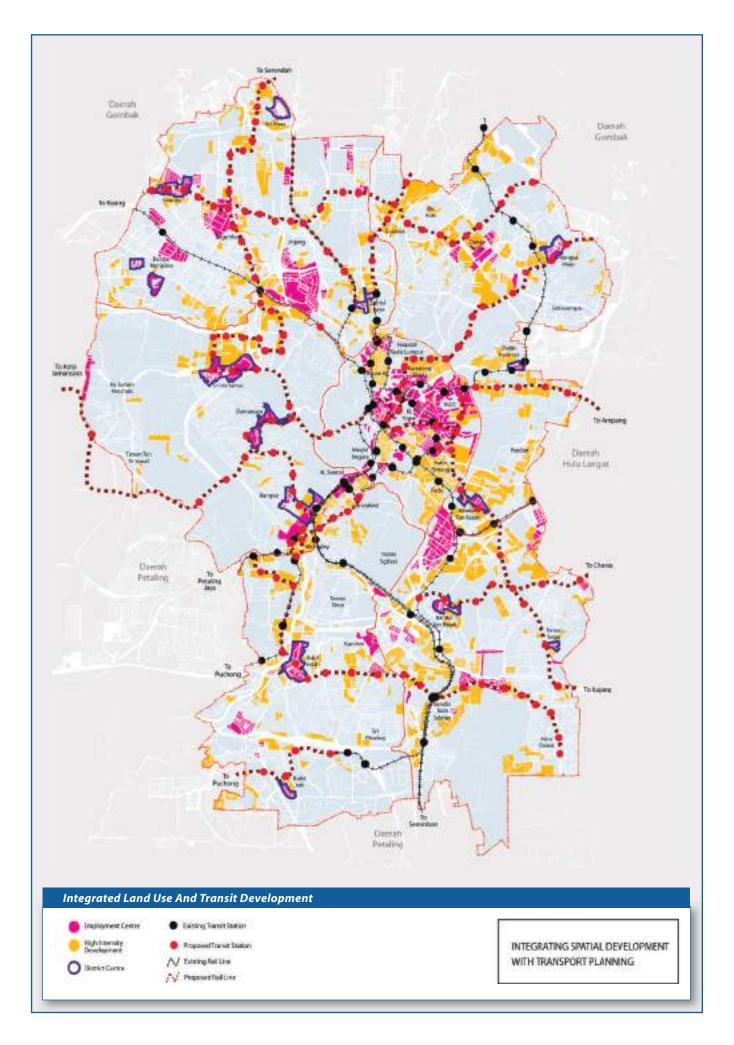
	2005	2020 and Beyond
Coverage of area within Transit Station	2,568.14 hectares (10% of	5,693.14 hectares (24% of
Within Hansit Station	Kuala Lumpur)	Kuala Lumpur)
Coverage of areas within rail corridors	5,920 hectares (24% of Kuala Lumpur)	12,646 hectares (52% of Kuala Lumpur)













**Strategic Direction 4.5 – Designating Transit Planning Zones** *Designate Transit Planning Zones at suitable locations and encourage developments to integrate with public transport facilities.* 

The Draft KL City Plan 2020 seeks to achieve a more efficient and effective city planning and development through the integration of land use planning and development with the public transport network. This is to be achieved by ensuring encouraging developments in location that are currently, or are planned to be, accessible to public Selected areas around transport. existing and future rail stations are identified as Transit Planning Zones (TPZ) where incentives are to be given for development through further intensification and mixed use developments.

Land and buildings within the Transit Planning Zones, which are located within 400m of either existing or future rail stations, are designated for mixed use zones and high to medium density residential zones. Commercial areas will be intensified to optimise investment in public transport, to minimise the need to travel or influence the

mode of travel towards public transport and provide integration between land development and transport planning. This aims to help alleviate traffic congestion problems particularly in the city centre and to optimise use of land around existing and future rail stations.

Developments in the Transit Planning Zones are encouraged to have the following characteristics or components:-

- Priority for transit supportive uses and high intensity activity nodes within 250m walking radius of a transit station.
- ii. High density residential and high plot ratio commercial development within transit planning zone.
- iii. Mixed-use development of commercial, residential, community facilities and other civic uses.
- iv. Pedestrian environment for all groups including the disabled.
- v. Good urban design.

Transit Planning Zone (TPZ) is defined as area within 400m around a Transit Station. Within this 400m radius, the immediate 250m radius is the Priority TPZ. The KL Development Control Plan 2008 (KLDCP 2008) provides further elaboration and guide on application and incentives for development in of TPZ.

Transit supportive land uses and activities encourage transit use and increase the transportation network efficiency. Transit supportive land uses are employment generation uses (commercial, office and industry), residential uses (high density), retail activities (pedestrian oriented street retail and shopping retail with strong pedestrian connections), civic use areas, entertainment, recreational and cultural facilities.

## 66 locations have been designated as Transit Planning Zones (TPZ) as follows:-

- 1. Desa Bakti
- 2. Intan Baiduri
- 3. Metropolitan Kepong
- 4. Taman Kepong
- 5. Fadason
- 6. Kampung Batu
- 7. Batu Kantomen
- 8. Taman Pelangi
- 9. Bandar Dalam
- 10. Padang Balang
- 11. Melati
- 12. Taman Tasik Danau Kota
- 13. Pusat Bandar Wangsa Maju
- 14. Taman Setapak Jaya
- 15. Taman Rainbow
- 16. Sentul Timur
- 17. Sentul
- 18. Matrade
- 19. Segambut Jaya
- 20. Sunrise
- 21. Taman Sri Hartamas
- 22. Jalan Dungun
- 23. Taman Bukit Damansara
- 24. Titiwangsa
- 25. Raja Muda Abd Aziz
- 26. Kampong Bharu
- 27. KLCC
- 28. Jalan Stonor
- 29. Jalan Kia Peng
- 30. Raja Chulan
- 31. Bangunan LTAT
- 32. Bukit Bintang
- 33. Wisma SPS
- 34. Imbi Plaza35. Hang Tuah
- 36. Pudu
- 37. Kuala Lumpur
- 38. Jalan Parlimen
- 39. Ampang Park
- 40. Taman U-Thant
- 41. Datok Keramat
- 42. Ampang Hilir
- 43. KL Sentral
- 44. Brickfields
- 45. Syed Putra
- 46. Bangsar
- 47. Abdullah Hukum
- 48. Kerinchi
- 49. Universiti
- 50. Taman Bukit Angkasa
- 51. Pantai Dalam
- 52. Petaling
- 53. Bukit Indah
- 54. Salak Selatan
- 55. Bandar Baru Bukit Jalil
- 56. Desa Petaling
- 57. Bandar Tasik Selatan
- 58. Desa Tun Razak
- 59. Desa Cheras
- 60. Bandar Tun Razak
- 61. Taman Tayton View
- 62. Seri Permaisuri
- 63. Cheras
- 64. Taman Bukit Ria
- 65. Cochrane
- 66. Masjid Alam Shah



Strategic Direction 4.6 – Encouraging Mixed Use Developments

Designate mixed use zone strategic locations in support of transit oriented development and at locations which necessitate redevelopment for improved urban environment.

The Draft KL City Plan 2020 encourages patterns and forms of development that reduce the need to travel especially by car. The Draft KL City Plan 2020 designates mixed use zones at locations where rail transport facilities already exist and supports high trip generating mixed use development at locations with both high levels of public transport accessibility and capacity.

Primary objectives of the mixed use zones are to:

 Allow a mixture of complimentary land use types, which may include housing, retail, offices, commercial services, and civic uses to encourage linking of trips;

- Promote transit-supportive development which is relatively dense, mixed use, and designed for the safety and convenience of pedestrians;
- Encourage street activity to support retail businesses;
- Provide housing especially in city centre to bring back population and city centre living;
- Provide flexibility in land use standards to facilitate demand and trend changes; and
- Reinforce streets as public spaces, and encourage pedestrian movement.

The Draft KL City Plan 2020 has designated three mixed use zones in the Land Use Zoning Map where mixed use developments will be encouraged to include, but are not limited to, any combination of housing, offices, retail and businesses, public and institutional uses and compatible industrial use, subject to conformity to Zoning Schedules of the respective mixed use zones.

Land uses may be mixed vertically or horizontally. For example, businesses, housing and civic uses may be mixed 'vertically' by constructing offices and residences above retail space or 'horizontally' by connecting different uses adjacent to each other with pedestrian pathways, plazas and other public spaces.

Mixed use development encourages a reduction in the need to travel long distances, by including a balance of housing, employment, commercial and other community facilities in the same area. Mixing uses can also help achieve intensive development by using the same space for more than one purpose. It contributes to vitality and safety by preventing areas becoming deserted and hostile. New developments should create or enhance a mix of uses within large buildings, within the development and/or between the development and its surroundings.

Example of vertical mixed use development where retail is located on the ground floor and residential on the upper floors



#### MIXED USE ZONES IN DRAFT KL CITY PLAN 2020 LAND USE ZONING MAP

#### **Mixed Use Commercial (MXC)**

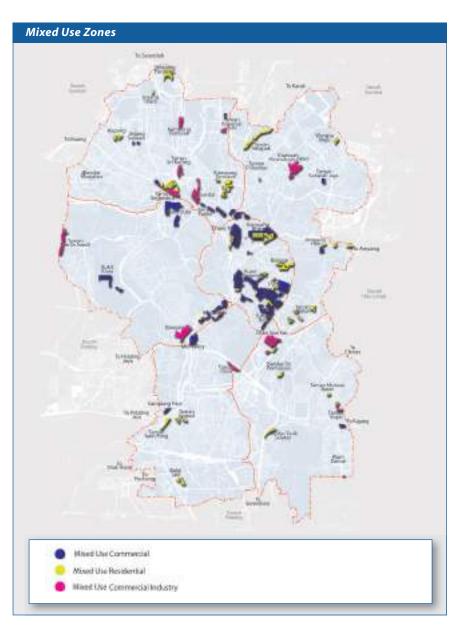
Refers to land zoned for mixed use commercial development in the Land Use Zoning Map. It is intended to promote residential component in city centre commercial zones, transit planning zones and international zones. The zone is predominantly commercial with minimum 30% residential.

#### Mixed Use Residential (MXR)

Refers to land zoned for mixed use development within general residential area as identified in the Land Use Zoning Map. It is intended to promote commercial component in residential zone and at transit planning zone. This zone is generally located within transit planning zone to encourage street level activities. This zone is predominantly residential where the commercial activity allowed is not more than 30% of the allowable gross floor area.

### Mixed Use Commercial & Industry (MXCI)

Refers to land zoned for mixed use industry and commercial development as identified in the Land Use Zoning Map. It is intended to promote clustering of similaractivities incorporating compatible and non-polluting industrial activities with support services such as storage, distributive trade and service industries. This zone is predominantly industrial with maximum 30% commercial use.







Ground/street level activities to promote street frontages for mixed use zones.



### PROJECTED LAND REQUIREMENTS, 2005 -2020

The land use distribution of the Draft KL City Plan 2020 takes account of future land requirements as set out in the KLSP 2020 and land available for developments. The KLSP 2020 projected that by 2020, additional 2921.12 hectares of land need to be released or developments of residential, commercial, industrial and community facilities. However, total vacant land available for development accounts for only 2,470.61 hectares, which means redevelopment or renewal of existing development areas or dilapidated buildings need to be considered.



## Transforming and Regenerating Brownfield Sites and Urban Villages

Regenerating older areas within the city provides opportunity to greatly improve the social, economic and environmental health of the city.

## Strategic Direction 4.7 – Regenerating Older Areas and Optimising Use of Previously Developed and Underutilised Land

Promote redevelopment of older housing and industrial areas, underutilised land and buildings and development on infill sites.

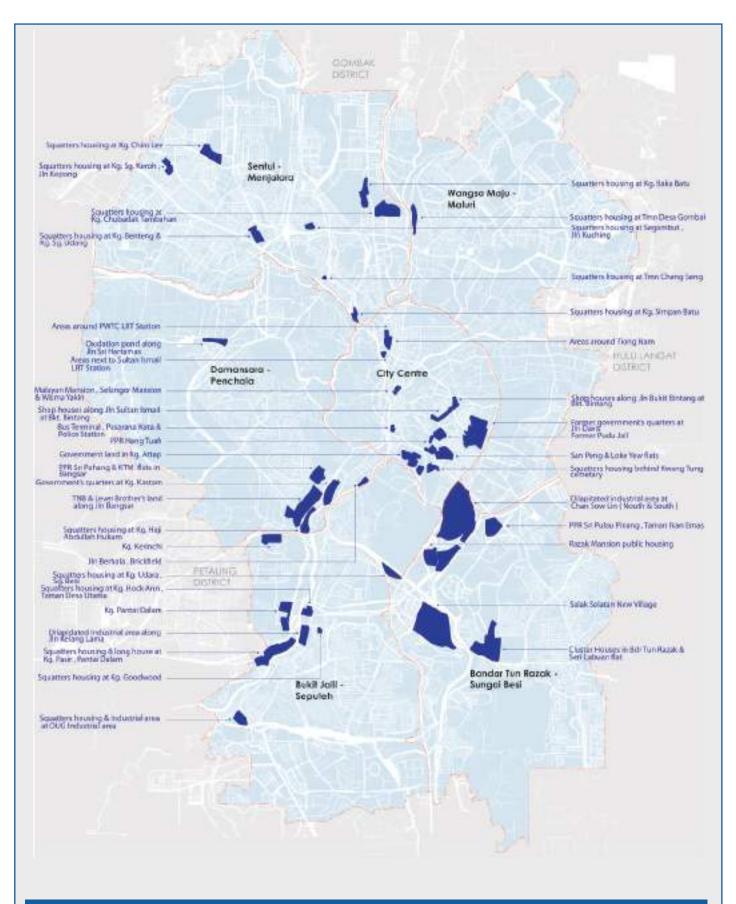
Vacant land ready for developments are becoming scarce in Kuala Lumpur and alternative land for development need to be explored for Kuala Lumpur to progress towards achieving its vision.

Draft KL City Plan 2020 promotes redevelopment of brownfield sites and dilapidated buildings, development on infill sites in city centre and other commercial centres and regeneration of abandoned projects. Concentrating development on infill sites and redevelopment of dilapidated sites can help to make the best use of existing services, such as transport and waste management. It can encourage more sustainable lifestyles by providing an opportunity to recycle land, clean up contaminated sites, and assist environmental, social and economic regeneration. It also reduces pressure to build on greenfield land and helps protect the residual forested areas, environmentally sensitive areas and open spaces within Kuala Lumpur.

A total area of 548 hectares has been identified for redevelopment in various parts of the city. Blighted housing and industrial areas will be redeveloped to present a positive and dynamic image of the city. Overcrowded residential areas will be transformed into modern living environment with sufficient facilities, infrastructure and utilities. Relocation of unsightly industrial premises away from main roads and landscape treatment will give a tidier introduction to the city.

The areas identified are to be redeveloped into a more intensified use in particular mixed use developments and high to medium density residential. All redevelopment areas will be required to provide for community facilities, improved infrastructures and urban parks/local play area. Proposals for redevelopment are required to be integrated with the surrounding urban fabric by ensuring continuity in public realm, green spaces and pedestrian network.

Where redevelopment areas are identified as Transit Planning Zones, provisions for transit facilities are to be made and the developments are to be integrated fully with transit facilities and high quality pedestrian environment.



#### Redevelopment Area



Area for Redevelopment

— Strategic Zone Boundary

#### **Redevelopment Areas**

No.	Location	Area (hectare)	Existing Use	Proposed Use		
City	City Centre					
1	Area near PWTC LRT Station	1.10	Open space, commercial, residential	Mixed use commercial (MXC)		
2	Areas around Tiong Nam	4.91	Open space, commercial, public facilities, residential	Mixed use residential (MXR)		
3	Areas next to Sultan Ismail LRT Station	1.33	Open space, commercial, SRJK Chung Kwok, residential	Mixed use residential		
4	Malayan Mansion, Selangor Mansion & Wisma Yakin	1.61	Mix used commercial, police beat, residential	City centre commercial		
5	Bus Terminal, Pasarama Kota & Police Traffic Office	0.91	Terminal, public facility	Mixed use commercial Public institution (police station)		
6	Government land in Kampung Attap	13.92	Open space, commercial, residential & institution	High density residential Medium density residential Public Institution		
7	San Peng & Loke Yew flats	15.53	Mosque, PPR Sri Selangor, open space, government's quarters, Merak & Melati flats	High density residential Mixed use residential Public Institution		
8	PPR Hang Tuah	1.05	Public housing	Mixed use residential		
9	Former Pudu Jail	6.57	Open space, former jail	Mixed use commercial		
10	Shop Houses along Jalan Sultan Ismail at Bukit Bintang	4.99	Terminal, open space, mixed use commercial, government's clinic, residential	City centre commercial Mixed use commercial		
11	Shop Houses along Jalan Bukit Bintang at Bukit Bintang	0.55	Mix used commercial, public facilities	City centre commercial		
12	Former Government's quarters at Jalan Davis	37.61	Car park & bus stop, open space, commercial, squatters	City centre commercial Mixed use commercial Mixed use residential Open space Public facilities		
Dama	ansara Penchala					
1	Oxidation Pond along Jalan Sri Hartamas	6.13	Car park, open space, commercial, utility	District centre commercial		
2	PPR Sri Pahang & KTM Flats	11.25	Open space, Flats	High density residential		
3	TNB & Lever Brother's land along Jalan Bangsar	36.03	Mixed use commercial, residential, open space, sport & recreation facilities, institution, industry	District centre commercial Mixed use commercial & industry Public facilities		

No.	Location	Area (hectare)	Existing Use	Proposed Use
4	Government's quarters at Kampung Kastam	12.50	open space, residential, commercial, public facilities, Industry	Public housing to accommodate residents from PPR Sri Pahang & KTM flats Mixed use commercial Mixed use residential Mixed use commercial & industry High density residential Public facilities
5	Brickfield around Jalan Berhala	2.09	Open space, public facilities, residential, sport & recreation facilities	Commercial Mixed use commercial Public facilities
Band	ar Tun Razak Sg Besi			
1	Dilapitated Industrial Area at Chan Sow Lin (North & South)	132.24	Open space, Industry, commercial, public facilities, residential, sport & recreation facilities	Neighbourhood centre commercial Mixed use residential Mixed use commercial & industry High density residential Commercial Industry
2	PPR Sri Pulau Pinang	15.23	Residential	High density residential Public facilities (hall & kindergarden)
3	Razak Mansion Public Housing	13.66	Commercial, school, residential, Industry	High density residential Commercial Public facilities (surau, market, school)
4	Cluster Houses in Bandar Tun Razak & Seri Labuan Flat	40.49	Car park, open space, commercial, public facilities, residential, sport & recreation facilities, Industry, Institution	High density residential Commercial Public facilities (surau, hall, school)
Bukit	Jalil Seputeh			
1	Kampung Kerinchi	9.75	Open space, PKNS flats	High density residential
2	Dilapitated Industrial Area along Jalan Kelang Lama	9.15	Open space, commercial, industry	District centre commercial open space
3	Kampung Pantai Dalam	12.91	Open space, commercial, public facilities, residential	High density residential Commercial Public facilities (surau, hall)
4	Kampung Pasir	26.01	Open space, mixed use commercial, public facilities, residential, industry	High density residential Mixed use commercial Commercial
5	Industrial area at Taman Industry OUG	7.89	Squatters, industry	Commercial Industry



### Strategic Direction 4.8 – Enhancing and Facilitating Development of Urban Villages

Enhance the physical environment, infrastructure provision and community facilities at urban villages.

urban villages in the city are generally associated with complex developmental problems, relative poverty, poor living conditions and inadequate infrastructure. Though they were originally conceived and planned as traditional villages, ad-hoc alteration to existing buildings, conversion of dwelling units into commercial and industrial use has resulted in substandard living environment and inadequate infrastructure and community facilities. Some of the urban villages are located close to areas where modern developments are taking place. These urban villages

need to be enhanced in terms of their development value, physical environment, infrastructure and community facilities.

It is effort to achieve vision for A World Class City; Kuala Lumpur's urban villages must be accorded similar opportunities to come into mainstream of the development. To achieve this, the Draft KL City Plan 2020 has designated the areas for development through the Land Use Zoning Map in the KLDCP 2008 in order to facilitate and encourage development within these areas.





DraftKuala Lumpur City Plan 2020 promotes a more sustainable and environmentally friendly transport system to enhance the city living environment. It redefines the priority for use of 'road space' and takes upon an integrated approach to enhance connectivity and accessibility within the City.

#### **Kuala Lumpur's Regional Role**

The Draft Kuala Lumpur City Plan 2020 shall move towards an integrated approach to promote a moresustainableandenvironmentally friendly transport system to enhance the city living environment. Transport proposals identified for the Draft Kuala Lumpur City Plan 2020 shall reinforce objectives of the Kuala Lumpur Structure Plan (KLSP 2020) for the development of urban transport plan and traffic management of Kuala Lumpur to create an efficient and equitable city structure for Kuala Lumpur and to enhance the living environment.

The Draft Kuala Lumpur City Plan 2020 aims to:

- provide a comprehensive and integrated transportation system that caters for the needs of inter and intra city travel;
- reverse the decline in public transport usage and to achieve a targeted public: private transport modal split of 60:40 by the year 2020:
- optimize the road and rail transportation infrastructure so that each will operate at its full capacity and maximum efficiency;

- ensure that the overall configuration of land use is integrated with road and public transport networks to optimize the development of land; and
- ensure that all areas within the City enjoy the same high quality and standard of provision of public transport services.

Kuala Lumpur must redefine the priority for use of the 'road space' for the betterment of accessibility within the City, rather than to achieve free flowing vehicular traffic on urban streets.

In order to create a city that is highly accessible for all its residents and users, in particular, one that is pedestrian and handicapped friendly, existing road space shall be reclaimed for implementing bus lanes, transit malls and barrier free pedestrian pathways. And in order to achieve a long term target modal split of 60% using public transport by the year 2020, an integrated approach and measures has been aligned for implementation by Draft KL City Plan 2020.

#### Building a More Sustainable, Integrated and Environmentally Friendly Transport Infrastructure

Strategic Direction 5.1

Implementing an Integrated Approach towards a Congestion Free City

Strategic Direction 5.2

Extending Coverage of Urban Rail Network and Integrating with Regional Network

Strategic Direction 5.3 **Providing Park & Ride Facilities** 

Strategic Direction 5.4 **Giving Priority to Buses** 

# Moving Towards Travel Demand Management Strategies

Strategic Direction 5.5 **Dispersing Peak-period Traffic** 

Strategic Direction 5.6

Reducing Single Occupancy Vehicles and Providing Incentives to Road Users to Change Travel Behaviour

Strategic Direction 5.7 **Restraining Traffic within City Centre** 

Strategic Direction 5.8

Managing Supply and Distribution of
Parking Facilities in City Centre

Strategic Direction 5.9
Using Data from ITIS KL for a
Comprehensive Traffic Management
System for Kuala Lumpur

Creating Streets for People

Strategic Direction 5.10 **Establishing Functional Road Hierarchy** 

#### Kuala Lumpur's Transport Challenges

- Reduce the negative impacts of Congestion;
- Improving Quality of Life;
- Minimise greenhouse gas emissions;
- Managing growth of cars; and
- Integrating transport with land use development.

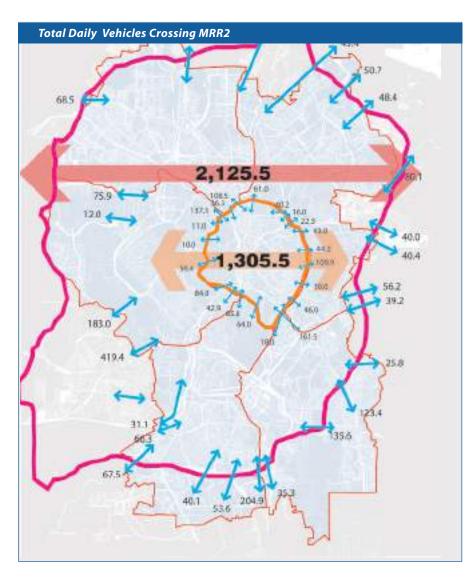
# Building a More Sustainable, Integrated and Environmentally Friendly Transport Infrastructure

A comprehensive and integrated transportation infrastructure that caters for the needs of inter and intra city travel will be integrated with land use planning and development.

Through effective land use and transport planning, Draft Kuala Lumpur City Plan 2020 aims to minimise the need to travel or to influence the mode of travel through the coordinated land use arrangement and siting of locations of traffic generating uses in relation to existing transport particularly the more sustainable transport mode, and ensuring proposals for new transit routes in line with land use planning. Investments shall be channelled to build infrastructure to support the public transport industry and operations to provide for a comprehensive and integrated transportation system.

### **Strategic Direction 5.1 – Implementing Congestion Free City**

Implementing an Integrated Approach towards a Congestion Free City.



Kuala Lumpur experiences high mobility of people, where movements into and within Kuala Lumpur are becoming increasingly unpleasant and unsafe. This high mobility pattern leads to congestion, where use of road network increases and traffic demand is greater than the capacity of the road.

The Road Network is Congested particularly on weekday peaks:

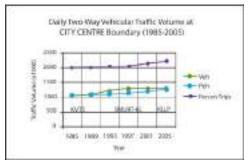
- 1.305 million vehicles cross the MRRI.
- 2.125 million vehicles cross the MRRII.
- 70% vehicle trips crossing MRRI (42,600 vehicles) and MRRII (86,500 vehicles) during AM peak hour are SOV.
- 65% crossing MRRI (39,100 vehicles) and MRRII (84,500 vehicles) during PM peak hour are SOV.
- Total Highway Capacity into/out of CPA = 930,000pcu.
- Total Volume Entering CPA in 2005 =1,260,000 pcu/day.
- This has already exceeded SMURT\_KL forecast of 1,187,000 pcu/day in 2010 (Base Masterplan).

#### City Centre Screenline

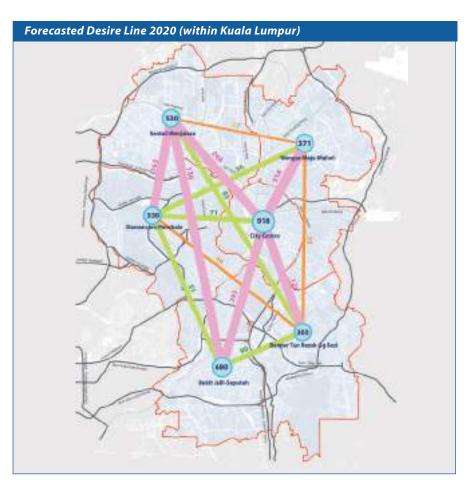
Year	Vehicle Trips x 1000	PCU Trips x 1000	Person Trips x 1000
1985	1035	1060	2005
1989	1106	1090	2024
1993	1181	1122	2042
1997	1262	1155	2061
2001	1284	1206	2145
2005	1305	1260	2233

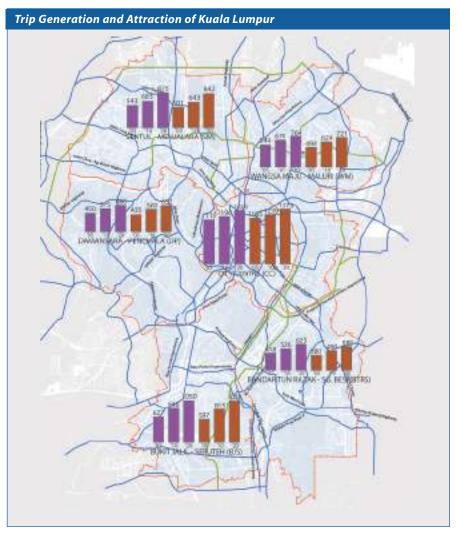
Daily more than 1.305 million vehicles cross the MRR1 and 2.125 million vehicles crosses the MRRII. This indicates strong inter and intra urban movement and the daily vehicular traffic on the arterial roads at the CPA boundary i.e. on MRR1 has increased by 1.7%pa (in vehicle units) and 0.7%pa (in PCU) from 1985 to 1997, and by 0.4%pa (in vehicle units) and 0.1%pa (in PCU) from 1997 to 2005. This has exceeded the SMURT–KL forecast of 1,187,000 pcu/day in 2010 from the SMURT-KL Study by JICA.

#### Daily Two-Way Vehicular Traffic Volume



Total For Kuala Lumpur (1000 person trip/day)					
Year 2005 2010 2020					
Trip Generation	3,699	4,498	5,304		
Trip Attraction	3,573	4,372	5,204		







#### Target Modal Split for Person Trips at City Centre Screen Line (MRRI)

		2005		2010			2020		
Mode	Person Trips / Day	Sha	are	Person Trips / Day	Sha	ire	Person Trips/ Day	Sha	re
Car / Taxi	943,751	42.3%		850,300	34.7%		557,400	20.8%	20.10/
Motorcycle	420,153	18.8%	61.1%	378,500	15.4%	50.2%	248,200	9.3%	30.1%
Bus	469,969	21.1%		646,880	26.4%		505,600	18.9%	
Rail	169,277	7.6%	28.6%	333,120	13.6%	40.0%	1,102,400	41.1%	60.0%
Small Van/ Utility/ Goods Vehicles	229,467	10.	3%	241,200	9.8	%	266,400	9.99	%
Total	2,232,617	100	.0%	2,450,000	100.	0%	2,680,000	100.0	)%
Ave Annual Growth Rate for period	1.00% since 1997		1.88%		0.90%				

Source: Estimated by KL Local Plan Team, 2005

#### Target Modal Split for Person Trips at MRRII

Mode	2005		2010			2020			
	Person Trips / Day	Sha	are	Person Trips / Day	Sha	ire	Person Trips / Day	Sha	re
Car / Taxi	2,138,175	67.4%	92.40/	2,431,800	63.0%	76.7%	3,158,100	58.0%	60.70/
Motorcycle	476,500	15.0%	82.4%	527,200	13.7%	70.7%	580,900	10.7%	68.7%
Bus	340,000	10.7%	14.50/	540,000	14.0%	20.00/	843,000	15.5%	27.50/
Rail	120,000	3.8%	14.5%	233,000	6.0%	20.0%	655,000	12.0%	27.5%
Small Van/ Utility/ Goods Vehicles	97,750	3.1	1%	128,000	3.3%		208,000	3.8%	
Total	3,172,425	100	.0%	3,860,000	100.	0%	5,445,000	100.0	)%
Ave Annual Growth Rate for period	4.00%		4.00%		3.50%				

Source: Estimated by KL Local Plan Team, 2005

#### Targeted Transport Capacity By Mode at City Centre Screen Line

Mode	2010	2015	2020
Forecast Daily Motorised Vehicle Demand (pcu)	1,242,000	1,137,000	993,000
Forecast Peak Hr Motorised Vehicle Demand (pcu/hr)	99,000	91,000	79,000
Targeted Road Capacity Required (pcu/hr)	110,000	98,000	86,000
Forecast Daily Bus Trips (Veh) <sup>1)</sup>	21,600	20,800	16,900
Targeted No. of Buses Required (veh)1)	2,200	2,100	1,700
Forecast Daily Rail Passengers (person)	333,120	671,040	1,102,400
Targeted Rail Capacity Required (ppdph)	83,300	111,800	183,700

Note: 1) Include Inter-city and excursion buses

The high mobility pattern is reflected in the traffic desire lines which are strongest between the City Centre-Kepong, followed by City Centre-Setapak and City Centre-Cheras. From this analysis, the most prominent corridor for diversion of car mode to public transport is the north-south corridor comprising Jalan Kuching/ Jalan Ipoh, KL-Seremban Highway and Jalan Cheras/Jalan Loke Yew.

The public transport share in the City Centre in 2005 has shown a significant increase of over 5% the situation recorded in 1997. The target is to increase the public transport share in City Centre to 40% in 2010, 50% in 2015 and subsequently to 60% in 2020. The public transport share for MRRII is also targeted to increase from 14.5% in 2005, 20% in 2015 and 27.5% in 2020.

The share of person trips at the CPA Boundary (i.e. crossing the Middle Ring Road 1) using public transport on a daily basis should be at least 40 % by year 2010, 50% by year 2015 and 60% by year 2020.

This shift to Public Transport mode will reduce motorized vehicle demand in 2020 and improved road capacity as well peak hour traffic condition.

### Strategic Direction 3.2 – Developing A World Class Retail Business Clusters

Developing a world class retail experience for Kuala Lumpur that is vibrant and internationally acclaimed.

#### **Urban Rail Network**

Urban rail network is a motorised transport mode that has been introduced in Kuala Lumpur. With three lines running – Putra, KTM Komuter and Star, the existing rail network shall be extended towards new areas not yet served by rail network and to link district centres, employment areas, high density residential neighbourhoods and new development areas that will support the economic and physical growth of Kuala Lumpur.

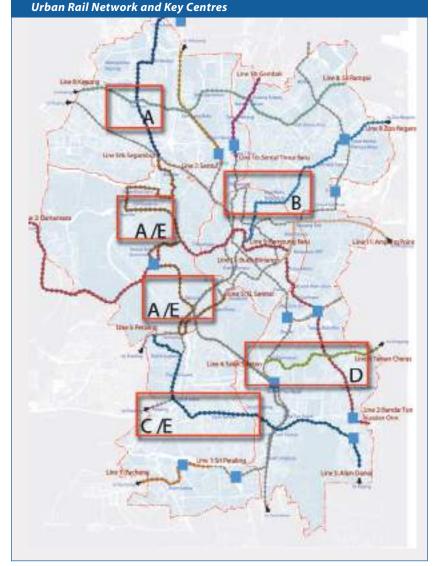
Currently, only approximately 10% (2,661.55 hectares) of Kuala Lumpur is directly served or within the transit catchments of existing transit stations and 20% of Kuala Lumpur (5894.51 hectares) are within transit corridor (i.e. areas within 400m on each side of rail line).

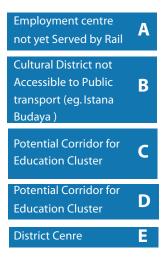
The urban rail network shall comprise of 11 (eleven) new and extension lines that shall increase



the total coverage of areas within transit corridor to be 53% (13,028.53 hectares) of Kuala Lumpur.

These new lines shall provide many opportunities for Kuala Lumpur; it will have great influence on land use planning and development where development will be more intensified along these rail corridors and district centres and employment centres will become transit nodes.



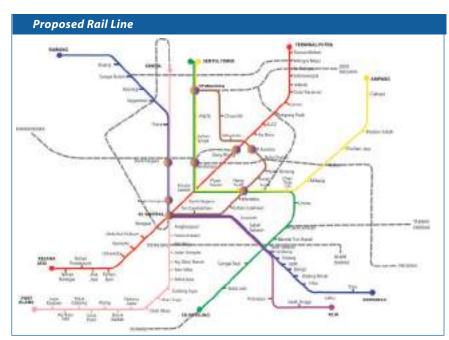


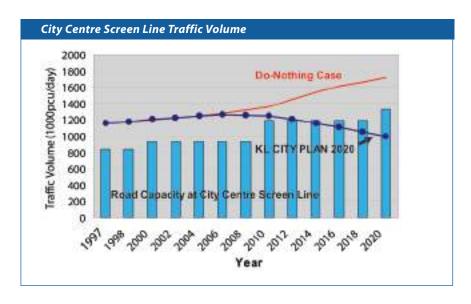


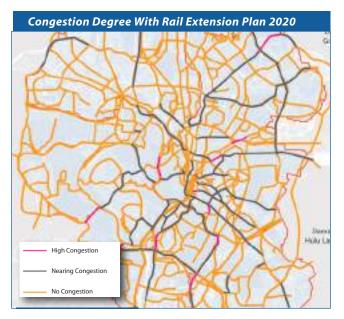
The urban rail network will have a total length of 233.61 km and shall serve 184 numbers of stations (65 existing stations and 119 new stations). These stations shall serve its immediate catchments areas where transit supportive uses are expected to take advantage of the lines and stations.

The urban rail network will change the mode of travel for city's population as it is expected that additional 250,000 people (800,000 persons trip per day) will now travel by rail in the City Centre and this would elevate the congestion problems the city is facing and will alleviate green house gas emission issue as it is an environmental friendly mode of transport.

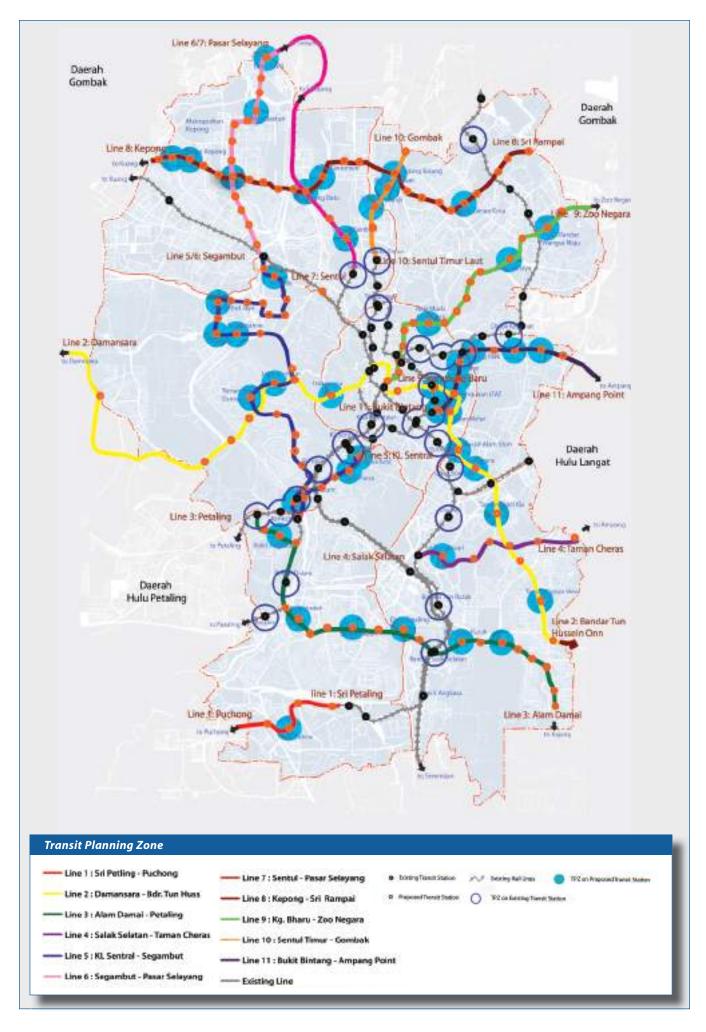
The need for an urban transportation network is most needed for the city and this is seen in the 'Do-Nothing' scenario where the congestion level in Kuala Lumpur will worsen should no form of public transportation be put in place by the year 2020.



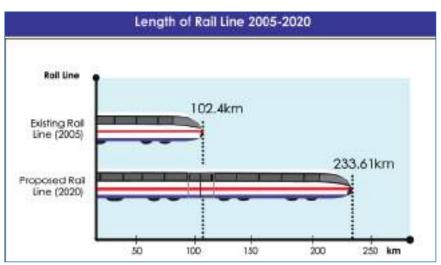






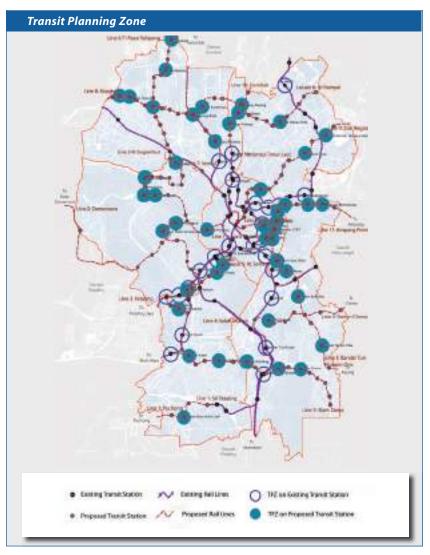






#### Proposed New Rail Line For Kuala Lumpur

Line No.	Description	Function/ System	Capacity (ppdph)	Approx, Length (km) in KL	No. of Stations in KL
1	KTM Bypass from KTM Segambut to Plaza Phoenix (finally to Kajang)	MRT (regional commuter + intercity)	12,000 – 54,000 (high)	18.35	16
2	One Utama to Ampang Point (part of Sg Buloh/Subang to Hulu Langat (Ampang) Line)	LRT (regional commuter)	5,000 -12,000 (medium)	21.18	18
3	KTM Segambut to KL Sentral (KL Monorail Extension)	Monorail (sub-urban feeder)	5,000 (low- medium)	16.42	19
4	STAR Sri Petaling to Sunway (Sri Petaling Line Extension)	LRT (regional commuter)	5,000 -12,000 (medium)	3.61	3
5	STAR Sentul Timur to Gombak (Ampang & Sri Petaling Lines Extension)	LRT (regional commuter)	5,000 -12,000 (medium)	3.89	4
6	STAR Salak Selantan to Taman Cheras (finally to Hulu Langat (Ampang))	Monorail (sub-urban feeder)	5,000 (low- medium)	5.59	7
7	KTM Petaling to Alam Damai (finally to Bdr Tun Hussein Onn/Kajang)	Monorail (sub-urban feeder)	5,000 (low- medium)	13.17	12
8	Metro Prima to Kolej TAR (part of Bdr Baru Selayang-Tmn Melawati Line)	LRT (regional commuter)	5,000 -12,000 (medium)	15.08	12
9	KTM Sentul-Pasar Selayang (part of extension to Gombak)	Monorail/LRT (sub-urban feeder + regional commuter)	5,000 -12,000 (medium)	9.85	5
10	PUTRA Kg Baru to Zoo Negara	Monorail (sub-urban feeder)	5,000 (low- medium)	9.29	10
11	KTM Segambut to Pasar Selayang	Monorail (sub-urban feeder)	5,000 (low- medium)	8.43	4
12	KL Monorail Titiwangsa to STAR Pudu	Monorail (urban distributor)	5,000 (low- medium)	6.35	9
	Total New Rail Network			131.21	119

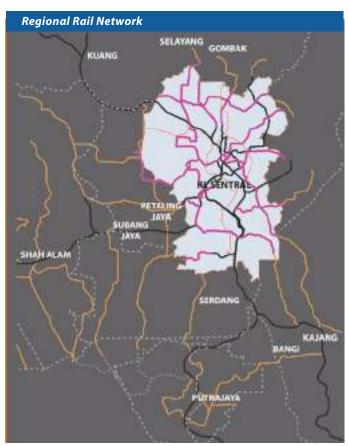


#### **Regional Network**

The public transport system in Kuala Lumpur, particularly the rail transport is more efficiently planned from a regional perspective (i.e. Klang Valley). The proposed extension and new rail lines in Kuala Lumpur shall be linked with existing rail lines and the future regional rail lines recommended under the Klang Valley Public Transport Study (KVPTS).

This regional rail network will provide for a comprehensive and integrated network which will enhance regional linkage as well overcome congestion issue for the Klang Valley. As a major employment centre for the region, Kuala Lumpur in particular inner city centre show very strong desire lines from the districts of Hulu Langat (Ampang), Petaling and Gombak.

The current high dependency on roads and highways to access city centre of Kuala Lumpur needs a large shift to other modes of motorised transport which are of higher capacity to overcome the congestion along all major roads in and out of Kuala Lumpur city.





#### Strategic Direction 5.3 – Providing Park & Ride Facilities

Providing Park & Ride Facilities in areas with frequent feeder bus operations.

Park-n-ride facilities shall be provided at rail stations with less frequent feeder bus operations. Feeder bus services in these rail ridership catchment areas are often not efficient due to the lack of well-organized local roads. The minimum capacity at each park-n-ride facility should be 250 car parking spaces based on a potential catchment area of 3km radius from the station.

Urban Rail Line	Park and Ride Facilities	
KTMB Commuter Line	Kepong, Segambut, Salak Selatan, Sungai Besi and Petaling	
Kelana Jaya Line	Bangsar, Setiawangsa, Wangsa Maju and Sri Rampai	
Ampang Line	Maluri	
Sri Petaling Line	Sri Petaling, Bukit Jalil, Sungai Besi, Bandar Tasik Selatan, Bandar	
Sitt etailing Line	Tun Razak, Cheras, Pudu, Sentul and Sentul Timur	
Dranged Future Line	Alam Damai, Pheonix Plaza, Pusat Bandar Damansara, Pasar	
Proposed Future Line	Selayang, Batu Cantonment and Sri Utara	

#### Strategic Direction 5.4 – Giving Priority to Buses

Giving Priority to Buses by Reclaiming Road Space for an Efficient Bus System

Buses have long been regarded as one of the most cost-effective mode of public transportation. The SMURT–KL Study had found in a 1997 survey that having a dedicated bus lane can reduce by more than 40% of the difference in travel speed between a bus and a car on a congested road. Thus it is obvious that having dedicated bus lanes can enhance Kuala Lumpur's public transport network. More importantly, it can be implemented immediately as it will be capitalising on existing infrastructure and resources.

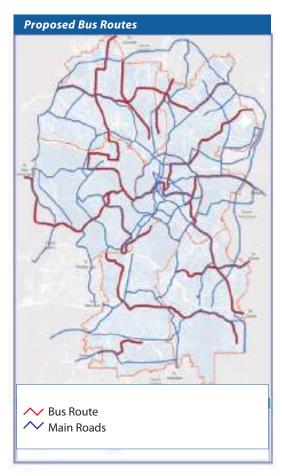
Existing bus lanes shall be strictly enforced and be extended to form a continuous network of bus lanes and bus priority lanes for smooth bus operations along the major arterials and urban streets in the city centre.

Traffic signals that give priority to bus movements shall be installed at junctions along bus routes to ensure reliable bus travel times. The Table below shows that without the proposed rail lines and in order

Planning for Buses	2010	2015	2020
Forecast Bus Passengers without Rail Extension	646,880	960,880	1,274,880
No of Bus Trips entering Kuala Lumpur City Centre	21,600	32,064	42,543
No of Stage Buses Required in Kuala Lumpur	2,200	3,237	4,279



to transport the same amount of person trips by bus transport, the number of stage buses providing services into the City Centre will be more than double the present bus fleet. As an interim measure to the future rail development plan, it is necessary to operate Bus Rapid Transit Systems on several roads running along the identified future rail transit corridors.





## **Moving Towards Travel Demand Management Strategies**

In achieving a sustainable transport for Kuala Lumpur, travel demand management provides strategies towards intervention to modify travel decisions so that more desirable transport mode, social, economic and/ or environmental objectives can be achieved, and the adverse impacts of travel can be reduced.

**Strategic Direction 5.5 – Dispersing Peak-period Traffic**Dispersing Peak-period Traffic to reduce congestion and share of use of roads

#### **KLSP 2020**

TT1: CHKL shall determine Travel Demand Management increase measures to public transport usage and liaise with the relevant authorities to ensure these that measures are implemented.

Peak hour congestion in Kuala Lumpur last for more than one and a half hours at times. Being caught in congestion is an economic loss where travel time is extended and time lost increases through congestion, road travel demand increases and air pollution contribution also increases.

Thus it is important to disperse peak period traffic through travel

demand management strategies. Two initiatives that Kuala Lumpur City Hall can create awareness to its city dwellers is:

- To maintain different starting hours for schools, government offices, private sector and retail activities; and
- To encourage flexible working hours for private sector.

These two strategies reduces share of use of roads during peak hours and will thus help reduce congestion.

### Strategic Direction 5.6 – Reducing SOV and Providing Incentives for Road Users

Reducing Single Occupant Vehicles (SOV) and Providing Incentives to road users to change behaviour of travel.

About 125,000 vehicles i.e. 65% (on average) of vehicles that crosses the MRRII during the morning peak and evening peak are single occupant vehicles. These vehicles are therefore travelling with very low efficiency and thus are a contributor to congestion and green house gas emission.

It is important to reduce SOV's from entering the City Centre and Kuala Lumpur City Hall can facilitate by providing incentives to those who Car Pool, especially to employees of government agencies and major corporations with offices in the City Centre. The incentives can include subsidized parking for registered pool vehicles.

Awareness program must also be created to SOV commuters on the impact caused by commuting alone as compared to car pooling or taking the public transport into the city. Comparatively, should the 125,000 SOV vehicles switch to high occupancy vehicle i.e. a Bus where more than 25 persons



#### **Target in SOV Reduction Entering City Centre**

Peak Time	2005	2010	2015	2020
Morning	70%	60%	50%	40%
Evening	65%	55%	45%	35%

travels in the same vehicle, thus the numbers will be reduced to 5,000 vehicles, a reduction of 96% vehicles. And if they switch more to Low Occupant Vehicle, where more than 3 person's travels in the same vehicle, the numbers will reduce to 42,000 vehicles, a reduction of 66% vehicles of the road users during peak time. Kuala Lumpur City Hall with the cooperation of Rapid Transit and urban rail operators can also provide discounted transit tickets (bus/rail)

to companies, government agencies or major corporations who have 25 workers or more to purchase annual transit passes as part of an Employer Pass Program.

"Route Integration", "Bus Priority Schemes", "Subsidized Fares/Public Transport Allowance", etc. - these efforts will create a shift from using the private mode to the public transport through provision of incentives for the users.

### Strategic Direction 5.7- Restraining Traffic within City Centre

Restraining Traffic within City Centre through Congestion Pricing to achieve an efficient use of road space

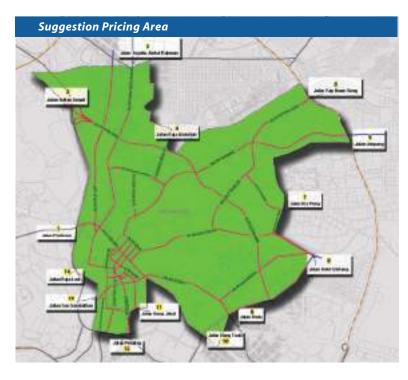
The objective of a measure to restrain traffic within the City Centre such as area road pricing (ARP) or congestion pricing scheme, is to achieve a more efficient use of road space in the City Centre in order alleviate traffic congestion, and as a result to reduce travel time and air pollution caused by vehicular emission. The measurable indices are improvement on the level of service on the designated roads, reduction in Single Occupant Vehicles (SOV), and increase in the share of public transport users in the City Centre.

ARP is proposed on the congested roads in the City Centre. Motorists crossing the Entry Barrier placed on the following 14 roads during the operation period shall be charged a user fees:

- 1. Jalan Parlimen
- 2. Jalan Sultan Ismail
- 3. Jalan Tuanku Abdul Rahman
- 4. Jalan Raja Abdullah
- 5. Jalan Yap Kwan Seng
- 6. Jalan Ampang
- 7. Jalan Kia Peng
- 8. Jalan Bukit Bintang
- 9. Jalan Pudu
- 10. Jalan Hang Tuah

- 11. Jalan Hang Jebat
- 12. Jalan Petaling
- 13. Jalan Tun Sambathan
- 14. Jalan Raja

Based on preliminary analysis of existing traffic conditions and future land use data, the ARP scheme is expected to reduce vehicular traffic demand by at least 15% in the first year, and affecting some 400,000 commuters in the City Centre by 2020.



#### **Strategic Direction 5.8 – Managing Car Parking**

Managing Supply and Distribution of Parking in City Centre to enhance Public Transport Usage



CHAN KAR LEONG
SJK DANAU KOTA
2nd Prize Winner (Category A) of Drawing Competition
in Conjunction with Preparation of Draft KL City Plan 2020.

In line with KLSP 2020, Kuala Lumpur City Hall shall use the control of supply and distribution of car parking facilities in the City Centre to reinforce the use of public transport and travel demand management objectives of the KLSP 2020 by making private transport a less attractive proposition than public transport.

The Kuala Lumpur City Plan 2020 initiative towards managing the supply of parking and its distribution is as follows:-

Future developments in City Centre shall provide offstreet parking facilities within their site to meet at the most only one-third (1/3) of their parking facilities requirement to be calculated based on the prevailing planning guidelines, while the balance two-third (2/3) parking requirement shall be provided within public parking facilities constructed by Kuala Lumpur City Hall using the 'Development Charge' to be contributed by the project owners in lieu of meeting the full parking requirements

- Future shop lots development shall provide sufficient off-street parking facilities required by occupants/tenants.
   Only limited duration parking shall be allowed to remain as on-street parking.
- Future land use development adjacent to existing and planned transit stations, hubs or terminals shall be required to provide 'Park-n-ride' facilities within the proposed development site, if deemed necessary by Kuala Lumpur City Hall.
- Construction of Public Parking Facilities by Kuala Lumpur City Hall includes 'Park-n-Ride' facilities at areas identified for 'Park-n-Ride' and other strategic locations, and link them to Parking Information and Guidance System to facilitate their efficient utilization and to improve traffic circulation within their service area. Public Parking Facilities can be constructed underground, below road space and other public areas (such as parks and open spaces).
- The control of parking charges shall be used to regulate parking demand in different parts of the city.
- The formulation and adoption of new parking standards and requirements for Kuala Lumpur.



#### **Strategic Direction 5.9 – Using ITIS**

Using Data from ITIS Kuala Lumpur towards a Comprehensive Traffic Management System for Kuala Lumpur

ITIS or Intelligent Traffic Information System is a comprehensive traffic management system and it is Kuala Lumpur City Hall initiative towards improving the management of Klang Valley's transport infrastructure system.

Presently the data from ITIS KL are used to provide information on the congestion level on the road network under surveillance by the Automatic Incident Detection (AID) cameras. These data or information is disseminated via Vehicle Message Signs (VMS) and also made available in the internet and through Call Centres.

However, ITIS sophisticated system and database will be further used in a much integrated manner by Kuala Lumpur City Hall for Parking Information System and Public Transport Service Information.

Automatic Vehicle Monitoring System or Bus Location System shall be implemented to enable the public transport operators to manage and control their business operations effectively and efficiently. Bus priority traffic signals shall be implemented to ensure better operation speed. More commuters will be encouraged to use the public transport when the services are more reliable and efficient.



#### **Creating Streets for People**

The move towards 'People Priority' is emphasize in this Draft KL City Plan 2020, where the priority use of road space must now take into consideration of the people's safety and comfort in travel and use of road space.

### Strategic Direction 5.10 – Establishing Functional Road Hierarchy

Establishing Functional Road Hierarchy and enhancing connectivity by adopting design standards that is flexible and adaptable to local issues.

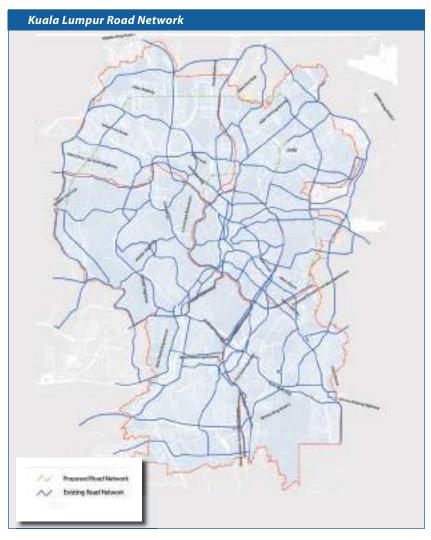


The total road length in Kuala Lumpur is some 1,213 km as of 31st December 2005. The road network of Kuala Lumpur is almost a complete network when all on-going and committed road projects are completed in the near future.

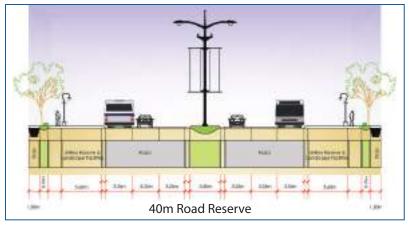
The consequences of pursuing a business as usual strategy for upgrading and building more road infrastructure in Kuala Lumpur are very clear:

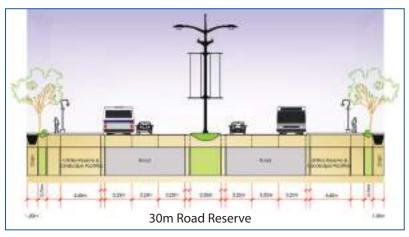
 increasing levels of congestion on roads;

- significant increases in greenhouse gases from motor vehicles;
- significant health impacts from lack of physical exercise, noise, respiratory disease, stress and road traffic accidents;
- significant damage to neighbourhoods, community and liveability as traffic levels damage social interaction and public use of streets; and
- significant costs in futile attempts to cope with the steep increase in traffic demand



Therefore, with the completion of the DUKE and the Pandan Corridor Expressway, there is no need for more urban highways within the City. Kuala Lumpur City Hall shall upgrade and construct roads and intersections in accordance with the established functional road hierarchy of the urban road network. The geometric design policies and practices must consider context-sensitive design or flexible design with objectives to simultaneously advance safety, mobility, enhancement of the natural environment, and preservation of community values. Such geometric design must be to meet the needs of specific sites rather than use the standardized solutions. to meet specific needs, they must consider the flexibility in the current design guidelines instead of automatically opting for the highend solution that gives capacity over environment, historical preservation,





and neighbourhood-protection concerns. Thus, Kuala Lumpur City Hall shall support the development of New Standards for Road Design and Materials, Guidelines for Road Safety Audit and Guidelines for Traffic Impact Assessment among others.

A road hierarchy system is established based on the current road network in Kuala where specific functions and minimum road reserve requirements is determined base on existing infrastructure and resources as well as meeting the needs functional needs of road space that make people the priority user.

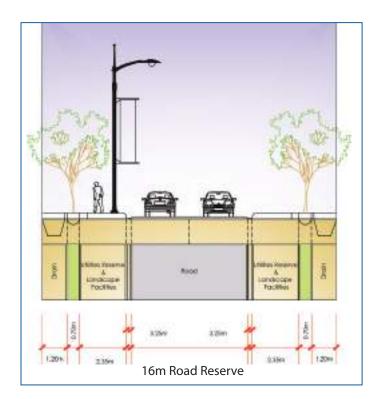
The major road network comprising the Highway and Arterial shall be developed to provide an efficient high to medium speed traffic circulation for vehicles. Highways and Arterials used by major bus routes shall be designed with continuous bus lanes to form a bus network in the City.

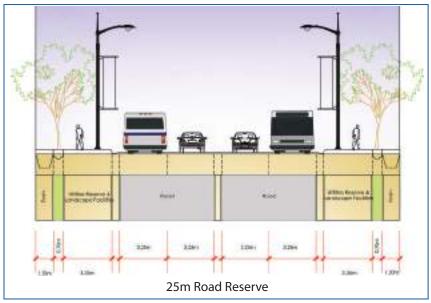
The minor road network comprising the Collector and Local Street shall be developed to provide an efficient medium to low speed traffic circulation for vehicles. Collectors used by major bus routes shall be designed with bus lanes (or even be reclaimed as transit malls) or bus priority lanes.

Local Street shall be developed for both motorists and pedestrians. Where necessary Local Street designs shall incorporate traffic calming measures to slow down traffic to a safer speed, minimize risk both to pedestrians and motorists, and improve the quality of life on these "calmed" streets.

The proposed Transit Oriented Development at rail stations with

the clustered land uses will encourage more walk trips within the City and support the viability of the rail services when the car mode can be made unnecessary at both ends of the transit trip. To encourage walking, suitable segments of Local Street or Minor Collector providing access to rail stations shall be converted to pedestrian roads.





#### **Functional Road Hierarchy**

Road	d Classification	Max. Speed	Road ROW	Function
Highway/Expressway		90 – 110 kph	30 – 40 m (100 – 132 ft)	<ul> <li>Free flow</li> <li>High speed</li> <li>Movements between KL and other urban areas in the Klang Valley</li> <li>Access through interchanges only</li> </ul>
Arterial	Major Arterial (include Ring Road)	80 kph	24 – 30 m (80 – 100 ft)	<ul> <li>Medium speed</li> <li>Dual carriageway with no direct access to individual premises</li> <li>Has 25ft service road</li> <li>Primarily distribute movements within KL</li> <li>Grade-separated or signalised intersections</li> </ul>
	Minor Arterial			<ul> <li>Medium speed</li> <li>Connect collector to major arterial road and highway</li> <li>Single carriageway with less or no direct access to individual premises</li> <li>Signalised intersections</li> </ul>
Collector	Major Collector	60 kph	15 – 20 m (50 – 66 ft)	<ul> <li>Low to medium speed</li> <li>Connect local road to arterial road</li> <li>Single carriageway with less or no direct access to individual premises</li> <li>Signalised control only when necessary or at crossing with arterial road</li> </ul>
	Minor Collector			Low to medium speed     Connect local road to arterial road     Still have direct access to individual premises
Local street		40kph	12 – 15 m (40 – 50 ft)	<ul><li>Low speed</li><li>Direct access to individual premises</li></ul>
Transit Mall/ Pedestrian Road		30 kph (bus/taxi only)	6-14 m ( 20 – 45 ft)	<ul> <li>No private vehicle is allowed on street</li> <li>Segments of local street or minor collector converted into bus/taxi only street or community spaces such as a bazaar street, pocket park, rest area, etc.</li> </ul>



Draft Kuala Lumpur City Plan 2020 emphasizes on the provision of adequate, safe, healthy and comfortable housing for all the City's citizens.

Kuala Lumpur is home to 1.6 million people (2006) and this is expected to grow to 2.2 million by the year 2020. With more than 464,996 homes today, the housing sector is fast growing and contributes significantly to the urban footprint of the City.

Housing is the most basic and fundamental need of a human being besides food, water and air. Thus, ensuring that every family in the city has a safe, comfortable and healthy abode to call home would be a great achievement for any city. The key

challenge is ensuring that the home not just meets the basic standards but has quality living space and provision of facilities needed by the communities. Community facilities act elements supporting for comfortable living environment as well as for community cohesiveness. The range, prices and affordability of housing act as physical indicators for a city's livability, whilst the adequacy and locational distribution of community facilities are social indicators of a city's livability.



SMK Desa Tun Hussein Onn 3rd Winner (Category B) of Drawing Competition in Conjunction with Preparation of Draft KL City Plan 2020.

### Meeting the Needs for Housing of the People

Strategic Direction 6.1

Ensuring Adequate Housing Land

Strategic Direction 6.2

Offering a Diversity of Housing Products

Strategic Direction 6.3

Encouraging Quality Affordable
Housing at Key Locations

Strategic Direction 6.4

Enhancing Kuala Lumpur City Hall's

Non Market Housing

Strategic Direction 6.5 **Providing Housing for Special Groups** 

#### **Promoting Inner City Living**

Strategic Direction 6.6

Increasing Residential Population

Strategic Direction 6.7 **Distributing Densities** 

### Providing Quality Living Spaces

Strategic Direction 6.8

Adopting Safe Community

Guidelines

Strategic Direction 6.9 **Promoting Sustainable Principle in Neighbourhood Planning** 

Strategic Direction 6.10

Providing Quality and Sufficient
Community Facilities

#### Meeting the Needs for Housing of the People

Providing adequate supply of housing to all income levels and ensuring that the living spaces, community needs and it's built environment meets the basic aspirations of living, which are safety and health. Provision of housing should also support economic growth and offering a diversity of housing contributes towards a lively social mix.

### **Strategic Direction 6.1 – Ensuring Adequate Housing Land** *Ensuring adequate housing land to meet housing needs and demand.*

Ensuring there is adequate land allocated for housing the City's population is the key responsibility of KLCH. This is to meet demands of the growing city of Kuala Lumpur. A total of 970 hectares of new land have been zoned to provide housing requirements of the future. This makes up the total housing land in the City to be 7423.84 hectares which are about 30.65% of the total City area.

In line with the concept of sustainable development and KLSP 2020 vision for a world class living environment, future housing development needs to be allocated so as to optimise the use of limited land and the supply of housing, cost and location is determined

through a range of activities including building and planning regulation, local infrastructure and community facilities provision.

The Draft KL City Plan 2020 has identified several areas for development of residential areas and they are prioritised at the following locations:-

#### i. Inner City Centre of Kuala Lumpur

In support of KLSP2020 emphasis on attracting more people to live in the City Centre which will help reduce dependence on private transport, create an increased liveliness in the City Centre and subsequently benefiting

and subsequently benefiting commercial and entertainment enterprises in the City Centre from increased residential areas.

#### ii. Areas served or to be served by public transport particularly rail transport or Transit Planning Zone

Locating residential developments at Transit Planning Zone will ensure optimum use of land and improve people's accessibility to public transport, thus encouraging the use of public transport in an effort to reduce traffic congestion especially into the City Centre.

#### Kuala Lumpur Housing 2020

Strategic Zone	Housing Units	% of Total units	Housing Areas (Hectare)	% of Total Housing Area
City Centre	69,975	11.17	125.90	1.70
Wangsa Maju-Maluri	126,414	20.18	1733.28	23.35
Sentul-Manjalara	126,788	20.24	1354.10	18.24
Damansara Penchala	73,812	11.79	1739.82	23.44
Bukit Jalil-Seputeh	132,271	21.12	1348.73	18.17
Bandar Tun Razak-Sg.Besi	97,057	15.50	1122.00	15.11
Total	626,317	100.00	7,423.48	100.00

### Strategic Direction 6.2 – Offering a Diversity of Housing Products

Offering a diversity of housing that meets the needs of all income levels and the changing needs of the communities.

### Snapshot of City's Population and Housing Requirements

#### **Population Profile**

- 1.6m people in year 2006
- 2.2m people in year 2020
- 563,629 household in year 2020
- Household Size 2000-2010 is 4.1
- Household Size 2010-2020 is 3.9
- Young Population is 25%
- Children 0-14 is 25%

#### **Housing Needs**

- 676,163 houses in 2005
- 534,403 houses needed in 2010
- Additional 91,914 houses needed 2020
- Land for housing 2005 is 6453.65 hectares
- 970.19 hectares of new residential land allocated in the City for year 2020

Fundamentally it is important to provide housing for those living and working in the City. The housing provision must reflect upon the City's population profile and ensure that housing needs of the populations are well met. The different preferences of a changing population will be met from the diversity of housing product.

#### **Affordable Houses**

In doing so, one key factor is the affordability of the City's population. Many who aspire to live in the City, particularly those in lower paid service industries, cannot afford to live in Kuala Lumpur. Housing prices and rents in the City has risen over the last years and there is a need to reclassify the range and prices of housing according to the forecasted future needs and demands as well as the expected economic framework for the City. New housing classification will be introduced and reflected in the composition of housing units to be provided by all future housing projects in the City.

#### **New Housing Classification**

Non-market Housing	Rented public housing owned by Kuala Lumpur City Hall
Affordable Housing	RM 42,001 – RM 74,999
Medium Cost Housing	RM 75,000 – RM250,000
High Cost Housing	>RM 250,000

**Affordable housing** includes low and low medium cost housing to cater for low income groups with a monthly household income between RM1,500 to RM4,000. Low cost housing is houses with a minimum floor space of 650 square feet.

#### **KLSP 2020 POLICY**

**HO2** KLCH shall encourage the private sector to develop a wider choice of innovative housing.

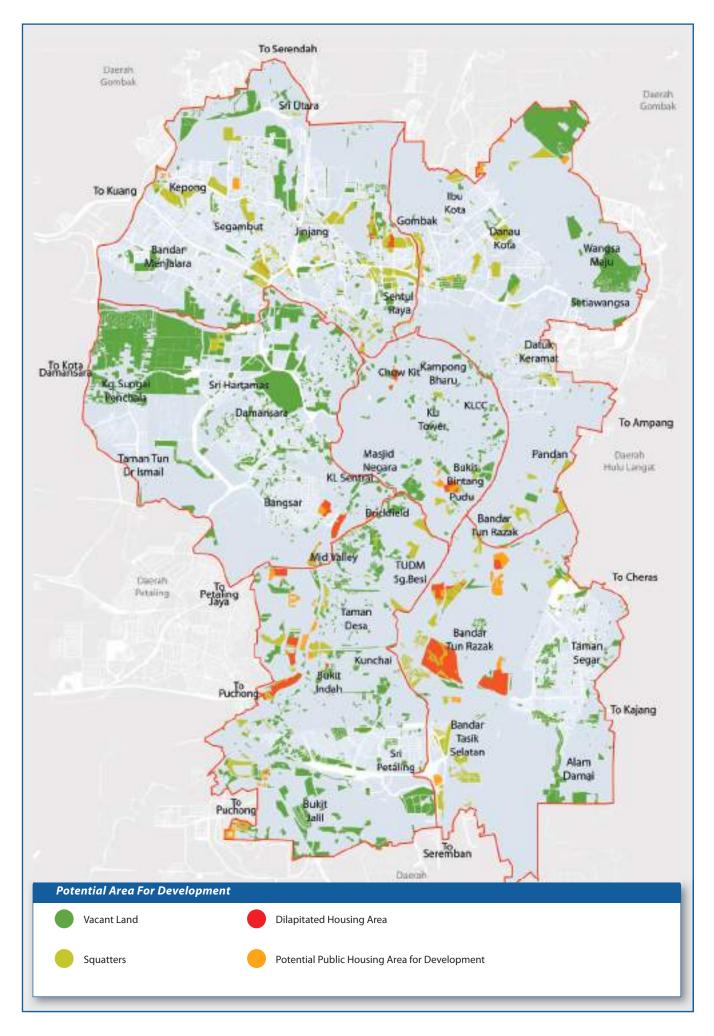
#### **KLSP 2020 POLICY**

**HO13** KLCH shall encourage the building of low-medium and medium cost houses.

#### **KLSP 2020 POLICY**

**HO19** KLCH shall ensure proper distribution of housing units by type.







Based on the new housing price classification and component, it has been estimated that Kuala Lumpur's total needs for housing in the year 2010 shall be 534,403 units and by 2020 it shall be an additional 91, 914 units (total of 626,317 units).

KLCH therefore has a duty to ensure that houses are built according to the different cost classification as targeted.

#### **Younger City Residents**

25% of the city's population is the younger households i.e. 20 to 40 years of age. These younger households has smaller households i.e. singles or with small family size. They are also young professionals who have just started working and are less stable in terms of income earned. Thus it is essential to consider the need of these young professionals in terms of housing types as well as facilities required i.e. active public spaces and sports and recreational centres. Studio type apartments or 2 room apartments are good options for these young city residents and they should be located within inner city of Kuala Lumpur.

In 2005, the total housing available is 676,163 units (this includes temporary houses) and the distribution of houses is more of the high cost housing where 43% of the total housing are high cost housing.

#### **Existing Residential Units By Cost Classification**

Strategic Zone	Low Cost & Low Medium [1]	Medium Cost	High Cost	Temporary Housing	Total (units)	Total (%)
City Centre	9,338	12,042	23,584	2,244	47,208	6.98
Wangsa Maju-Maluri	41,200	46,323	54,535	10,765	152,823	22.60
Sentul-Manjalara	45,744	36,181	66,927	10,885	159,737	23.62
Damansara Pencala	2,375	8,570	47,063	1,708	59,716	8.83
Bukit Jalil-Seputeh	38,152	28,593	60,277	8,820	135,842	20.09
Bdr. Tun Razak-Sg.Besi	51,801	26,598	36,431	6,007	120,837	17.87
Total (units)	188,610	158,307	288,817	40,429	676,163	100.00
Percentage (%)	28	23	43	6		

Note: [1] RM42,000-RM85,000 Homes

### Planning for Housing Needs in Kuala Lumpur

#### Residential Zones in Draft KL City Plan 2020

- 1. Residential R1
- 2. Residential R2
- 3. Residential R3
- 4. Established Housing
- 5. Public Housing
- 6. Mixed Use Residential

### Distribution of Housing by Cost Classification

- Non-market Housing
- Affordable Housing
- Medium Cost Housing
- High Cost Housing

#### 2020 Target for Housing

Non-market Housing 50,105 units

#### Affordable Housing

275,579 units

Medium Cost Housing 244,264 units

**High Cost Housing** 

56,369 units

# INITIATIVES TOWARDS PROVIDING DIVERSITY IN HOUSING

- 1. Facilitate the supply of housing by the private market.
- 2. Work with stakeholders including developers to identify opportunities and solutions to achieve the diversity in housing distribution.
- Monitor the supply and demand of different housing types and levels through approvals of new housing developments.
- Increase the supply of affordable housing through joint venture programmes with private developers, Ministry of Housing, local council and non-profit sector.
- 5. Promote partnerships and develop advocacy strategies for the delivery of housing for very low through to medium income households.

The KLSP 2020 continues this trend and has projected a total of 626,317 units of houses required by the year 2020. Out of this 25% shall be low cost, 28% medium cost and 46% are high cost housing.

The Draft KL City Plan 2020 initiatives towards addressing the issue of declining housing affordability and ensuring there is diversity in the types of houses provided in the City

to meet the new economic growth and its key issues in housing. The Plan now calls for the new housing provide for have higher quantum of affordable housing and a reduction in higher cost housing.

However, much of these housing units has either been absorbed by the numbers of housing application made over the last three years and as such much of these housing categories has met its targets except for medium cost housing.

#### Projected Housing Needs 2020 By KLSP 2020

Strategic Zone	Low Cost	Medium Cost	High Cost	Total (units)	Total (%)
City Centre	10,496	20,992	38,486	69,974	11.17
Wangsa Maju-Maluri	31,604	42,981	51,830	126,415	20.18
Sentul-Manjalara	36,768	32,965	57,055	126,788	20.24
Damansara Pencala	5,167	28,048	40,596	73,811	11.78
Bukit Jalil-Seputeh	35,713	30,422	66,136	132,271	21.12
Bdr. Tun Razak-Sg.Besi	38,823	25,235	32,999	97,057	15.50
Total (units)	158,571	180,643	287,102	626,316	100.00
Total (%)	25.32	28.84	45.84	100	

Source : KLSP 2020





### Strategic Direction 6.3 – Encouraging Quality Affordable Housing at Key Locations

Encouraging quality affordable and medium cost housing in focus areas such as transit stations and areas with high employment.

Quality affordable and medium cost housing is very much needed in the City. This will enhance the quality of life of these communities and make Kuala Lumpur much more liveable. Quality homes include providing the communities with quality community facilities, safe, clean and attractive built environment as well as easy access to markets and employment opportunities. 44% of households in Kuala Lumpur are from the middle income group whose monthly household income is RM 1,500 to RM 3,500. This is a large segment of the City's population and their needs must be given priority.

Locating these houses near existing and planned transit stations would be most ideal as it will reduce travel cost and create the ridership needed to support the transit stations. Thus, new development for affordable housing should be encouraged to be located within the transit station areas.

To encourage these development, most residential zone within Transit Stations has been identified as Residential 3 (R3), where the maximum allowable density is 100 units per acre.

#### **KLSP 2020 POLICY**

**HO1** KLCH shall encourage responsible parties in the housing sector to develop good quality housing and living environments.

#### **KLSP 2020 POLICY**

**HO 5** KLCH shall upgrade the standards of low cost housing.

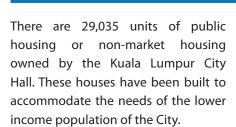
#### **KLSP 2020 POLICY**

**HO 6** KLCH shall implement measures to improve the the environmental quality of low cost housing.





Enhancing Kuala Lumpur City Hall's non-market housing to improve living environment.



It is important to retain selected numbers of this existing public/non-market housing particularly to accommodate the needs of the urban poor which is estimated to be 1.5% of the total population. Though the numbers of urban poor will eventually decrease over time, it is crucial to retain these homes which, can later accommodate transient population.

These KLCH houses should then be refurbished and upgraded to provide proper facilities in order to maintain good quality living standards. Ideally too these non-market housing should be integrate with other types of housing such as medium cost and high cost houses to reduce negative perception towards the non-market housing.

#### **KLSP 2020 POLICY**

**HO 18** KLCH shall, encourage the redevelopment of designated underutilised government land to provide high density housing for public sector employeeshigh density housing for public sector employees.

#### **KLSP 2020 POLICY**

**EN 17** KLCH shall, together with the relevant agencies and private sector, ensure the adequate provision of affordable housing for public sector employees.

#### **Non Market Housing**

Strategic Zone	Non Market Housing				
c .	PA Seri Serawak				
City Centre	PA Seri Selangor 1&2				
	PA Jalan Hang Tuah				
	PA Loke Yew				
Damansara Penchala	PA Seri Pahang				
Bukit Jalil	PA Bukit Kerinchi				
Seputeh	PPR Seri Pantai				
	PA Seri Labuan				
	PA Seri Pulau Pinang				
Bandar Tun Razak	PA Razak Mansion				
Sungai Besi	PA Seri Sabah 3B				
	PA Seri Sabah 3A				
	PA Seri Johor 4C				
	PA Seri Johor 4AB				
	PA Seri Melaka				
	PA Seri Kota				
	PPR Desa Tun Razak				
	Taman Ikan Emas				
	PPR Taman Mulia				
	PA Seri Kedah				
	PA Gombak 2				
Vangsa Maju	PA Seri Perlis I				
Vangsa Маја Лаluri	PA Seri Tioman 1&2				
	PA Seri Perlis II				
	PA Seri Terengganu				
	PA Wangsa Maju R1				
	PA Setapak Jaya 1A				
	PA Wangsa Maju R10				
	PPR Gombak Setia				
	PPR Jelatek				
	PA Desa Rejang				
	Kelompok Kuang/Kenar				
Sandral Manustalana	PA Seri Pangkor I				
Sentul Mernjalara	PA Seri Perak				
	PA Seri Kelantan				
	PA Seri N.Sembilan				

#### List of Basic Common Facilities to be included in Non Market Housing

- Convenience Store
- Kindergarten/Day Care Centre
- Launderette
- Cafe
- Playground
- Surau
- Library with Internet facilities
- Community Hall (integrated with multipurpose hall)

### Strategic Direction 6.5 – Providing Housing for Special Groups

Providing for housing needs of the 'Special Group' in suitable location

Housing needs for 'Special Group' communities include housing for persons with disabilities, singles and the ageing population. These special group communities, though a minority, comprise of about 1% of the City's population.

In making the City liveable, providing housing for this group is important and efforts should include partnership with various parties to ensure this community's needs is not neglected. Initiatives include:

# Community hostels for rent to transient workers in the City

Kuala Lumpur being the centre of employment experience rural-urban migrants in search of work in the city. These young and single group of people most often rent rooms or houses that are cheap in the city. Preferred areas such as Kampong Bharu and Kampung Datuk Keramat are well known areas for cheap rental accommodation, provided by the landlords or house owners wanting the extra income. One of the key contributing factors to squatters' settlement is also the need for housing from this group of people. Thus the provision of hostels or boarding homes can be a solution and this should be encouraged within the city of Kuala Lumpur. Hostels would be a better option for these people as temporary homes whilst they find their feet and earn for themselves enough to rent/purchase better accommodation.

# Making residential developments accessible and habitable by all members of the community

In creating an inclusive community for the City, it is important to make residential development accessible and habitable by all groups of the community. Adopting universal design principles in residential development would not only make it accessible for the special group but for all walks of communities.

Thus residential development in Kuala Lumpur must meet the mobility standards of people with disability especially the

#### **KLSP 2020 POLICY**

**EN 16** KLCH shall together with the relevant agencies and the private sector, ensure the adequate provison of housing for special needs group affordable housing for public sector employees.



affordable residential development as well as the non-market residential. Provisions for these standards can be applied as follows:

- 1.2% of total units or 4 units for every 350 units are designed to accommodate the needs of this special group community.
- For apartment type residential Ground floor units must be built to mobility standards i.e. accessible to people with disability.
- Provision of ramps from public use areas (car park, corridors, etc) to the main door of the unit must be provided.
- Units to comply with Standard in MS1331:2003 Code of Practice for Access of Disabled Persons outside Buildings must be complied.
- Certification of Fitness (CF) shall not be issued unless the developments incorporate the above.



#### **KLSP 2020 POLICY**

**HO 20** KLCH shall implement majors to increase the number of medium and high cost residential units in the City Centre

#### **KLSP 2020 POLICY**

HO 21 KLCH shall improve the quality of low cost houses and low cost housing environment in the City Centre

#### **INITIATIVES TO PROMOTE INNER CITY LIVING**

Allowing the conversion of commercial spaces, especially office blocks, into residential or service apartments;

Providing planning incentives for residential developments such as reduction of car parking standards (as most residents would be encouraged to walk and to use public transport) for land within Transit Planning Zone:

Allow for street level commercial spaces, especially for blocks near major route networks, within residential developments;

Increase the supply of well-located affordable and medium cost housing and distribute it to various areas in KL City Centre'

Promoting Inner City Living
Providing adequate supply of housing to all income levels and ensuring that the living spaces, community needs and it's built environment meets the basic aspirations of living, which are safety and health. Provision of housing should also support economic growth and offering a diversity of housing contributes towards a lively social mix.

#### Strategic Direction 6.6 – Increasing Residential Population *Increasing residential population in the City Centre*

The need to encourage more people to live in the City is important. This is to support the KLSP 2020 call for housing in the City Centre to enhance the business and financial precincts.

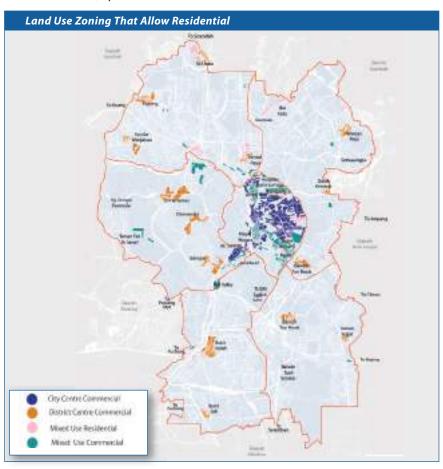
KLCH will facilitate private sectors who want to build residential development in inner city Kuala Lumpur. Several zones allow for residential development and this includes:-

· City Centre Commercial Zone i.e. areas zoned for commercial in inner city Kuala Lumpur, allows for multi dwelling residential i.e. condominium, apartments etc.

- Mixed Use Residential 70% residential use
- Mixed Use Commercial 30% residential use

212 hectares of land has been allowed for residential use in the City Centre area and this shall support the housing development in these areas.

Here plot ratio's is assigned to these zones and development should look into a more comprehensive and integrated manner where public realms must be provided to enhance the development in Kuala Lumpur.



#### **Strategic Direction 6.7 – Distributing Densities**

Distributing densities in Kuala Lumpur to meet housing needs as well to support economic activities in the City.

#### **KLSP 2020 POLICY**

**EN 19** KLCH ensure the proper distribution of housing units by type.

52% of the city's housing stock in Kuala Lumpur is made up of apartments. The density map of Kuala Lumpur shows that the City has a high average housing density of about 108 persons per hectare. This signifies that Kuala Lumpur is a compact city. This has not been a deliberate strategy as the move towards more high rises and apartment living was mainly due to the rising land costs within the city boundaries, due to lack of land.

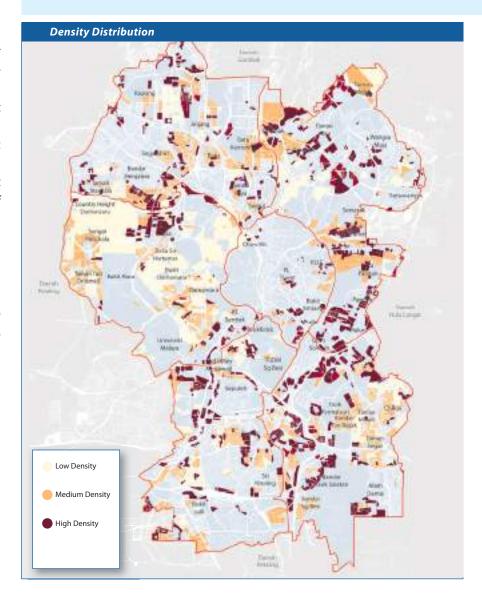
High density and high-rise living should continue in Kuala Lumpur to make it a compact city. These developments should be encouraged to concentrate mainly at transit points of public transportation. In this way, high ridership of public transport will be ensured. Being close to transit points throughout the City will also reduce the use of private transportation and the need for a high number of car parking spaces both within the residential areas as well as in the City. Living close to public transportation will mean faster commuting time into the City and less time fighting traffic. All in all, high-rise, high-density living leads to a more sustainable city.

#### **INITIATIVES**

Provide adequate and appropriate land for the development of the housing product. These should include the possibility of redevelopment opportunities of the dilapidated housing sites within city centres;

Fast track approval for affordable and medium cost high rise development especially located near public transport station; and

Develop a strict minimum standard housing guideline for high rise developments to provide well-designed housing at strategic sites.



#### **Providing for Quality Living Spaces**

Successful urban communities are places where people want to live and will continue wanting to live. Central to this is the provision of quality living in safe and attractive neighbourhoods that offer shops, employment, local leisure and good open spaces within walking distance.

### **Strategic Direction 6.8 – Adopting Safe Community Guidelines** *Adopting Guidelines that Promote a Safe Community Environment*

One of the elements of world class cities is the low levels of crime and high perceptions of personal safety. Safe cities attract greater investments and economic activities. When people and businesses feel safe, there will be greater prosperity. Thus creating a safe living environment is pertinent to achieving A World Class City status.

In creating a safe living environment, the responsibility to ensure the security and safety of the City is not only depending on the police force, but is also the responsibility of city planners and urban designers as well as developers. They have the role to create a safe environment for the community. Hence, objectives towards the creation

of safe living environments especially in new developments for residential areas and other parts of the City, guidelines on crime prevention through environmental design shall be considered.

It is recommended that the concept crime prevention through environmental design (CPTED) is formulated in greater details to guide in the planning of townships residential neighbourhood. The need to incorporate such process right from the start is KLCH's responsibility as preventing crime through designing a physical environment can positively reduce crime at common public area which becomes an easy area to attack; create safe and better environment for Kuala Lumpur's citizen and tourists.

#### Planning for Safe Communities Key Strategies

#### **Natural Surveillance**

A planning concept that maximize visibility of people, public areas and building entrances, door and windows that look onto public areas.

#### **Territorial Reinforcement**

Utilizing landscape plantings to define property lines and distinguish public with private spaces.

#### **Natural Access Control**

Design concept that deny access to crime targets.

#### **KLSP 2020 POLICY**

**EN 10** KLCH shall eradicate all squatter settlements and long house.

#### **KLSP 2020 POLICY**

**EN 11** KLCH shall encourage the improvement and redevelopment of dilapidated housing areas.

#### **INITIATIVES**

- · KLCH to develop CPTED guidelines.
- New Housing development must adopt these guidelines.
- Development areas that require CPTED are:-
  - City Centre
  - District Commercial Centre
  - Neighbourhood Commercial Centre.
  - Mixed-use residential/ commercial development
  - Medium and high rise density residential areas
  - Parks, open spaces and public accessible area
  - Community Centre
  - Sports, recreation and entertainment area

CPTED guidelines may include design elements with significant influence on the safety and security of spaces such as:-

- Visibility
- Access
- Amount of natural light
- Location and proximity to other facilities
- Orientation (where the building faces)
- Landscaping
- Buildings materials and finishes
- Building design and cubicle configuration
- Management, maintenance and security



### Strategic Direction 6.9 – Promoting Sustainable Principles of Neighbourhood Planning

Promoting the Sustainable Principles of Neighbourhood Planning to achieve quality living spaces.

Neighbourhood planning has always been the traditional practice in plan making for residential township development. This however has evolved when neighbourhood planning starts to responds to market demands and community sustainability most often is taken for granted.

In pursuit of liveability for Kuala Lumpur, neighbourhood planning must be revived and adopted fully in any plan making process. The sustainable principles of neighbourhood planning will result in better planned residential where community liveability is not compromised.

#### Walkability

Walkability is an important principle. It describes upon a 5 minutes walking radius that also can be used to determine the neighbourhood size. Walkability is important as it brings about access to facilities and parks within a 5 minute walking distance and this will reduce the need to use their cars. Thus a neighbourhood design must be as permeable as possible, to allow easy access with no barriers along the walking routes/paths.

### Mixed Uses and Varying Residential Densities

One of the factors of sustainable housing/residential is mixed uses and diversity in housing types. This means having different types of residential in one neighbourhood community.

Mixed uses or mixed type of residential units will create a more vibrant social structure of the community; it will also create an interesting urban structure for the neighbourhood as there is varying densities and varying heights of development that will make the neighbourhood interesting.

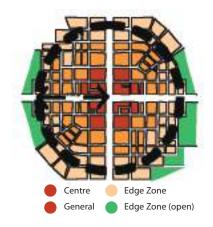


Neighbourhood Design Concept within walking distance

Mixed uses and varying residential densities also allows for economic sustainability as its densities allows it to support retail activities, public transportation and other activities.

### **Sustainable Neighbourhood Design Elements**

Many people want to live in an area where a sense of place and belonging is evident. In order to create this sense of place, there must be design elements that will evoke a sense of being, identity and belonging. Paths, edges, districts, nodes, and landmarks are the elements that give form to a neighbourhood by evoking an image which is recognized by the community.



Neighbourhood Structure - Divided into Centre, General & Edge Zone

#### **INITIATIVE**

Provide incentives for development that promote mix product of non-market housing and other development;

Enhance paths, roads, lanes, walkways which are the main corridors through which most of the community activities go through. They provide the linkage within the area as well as between areas; Ensure footpaths are connected and remain unobstructed throughout the neighbourhood;

Make edges demarcating neighbourhoods via landscaping features; edges are another feature that marks distinct neighbourhoods, as they provide visual clarity that evokes an identity;

Create nodes at the centre or core of an area which also provide neighbourhood identity;

Provide for street design with interconnected circulation that creates focal points for reference and community identity;

Provide planting of shady trees that will reinforce pedestrian scale along neighbourhood streets.

#### **KLSP 2020 POLICY**

**CF 5** KLCH shall, in consultation with the approariate authorities and agencies ensure that all community facilities and services are adequately distributed according to the needs of the population, and that they are conveniently accessible to all members in the public.

#### KLSP 2020 POLICY

**CF 2** KLCH shall, in consultation with the approariate authorities and agencies ensure that community facilities are adequately planned and provided for in accordance with the new planning standards.



### Strategic Direction 6.10 – Providing Quality and Sufficient Community Facilities

Integrating residential development with community facilities to achieve cohesiveness and stimulate communities interaction.

A neighbourhood as a living space will not be complete without facilities that the people living in them can use. People need facilities to provide their sustenance and daily needs. Thus, community facilities form a very tangible and pertinent element of the urban environment. These community facilities will be provided by different agencies as well as by the private sectors. It is thus important KLCH to monitor the implementation of the community facilities.

## Ensure adequate provision of facilities at the appropriate locations

It is important that all new development provide adequate facilities at locations which are within the walkable distance of a community.

Community Facilities to be provided in the Neighbourhood includes:

- i. Community Library
- ii. Multi-Purpose Hall
- ii Post Office
- iv. Bill Paying Facilities
- v. Surau
- vi. Clinic
- i. Recreational Facilities
- ii. Police Beat
- iii. Day Care Centre
- iv. Food Court
- v. Information Centre
- vi. Retail and Office
- vii. Public Toilet

### Community Facilities within Commercial and Public Buildings

Many of these community facilities mean the take up of land in an area with high land prices, such facilities may not be built nor land contributed by the land owners/ developers. Furthermore, to attract more people to live in the city centre, community facilities must be provided. Yet, land prices are so exorbitant that such the provision of such facilities may be prohibitive. Innovative ways are encouraged to integrate facilities within other uses, such as within commercial buildings.





#### **Projected Needs for Community Facilities, 2010-2020**

	Projected Numbers of Community Facilities																	
Community Facilities	City Centre			Damansara Penchala		Sentul Menjalara		Wangsa Maju Maluri		Bandar Tun Razak Sg Besi		Bukit Jalil Seputeh						
	2010	2015	2020	2010	2015	2020	2010	2015	2020	2010	2015	2020	2010	2015	2020	2010	2015	2020
District Police HQ	1	1	1	1	1	1	2	2	2	2	2	2	2	2	2	2	2	2
Police Station	4	5	5	4	5	5	8	8	9	8	9	9	6	6	7	9	9	9
Police Post	10	11	12	11	12	13	20	21	22	21	22	22	15	16	17	22	22	23
Fire Station	1	2	2	2	2	2	3	3	3	3	3	3	2	2	2	3	3	3
Neighbourhood Watch Centre	14	15	16	15	16	17	26	28	30	28	29	30	20	22	23	29	30	31
Hospital	1	1	1	1	1	1	0	0	0	0	0	0	1	1	1	0	0	0
Clinic	8	9	10	9	10	10	16	17	18	17	17	18	12	13	14	17	18	19
Mosque	8	9	10	9	10	10	16	17	18	17	17	18	12	13	14	17	18	19
'Surau'	20	23	25	22	24	26	39	42	45	42	43	44	30	32	34	43	45	46
Other Religious Use	20	23	25	22	24	26	39	42	45	42	43	44	30	32	34	43	45	46
Secondary School	10	11	12	11	12	13	20	21	22	21	22	22	15	16	17	22	22	23
Primary School	20	23	25	22	24	26	39	42	45	42	43	44	30	32	34	43	45	46
Kindergarten	41	45	49	44	48	52	78	84	89	84	86	89	61	65	68	86	90	93
Community Centre	10	11	12	11	12	13	20	21	22	21	22	22	15	16	17	22	22	23
Market	8	9	10	9	10	10	16	17	18	17	17	18	12	13	14	17	18	19
Public Library	5	6	6	6	6	6	10	10	11	10	11	11	8	8	9	11	11	12
TOTAL	181	204	221	199	217	231	352	375	399	375	386	396	271	289	307	386	400	414

#### **INITIATIVES**

Review the existing location and provision of public facilities to provide well spread and appropriate facilities within city centres;

Facilities such as libraries, police beat base or police stations, community halls, market, kindergarten, crèches, government clinics, and neighbourhood watch centres be centrally located within shopping centres or complexes, which should be located within transportation nodes or hubs;

Further develop and improve existing public facilities to promote concentration of new housing development in city centres;

Burial grounds for Kuala Lumpur be shared on a regional / metropolitan basis with adjacent or peripheral local authorities;

Schools allowed to be multi-storey facilities with comprehensive safety features, incorporating indoor and rooftop recreational facilities, such as those in Hong Kong;

A policy change be undertaken to allow the public/ community to use school recreational grounds such; and

Encourage 24-7 convenience stores and supermarkets for the use of residents and visitors to the city.

#### **KLSP 2020 POLICY**

**CF 17** KLCH shall liase with the relevant authorities to provide adequate government health facilities in approapriate locations.

#### **KLSP 2020 POLICY**

**CF 16** KLCH shall, in consultation with the relevant agencies, ensure that there is adequate provision of school for disabled children.

#### **KLSP 2020 POLICY**

**CF 18** KLCH shall in consultation with the approapriate authorities, ensure that suffient police stations, police posts and neighbourhood watch centres are provided and adequately distributed.

# Protecting and Enhancing the Environment

As pressure for development in Kuala Lumpur become more intense, the need for protection of the environment becomes greater. Ensuring environmentally sound and sustainable development and achieving world-class environment becomes more challenging and demanding for Kuala Lumpur.



Trend all over the world has shown that sustainability is the way forward for cities. After years of development and taking cognizance of such trend, it is vital for Kuala Lumpur to adopt effective land use planning, wise resource consumption and active wastes reduction in order to minimize impacts to the environment. Declining urban environmental quality coupled with increasing environmental issues and dwindling natural resources makes it imperative for KLCH to embrace concept of sustainability in managing Kuala Lumpur.

The key to improving the urban environment lies in protecting its natural environment and enhancing the environmental quality of Kuala Lumpur. Principles of green infrastructure shall be adopted to improve level of urban services for drainage, sewerage, public transportation and solid waste management. In addition, green spaces shall be considered as part of urban infrastructure.



TAN MEI KEE
SMK Kepong Baru
1st Winner (Category C)
of Drawing Competition in Conjunction with Preparation
of Draft KL City Plan 2020.

### **Conserving Natural Environment**

Strategic Direction 7.1

Protecting Environmentally Sensitive Area

Strategic Direction 7.2 **Preserving Forest and Wildlife Reserves** 

Strategic Direction 7.3

Preserving Steep and Hilly Areas

Strategic Direction 7.4

Preserving Lakes and Former Mining
Ponds

### **Enhancing River Value and River Water Quality**

Strategic Direction 7.5 **Regulating Development along River Reserve** 

Strategic Direction 7.6 **Improving River Water Quality** 

### Ensuring Safety and Best Use of Contaminated Sites

Strategic Direction 7.7 **Regulating Use of Former Landfill Site** 

#### Improving Urban Environmental Quality

Strategic Direction 7.8

Managing Noise to Acceptable Level

Strategic Direction 7.9 **Improving Air Quality** 

#### **Conserving Natural Environment**

Hills, residual forested areas and rivers are valuable natural resources that must be protected and conserved for sustainability of Kuala Lumpur.

### Strategic Direction 7.1 – Protecting Environmentally Sensitive Area

Protect and properly manage areas that are environmentally sensitive to safeguard against hazard risks of development.

Identification and protection of environmentally sensitive areas (ESAs) in Kuala Lumpur is critical to protect areas that are sensitive to development. Protection and conservation of these areas are deemed important to meet the following objectives:-

- Protection against insensitive urban development;
- Protection of limited natural assets of Kuala Lumpur such as forest reserves and wildlife reserve and the need to provide more green areas to support urban biodiversity as well as green lungs for the City;
- Protection from hazard risks such as erosion and land slides that could result in loss of life and properties;
- Protection against flood risk and improved drainage; and
- Fulfill the recreational needs for urban dwellers and provide urban landscape for Kuala Lumpur.

Protection of the natural environment as provided in the Draft KL City Plan 2020 is as follows:-

- Identification and classification of environmentally sensitive areas (ESAs) as a framework for an integrated land development and protection of the environment;
- Control land use development, development intensity and implementation via land use zoning, density and plot ratio controls and environmental protection zone (EPZ). Land located within EPZ is subjected to a more stringent control to assess its suitability for development in terms of type, scale and intensity of development.

#### KLSP 2020 POLICY

**EN 13** KLCH shall conserve residual forest areas and maintain a sustainable variety and population of wildlife within the City boundaries.

#### KLSP 2020 POLICY

**EN 18** KLCH shall designate environmentally sensitive areas and prepare guidelines for their control and management.



NPP 18: Environmentally Sensitive Areas (ESA) shall be integrated in the planning and management of land use and natural resources to ensure sustainable development.

### ENVIRONMENTALLY SENSITIVE AREA (ESA) FOR KUALA LUMPUR

Environmentally sensitive area (ESA) refers to area that is highly sensitive to development or changes in land use and needs to be conserved for its limited biodiversity value, to be maintained and enhanced for its life support and recreational functions and/or to be protected to mitigate the hazard risks to the surrounding areas.

### ESA RANK 1: TOTALLY PROTECTED AREAS

No development except for limited eco-tourism, research activities and remediation/rehabilitation work.

- 1. Forest reserves
- 2. Contaminated sites

#### ESA RANK 2 : CONSERVATION AREAS

No development except for low impact recreational activities (Refer to Chapter 4 in KLDCP 2008).

- 1. Rivers and ponds
- 2. Residual forested areas
- 3. Hill land

# RANK 3: CONTROLLED DEVELOPMENT AREAS

Development subject to compliance to requirements specified by KLDCP 2008 (Refer to Chapter 4 in KLDCP 2008).

1. Hillside





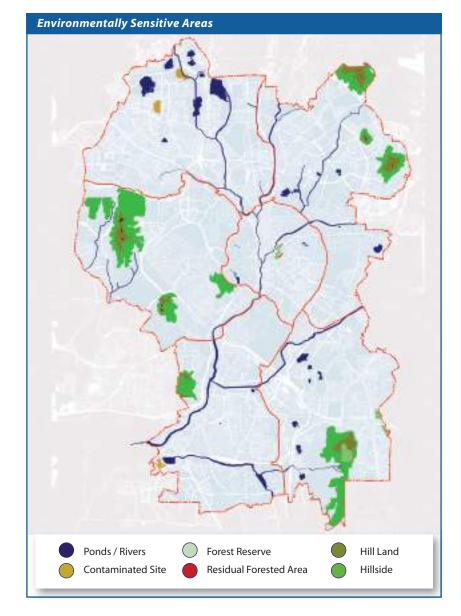
### Strategic Direction 7.2 – Preserving Forest and Wildlife Reserves

Preserving forest and wildlife reserve is important in maintaining habitat for urban wildlife, providing green lung and urban sanctuary for Kuala Lumpur.

With only 61 hectares of forest reserve remaining, protection and conservation of forest and wildlife reserve of Kuala Lumpur must be accorded highest priority in the development planning and management of Kuala Lumpur. Apart from its biological importance in providing habitat for urban wildlife, it serves as green lung and urban

sanctuary for the community besides improving air quality.

These reserves are protected through Land Use Zoning Control and its associated use classes controls where only limited and very low impact eco-tourism are allowed.





Bukit Nanas Forest Reserve is one of the oldest forest reserves in Malaysia. It was gazetted as Wildlife and Bird Sanctuary Reserve in 1934.

#### **SPECIAL PLANNING CONTROL FOR ESA**

Control on developments in areas ranked as ESA is through additional planning control in the KLDCP 2008 which designates an Environmental Protection Zone (EPZ) within the development control plan for Kuala Lumpur.

#### **RESIDUAL FORESTED AREA**

Areas surrounding existing forest reserves in:

- 1. Bukit Nanas Forest Reserve
- 2. Bukit Sungai Puteh Forest Reserve
- 3. Bukit Sungai Besi Forest Reserve

#### **HILL LAND**

Areas in:

- 1. Bukit Kiara
- 2. Bukit Arang
- 3. Part of Bukit Batu Tabor
- 4. Bukit Dinding
- 5. Bukit Sg. Besi
- 6. Bukit Gasing
- 7. Bukit Wangsa Maju

#### HILLSIDE

Areas in:

- 1. Bukit Kiara
- 2. Bukit Lanjan
- 3. Bukit Arang
- 4. Bukit Batu Tabor
- 5. Bukit Dinding
- 6. Bukit Sg. Besi
- 7. Bukit Gasing
- 8. Bukit Persekutuan
- 9. Bukit Wangsa Maju

#### RIVERSIDE CORRIDOR

Areas along:-

- 1. Sungai Klang
- 2. Sungai Gombak
- 3. Sungai Jinjang
- 4. Sungai Batu
- 5. Sungai Bunus
- 6. Sungai Penchala
- 7. Sungai Kayu Ara
- 8. Sungai Kerayong
- 9. Sungai Kuyoh

#### **Strategic Direction 7.3 – Preserving Steep and Hilly Areas**

Preserving the hills of Kuala Lumpur that form part of the natural urban landscape of the City for protection against geo-hazards and fulfilling recreational needs of urban dwellers.

The hills of Kuala Lumpur are under tremendous threat for development. Recognizing its importance, the hills shall be protected against development via Environmental Protection Zone (EPZ) control. Under such zoning, development and types

of activities need to adhere to specific guidelines and requirements. addition, development can only be approved upon fulfilling requirements imposed by Special Committee on Environmentally Sensitive Areas (Jawatankuasa Penyelarasan Pembangunan Kawasan Sensitif Sekitar Alam Wilayah Persekutuan).

#### **KLSP 2020 POLICY**

**EN 6** KLCH shall not permit development on hillside with slope that exceeds the allowable level, rules and regulation set by the Federal Government.

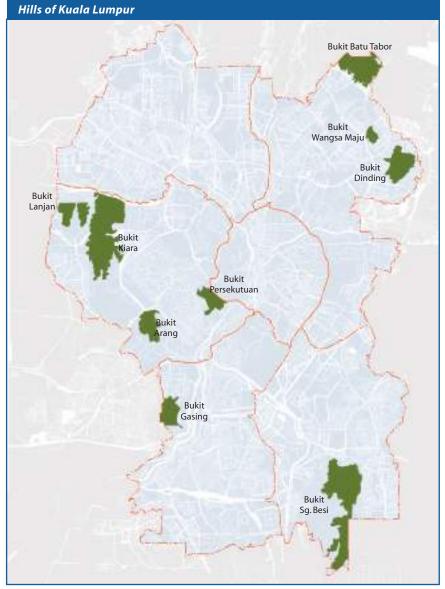
#### **KLSP 2020 POLICY**

**EN 7** KLCH shall ensure that geotechnical study is carried out for all hillside developments.

#### **HILLS OF KUALA LUMPUR**

	Hill	Highest Peak
1.	Bukit Kiara	240 metres
2.	Bukit Lanjan	130 metres
3.	Bukit Arang	140 metres
4.	Bukit Batu Tabor	320 metres
5.	Bukit Dinding	290 metres
6.	Bukit Sg. Besi	295 metres
7.	Bukit Gasing	135 metres
8.	Bukit Wangsa Maju	190 metres
9.	Bukit Persekutuan	105 metres







# Strategic Direction 7.4 – Preserving Lakes and Former Mining ponds

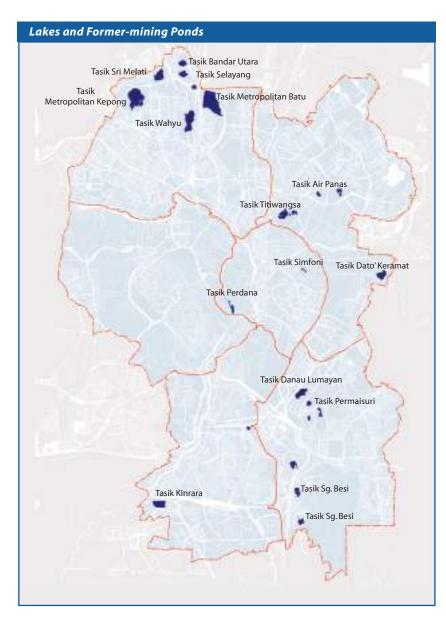
Preserving existing lakes and former mining ponds to improve drainage system and mitigate flood as well provide recreational benefits to the community.

#### **KLSP 2020 POLICY**

**EN 4** KLCH shall ensure the landscaping of rivers and the rehabilitation of ex-mining land.

As part of the initiatives to improve drainage infrastructure and alleviate flooding problem, existing lakes and former mining ponds should be preserved and protected. A total of 22 lakes have

been identified for such purpose. In addition, such lakes shall also be used for recreational purposes subject to compliance to permissible use as stated in the Zoning Schedule.





#### **River Amenity Value and River Quality**

Rivers are valuable natural resources for human life and the environment and can become main feature and pride to Kuala Lumpur.

# Strategic Direction 7.5 – Regulating Development along River Reserve

Integrating and enhancing potential and amenity value of rivers and their reserves for the benefit of the general public.

Draft KL City Plan 2020 acknowledges the importance of rivers other than being a mere conduit to convey stormwater and therefore places greater emphasis on improving and enhancing amenity and aesthetic values of major rivers. Measures to improve the quality are:-

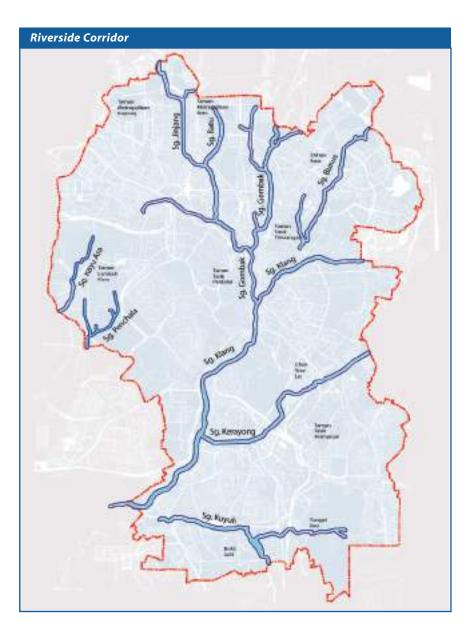
- Designating Riverside Corridor where development on land located within the corridor will need to be more sympathetic to the river by taking measures to front the river and to provide public spaces fronting the river.
- Removing industrial activities along river reserves and within river corridor.
- Controlling land use and activities on land located within Riverside Corridor to eliminate polluting activities directly abutting rivers.



#### **RIVERSIDE CORRIDOR**

Land located within 50 metres from river reserves of the following rivers where specific guidelines are prescribed in the KLDCP 2008:-

- 1. Sungai Klang
- 2. Sungai Gombak
- 3. Sungai Jinjang
- 4. Sungai Batu
- 5. Sungai Bunus
- 6. Sungai Penchala
- 7. Sungai Kayu Ara
- 8. Sungai Kerayong
- 9. Sungai Kuyoh



#### **INITIATIVES TO IMPROVE RIVER WATER QUALITY**

- 1. River catchment management in terms of development planning and controls for various land uses and activities:
  - Effective development planning in terms of land use zoning and propercontrolofpollutingactivities within catchment of major rivers should be implemented. Some of the measures include installation of appropriate instruments or equipment to ensure discharges into rivers meet the relevant requirements.
  - Protect suitable natural environment or riverine vegetation along major rivers. Sg Kuyoh and Sg Kerayong have been identified for this purpose.
- 2. Efficient management of storm water or surface runoff to prevent pollutant from entering the river system:
  - Installation of gross pollutant traps or trash screens at major drainage outlets into Sg Klang, Sg Gombak, Sg Jinjang, Sg Kuyoh and Sg Kerayong.
  - · Enhanced aeration by creation of small water falls or weirs along Sg Klang and Sg Gombak.

#### 3. River water quality monitoring:

- Installation of water quality monitoring stations along major outlets of major rivers for long term water quality monitoring database. The monitoring shall be carried out by KLCH.
- Set-up River Protection and Monitoring Unit within KLCH to monitor, assess and implement water quality improvement programmes.
- Initiate sewage treatment plants monitoring programme to ensure compliance to environmental requirements.
- Initiate Industrial Areas Monitoring Programme to monitor quality of discharge from industrial areas. Some of the industrial areas that have been identified include:
  - a) Chan Sow Lin/Sq Besi Industrial
  - b) Segambut Industrial Area
  - c) Kepong Industrial Area
  - d) Setapak Industrial Area

#### **Strategic Direction 7.6 – Improving River Water Quality**

Implement measures to tackle major sources of pollution which include sewage discharge, industrial effluent discharge and dumping of wastes into the river system.

The status of the river water quality is very much related to land use and activities taking place within the river catchment. Being the federal capital and highly urbanized area, the focus of the Draft KL City Plan 2020 is to

#### **KLSP 2020 POLICY**

EN 5 KLCH shall initiate an appropriate strategy integrating the major rivers and abandoned mining ponds as an amenity and feature of the City's urban design.

#### **KLSP 2020 POLICY**

**EN 8** KLCH shall, in co-operation with the sewerage concessionaire, ensure that tree shall no be further discharge of untreated domestic wastewater into the rivers and drainage system.

minimize and eliminate major polluting land use and activities via prudent land use zoning and development controls, control of discharges and river monitoring.

#### **KLSP 2020 POLICY**

**EN 9** KLCH shall investigate the feasibility of new approaches to increase oxygenation, aeration and water quality of the City's rivers to support aquatic life in rivers.

#### **KLSP 2020 POLICY**

EN 10 KLCH shall re-active the rehabilitation programme of Sungai Klang and Sungai Gombak.

#### **KLSP 2020 POLICY**

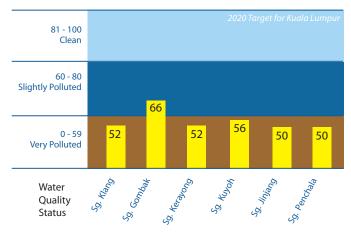
**EN 11** KLCH shall not approve permanent development involving structures in river reserves.

#### 2020 TARGET FOR SELECTED RIVERS

To improve water quality of major rivers such as Sq Kelang, Sq Gombak. Sq Kerayong, Sq Jinjang, Sq Penchala and Sq Kuyoh from polluted to clean by implementing pilot projects within the river subcatchments.

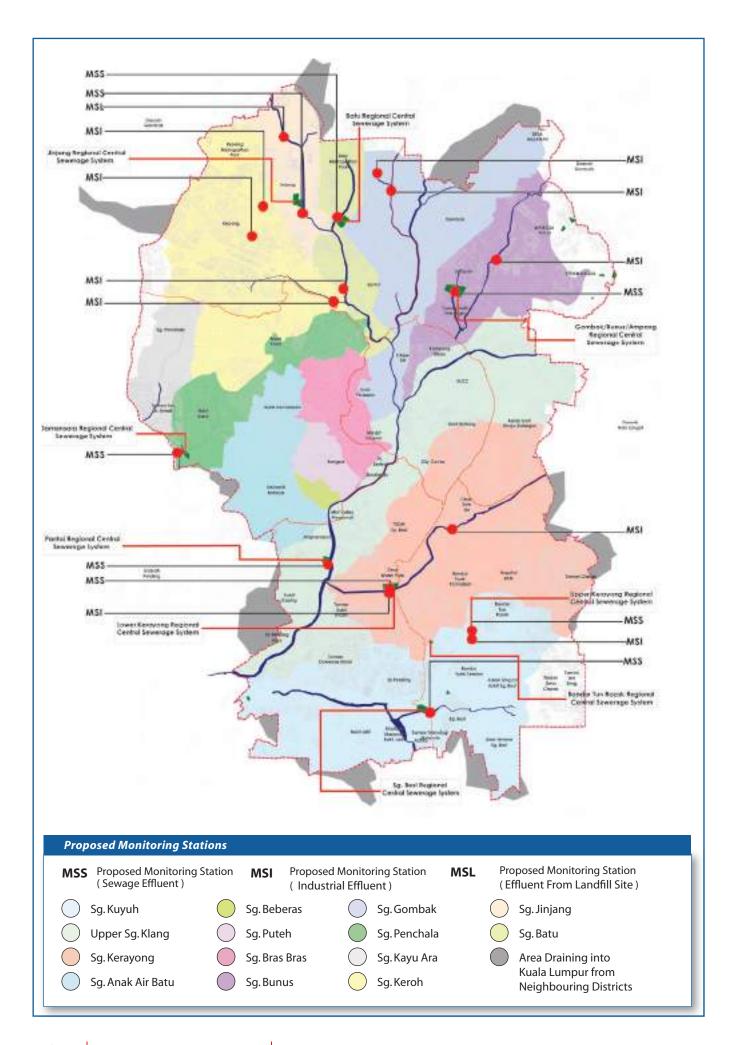
#### **River Water Quality Status 2004**

Water Quality Index Classification



Major sources of pollution include sewage effluent, industrial discharge and runoff from construction and development sites. Other sources include dumping of wastes or rubbish into the rivers and waterways as well as sullage discharge from residential and commercial areas, from markets and food courts or stalls. Pollutants from such activities include litter/rubbish, organic material, oil and grease, nutrients and sediments.

SOURCE: MALAYSIA ENVIRONMENTAL QUALITY REPORT, DOE, 2004.



# Ensuring Safety and Best use of Contaminated Sites

Proper management of contaminated sites in Kuala Lumpur is crucial due to previous land uses that may have contaminate these areas and pose health and safety risks to the people.

# Strategic Direction 7.7 – Regulating Use of Former Landfill Site

Designate former landfill as green area.

Draft KL City Plan 2020 has zoned the former landfill site at Taman Beringin as green area where proper closure and rehabilitation must be undertaken. Strict and close monitoring for leachate and landfill gases must be carried out as the site is surrounded by residential areas. The site will remain as green area for at least 20 years prior to other use including active recreational activities or redevelopment.

Some of possible contaminated sites include former landfill sites at Taman Beringin, former industrial land, former depot and workshops.

Redevelopment of such areas requires detailed site investigation to determine contamination level (if any) and remediation measures that will be required to ensure such areas can be developed and risks to the community are minimal.

Initiatives ensure proper management potential of contaminated include sites policy on contaminated sites, identification of potential contaminated sites and formulation of guidelines for management of contaminated sites.

#### Improving Urban Environmental Quality

Improve urban environmental quality requires holistic and integrated approach. It requires implementation of direct and indirect measures and by working together with relevant agencies.

### Strategic Direction 7.8 - Managing Noise to Acceptable Level

Controlling high-noise generating activities to reduce impacts to the community.

Noise can be a major nuisance in urban areas. Excessive noise levels can cause stress and other related health and social problems to the community. The main source of noise has been identified to be from traffic or vehicular movement.

Spatial and physical initiatives towards reducing noise impact entail controlling, segregating and eliminating high-noise generating activities in noise sensitive areas. These include:-

- 1. Confine high noise generating activities such as late night entertainment and other 24 hour activities within selected areas through zoning and associated permissible activities.
- Introduce noise reducing measures at strategic location and noise-sensitive areas to alleviate noise level. Some of the measures include vegetated buffer zone and noise barrier.

#### **KLSP 2020 POLICY**

**EN 17** KLCH shall ensure that contaminated land be treated and designated as sensitive green areas.



# DESIGNATED LOCATIONS FOR NOISE GENERATING LATE NIGHT AND ENTERTAINMENT ACTIVITIES

- Permitted in City Centre Commercial Zone (CCC).
- In District Centre Commercial (DCC) zones, Mixed Use Residential (MXR) zones and Mixed Use Commercial (MXC), the activity may be considered by KLCH subject that the proposed activity satisfies conditions imposed as per the Zoning Schedule.

#### **KLSP 2020 POLICY**

**EN 16** KLCH shall implement measures to reduce noise level in the City.



# INITIATIVES TO IMPROVE AIR QUALITY

- Improve integration of land use and transport policy that support better public transportation and land use zoning through mixed use developments and transit planning zones
- Retain and enhance green spaces and vegetated areas within the City.
- Increase efforts for tree plantings and creating effective green spaces along identified hot spot and to promote better pedestrian linkages.
- Improve and increase efficiency of public transportation system to minimize number of private vehicles on the road.
- Monitor and assess air quality at key areas such as industrial areas, bus terminal or depot and other high traffic areas.

#### **KLSP 2020 POLICY**

**EN 15** KLCH shall, in co-operation with the Department of Environment, undertake measures to reduce air pollution in the City.

#### **Strategic Direction 7.9 – Improving Air Quality**

Good air quality is vital for the health and well being of the urban community.

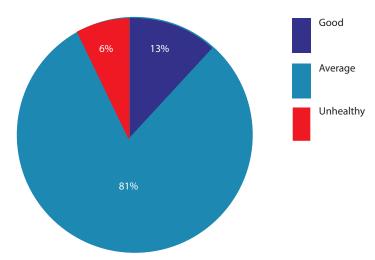
Ensuring good air quality is challenging due to numerous sources of pollution, both stationary and mobile sources, and its transboundary nature. For the city of Kuala Lumpur, vehicular emission has been identified as

the key pollution sources. Therefore, fundamental change in terms of transportation policy and significant improvement of the existing public transportation system is crucial in tackling the air quality issue.

#### 2020 TARGET FOR CLEANER AIR IN KUALA LUMPUR

Strive for Air Pollutant Index (API) to be within the 'good range' for 20% of the year and within 'moderate' range for the remaining 80% of the year.

#### % Breakdown of Mean API in KL (2000-2003)



- 81% of air pollution is contributed by mobile or non-point sources from vehicular emission.
- · Point sources are mainly from industrial activities.
- Transboundary pollution from haze and other activities from areas outside
   Kuala Lumpur.

# Enhancing Green Network and Blue Corridor

Kuala Lumpur's vision is of a network of high quality, accessible parks and green spaces which promote recreation, health, education and economic regeneration, helping to make Kuala Lumpur a significantly more attractive city in which to live and work.

Parks, green spaces and rivers benefits cities socially, economically and environmentally. As a tropical city, Kuala Lumpur is blessed with natural resource areas, greeneries and a river system; however it is always threatened by encroachment of development. Many policies at national and city level have been put in place to protect and enhance parks and rivers in the cities. These policies need to be implemented at local level, hence the Draft KL City Plan 2020 has identified initiatives and plans for implementation in the next 12 years.

The Draft KL City Plan 2020 seeks to provide an integrated network of green spaces by linking the major parks in the city through both natural and man-made connectors, which include rivers, utility reserves, roadside landscape areas and pedestrian walkways, both in a neighbourhood setting and in an urban setting.

The Plan ultimately aims to achieve a standard provision of parks, open spaces and recreational facilities of a comparable standard with other liveable cities which are easily reachable and accessible to the communities. This includes provision of good quality recreational and sports facilities.



#### **Increasing Access to Public Park**

Strategic Direction 8.1 **Providing Public Parks and Recreation Grounds** 

Strategic Direction 8.2
Securing City Urban Spaces

Strategic Direction 8.3 **Providing Quality Recreational Facilities** 

**Improving Amenity Value of River Corridor** 

Strategic Direction 8.4

Improving River Corridor

Strategic Direction 8.5 **Designating River Corridor Activity Zone** 

#### **Increasing Access to Public Park**

Parks and public open spaces contribute towards Kuala Lumpur's liveability and have positive impact on people's health and well-being.

# Strategic Direction 8.1 – Providing Public Parks and Recreation Grounds

Protect existing parks, open spaces and recreation grounds and secure new parks, open spaces and recreation grounds for residents and visitors.

Parks and open spaces provide settings for a wide range of social and recreational activities; promotes biodiversity and nature conservation; enhances the visual environment and can contribute to the economic development of the city. The Draft KL City Plan 2020 has identified measures to increase land allocated for parks and open spaces in the city through:-

- Expansion to existing city park in Taman Tasik Perdana towards the City Centre along Jalan Parlimen and the area previously occupied by government offices north of Jalan Parlimen.
- New district and local parks for wider variety of recreational facilities including sports facilities.

 Locally accessible urban and neighbourhood parks located closer to residential neighbourhoods and major public locations in city centre and other commercial centres in order to improve distribution of recreational facilities to all areas within Kuala Lumpur.

The new parks will increase coverage area of public parks and open spaces in Kuala Lumpur from currently 5% of Kuala Lumpur's total area to 8% in the year 2020.

The Draft KL City Plan 2020 has designated two zones related to open spaces i.e the Public Open Space and the Private Open Space in the 2020 Land Use Zoning Map.

#### Parks And Open Spaces, 2020

Hierarchy	Total Area (hectare)	%
Public Parks and Open Spaces		
City Park	453.87	18
District Park	422.46	17
Neighbourhood Park	221.19	9
Local Park	93.74	4
Local Play Area	463.08	19
Sports Complex	132.81	5
Open Spaces/Landscape Areas	94.93	4
Sub-total	1882.08	77
Private Open Spaces	563.30	23
TOTAL PARKS AND OPEN SPACE	2,445.38	100

# INITIATIVES FOR NEW MAJOR PARKS AND PUBLIC OPEN SPACES

#### 1. Lake Garden Expansion

A major expansion of the City's premiere park northwards to include areas occupied by the old government offices, and areas behind the Bank Negara and Federal Hill area. The expansion will almost triple the size of the park and put it on an equivalent footing with some of the other major internationally renowned parks.

#### 2. Youth Park at Jalan Cenderasari

An active youth park will be created next to the proposed Youth Sports Academy on Jalan Cenderasari. The park shall accommodate extreme sports facilities for activities such as rollerblading and skateboarding. Future extensions to the park could be accommodated in the existing hawker area next to the Methodists Girls school as well as the undulating area between Jalan Cenderasari and Jalan Lembah. The close proximity to the police headquarters at Bukit Aman should ensure security and also provide opportunities for police initiated youth programs.

#### 3. Urban Parks

The urban parks will be secured on all redevelopment sites in the city centre and the Comprehensive Development Areas (CDA) of Kampong Bharu and Duta Government Complex.

#### 4. Bukit Gasing District Park

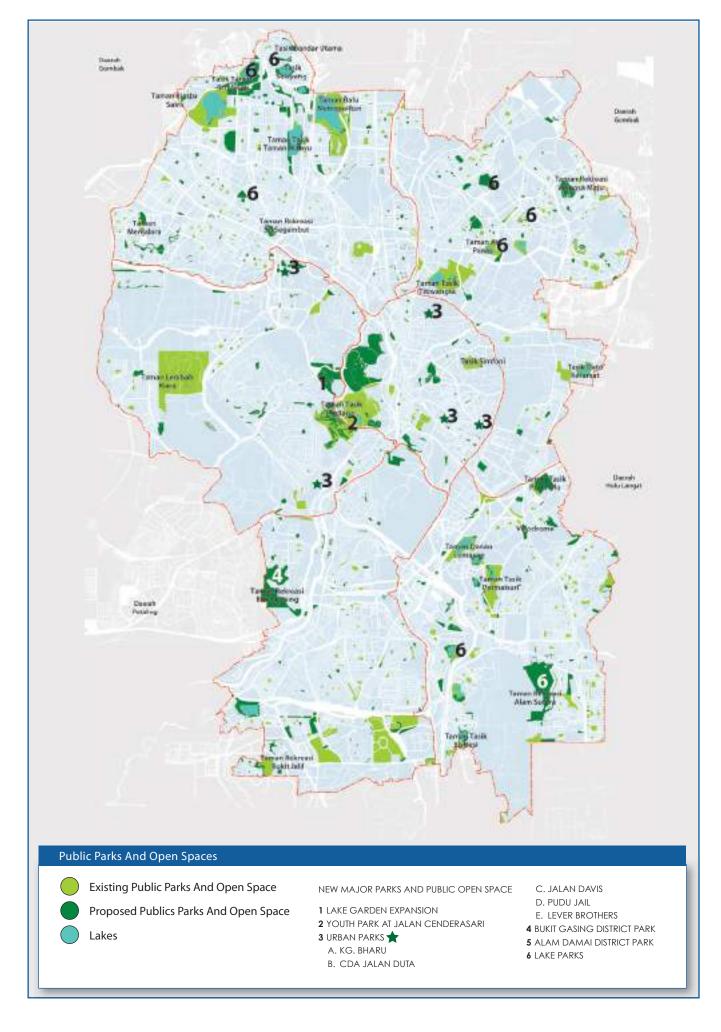
New district park focusing on nature recreation in keeping with the objective of retaining the hills in Kuala Lumpur which are also identified as environmentally sensitive area.

#### 5. Alam Sutera District Park

New district park located at the hillside of Bukit Sg. Besi. Recreational facilities in the park are to be linked to the natural characteristics of the Bukit Sg. Besi Forest Reserve to include nature as its key theme.

#### 6. Lake Parks

Parks surrounding all existing lakes which are also to be retained for drainage retention. These parks are Rampai Lake Park, Ayer Tawar Lake Park, Ayer Panas Lake Park, Danau Kota Lake Park, Intan Baiduri Lake Park, Sri Utara Lake Park, Jinjang Lake Park, Tasik Selatan Lake Park, Malaysia Raya Lake Park.





#### **KLSP 2020 POLICY**

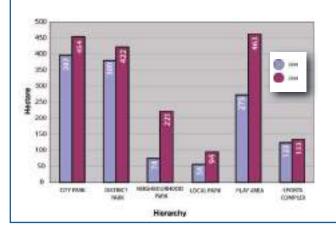
**EN 15** KLCH shall establish hierarchy of parks and implement programme to develop more district parks, neighbourhood parks, local parks and local play area.

#### 2020 TARGET FOR PROVISION OF PUBLIC PARKS AND OPEN SPACES

Kuala Lumpur targets to increase its public parks and open space area to 10% of the total Kuala Lumpur area in 2020 in which the ratio of public open space for every person is targeted to be 11 meter square per person as compared to current 7 meter square per person.

	Existing (Year 2005)	2020 Target
Population (persons)	1,620,100	2,198,400
Total Land Allocated for	1543	1,882
Public Parks (hectares)		
% Of Public Parks in	5%	10%
Kuala Lumpur		
Ratio of Public Parks and		
Open Spaces to Popula-	7	11
tion (metre square per		
person)		
Area Required to Cater		
for Ratio of 11metre	-	2,418
square/person in 2020		hectares

The Draft KL City Plan 2020 has allocated a total of 1,882 hectares of land for public open spaces of various hierarchies. To achieve a target of 11 metre square per person in 2020, a total of 2,418 hectares of public open space will be required. The balance of 536 hectares required to achieve the set target will be secured through provision by private developments as part of the requirement to provide open spaces within developments. This is expected to come from local play areas, urban plaza and pocket parks.



#### PARK HIERARCHY FOR KUALA LUMPUR

#### City Parks

City park is the highest hierarchy of parks for Kuala Lumpur consisting of the Taman Tasik Perdana and the Taman Lembah Kiara. These parks serve Kuala Lumpur and its surrounding areas within the Kuala Lumpur conurbation. The existing Taman Tasik Perdana will be extended thus making the whole area allocated for city parks to 453 hectares or 19% of the total land areas allocated for public open spaces in Kuala Lumpur by the Draft KL City Plan 2020.

#### **District Parks**

Large parks located within various parts of Kuala Lumpur catering for a catchment population for 200,000 population. The district parks cover a total area of 422.46 hectares or 17% of total land allocated for public open spaces in Kuala Lumpur. The park can include other sports and recreational facilities such as stadium and playing courts for integration of facilities and optimum use of land. The district parks are Taman Rekreasi Bukit Gasing, Taman Tasik Titiwangsa, Taman Antarabangsa Bukit Jalil, Taman Tasik Permaisuri, Taman Danau Lumayan, Taman Tasik Pudu Ulu, Taman Tasik Taman Wahyu, Tasik Metropolitan Kepong, Taman Batu Metropolitan and Taman Rekreasi Alam Sutera.

#### **Neighbourhood Parks**

Parks within neighbourhood areas for a catchment of 50,000 population. Accommodates facilities for large group activities and organised sports play.

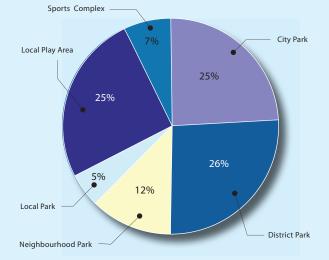
#### **Local Parks**

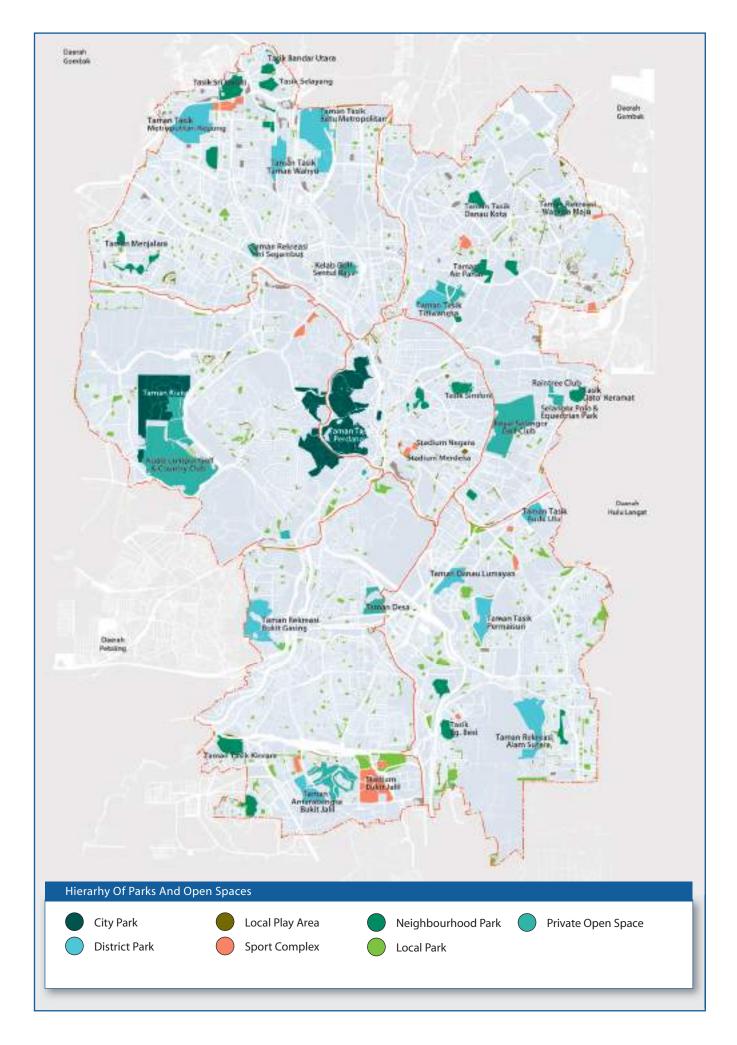
Parks located within residential areas and locally accessible to a population catchment of 10,000 people. The parks provide daily recreational facilities within bicycling and walking distance from their catchment areas. In urban or commercial centres, the park can also be identified as urban park.

#### Local Play Areas / Urban Plaza / Pocket Parks

Open spaces located in residential areas or in urban and commercial centres within walking distance of users. These will be secured from developments through development control mechanism

#### BREAKDOWN OF PUBLIC OPEN SPACE BY HIERARCHY, 2020







#### **Strategic Direction 8.2 – Securing City Urban Spaces**

Identify and secure areas for urban pocket parks and urban plazas.

Urban spaces in the forms of pocket parks and urban plazas which are of smaller hardscaped and landscaped spaces provide open spaces for highly intensified urban environment in the City Centre and other district centres, where land for green parks and open spaces are limited. However, they provide breathing spaces in the City Centre and places for people to gather, socialise, rest and relax.

Securing all these spaces through limited public funding is an uphill task for KLCH alone. The Draft KL City Plan 2020 has identified the needs for these spaces in the City Centre and they will be secured through various mechanisms other than acquisition using public funds. Provision will be secured through development control process where development proponents will be required to surrender the identified spaces for public use. Provision of private open spaces such as urban courtyards will also be encouraged in accordance to the planning guidelines and standards.

### WHY HAVE URBAN PARKS/POCKET PARKS?

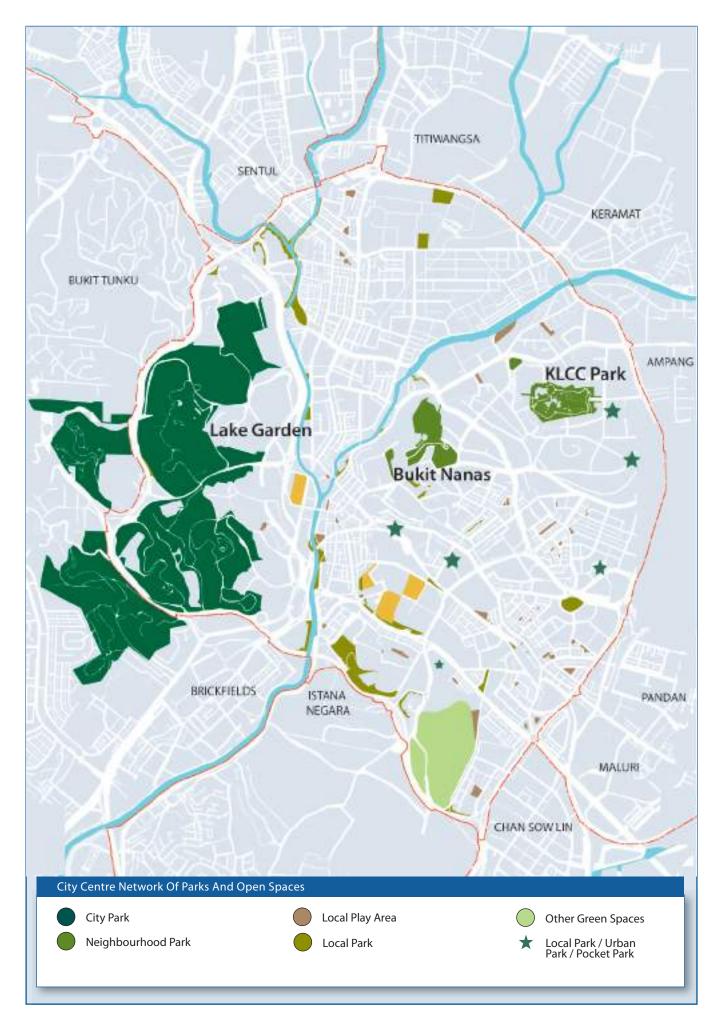
- 1. To increase the amount of green space in the City Centre and promote environmentally friendly practice.
- 2. To distribute green spaces through the City Centre.
- 3. To create a network of useful and user friendly green spaces in the City Centre where people enjoy visiting on a regular basis.
- 4. A showcase for new development and investment.
- 5. To enhance a sense of pride in a locality.
- 6. To increase amount of seating/ sitting places in the City Centre.
- 7. To enhance view from buildings which overlook the urban parks.





#### REQUIREMENTS FOR URBAN PARK / POCKET PARK IN REDEVELOPMENT PROJECTS

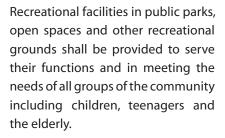
- All redevelopment sites are required to provide urban park within their developments areas.
- The parks are to be made accessible to public and linked to primary pedestrian network within its surrounding vicinity. A guide to pedestrian network is specified in the Development Guide Plan prepared by KLCH.







Provide functional and user friendly recreational facilities to meet requirements and needs of all groups of the community and users.



Parks must be designed to be functional with emphasis given on design that is safe incorporating the principles of Crime Prevention Through Environmental Design (CPTED) and user friendly by



Waterplay activities will be encouraged in urban park / local play area / pocket park

applying universal design principles. Landscape elements within the park must reflect its function with sustainable with landcaping principles incorporated into the design. Public arts are encouraged within an urban park.

Standard For Provision Of Local Parks, Local Play Areas, Urban Parks And Plazas							
Park Hierarchy	Requirements : Land area per 10,000 population (hectare)	Minimum Land Area per Unit	Minimum Facilities				
Local Park	1.00 hectare per 10,000 population	2 hectares	<ul> <li>Mini football field, and/or</li> <li>Courts for courts games,</li> <li>Playground with play equipments</li> <li>Shaded area for passive activities such as reading, traditional games, picnic, etc.</li> </ul>				
Local Play Area	1.00 hectare per 10,000 population	0.5 hectares	<ul><li>Courts for courts games,</li><li>Playground with play equipments</li></ul>				
Urban Parks and Plazas	10% of total development area	70% of the area allocated must be centralised	<ul><li>Plaza, courtyard, forecourt</li><li>Seating areas</li><li>Landscaped areas</li></ul>				

#### **Improving Amenity Value of River Corridor**

The quality of the built environment and public realm alongside the rivers makes an important contribution to Kuala Lumpur's image and status as A World Class City.

**Strategic Direction 8.4 – River Corridor Improvement** 

Undertake measures to achieve the highest possible quality of building development and the treatment of the river's edge within the river corridor.

Reshaping the face and character riverfront of Kuala Lumpur's will take time. However, Kuala Lumpur is committed to prevent further deterioration of the river environment and river amenity by ensuring the highest possible quality for both building developments and the treatment of the river's edge. Initiatives incorporating urban design, public amenities and spaces along the river, pedestrian connectivity, river beautification as well as improving river water quality shall be implemented so that the river will become a more attractive place and a source of civic pride.

Priority focus for improvement within this Draft KL City Plan 2020 planning period, is for the Sg. Klang and Sg. Gombak located within the City Centre and along other rivers located outside the City Centre where opportunities to create localized pedestrian connectivity along the rivers exist.

#### **KLSP 2020 POLICY**

**UD 15** KLCH shall designate river corridors, implement measures to impove the amenity value of the rivers and implement guidelines for development within or abutting the river corridors.

#### **INITIATIVES TO IMPROVE AMENITY VALUE OF RIVER CORRIDOR**

- 1. Providing a continuous walkway and cycle path along river edge.
- 2. Removing impediments and eyesores along the river.
- 3. Introducing riverside beautification and landscaping measures.
- 4. Encouraging riverside activities and ensuring that developments orientate themselves towards the river.
- 5. Initiating measures to improve river water quality and removing polluting activities within river corridor (see Chapter 7).
- 6. Controlling development within areas designated as River Corridor through Environmental Protection Zone as part of the development control elements of the Development Control Plan (DCP).
- 7. Implementing River Corridor Design Principles for all new developments and redevelopments within the river corridor.

#### **DESIGN PRINCIPLES FOR DEVELOPMENT WITHIN RIVER CORRIDOR**

- 1. Developments within the River Corridor should integrate successfully with the water space in terms of use, appearance and physical impact and should in particular:
  - Include a mix of uses appropriate to the water space, including public uses and open spaces, to ensure an inclusive accessible and active waterside and ground level frontage;
  - Integrate into the public realm, especially in relation to walking and cycling routes;
  - Incorporate built form that fronts the river and has a human scale of interaction with the street, public spaces and the riverside.
- 2. No further permanent structures such as LRT or monorail viaducts will be permitted within the river reserves to prevent further deterioration of river amenity.

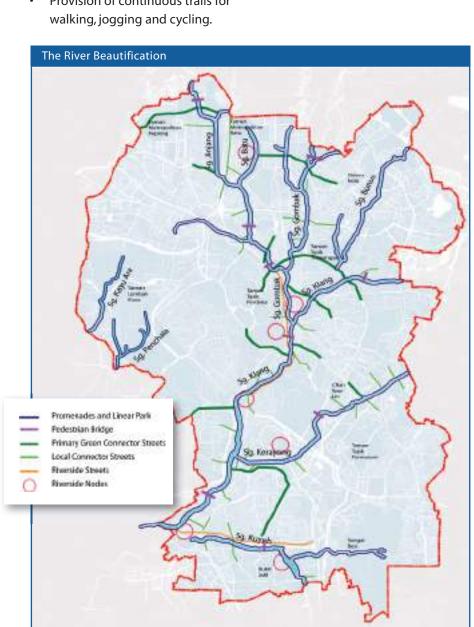


Sg Klang as it was Sg Klang as it is now

Sg Klang as it is envisioned by 2020

The proposed beautification involves the Sungai Jinjang, Sungai Klang, Sungai Gombak, Sungai Kerayong and Sungai Kuyoh. The beautification components proposed are:-

- Landscaping and treatment of river edge
- Treatment on river buffer
- Integration of activities as river nodes
- Provision of continuous trails for walking, jogging and cycling.









Illustrative views of riverfront landscaping of Sg. Klang with pedestrian facilities and hardscape features along the river



Illustrative views of pedestrian facilities along river suitable for other parts of Kuala Lumpur

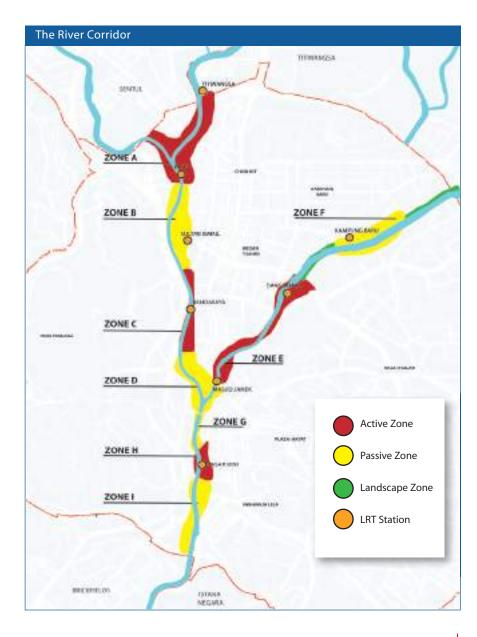




# **Strategic Direction 8.4 – Designating River Corridor Activity Zone**

Designating river corridor activity zone to integrate active and passive activities along the river reserves and to coordinate public amenities and facilities provided within the river corridor.

Along the length of the river, activity zones are defined which are alternately active and passive. The nature of each of the zones is determined by the existing activities nearby and the opportunities and constraints that they present. Within the City Centre, the activity zones are specified into various sections where land and building activities will support and integrated with the public spaces along the river reserves.





#### RIVER ACTIVITIY ZONES FOR SUNGAI KLANG AND SUNGAI GOMBAK WITHIN CITY CENTRE

- Zone A Active area along Sungai Batu and Sungai Gombak between PWTC LRT Station and Jalan Tun Razak
  - The existing shophouses along this stretch will be encouraged to develop river front al fresco activities and the roads running alongside Sungai Gombak will be pedestrianised. Similarly, opportunities for opening up the riverside footpaths and creating more river related activities at the PWTC Exhibition Centre will be explored.
- Zone B Passive Area between the Puteri and Putra condominiums and the Tamil School on Jalan Raja Laut
  This will be maintained as an area with a relaxed residential character with the new riverside park acting as a bridge between the existing condominiums next to the Mall Shopping Centre and the new residential developments at Tiong Nam.
- Zone C Active Area between the Quality Hotel and KLCH

Buildings along this stretch will be encouraged to develop riverside activities which could be accessible to the public and to modify the appearance of the lower floors so they address the river. Building owners will be eencouraged to investigate the possibility of making alternative provision for the service roads running alongside the rivers so that wider footpaths and cycle ways can be provided. In addition the existing footpath will be widened by cantilevering out over the river bank.

• Zone D Passive area along Sungai Gombak and Sungai Klang between Jalan Parlimen and Leboh Pasar Besar

Due to the proximity of Masjid Jamek, riverfront activities, although encouraged, will be controlled so as to ensure that incompatible or noisy activities do not compromise the serene and dignified ambience around the mosque. Buildings between Medan Pasar and Sungai Klang will be encouraged to develop double frontages facing onto the river and the more modern, less attractive, buildings to retrofit their facades to create more attractive riverfront elevations which reflect the character of the adjacent historic buildings. The footpath along this stretch will be widened by cantilevering over the river bank and some limited al fresco activities encouraged. The existing pumping station will be placed underground so as to enable the provision of a continuous footpath leading towards the Masjid Jamek LRT.

• Zone E Active Zone along Sungai Klang between Masjid Jamek LRT and Jalan Sultan Ismail

New developments such as the new riverfront plaza at Masjid India and the al fresco eating and entertainment plaza at Jalan Sultan Ismail together with other proposed new developments along the river front will create a lively stretch of river connecting the City's Historic Centre to the main hotel belt along Jalan Sultan Ismail. Existing and new developments along this stretch will be encouraged to provide active commercial frontages accessible from the public riverside footpaths. This will become the prime eating and entertainment riverside zone and be the focus for river related activities.

• Zone F Active/ Passive Zone between the Muslim Cemetery and Phileo Avenue

When the SMART tunnel is operational, the flood walls that run under the elevated highway and cut off the river from areas either side of it will be removed. Once access to the river is restored, this zone will be developed as a green passive area on one side of the river with the muslim cemetery and new local park and an active area on the Kampong Bharu side with low key river related activities. The road running along side the river will be made into a pedestrian priority road with traffic calming measures introduced.

• Zone G Passive Zone between Leboh Pasar Besar and Jalan Tan Cheng Lock

There are no buildings with frontages onto the river along this stretch and emphasis will be placed on creating a dignified riverside walkway on both sides of the river leading up towards the Masjid Jamek mosque. The new cultural bridge cum plaza that connects Dayabumi to Central Market will be the focus of this stretch of river providing a connection between the proposed cultural activities within Dayabumi to the Heritage Zone around Central Market.

• Zone H Active Zone between Jalan Tan Cheng Lock and the site of the existing Klang Bus Station

This will be the focus for tourism related activities around the new commercial and tourism complex associated with the heritage shophouse area. Al fresco activities along the river bank plaza under the LRT viaduct and along the proposed new park will include a nightly pasar malam. The three electrical substations along this stretch will be relocated and incorporated into new developments.

• Zone I Passive Zone between Pasar Seni and Jalan Damansara

This will be a passive green link with a semi rural flavour connecting the City Centre to Brickfields. Limited commercial and recreational activities fronting the river will be permitted.



Kuala Lumpur is a city of different races and cultures. As a fast developing city, Kuala Lumpur has to define its image and identity to ensure that it's heritage is conserved and new developments within the City reflect greater awareness towards urban design excellence that shapes a distinctive Kuala Lumpur.

Kuala Lumpur has evolved from the 1860's mining town to a modern and vibrant developing city. Colonization from the western countries has brought some of the most endearing features to the City, where 19th century buildings were influenced by the Islamic heritage of Mughal and pre-World War II shop houses became a key feature to business and retailing activities. Today, Kuala Lumpur's economic growth has changed the skyline of the city. Ultra-modern buildings and large scale developments now frames the city's urbanscape. The challenge now is to make Kuala Lumpur distinctive, where its urban development has a high level of cultural vibrancy and creativity that will make Kuala Lumpur liveable and attractive.



#### **Conserving Cultural Heritage**

Strategic Direction 9.1 **Designating Heritage Zone** 

Strategic Direction 9.2

Conserving Heritage Buildings

# City Landmarks and Urban Profile

Strategic Direction 9.3 **Enhancing City Landmarks and Skyline** 

Strategic Direction 9.4

Enhancing City Centre Urban Profile

Strategic Direction 9.5 **Enhancing Urban Profile of Other Centres** 







#### **Conserving Cultural Heritage**

Conserving cultural heritage portrays a strong identity for a city and reflects upon an urban character that helps nurture a sense of place to the City's communities.

#### **Strategic Direction 9.1 - Designating Heritage Zone**

Designate heritage zones in the City of Kuala Lumpur to promote a culturally rich capital city.

Kuala Lumpur's historical buildings and sites is a physical legacy inherited from the different era of colonization. They are unique and irreplaceable and its preservation for the future is an important part of our culture.

The Draft KL City Plan 2020 has designated 2 key areas of heritage zones in Kuala Lumpur. The heritage zone includes:-

- The historic core centred around the confluence of Klang and Gombak Rivers, extends up to the north of Jalan Tuanku Abdul Rahman, the west most point of Jalan Pudu and the southern most tip of Jalan Tun Sambanthan; and
- Shophouse areas in Sungai Besi.

The designation of the heritage zone is in response to addressing issues such as the threat caused by rapid development that caused many deemed historic buildings and structures to disappear in exchange for modernity. Development control and guidelines<sup>1</sup> shall ensure that developments within this area shall exercise care and sympathy to form, scale and character of the existing historical sites and buildings.

The heritage zone is not homogenous in character; there are certain areas which are ranked above others in terms of architectural and historic integrity. Areas designated to be conserved are categorized according to their architectural or historic significance as well as their degree of contiguity.

#### Strategic Direction 9.2 - Conserving Heritage Buildings

Blend Heritage Buildings with overall urban fabric of the City to provide an interesting and contrast urban setting for the City.

1,819 buildings in Kuala Lumpur have been identified for preservation<sup>2</sup> within the heritage zone of Kuala Lumpur. Most of these heritage buildings were built before the World War II and designed with different architectural styles such as the Early Shop Houses, Neo-Classical, Eclectic and Colonial styles. It is these early buildings that defines the city's unique architectural identity, adds variety and shapes Kuala Lumpur's character to define a special sense of 'place' for the City.

On a city wide scale, these heritage buildings provides interesting and contrast setting to the urban fabric of Kuala Lumpur. More economic use of these buildings is provided for where adaptive reuse of these buildings is allowed in line with the zones and uses identified in the development control documents<sup>3</sup>.

Thus it is the responsibility of KLCH to protect these heritage buildings for its residents and the objective is to ensure that new development

#### **KLSP 2020 POLICY**

**UD 20** CHKL shall designate the conservation of areas, places, landscapes and structures of historical and architectural value and significance, and ensure that all developments in their vicinity are sympathetic in form, scale and character.

#### KUALA LUMPUR HERITAGE ZONES

#### **Primary Zone**

A core area for conservation which is contiguous and contains groups of buildings gazetted under the National Heritage Act 2005.

#### Secondary Zone

Area that is less contiguous and contains a mixture of newer and older buildings with significant historic merit.

#### **Tertiary Zone**

More recently developed shophouse areas that has buildings of little or no historic significance.

#### **Buffer Zone**

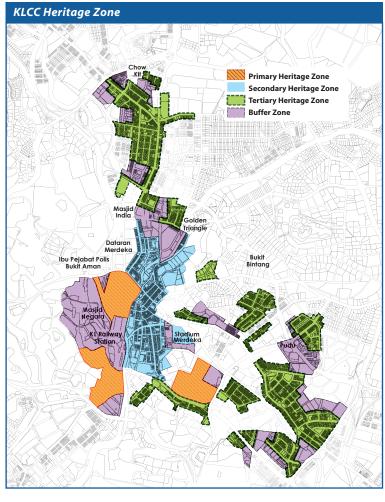
Areas that adjoin or connect heritage zones to others and has no intrinsic existing character which requires conservation.

blends with the heritage buildings and that the urban character of the heritage zone is maintained and protected. Such new developments should add to this continuing tradition of variety and cosmopolitan change in creating heritage for tomorrow.



<sup>&</sup>lt;sup>1</sup> Badan Warisan Malaysia has identified the buildings to be preserved.

<sup>&</sup>lt;sup>2</sup> Refer to the full list in Attachment 2 of the KLDCP 2008 <sup>3</sup> Refer Chapter 2, KLDCP 2008, Land Use Zoning





#### KUALA LUMPUR HERITAGE BUILDING CATEGORIES

#### **Category 1 Heritage Buildings**

These are buildings previously gazetted under the Antiquities Act and now covered by the National Heritage Act 2005 and fall under the jurisdiction of the Commissioner for Heritage.

#### Category 2 Heritage Buildings

These include buildings of significant historical and/or architectural importance which have not previously been gazetted principally because they are or were not more than 100 years old and therefore fell outside the provisions of the Antiquities Act. However, it is likely that most if not all of these buildings will be gazetted in the future under the National Heritage Act.

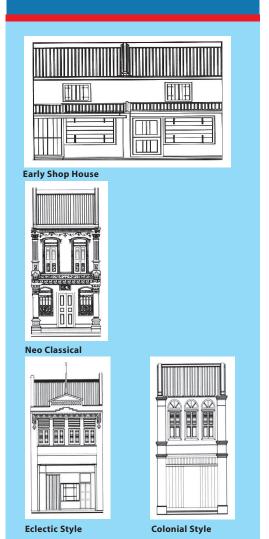
Heritage Zone guidelines for this category are the most stringent and decisions relating to major alterations and additions will be referred to the Design Review Panel.

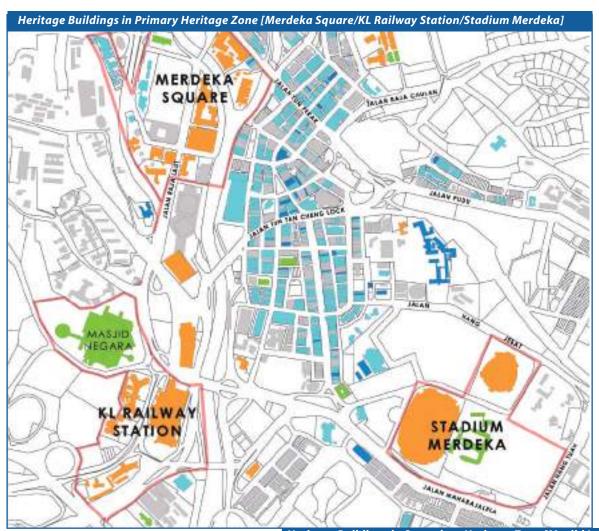
#### **Category 3 Heritage Buildings**

These include other buildings which contain elements or characteristics of some historical or architectural significance which are recommended to be conserved.

#### Category 4 Heritage Buildings

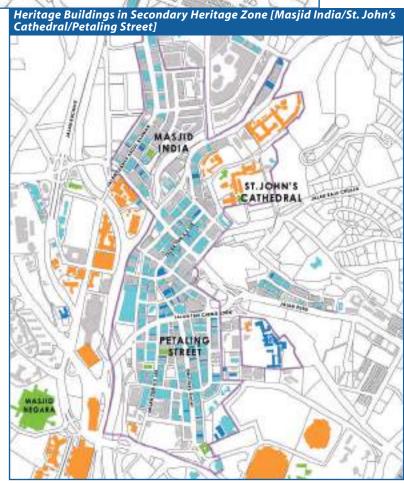
These include other shophouse buildings which are purely contextual value.





#### KUALA LUMPUR HERITAGE BUILDINGS





# Heritage Buildings in Tertiary Heritage Zone [ Bukit Nanas ] BUKIT NANAS BUKIT BINTANG

# Heritage Buildings in Tertiary Heritage Zone [Sungai Besi]

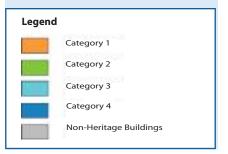
# Heritage Buildings in Tertiary Heritage Zone [ Brickfields ] Kuala Lumpur Sentral Stallon

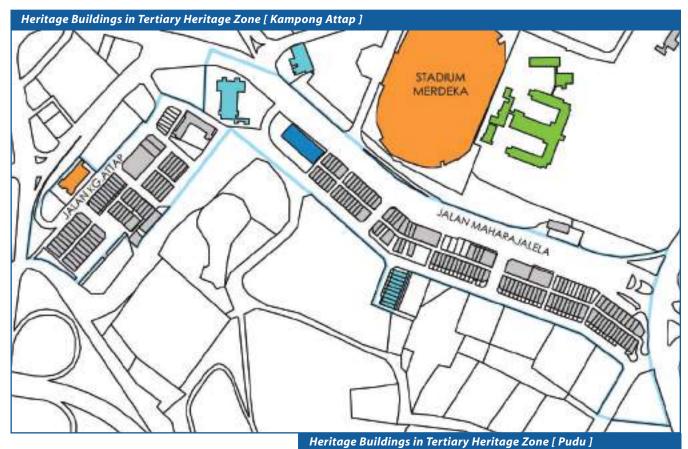
#### KUALA LUMPUR HERITAGE BUILDINGS



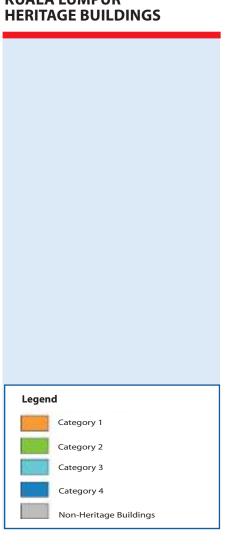
#### SHAMSUL KAMAL BIN SAMSUDIN

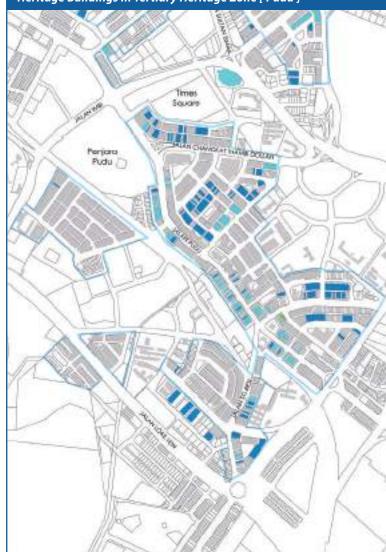
SMK Desa Tun Hussein Onn 3rd Winner (Category C) of Drawing Competition in Conjunction with Preparation of Draft KL City Plan 2020.

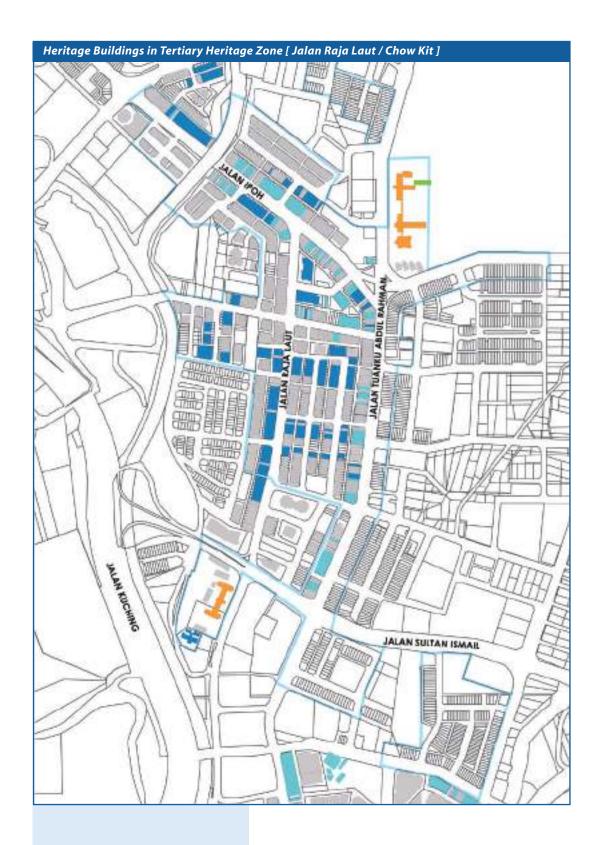




### **KUALA LUMPUR**







#### KUALA LUMPUR HERITAGE BUILDINGS





#### **INITIATIVES FOR HERITAGE ZONES**

Promote urban walks along heritage trails in Kuala Lumpur to strengthen their appeal to international tourists.

Urban walks are based on the concept of walkabouts which provide an alternative way for tourists to experience Kuala Lumpur at ground level. The array and mix of urban walks that combine heritage buildings, lifestyle, and parks allow international tourists the opportunities to appreciate the intrinsic attractions of Kuala Lumpur. There are numerous heritage trails in the City Centre located at Merdeka Square, Market Square, Chinatown and ending in Jalan Tuanku Abdul Rahman.

#### **City Centre**

- Improvements to road crossing, removal of high kerb obstacles, vehicle restraints, foot path widening, and provision of benches for walkers to rest.
- (2) Improve street lightings along major routes and secondary routes to encourage early morning and evening walks.
- (3) Ensure signage and cleanliness of the areas around the walks are in excellent conditions.
- (4) Link all the heritage trails to the hopon/hop-off bus system which should have a regular, punctual routine with an interval of 20 minute to reduce waiting time in the humid weather of Kuala Lumpur.
- (5) Create a river walk along the Sungai Klang as alternative route to link Chinatown to Merdeka Square.
- (6) Initiate river improvement program.
- (7) Create pedestrian walkways along Sungai Klang.
- (8) Provide public facilities such as restrooms, food stops for refreshments and snacks.

(9) Focus on landscaping that is sensitive towards pedestrian movements along the heritage trails.

#### **Brickfields**

- Ensure public areas are clean including back lanes and stormwater drains.
- (2) Create and maintain pedestrian walkways.
- (3) Improve signage.
- (4) Provide connectivity to public transport system to create seamless movements for travelers.
- (5) Provide a safe environment for travelers.
- (6) Encourage early morning and evening walks through improved street lightings.

# Encourage improvements and adaptive reuse of heritage and cultural buildings to enhance their tourism appeal.

Heritage buildings can be reuse for functional activities that are tourism-based. Adaptive reuse of such buildings breathe life into them, allowing them to showcase their aesthetic features while continue to serve their functional roles.

The proposed measures are:

- (1) In the City Centre, buildings that could be enhanced for reuse to promote tourism include the following:
- KL Railway Station: The possibility of developing an international class National Railway Museum at the platform areas should be considered.
- KTM Offices/Bangunan Sultan Abdul Samad: The possibility of using this splendid architectural heritage as a museum for cultural arts, craft centre, interpretation centre and art gallery with some commercial elements should be considered.

- Dayabumi: Architectural visage of the City with its Islamic overtones may interest some visitors. It is proposed that the lower block could be adapted as a theatrette which show audio-presentations of Kuala Lumpur and Malaysia's rich cultural heritage. A visitors' centre could also be housed here together with souvenir outlets.
- (2) In the City Centre, it is suggested that the Chinese Cemetery in the City Centre be retained as a possible tourism attraction. The underlying belief here is that cemeteries do have value as tourist destinations.
- (3) In Wangsa Maju-Maluri Strategic Zone, it is proposed that an extension to the P.Ramlee Memorial Museum be undertaken. The proposed extension involves the building of a new wing to the existing museum. With the new wing, additional components can be added such as a mini theatre to show case P. Ramlee's movies; souvenir kiosks; gallery displaying the history of film making; an exhibition/convention center targeted at the film/video industry and a mini studio for staging small plays.
- (4) In Sentul-Menjalara Strategic Zone, it is proposed that a Railway Museum be established within the KTMB train depot at Sentul covering an area of 4.7 acres. Another proposal is to establish new museum to showcase tin mining in Malaysia and Kuala Lumpur and to be known as the Tin Mining Museum (Kuala Lumpur). It is to be developed within the Batu Metropolitan. The proposed development will have exhibition halls, souvenir kiosks, archives, and a floating restaurant.



#### **City Landmarks and Urban Profile**

A land mark is important to enhance the legibility of the city at a strategic scale as well as create a distinct identity for the City.

# Strategic Direction 9.3 – Enhancing City Landmarks and Skyline

Petronas Twin Towers and KL Tower shall become the city Landmark for Kuala Lumpur, thus view corridors to the city Landmarks must be enhanced and protected

Urban windows and scenic view are important to a city in order to maintain visual permeability. It creates a visual interpretation of the city with a sense of recognition. Views to this landmarks and features are to be protected to create a sense of orientation for the city as well as create a city-wide identity for Kuala Lumpur.

#### **KLSP 2020 POLICY**

**UD 1** CHKL shall ensure the protection and enhancement of the City's gateways and major vistas.

#### **KLSP 2020 POLICY**

**UD 2** KLCH shall maintain and enhance the character and sequence of visual experiences along the major road corridors in particular those that focus in the City Centre.

#### **KLSP 2020 POLICY**

**UD 3** KLCH shall enhance the definition of existing view corridors and where practicable establish new corridors within the City Centre.

#### **KLSP 2020 POLICY**

**UD 8** KLCH shall encourage the development of additional major landmark buildings or complexes at key locations.

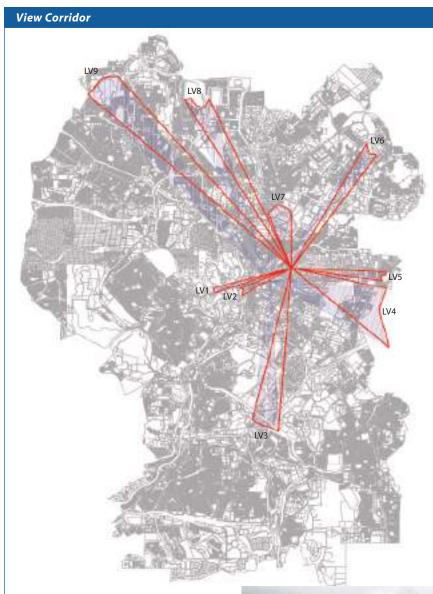
#### **KLSP 2020 POLICY**

**UD 9** KLCH shall control building heights to ensure the visual primacy of certain designated areas in the City Centre, the protection of special character areas and the accenting of entry gateways and activity nodes.

The Petronas Twin Tower and Kuala Lumpur Tower are synonymous to Malaysia and has become the primary landmark for Kuala Lumpur. The Petronas Tower soars up to 80 floors of about 400 meters high whilst the Kuala Lumpur Tower is of 300 meter high. Both these buildings can be seen from the periphery areas of the City and is a major tourist destination for the City.

Panoramic view corridors from major gateways of the City towards these two landmarks shall be protected. The principle of preserving the views are for the purpose of orientation and city's legibility and at the same time create a sense of pride and identity for Kuala Lumpur. This can be achieved through building height limitation imposed for structures or buildings within these view corridors.





#### LANDMARK VIEW CORRIDOR (LV)

View looking directly towards either one of the landmark buildings

- LV1 View from Tugu Negara
- LV2 View from Dataran Merdeka
- LV3 View from Sungai Besi Airport
- LV4 View from MRR2 Kampung Pandan
- LV5 View from Tasik Ampang Hilir
- LV6 View from Wangsa Maju Park
- LV7 View from Taman Tasik Titiwangsa
- LV8 View from Taman Metropolitan Batu
- LV9 View from MRR2/Taman Metropolitan Kepong



LV2 – View from Dataran Merdeka



LV7 – View from Taman Tasik Titiwangsa

# Strategic Direction 9.4 – Enhancing City Centre Urban Profile

To emphasize upon an urban profile that distinct Kuala Lumpur's Central Business District with other growth centres.

The growth and development progress in Kuala Lumpur can be recognized by construction of tall buildings established since the 1970s and 1980s. Rapid developments of high rise continued since then, distinguishing inner city of Kuala Lumpur from other suburban areas. This natural development of clusters of tall buildings is very much linked to the clustering of the financial and businesses sector which cluster together so as to concentrate sufficient activity to support ancillary activities.

Thus, the Draft KL City Plan 2020 sets the framework for an urban profile that can distinguish the city centre of Kuala Lumpur with other centres. In general, the city centre functions as the financial and prime business, will comprise of the most modern and high-rises in the city and this urban profile decreases as it moves towards the edge of the city centre. However this is subject to other strategic planning requirements being satisfied (i.e. Intensity Control, Heritage Zone, Height Control as well as Visual Corridors Height Control Zones).

#### **KLSP 2020 POLICY**

**UD 7** CHKL shall ensure the retention and enhancement of important views of the City's skyline and landmarks visible from urban centres and public open space outside the city centre.







# Strategic Direction 9.5 – Enhancing Urban Profile of Other Centres

To allow for clustering of tall buildings in specially identified areas to create interesting urban profile for the City.

However, the City shall also support the clustering of tall buildings in other centres defined within the Comprehensive Development Area (CDA), urban regeneration areas and the International Zone area. This localised peaking situation will create an urban profile that is more interesting for the City and provides for local landmarks and local identities. Such areas however shall not in anyway compete in terms of scale and height with Kuala Lumpur inner city, so as to differentiate the importance of centres as well as create different forms of focal points for the City.



#### **KLSP 2020 POLICY**

**UD 9** KLCH shall control building heights to ensure the visual primacy of certain designated areas in the City Centre, the protection of special character areas and the accenting of entry gateways and activity nodes.

#### Kuala Lumpur City also supports clustering of tall buildings in areas such as:-

# Comprehensive Development Area (CDA)

CDA Kampung Bharu CDA Jalan Duta

#### International Zone

Mont Kiara – Sri Hartamas Jalan Ampang

#### **Urban Regeneration Areas**

Jalan Davis – Jalan Selatan Jalan Pekeliling Jalan Pudu (old Pudu Jail area) Sentul

#### **District Commercial Centres**

This is subject to other strategic planning requirements being satisfied (i.e. Intensity Control and Height Control).



To become a sustainable world city, Kuala Lumpur must use natural resources more efficiently, increase its re-use of resources and reduce levels of waste and environmental degradation. As Kuala Lumpur grows, these objectives will become even more important and provision of infrastructure and utilities that meet up to its sustainable vision should become priority for actions.



Kuala Lumpur's vision to become a world class city requires supporting infrastructure and utilities that facilitate its growth and at the same time reduces its impact on future generations by minimising degradation of natural resources.

The Draft KL City Plan 2020 promotes and aims to achieve environmental stewardship in its development and management of the City whereby initiatives for a more sustainable approach to provision of infrastructure and utilities are to be implemented. The implementation however requires coordination and participation of all stakeholders including the people of Kuala Lumpur.

As A World Class City, Kuala Lumpur also needs to provide the highest standard and quality of infrastructure and utilities services. The quality of the services expected for the city shall be reliable, which means uninterrupted, clean and efficient, whilst at the same must be sustainable. While Kuala Lumpur is aspiring to be A World Class City, all the stakeholders including KLCH, infrastructure and utility service providers and the public must constantly be aware of the importance of operating or living in a sustainable manner.

# Providing Infrastructure and Utilities to Support Growth of the City

Strategic Direction 10.1

Meeting Future Infrastructure and
Utility Requirements

Strategic Direction 10.2

Achieving Highest Reliability and Standard in Utility Provision

#### Mitigating Floods and Managing Stormwater

Strategic Direction 10.3 Implementing Kuala Lumpur's Drainage Master Plan

Strategic Direction 10.4

Managing Urban Stormwater in A

Sustainable Manner

#### Meeting Demand for Energy, Water and Waste Resources in a Sustainable Manner

Strategic Direction 10.5 **Promoting Energy Efficient Development** 

Strategic Direction 10.6

Promoting Rain Water Harvesting,
Recycling and Water Savings

Strategic Direction 10.7 **Reducing Waste** 

# Providing Infrastructure and Utilities to Support Growth of the City

Infrastructure services and utilities are central to Kuala Lumpur in achieving its "World Class" vision. The quality of its infrastructure and utility services are measured against the level of satisfaction of its citizen and businesses. Delivery of essential services must be efficient with the highest quality and reliability.

# Strategic Direction 10.1 – Meeting Future Infrastructure and Utility Requirements

Coordinate infrastructure and utility provision to meet projected demand.

Provisions of City's infrastructure and utilities are the responsibilities of various service providers which are outside KLCH. Although it is beyond KLCH jurisdiction to have control over their provision, service and maintenance, any inadequacy will reflects the City as a whole. Therefore, KLCH as city manager has an important role to play in coordinating all infrastructures and utilities provision and maintenance works.

Growth and development of the City needs to be supported with sufficient services to meet the requirement of 2.2 million populations, 1.4 million employments and to support economic growth towards becoming an attractive and dynamic business city. With the projected future population and employment, planning for infrastructure and utilities need to be projected to cater for the expected demand in the future, and be coordinated with the spatial and land use development of the City.

The utility services that are so essential to keep the city going are laid underground in uncoordinated manner. This has created unnecessary problems to the public and the residents whenever there is a major repair to be carried out on any of the utility services. Currently a permit is required for any utility service to lay their pipes or cables across the road or in places of heavy traffic such works have to be carried out in the night or during the holiday. These inconveniences can be minimized or avoided if an Integrated Utility Infrastructure Master Plan is prepared and implemented. Preparation of this Integrated Utility Infrastructure Master Plan for the City shall become the ultimate objective for planning implementation of infrastructure and utility for Kuala Lumpur and KLCH shall seek to find means and measures to facilitate the formulation of such master plan.



**UT 1** CHKL shall, in coordination with the appropriate authorities and agencies, enhance the provision of infrastructure, utilities and waste disposal services and ensure that they are reliable and sufficient to meet the needs of the people.

#### **KLSP 2020 POLICY**

**UT 2** CHKL shall, in coordination with the appropriate authorities and agencies, ensure that infrastructure, utilities and waste disposal services development plan complement the City's planned requirements.



# INITIATIVE FOR COORDINATED AND INTEGRATED INFRASTRUCTURE AND UTILITY SERVICES FOR KUALA LUMPUR

- Set up a division within KLCH that liase, monitor and coordinates all infrastructure and utility providers in terms of planning, implementation and maintenance.
- Assist service providers in their long term planning by integrating projected demands generated from the proposal of the Draft KL City Plan 2020 and by monitoring development implementation and distribution.
- 3. Set targets for waste, water, energy, resources and air within the City and monitor achievement of the targets and performance and quality of services.
- 4. Seek, where possible, implementation of underground utility tunnel or trenches in particular for large comprehensive development or redevelopment.

# TARGET FOR IMPROVED RELIABILITY IN ELECTRICITY SUPPLY TOWARDS WORLD CLASS KUALA LUMPUR

#### **TARGET**

The Draft KL City Plan 2020 seek to encourage service and provision of electricity supply to the City to meet the following targets which in the long term would also be in line with the MSC requirements of ensuring 99.9% electrical supply reliability.

- Improve System Average Interruption
   Duration Indexes (SAIDI) from the
   current 99 minutes/customer/year
   (2004) to less than 10 minutes/
   customer/year.
- 2. Improve System Average Interruption Frequency Indexes (SAIFI) from 0.84 (2004) to less than 0.5.

#### **CURRENT STATUS**

- For Kuala Lumpur, the System Average Interruption Duration Indexes (SAIDI) is 83, 101.2 and 99 minutes/customer/year for the years 2002, 2003 and 2004 respectively. For comparison, Putrajaya has a SAIDI index of 35.2 (2003) and 2 (2004).
- For Kuala Lumpur, the System Average Interruption Frequency Indexes (SAIFI) is 1.06 and 0.84 for the years 2003 and 2004 respectively. For comparison, Putrajaya has a SAIFI index of 0.57 (2003) and 0.01 (2004) and Singapore has a SAIFI Index of 0.1 (2003)

In most world class cities, tunnels are constructed for running of services like water, gas, telephone cables, signals and data lines, power cables of low and high voltages (LV & HT). Services requiring rigid pipes are located on one side of the tunnel while services utilizing cables or wires that are more flexible are stacked in trays on the other side of the tunnel.

In areas where traffic volume is low, services can be laid in the trench located on one side of the road. Services pipes and cables are coordinated for easy detection and identification wherever maintenance or any work need to be carried out. In areas where pavement is spacious, trench can be constructed under it instead of being under the road. Trenches can be laid on both sides of the road if there are difficulties for services to run across the road. Width of trenches varies depending on number of pipes and cables to be placed in.

# Strategic Direction 10.2 – Achieving Highest Reliability and Standard in Utility Provision

Monitor the performance of service providers in ensuring that the target for improved supply and service reliability can be met in City Centre.

Reliability of water and electricity supply and telecommunication services is paramount for Kuala Lumpur and achieving maximum reliability shall remain as long term target for Draft KL City Plan 2020. Achieving this target requires participation and commitment by all the relevant service providers.







#### **Mitigating Floods and Managing Stormwater**

Resolving flooding problems will become a priority for Kuala Lumpur which requires solutions beyond physical drainage investments. Drainage system must now be looked at in an integrated and holistic approach towards sustainable stormwater management.

# Strategic Direction 10.3 – Implementing Kuala Lumpur's Drainage Master Plan

Implementing fully the proposals of the Kuala Lumpur's Drainage Master Plan.

The rapid urban development growth in Kuala Lumpur in the last few decades has imposed severe loads on drainage system resulting increase in runoffs, mudflows, water pollution and most importantly flash floods. Clogging up of the drains due to silting or due to rubbish reduces the drain capacity further hence increasing the magnitude of the problem.

The KLSP 2020 has identified two policies with respect to drainage. The first policy is that KLCH shall in coordination with the Department of Drainage and Irrigation take measures to mitigate flash floods and the second policy concerns mining ponds and existing lakes in the City where they are to be used also for flood mitigation and stormwater detention.

The relevant authorities namely KLCH and the Department of Drainage and Irrigation have taken a two-prong approach involving flood mitigation and preparation of the Kuala Lumpur Drainage Master Plan to solve the problem of flooding. KLCH as the local planning authority shall seek to ensure that the drainage master plan is implemented fully and measures to control sedimentation resulting from developments are imposed and monitored closely.

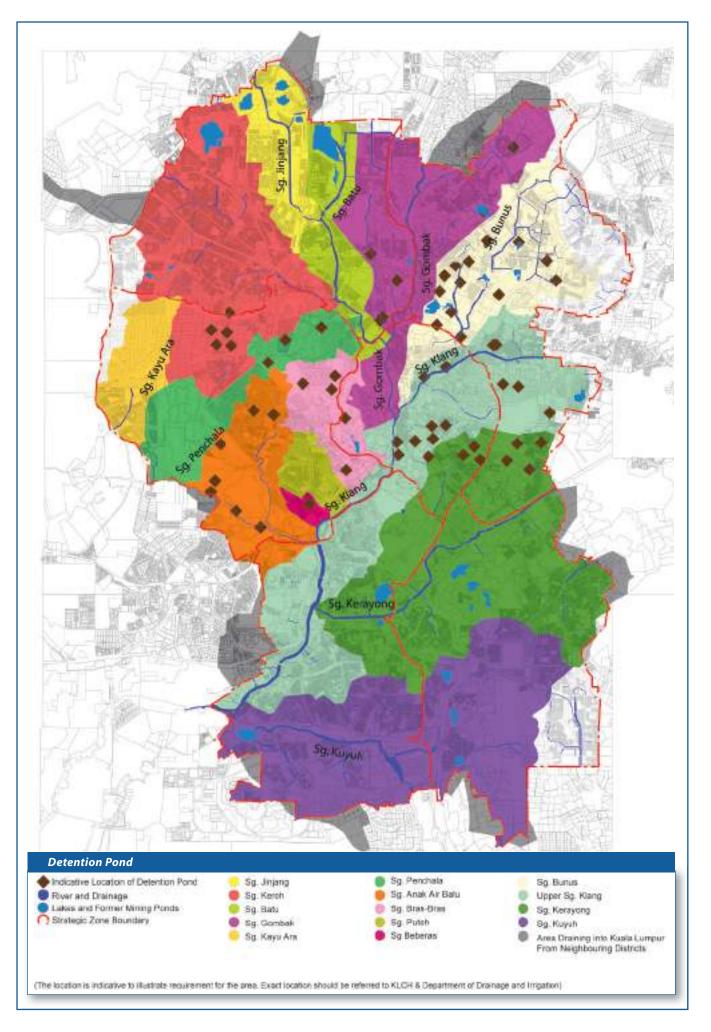
The Drainage Master Plan has proposed various measures to improve drainage. These include but are not limited to:

- Introducing detention ponds,
- · Increasing drain sizes,
- Improving culverts,
- Installing gross pollution traps.

Some of the detention ponds as proposed in the Drainage Master Plan are located on privately owned land. In some instances these lands are already being developed. KLCH shall either acquire the land for the detention ponds or alternatively seek other suitable locations for these proposed ponds. developments shall be required to provide detention ponds within their developments in accordance to requirements whilst all existing lakes and former mining ponds shall be preserved for detention purposes (see also Strategic Direction 7.4: Preserving Lakes and Former Mining Ponds).

#### **KLSP 2020 POLICY**

**UT 4** CHKL shall, identify, gazette and utilize former mining ponds as flood retention and recreation facilities.





Kerb prevents access while the breaks in the kerb allow stormwater inflows

# **Strategic Direction 10.4 – Managing Urban Stormwater** *Managing urban stormwater in a more sustainable manner.*

Management of stormwater run-off impacts directly on the community's quality of life by either enhancing or adversely affecting both the built and natural environments. Cities all over the world are now addressing urban stormwater in a more sustainable manner with objective to protect natural resources and sustainable development. The Draft KL City Plan 2020 seeks, as its long term objective, to continuously improve the management of Kuala Lumpur's urban stormwater towards sustainable drainage practices.



Bio-retention swale in a median strip

#### **Water-Sensitive Urban Design**

Water-sensitive urban design (WSUD) integrates urban planning and development with management, protection and conservation through the whole water cycle. It is about a balanced approach – the right applications in the right locations to achieve sustainability.

Planned and implemented correctly, its concepts and technologies allow new developments and the water cycle to complement each other.

#### Its key principles are:

- protect and enhance natural water systems
- integrate stormwater treatment into the landscape
- protect quality of water
- · reduce run-off and peak flows
- add value while minimizing drainage and infrastructure costs.

# INITIATIVES FOR SUSTAINABLE URBAN STORMWATER MANAGEMENT

- Ensuring implementation of Manual Saliran Mesra Alam by all development proposals submitted for planning, drainage and earthwork permissions.
- Introducing and encouraging implementation of water sensitive urban design in new development and retrofitting where possible WSUD in existing area.
- 3. Develop and implement "Best Environmental Management Practices for Runoff for Various Land Uses and Activities". This shall include environmental control measures for residential, construction site, commercial areas (eateries, markets, restaurants, hawker centres), streets and open spaces and recreational areas.
- Develop and implement a planning policy requiring GPT or other devices to be installed in new and existing commercial areas. This requirement should be made clear in planning approval.



# Meeting Demand for Energy, Water and Waste Resources in a Sustainable Manner

World class Kuala Lumpur needs to demonstrates its commitments to reducing the effect of its growth on the environment and its share of the nation's carbon footprint.

# Strategic Direction 10.5 – Encourage Energy Efficient Development

Encourage the inclusion of new energy efficiency standards for new development.

Human activities are already interfering with the earth's climate. Malaysia, like any other countries in the world is expected to face climate change impacts that range from reductions in rainfall and water availability, to extreme weather events and sea level rise. As A World Class City, Kuala Lumpur shall promote the inclusion of new energy efficiency standards for new development in Kuala Lumpur.

Energy efficiency in buildings means using less energy for heating (water heating), cooling and lighting. also means buying energy-saving appliances and equipment for use in a building. Cooling and lighting systems typically use the most energy in a building in Malaysia. Efficient cooling controls such as a programmable thermostat can significantly reduce the energy use of this system. For hot water normally used in hotel and hospital, the energy used to heat water can be reduced by both heating water more efficiently and by reducing hot water use. Reducing hot water used can be achieved by a wide variety of fixtures, such as low-flow showerheads and faucet aerators. In a home, the older water heater can be replaced with a newer, more energyefficient one, and the water heater and hot water pipes can be insulated to minimize heat loss.



Seattle City Hall is a 20,000 square-foot building that was completed in 2003. It includes a green roof and rainwater harvesting system for toilet flushing and on-site irrigation. The rainwater collection system can store up to 30,000 cubic-meter of water in a cistern located in the basement of the municipal building.





Ministry of Energy Water & Communication (MEWC) new office in Putrajaya known as MEWC Low. Energy Office (LEO) Building incorporates features of photovoltaic system and energy efficiency for minimum energy consumption.

#### INITIATIVE TO ACHIEVE REDUCTION IN DEMAND FOR ENERGY

- 1. Implement measures to reduce heat gain into buildings such as external devices that are able to reduce heat gain into buildings which reflects the character of tropical architecture and at the same time practice energy efficient space planning and management to reduce energy cost. These measures are elaborated in the MS 1525:2001 Code of Practice and Energy Efficiency and Use of Renewable Energy for Non-residential Buildings and KLCH shall progressively work with Malaysian Energy Centre to fully implement the code of practice.
- Encourage passive architectural design strategy which includes building orientation and configuration, interior space arrangement, façade design, natural ventilation, and strategic landscaping.
- 3. Implementation of the Energy Efficient Building Codes which is a set of requirements that specifies control on the design of the building envelope by using the overall thermal transfer value (OTTV) method. This can be implemented by KLCH to measure the energy efficient design in buildings. The code can be prepared with collaboration with Malaysian Energy Centre.
- 4. Improving the integration of land use and transport development and reducing the need to travel by car.



# Strategic Direction 10.6 – Promoting Rain Water Harvesting, Recycling and Water Savings

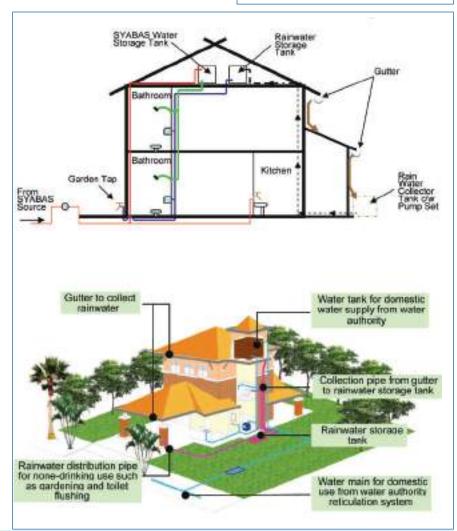
Promote rain water harvesting, recycling and water saving initiatives to reduce the burden of water supply system and water resources.

As population in the City is expected to increase to 2.2 million people by the year 2020, additional water infrastructure will be required to facilitate Kuala Lumpur's growth and a clean and reliable supply of water is a fundamental need of everyone. Unless Kuala Lumpur practice water conservation or alternative water source is found soon, water supply in Kuala Lumpur will be stressed in the near future. This pressure is likely to increase in the future with the predicted changes to our climate. Warmer temperatures are likely to increase the overall demand for water and climate change is likely to make it more difficult to retain the water. When combined with the levels of growth forecast for Kuala Lumpur, it is vital that a precautionary approach is taken in order to ensure that Kuala Lumpur's future water resource needs are sustainable.

> Concept Schematic Diagram of Rainwater Collection and Recycling

#### **KLSP 2020 POLICY**

**UT 6** CHKL shall implement measures to reduce water demand by encouraging users to install rainwater collection equipment and to recycle wastewater for non-drinking purposes.



# INITIATIVE TO ACHIEVE REDUCTION IN DEMAND ON POTABLE WATER SUPPLY

 Encourage rain water harvesting and water saving for all developments within the short term up to year 2010.
 KLCH shall formulate incentives for developments that implements rain water harvesting as part of the initiatives to encourage rain water harvesting, water recycling and water savings. 2. Formulate guidelines to make rain water harvesting and water saving mandatory in development of certain types and characteristics by the year 2020.

# Strategic Direction 10.7 - Reducing Waste

Reduce the amount of waste generated and encourage increased reuse and recycling of waste materials.

# PROJECTED WATER DEMAND 2020

The total water demand in year 2020 is estimated to be:-

- Low growth demand 6, 308 mld
- Plan growth demand 7,529 mld
- High growth demand 7,888 mld

The water demand is based on projected combined population for both Kuala Lumpur and state of Selangor.

Whereas the availability of the river sources is only: -

Sg. Kelang - 150 mld
 Sg. Batu - 120 mld

• Sg. Langat /

Sg. Semenyih - 1,120 mld

Sg. Selangor - 3,000 mld

Total - 4,390 mld

.,...

Note: mld - million litre per day

#### **KLSP 2020 POLICY**

**UT 10** CHKL shall, in coordination with the appropriate agencies, promote the strategy of reduce, recycle and reuse of waste products.

Waste management in Kuala Lumpur is dependent on landfill facilities outside its boundary. Currently waste is sent to the transfer station at Taman Beringin and trucked to Bukit Tagar, more than 70km away from Kuala Lumpur. The anticipated increase in population to 2.2 million in the year 2020 will further increase the amount of wastes generated and pressure will be on the relevant authority to find suitable sites for waste disposal facilities.

Kuala Lumpur which depends on landfill outside its territory to which it has no direct control needs to deal with its own wastes and find alternative sites and new technologies for waste disposals. The Draft KL City Plan 2020, recognizing the need to safeguard existing waste disposal site, has designated that the Taman Beringin site to remain as site for waste disposal facilities. By the year 2015, a solid waste treatment plant shall be built here using current technologies that convert waste into energy or reusable products such as ethanol or refuse derived fuels (RDF).

The Draft KL City Plan 2020 also aims to ensure that all sections of the community contribute to progressively reducing waste generated within the City. The facilities that support waste should be retained or replaced in a suitable way. As Kuala Lumpur grows and recycling performance improves, new facilities will be needed. These include materials recycling facilities and depots, inertwaster ecycling plants, composting facilities, waste treatment and energy recovery facilities, and reprocessing of recyclables. shall work with the communities through Local Agenda 21 programmes to promote and encourage more recycling efforts and participation of the local communities.

# 2020 TARGET FOR SOLID WASTE REDUCTION

### By 2010,

**20%** of total waste generated being recycled and target solid waste at 0.65 kg/d

### By 2015,

**30%** of total waste generated being recycled and target solid waste at 0.62 kg/d

#### By 2020,

**40%** of total waste generated being recycled and target solid waste at 0.60 kg/d

Note: kilogram per cubic per day





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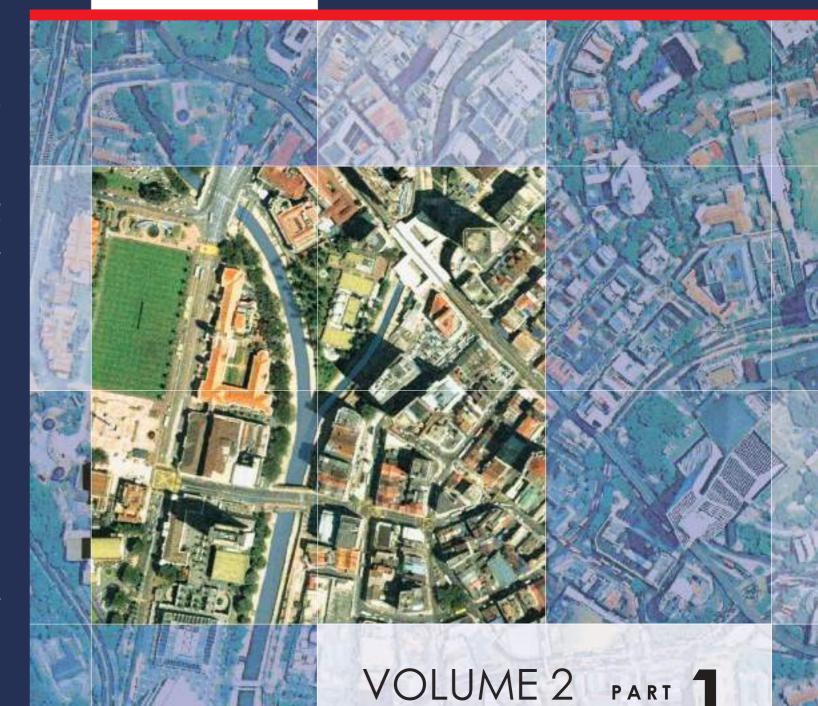


DRAFT KUALA LUMPUR CITY PLAN 2020





Towards a World Class City



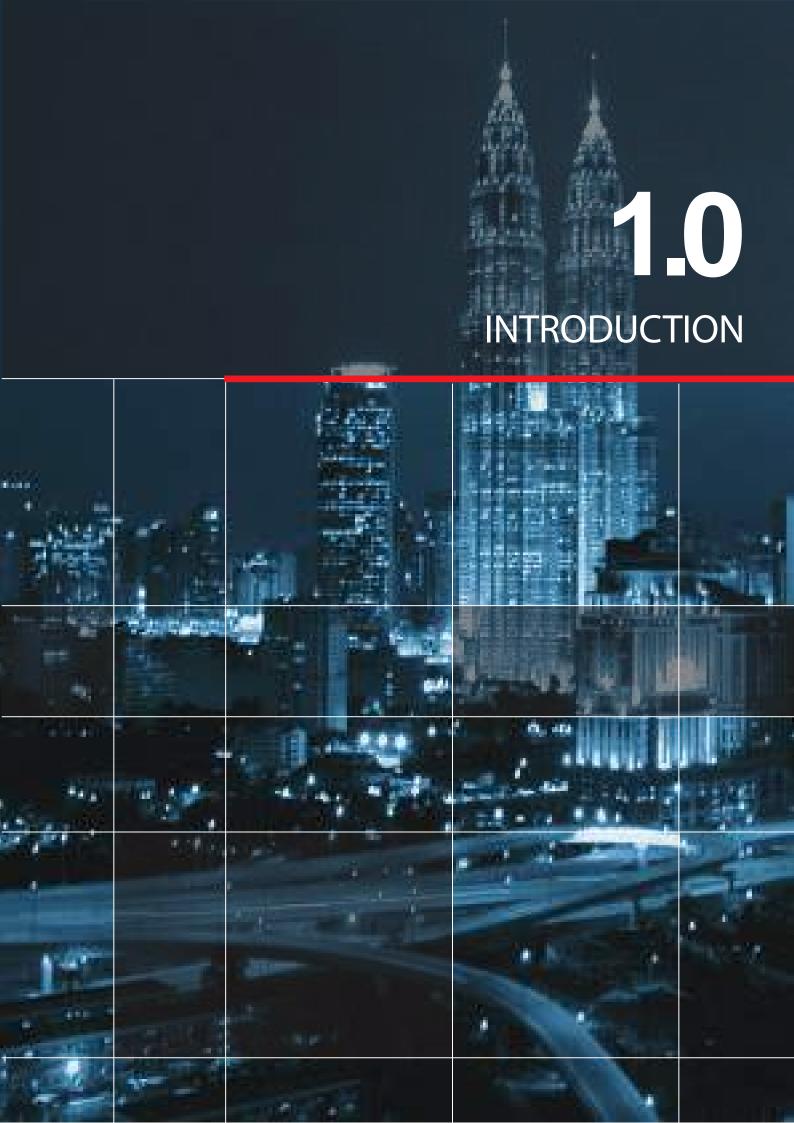
KL Development Control Plan 2008

Volume 2 - Part 1







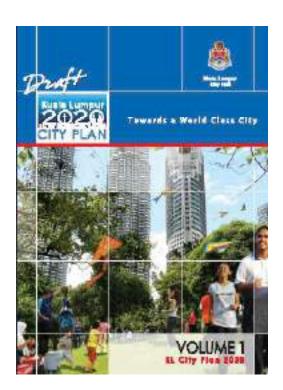


#### **Kuala Lumpur Development Control Plan, 2008** 1.1

The Kuala Lumpur Development Control Plan 2008 conforms to the provisions of the Draft Kuala Lumpur City Plan 2020 (Draft KL City Plan 2020) which is the local plan of Kuala Lumpur prepared under the provisions of Section 13 of the Federal Territory (Planning) Act 1982 (Act 267).

The Kuala Lumpur Development Control Plan 2008 shall be referred to as the **KLDCP 2008** throughout this document. It contains detail provisions with regards to development and use of land for the Kuala Lumpur city and it is intended for use by property owners, developers and the community for the assessment of all planning and development applications in Kuala Lumpur.

The **KLDCP 2008** shall also contribute to the growth and character of Kuala Lumpur where provisions are made for all aspects of development in Kuala Lumpur that shall contribute towards the improvement of the natural and built environment, transportation, community and social amenities as well as the infrastructure and utilities to achieve the desired vision and objectives of the Draft KL City Plan 2020.



#### 1.2 **Application of Kuala Lumpur Development Control Plan 2008**

The plan applies to all areas within Kuala Lumpur, which is 24,221.05 hectares and falls under the jurisdiction of Kuala Lumpur City Hall as the local planning authority. The plan will include the planning and development of six strategic zones, special areas which includes new villages, Malay Reservation land and traditional villages as well as government-owned and private owned properties in Kuala Lumpur.

Application of KLDCP 2008 shall be from the time it is approved by the Minister and adopted by the Commissioner of Kuala Lumpur City Hall.

#### 1.3 Relationship To Kuala Lumpur Structure Plan 2020 and **Draft Kuala Lumpur City Plan 2020**

KLDCP 2008 shall be read in conjunction with the Draft Kuala Lumpur City Plan 2020, where the key initiatives and implementation strategies have been identified as the guiding principles of the KLDCP 2008. The Draft KL City Plan 2020 is focus towards achieving the Vision and policies of the Kuala Lumpur Structure Plan 2020 (gazetted in 2004), where seamless maps accompany the Draft KL City Plan 2020 showing existing features as well as proposed changes to all aspects of land and development in Kuala Lumpur.



**District Commercial Centre** 

This plan also repeals Kuala Lumpur's Comprehensive Development Plans (CDP) and other technical policies of Kuala Lumpur City Hall. These includes:-

- CDP 1039 Central Commercial Area
- CDP 1040 Density Zoning
- CDP 1041 Land Use Zoning
- All Gazetted Plans (see Attachment 1); and
- All technical policies approved by Technical Planning Committee of Kuala Lumpur City Hall
- Use class rules under the City of Kuala Lumpur (Planning) (Use Classes) Rules 1980.

## **Intent of Development Control**

The KLDCP 2008 consist of several key planning measures that are used to regulate, control and manage physical development of land through development control process and procedures. Any persons wanting to develop any piece of land must refer and conform to the provisions and requirements specified in this document. Compliance with the provisions of the **KLDCP 2008** shall facilitate the consent to an application and leads to planning permission being granted.

With KLDCP 2008, development in Kuala Lumpur City will be allowed if planning conforms to three key measures, i.e.:

- Conformity to assigned Zoning and Land Use Activities;
- Conformity to requirements of Special Planning Zones; and
- Conformity to the specified Development Intensity

A series of schedules and maps is contained in this **KLDCP 2008** that shall provide the regulations and requirements for types of development to be undertaken as well as the standards to be met in any development application in the city.

#### **Structure of KLDCP 2008** 1.5

The **KLDCP 2008** contains two key components that will be measured towards development control in the City of Kuala Lumpur. These two key components are:-

- Key Planning and Development Control; and
- Special Planning Control

#### **Key Planning and Development Control**

The key planning and development control of **KLDCP 2008** shall be the main requirement and guide for any development application. It provides specific information to those wishing to carry out development on land zoned for residential, commercial, mixed-use, industrial, technology park, public open space, private open space, institution and 12 other zones identified in Kuala Lumpur.



It also provides specific guide towards the development intensity of a plot of land as all maps and schedules are assigned to every single plot of land in Kuala Lumpur.

Three key maps will control use and intensity of land development on a city-wide scale. The maps are:

- DCP 1 Land Use Zoning Map
- DCP 2 Density Map
- DCP 3 Plot Ratio Map

Development Control Plan 1 - Land Use Zoning Map (Figure 2.1) will be supported by a **Zoning and Use Class Schedule** which defines the allowable and prohibited use and activities within each land use zone specified on the map. These two documents must be read concurrently to fully comprehend the needs and requirements of the plan.

Development Control Plan 2 - Density Map will prescribe the permissible intensity of development allowed in residential zones (other than city centre area). The density reflects upon the number of persons to the land area and this would assist in population distribution and planning for social amenities as well as infrastructure and utilities.

Development Control Plan 3 - Plot Ratio Map will prescribe the permissible intensity of development allowed in residential zones in city centre area, commercial zones, mixed use zones as well as industrial zones. Plot Ratio reflects upon the total permissible built up area of a building plot.

### **Special Planning Control**

Special planning controls is also imposed as an overlay zone to the underlying Land Use Zoning Plan (DCP 1). These overlay zones are additional planning and development controls assigned to specific areas of concern that is important to the built and natural environment of Kuala Lumpur in creating a World Class City.

The special planning control relates to four zones which require special care and attention in planning and development. These areas shall be referred to as Special Planning Zones and are as follows:

i. SPZ 1 - Environmental Protection Zone - refers to area that is highly sensitive to development or changes in land use and needs to be conserved for its limited biodiversity value, to be maintained and enhanced for its life support and recreational function and/or to be protected to mitigate the hazard risks to the surrounding area.

## **KEY PLANNING AND DEVELOPMENT CONTROL**

Maps

**Development Control Plan** DCP1: Land Use Zoning Map Development Control Plan DCP2: Density Map Development Control Plan **DCP3: Plot Ratio Map** 

Schedule

**Zoning and Use Class Schedule** 

#### **SPECIAL PLANNING CONTROL**

Special Planning Zones Maps

SPZ1 - Environmental **Protection Zone** 

SPZ 2 - Heritage Zone

SPZ 3 - Height Control Zone SPZ 4 - Transit Planning Zone

#### **How To Use The KLDCP 2008**

#### STEP 1

Refer to DCP 1 - Land Use Zoning Map and read with the explanatory notes on the Land Use Zoning Chapter 2.0 of **KLDCP 2008.** 

For easy reference, DCP 1 - Land Use Zonina Map is grid indexed. Refer to Key Index Map for reference number in Chapter 2.0 and the indexed plans in document Part 2 of this KLDCP 2008 documents.



#### STEP 2

Refer to Zoning and Use Class Schedule to determine the allowable use and activities for the land or/and building of the said zone in which the land falls within.

The Schedule is prepared generally for each zone as well as for specific areas. Refer to listing in Chapter 2.0



Refer to DCP 2 - Density Map or DCP 3 - Plot Ratio Map and determine maximum allowable plot ratio or density for the particular lot/plot and read with the explanatory notes on Chapter 3.0 of KLDCP 2008



#### STEP 4

Check to see if land falls within any **Special Planning Zones on the Land Use** Zoning Map.

Refer to Special Planning Zone to determine further restriction, control or incentives

- **Environmental Protection Zone** (refer SPZ 1 Map and Chapter 4.0)
- Heritage Zone (refer SPZ 2 Map and Chapter 5.0)
- **Height Control Zone** (refer SPZ 3 Map and Chapter 6.0)
- Transit Planning Zone (refer SPZ 4 Map and Chapter 7.0)

- ii. SPZ 2 Heritage Zone where land and buildings within the designated areas are subject to additional policies and set of guidelines in ensuring that developments within this areas are undertaken with care and sympathy to form, character and scale of existing historical sites and buildings within the designated area. Heritage Zone includes areas with natural and cultural and having aesthetic, historic, scientific or social significance, or other significance, for current and future generations of Kuala Lumpur.
- iii. SPZ 3 Height Control Zone refers to additional overlay on control on height of new buildings that fall within the designated view corridors which are established to protect and ensure visibility of two primary city landmarks; the KLCC Twin Towers and the Kuala Lumpur Tower, from various preidentified entrances or gateways to the city and from various local public spaces within various parts of the city towards these two primary towers.
- iv. SPZ 4 Transit Planning Zone refers to additional overlay to designated transit stations where intensification of development for areas within the priority transit planning zone i.e. 250 metres radius from a transit station is allowed. Such intensification shall be reflected in the additional permissible densities and plot ratios allowed in these areas to encourage transit supportive activities and create an integrated land use and transportation planning for the city.

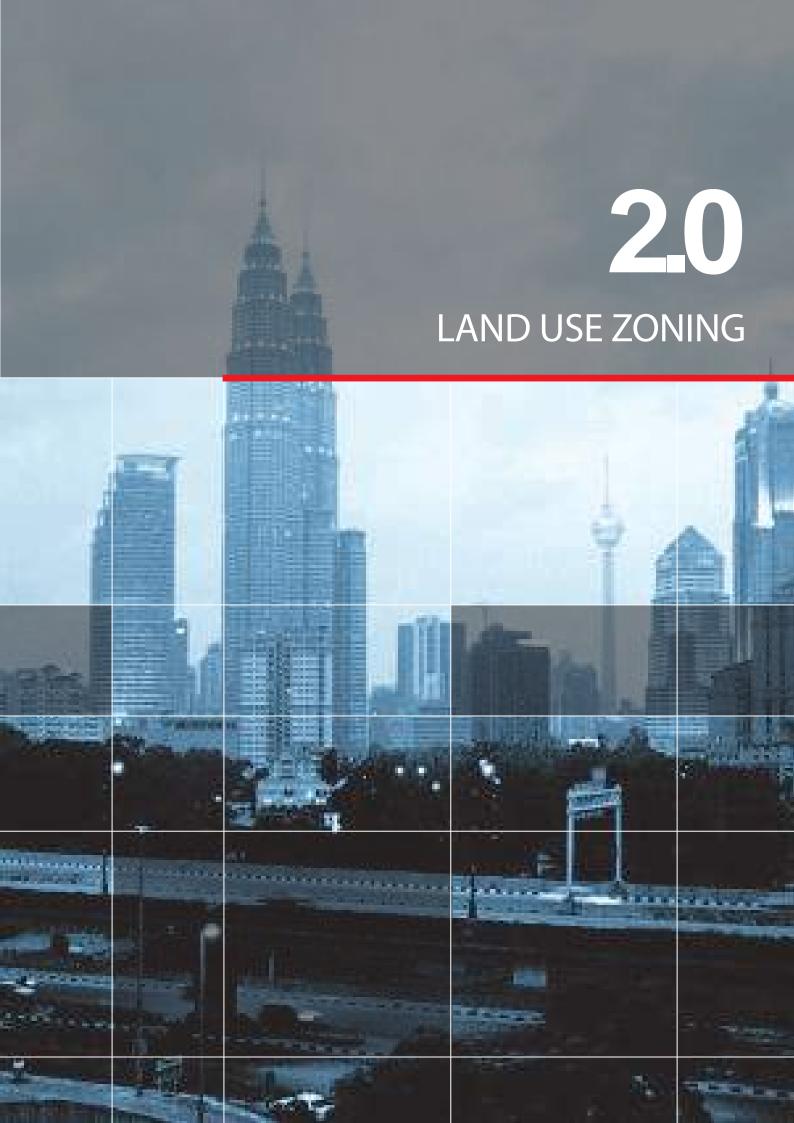
Please note that should any land proposed for development falls under these four specific zones, the requirements of the special planning control of these Special Planning Zones shall prevail over all other key planning and development control provisions i.e. DCP 1, DCP 2, DCP 3 and its Zoning and Use Class Schedule.

#### How to Use the KLDCP 2008

The KLDCP 2008 contains five main parts that is explained in the next five chapters of this document i.e.:-

- Land Use Zoning
- **Environmental Protection Zone**
- Heritage Zone
- Height Control Zone
- Transit Planning Zone

All users of this document shall find four easy steps to guide them in making preparation for plan making for development proposals.



#### **Sustainable Land Use for Kuala Lumpur** 2.1

The Draft KL City Plan 2020 has called for sustainable use of land to overcome the rising issues of urbanisation by managing land use in a manner that the needs of the present communities are met without compromising the needs of the future communities in Kuala Lumpur. Key initiatives and strategies have been identified in the Draft KL City Plan 2020 to achieve these objectives and the KLDCP 2008 has established a Land Use Zoning Map that will reflect the tenets of sustainable use of land which includes:-

- Strengthening the development of existing commercial and employment centres
- Meeting the needs of new economies that will enhance Kuala Lumpur as a global city
- Integrating land use development with transportation network
- Protection of environmentally sensitive areas
- Encouraging development on Infill sites and redevelopment/ revitalisation of existing sites that is no longer economic Kuala Lumpur
- Protection of heritage areas and sites in Kuala Lumpur
- Improvement of quality of living for new villages, Malay Reserve Land, traditional villages as well as public housing areas in the city
- Protection and provision of parks and open spaces in the city
- Enhancement of its natural assets including rivers and hills
- Protection of stable areas in the city
- Encouraging the development of varied types of housing in the city to meet the needs of all its local communities

#### 2.2 **Land Use Zoning and Planning Permission**

The **KLDCP 2008** has designate land use zone that reflects the predominant use of land within each plot of land in Kuala Lumpur. The DCP 1 - Land Use **Zoning Map** shall regulate land use, prevent land-use conflict, and allow growth to occur in a sustainable manner. Thus all applications for planning permission that involves any change in the use of land or building must refer to DCP 1 - Land Use Zoning Map (Section 20 (2 (i)) of the Federal Territory Planning Act 1982 (Act 267) states that a planning permission will be required for any change in use of land and building unless and otherwise prescribed by the Commissioner as not requiring planning permission.

This also means that no planning permission shall be granted, if any such propose activity is not allowed for in the designated land use zone of the DCP 1 - Land Use Zoning Map.

# DCP 1 – Land Use Zoning Map and Zoning Schedule

## 2.3.1 DCP 1 - Land Use Zoning Map

The DCP 1 - Land Use Zoning Map shows the distribution of land use zoning in a spatial form for the entire Kuala Lumpur city. It is a seamless map that has designate 20 land use zones to reflect its predominant use allowed for that particular plot of land. This essentially means that the plot of land can only be allowed to be developed as per the designated land use zone (if currently differs). However, such rights are only excise upon application and approvals for planning permission.

The 20 designated land use zones is characterised by its function as well as it primary use. In general, it can be reflected that the land use zones in Kuala Lumpur are within 7 main classification of use as follows:-

Table 2.1 - Types of Land Use Zones in Kuala Lumpur

Primary Use / Activity	Land Use Zones
Commercial	City Centre Commercial District Centre Commercial Neighbourhood Centre Commercial Commercial
Mixed Use	Mixed Use Commercial Mixed Use Residential Mixed Use Commercial and Industry
Residential	Residential 1 Residential 2 Residential 3 Established Housing Area Public Housing
Industrial and Special Industries	Industrial Technology Park
Institution	Public Institution Private Institution
Open Space	Public Open Space Private Open Space
Special Use	Forest Reserve White Zone (community facilities, utility and infrastructure)

For the purpose of KLDCP 2008, the above 20 land use zones is seen as one comprehensive list of classification and its detail description is described in Table 2.2: Land Use Zoning Classification.

Table 2.2 - Land Use Zoning Classification

Land Use Zoning	Abbreviation	Description
City Centre Commercial	CCC	Refers to commercial areas generally located within the Kuala Lumpur City Centre and some part of Damansara Penchala Strategic Zone as identified in the Zoning Plan. It is the highest hierarchy of commercial zone that reflects its function as the main commercial zone within Kuala Lumpur.
District Centre Commercial	DCC	Refers to commercial areas located within the boundary of district growth centres as identified in the Zoning Plan. Neighbourhood NCC Refers to commercial land located within Centre Commercial residential neighbourhood and serves the neighbourhood area to provide small-scale retail and services Intensity of development and type of activities permissible should be consistent with the character of the neighbourhood area in order to limit adverse impacts on nearby residential lots.
Neighbuorhood Centre Commercial	NCC	Refers to commercial areas located within residential neighbourhood and serves the neighbourhood area to provide small-scale retail and services. Intensity of development and type of activities permissible should be consistant with the character of the neighbourhood area in order to limit adverse impacts on nearby residential lots.
Commercial	С	Only commercial uses and activities are allowable in this area i.e. 100% commercial.
Mixed Use Commercial	MXC	Refers to land zoned for mixed-use commercial development in the Zoning Plan. It is intended to promote residential component in commercial zone and at transit planning zones. The zone is predominantly commercial with minimum 30% residential.
Mixed Use Residential	MXR	Refers to land zoned for mixed-use development within general residential area as identified in the Zoning Plan. It is intended to promote commercial component in residential zone and at transit planning zone. This zone is generally located within transit planning zone to encourage street level activities. This zone is predominantly residential where the commercial activity allowed is not more than 30% of the allowable gross floor area.

Land Use Zoning	Abbreviation	Description
Mixed Use Commercial	MXCI	Refers to land zoned for mixed-use industry and & Industry commercial development as identified in the Zoning Plan. It is intended to promote clustering of similar activities incorporating compatible and non-polluting industrial activities with support services such as storage, distributive trade, and service industries. This zone is predominantly industrial with maximum 30% commercial use.
Residential 1	R1	This residential zone refers to low density residential of 4 persons to 40 persons per acre.
Residential 2	R2	This residential zone refers to medium density residential of 48 persons to 120 persons per acre.
Residential 3	R3	This residential zone refers to high density residential of 160 persons to 400 persons per acre.
Established Housing Area	ЕНА	Areas which consist of housing development (old or new) that are in existence and well planned and is expected to remain residential within the planning period of the Local Plan.
Public Housing	РН	Refers to land designated for public housing only and these includes KLCH's and government's public housing areas.
Industrial	IP	Area designated and zoned for manufacturing and its associated activities in the Zoning Plan
Technology Park	TP	Zone designated for Technology Park Malaysia in Bukit Jalil.
Public Institutional	PI1	Major institutional and civic uses such as art galleries, museum, government offices, palace reserve, military reserve, health & emergency such as police head quarters, hospitals, universities, and other civic use.
Private Institutional	PI2	Institutional uses such as private art galleries, private museum, health services etc.
Public Open Space	OS1	Parks and open spaces are public spaces that provides for a variety of active and passive needs of ages of the community and local residents.

Land Use Zoning	Abbreviation	Description
Private Open Space	OS2	Private open space refers to parks and green areas that is owned by individuals or businesses and are used at the discretion of the owner. Private parks are privately maintained and usage to the public is at the discretion of the owners. One example of such park in Kuala Lumpur is the golf courses.
Forest Reserve	FR	Bukit Nanas Forest Reserve, Bukit Sungai Besi Forest Reserve and Bukit Sungai Puteh Forest Reserve.
White Zone	WZ	Refer to public facilities, infrastructure and utilities services.

The 20 land use zones described above is illustrated on the Land Use Zoning Map (Figure 2.1). As mentioned, the land use zoning is prescribed for every plot of land in Kuala Lumpur. For more detail reference, the grid indexed maps is provided in Volume 2 Part 2 – Land Use and Intensity Maps by Index.

#### **Mixed Use Commercial and Mixed Use Residential**

Mixed use development encourages a more integrated form of development, where living and work spaces integrates together. Thus in these zones, it is important to ensure both uses (commercial and residential) are planned with the highest consideration for the other.

Mixed-use refers to the combining of retail/commercial and/or service use with residential or office use in the same building or on the the same site in one of the following ways:

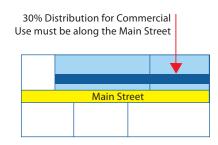
- Vertical Mixed-use. A single structure with the elevated floors used for residential or office use and a portion of the ground floor for retail/commercial or service use.
- Horizontal Mixed-use Attached. A single structure, which provides retail/commercial or service use in the portion fronting the public or private street with attached residential or office use behind this.
- Horizontal Mixed-use Detached. Two or more structures on one site which provide retail/ commercial or service use in the structure (s) fronting the public or private street, and residential or office use in seperate structure (s) behind or to the side.

The specific purpose of these zones are:-

- To encourage residential use in conjunction with commercial and other compatible activities in order to create an active street life, enhance the vitality of business, and reduce vehicular traffic.
- Residential, commercial and other non-residential uses shall be provided in such a manner as to be co-dependent, functionally integrated and complementary of each other rather than random, non-integrated and having non-associated elements.

- To encourage intensification of developments at identified transit-planning zones.
- To provide, where appropriate, higher density residential use integrated into the overall mixeduse development.
- To assure compatibility of the proposed land uses by incorperating higher standards of project and site planning.

As the zoning prescribed are on a lot base manner, it is also important to note that the application of mixed use development for large plots of land or contiguous plots meets the planning and urban design intent for such zoning. This includes consideration for distribution of commercial spaces in a Mixed Use Residential where the commercial floor space (maximum of 30%) must be placed at the main street and at the same level of the street, to encourage street level activities and public surveillance.



## **Special Planning Zones**

Please also note that the Special Planning Zones as mentioned in Chapter 1 of the KLDCP 2008 is shown as an overlay on the Land Use Zoning Map. For easy reference, a Special Planning Zone Map (Figure 2.2) shows the areas in Kuala Lumpur where these Special Planning Zones has to be given due consideration in any planning application. All applications within these zones must make further reference to the specific zone indicated i.e. to make reference to:-

- SPZ 1 Environmental Protection Zones Map (refer Chapter 4)
- SPZ 2 Heritage Zone Map (refer Chapter 5)
- SPZ 3 Height Control Zone Map (refer Chapter 6)
- SPZ 4 Transit Planning Zone Map (refer Chapter 7)



#### 2.3.2 Zoning Schedule

The land use zones is supported by a Zoning Schedule which identifies specifically the ancillary, related or compatible activities within the permissible predominant use of each particular land use zone. The Land Use Zoning Map and the Zoning Schedule are to be read together in determining the type of all ancillary or related uses that may be allowed for the support or management of the predominant use.

The Zoning Schedules form an integral part of the Development Control Plan and must be referred to each time planning and development is to be undertaken in Kuala Lumpur. There are two forms of Zoning Schedule i.e.:-

**Zoning Schedule (General)** which applies generally to the zone identified for each of the 20 land use zoning classification

Example:

Table ZS 2.0 Zoning Schedule (General) Land Use Zone : District Centre Commercial Zone

Zoning Schedule (Specific) which applies to a specified area of the same land use zoning classification

Example:

Table ZS 2.1 Zoning Schedule (Specific) Land Use Zone : District Centre Commercial Zone

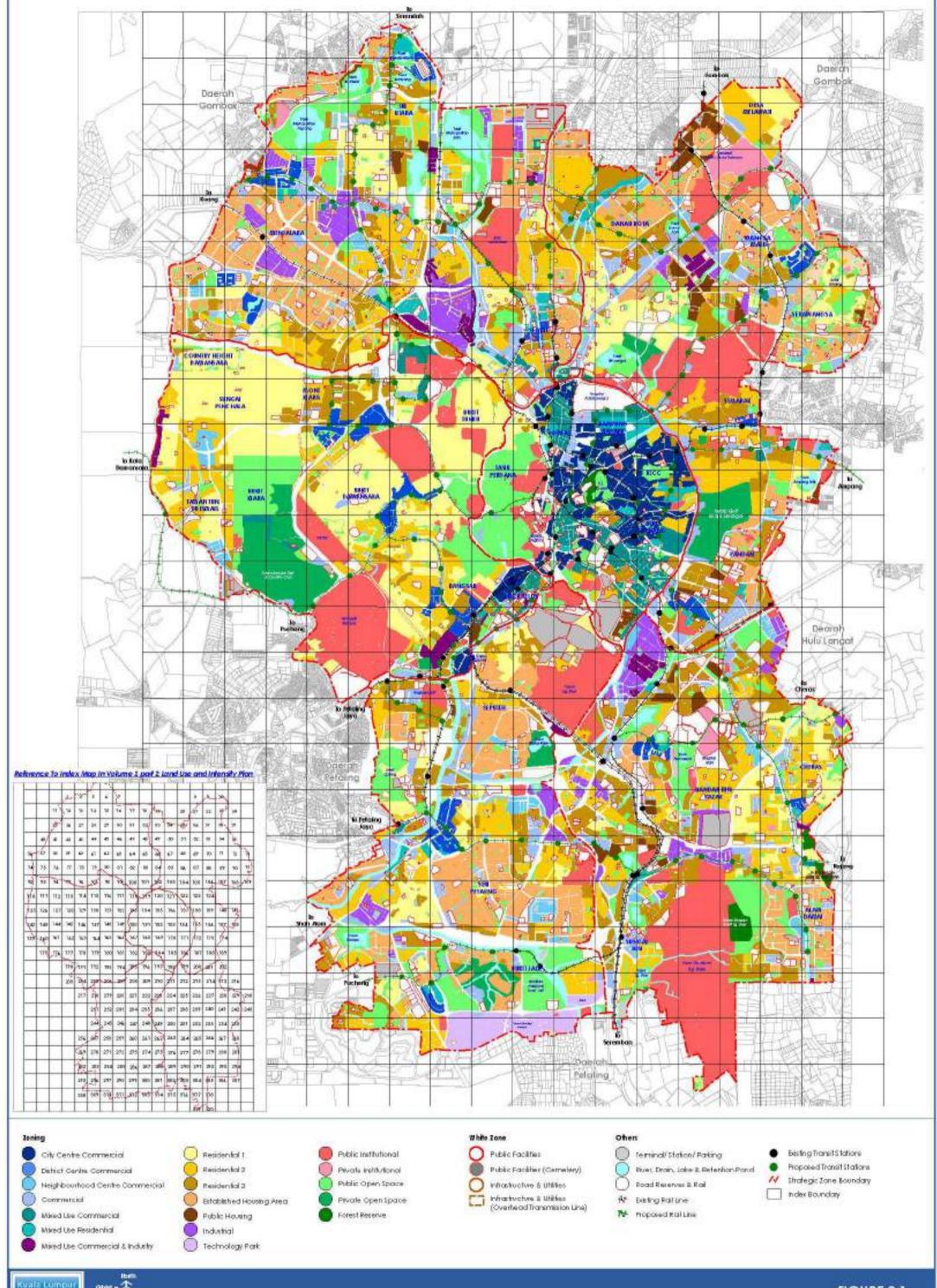
Location Bandar Bukit Jalil

## Zoning Schedule (Specific)

Unless otherwise stated in the Zoning Schedule (General), Zoning Schedule (Specific) is to be prescribed to certain areas of the land use zones only. The situation where specific areas are prescribed or limited to specific use classification can vary as follows:-

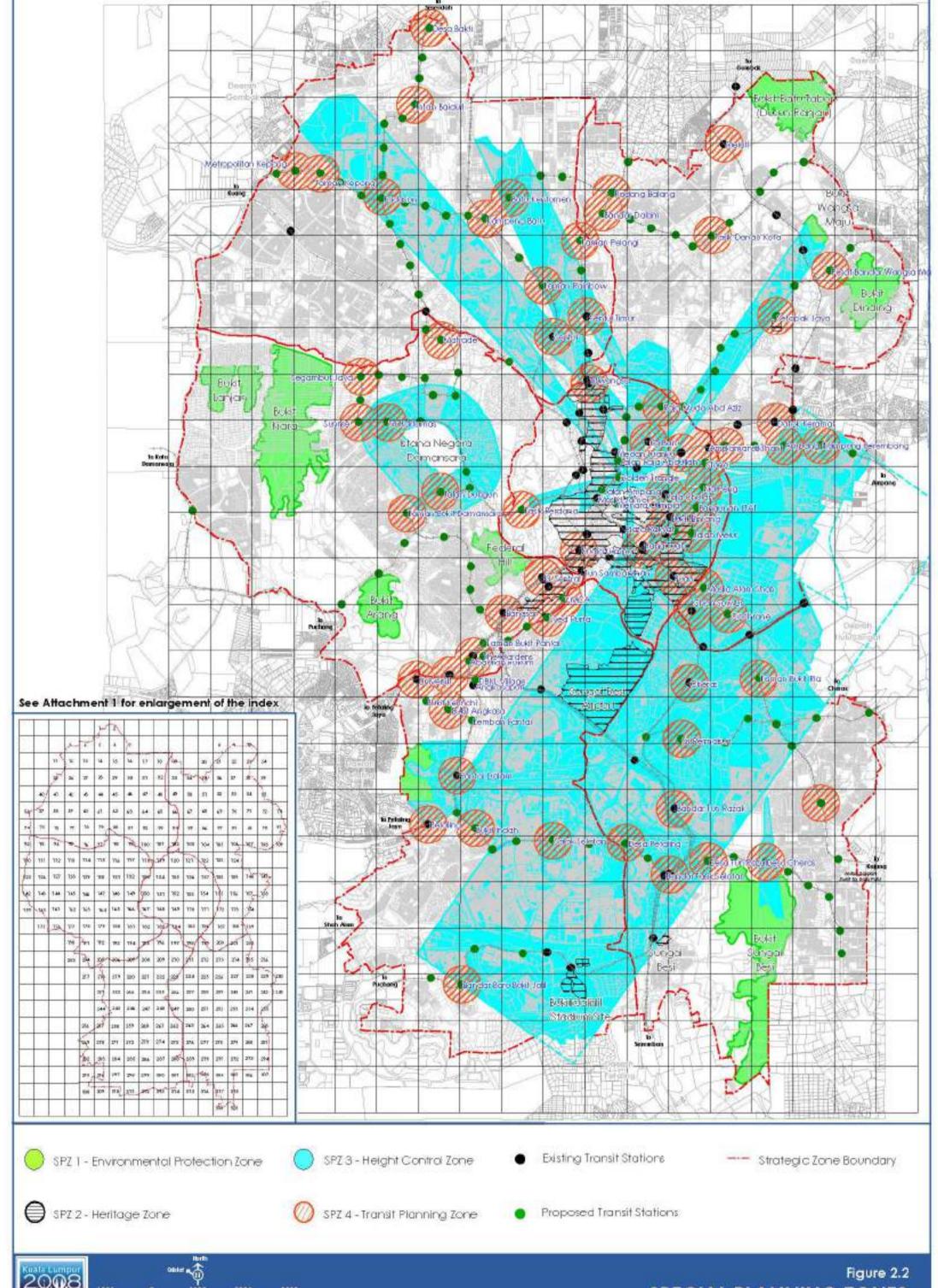
- · Where the area has characteristics peculiar only to the area compared to other areas zoned as the same land use zone;
- · Where only limited classes of use can be allowed within the area due to reasons such as compatibility of activities with the surrounding area or impact of the activities onto surrounding areas in terms of local amenity or the environment; and
- Where certain activities are to be promoted to the area specified which generally might not be permissible to other areas within the same land use zones.

Table 2.3 shows the Listing of Zoning Schedules for KLDCP 2008 while the Zoning Schedules detail description for the 20 Land Use Zones are listed from Table ZS 1.0 - ZS 20.0.











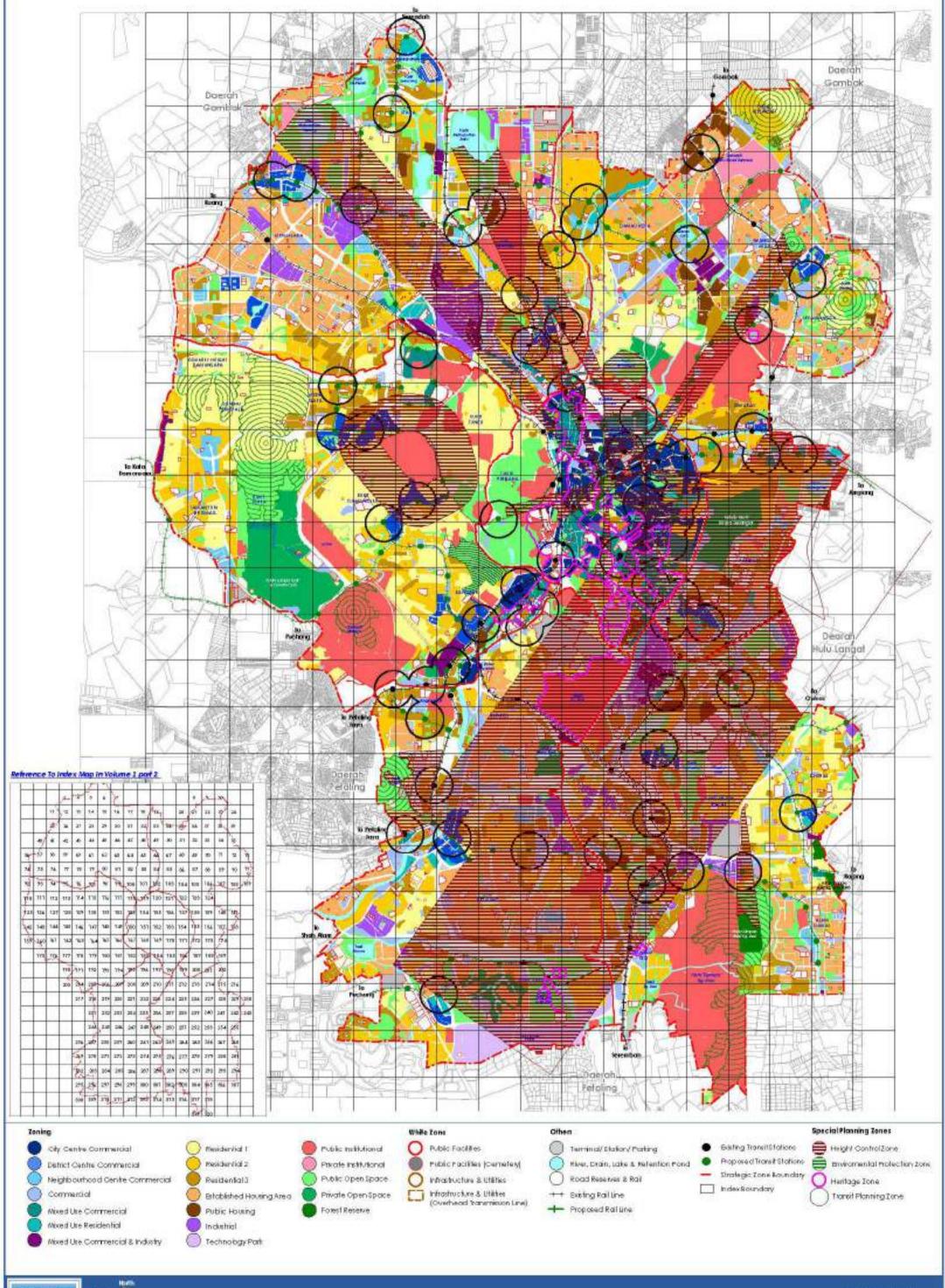






Table 2.3 - Listing of Zoning Schedules for KLDCP 2008

Ref No	Zoning Schedule	Ref No	Zoning Schedule
	(General)		(Specific)
ZS 1.0	City Centre Commercial Zone	ZS 1.1	Kampong Bharu Precinct A
ZS 2.0	District Centre Commercial	ZS 2.1	Bandar Bukit Jalil
		ZS 2.2	Bukit Indah
		ZS 2.3	Bandar Seri Permaisuri
ZS 3.0	Neighbourhood Centre	ZS 3.1	Chan Sow Lin
	Commercial Zone		
ZS 4.0	Zoning Schedule (General)	ZS 4.1	Kg Pasir
	Commercial	ZS 4.2	Kg Kerinchi
		ZS 4.3	Jalan Maarof
		ZS 4.4	Pekan Sungai Besi
ZS 5.0	Zoning Schedule (General)	ZS 5.1	Jalan Raja Muda Abdul Aziz
-	Mixed Use Commercial	ZS 5.2	Jalan Raja Muda Abdul Aziz - Jalan Raja Bot
ZS 6.0	Zoning Schedule (General)	ZS 6.1	Bukit Indah
	Mixed Use Residential	ZS 6.2	Chan Sow Lin
		ZS 6.3	Bandar Sri Permaisuri
ZS 7.0	Mixed Use Commercial	ZS 7.1	Chan Sow Lin
	and Industry		
ZS 8.0	Residential 1		
ZS 9.0	Residential 2		
ZS 10.0	Residential 3	ZS 10.1	Pantai Dalam
		ZS 10.2	Chan Sow Lin
		ZS 10.3	Kg Baru Salak Selatan
		ZS 10.4	Rumah Murah Bandar Tun Razak
		ZS 10.5	Razak Mansion
ZS 11.0	Established Housing Area		
ZS 12.0	Public Housing		
ZS 13.0	Industrial	ZS 13.1	Chan Sow Lin
		ZS 13.2	Taman Sg Besi
ZS 14.0	Technology Park		
ZS 15.0	Public Institutional		
ZS 16.0	Private Institutional		
ZS 17.0	Public Open Space		
ZS 18.0	Private Open Space		
ZS 19.0	Forest Reserve		
ZS 20.0	White Zone		

## 2.3.3 Zoning Schedule and Permitted Use Classes

The Zoning Schedule relates to a list of uses and activities that is allowed within the predominant use of the Land Use Zone. As such these lists of uses and activities have been classified into a comprehensive list of Use Classes for the KLDCP 2008.

The Use Classes consists of activities that are group together within certain similar uses or classification. Five (5) main use classes has been identified and these use classes reflects the activities that shall be out rightly permitted, conditionally permitted or totally prohibited for land and buildings located within each particular land use zone.

The permissible, conditionally permissible or prohibited use of land and building also refers to the planning permission granted to a planning application made in a particular land use zone in Kuala Lumpur. The following is the interpretation of the three conditions of allowable

Table 2.4 - Conditions For Allowable Uses

Conditions	Description
Permissible (P)	Land and building use and activities that <b>IS ALLOWED</b> within a particular land use zone.
Conditionally Permitted (C)	Land and building use and activities that SHALL BE ALLOWED WITH SPECIAL CONDITIONS imposed on a particular plot or plots of land within a land use zone. The City Hall of Kuala Lumpur however has the right to reject the planning application if the application does not meet the special conditions imposed onto the particular plot/plots of land within a land use zone.
Totally Prohibited (N)	Land and building use and activities that SHALL NOT be allowed within a particular land use zone.

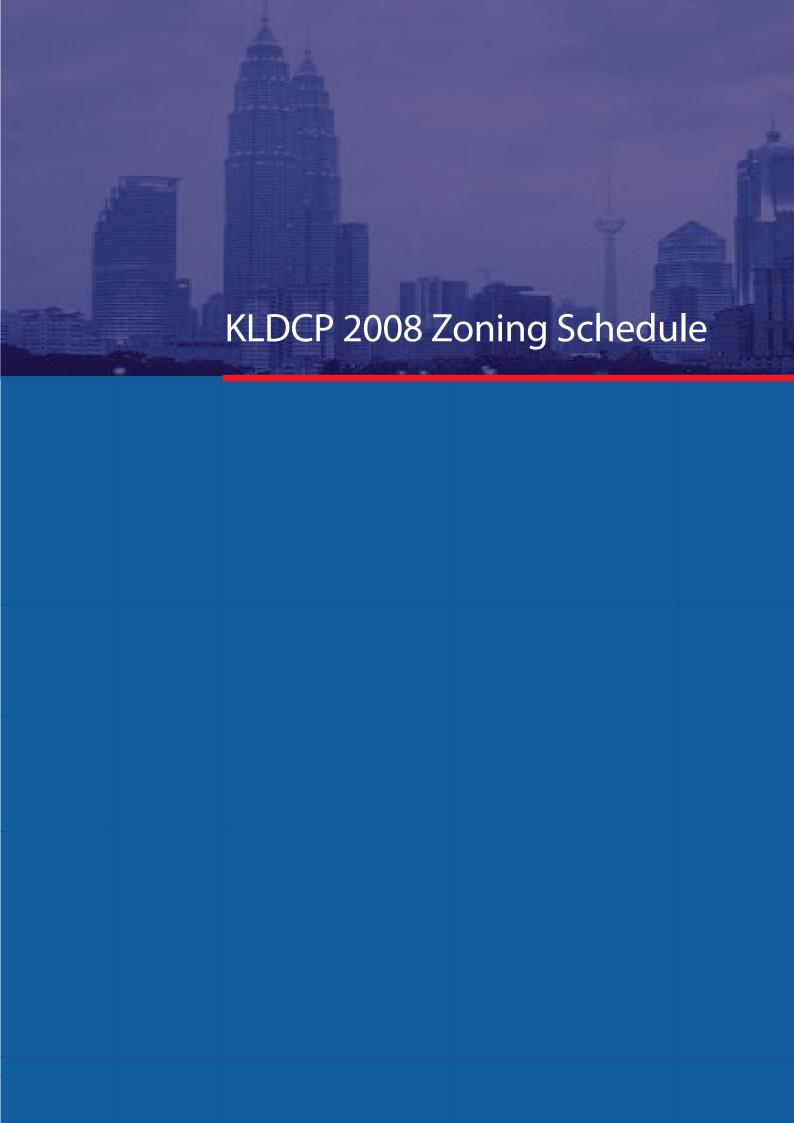
A Permitted Use Table of Land Use Zoning and Use Classes in Table 2.5 best illustrates the application of the Zoning Schedule and its Permitted Use Classes.

Table 2.5 - Permitted Use Table

# **Land Use Zoning**

										und	030	. 20.	5	<u> </u>						
Class	SSS	DCC	NCC	U	MXC	MXR	MXCI	R1	R2	R3	ЕНА	PH	ط	TP	P1	P2	051	052	FR	WZ
Class A1																				
Single Dwelling Units	N	Ν	Ν	Ν	Ν	Ν	Ν	Р	Р	Ν	Ν	Ν	Ν	Ν	Ν	Ν	Ν	Ν	Ν	Ν
Class A2																				
Multi Dwelling Units	Р	Р	Ν	Ν	Р	Р	Ν	C	Р	Р	Ν	Р	Ν	Ν	C	Ν	Ν	Ν	Ν	Ν
Class A3																				
Workers quarters	N	Ν	Ν	Ν	C	Р	C	C	Р	Р	Ν	Ν	C	C	Р	Р	Ν	C	Ν	C
Class A4																				
Hostels	Р	C	C	C	C	C	N	Ν	Ν	C	N	C	Ν	Ν	Р	Р	Ν	Ν	Ν	C
Class A5																				
Residential Care Homes	Р	P		N		C	N		C	С	N	С	N	N	P	P	N	N	N	
Class	SSS	DCC	NCC	U	MXC	MXR	MXCI	R1	R2	R3	ЕНА	PH	Ы	TP	P1	P2	051	052	FR	WZ
Class B1																				
General Retail and Office	Р	Р	Р	Р	Р	C	C	Ν	Ν	C	Ν	C	Ν	C	C	C	C	Ν	Ν	N
Class B2																				
Food and Drink	Р	Р	Р	Р	Р	C	C	Ν	C	C	N	C	C	C	C	C	C	C	Ν	C
Class B3																				
Entertainment	Р	C	N	C	C	Ν	N	N	N	N	N	N	Ν	Ν	Ν	Ν	Ν	N	Ν	N
Class B4																				
Leisure	Р	C	N	C	C	C	N	N	N	N	N	N	N	N	N	N	C	C	N	N
Class B5																				
Theme Park	С	С	N	С	С	N	N	N	N	N	N	N	N	N	N	N	N	Р	N	N
Class B6		_				_									_					
Hotels	Р	С	С	С	С	С	N	N	N	N	N	N	N	N	С	С	N	С	N	N
Class B7		_		_	-	_										_				
Service Apartment Class B8	Р	Р	N	С	Р	С	N	N	N	N	N	N	N	N	N	C	N	N	N	<u>N</u>
Home Based Business	Р	Р	N	NI	Р	Р	NI	_	C	C	NI	C	NI	NI	N	NI	NI	NI	NI	NI
Class B9	Р	<u> </u>	IN	N	<u> </u>		N	С			N		N	N	N	N	N	N	N	<u>N</u>
Hypermarket	N	C	N	N	N	N	Р	N	N	N	N	N	N	N	N	N	N	N	N	Ν
Class B10	IN		11	111	14	14	<u>'</u>	14	14	111	IN	IN	14	IN	14	11	14	11	114	
Funeral Homes & Services	N	N	Ν	C	Ν	N	Ν	N	N	Ν	N	C	C	Ν	N	Ν	N	N	N	C
Turicial Florries & Scrvices	11	- 14	- 11					- 14	- 14	- 1 1	- 1 1			- 1 1		- 14	14	14	- 14	
Class	222	DCC	NCC	U	MXC	MXR	MXCI	R1	R2	R3	EHA	PH	≙	T	P1	P2	051	052	FR	WZ
Class C1																				
Showroom	c	C	C	Р	C	N	C	N	N	N	N	N	Р	N	N	N	N	N	Ν	Ν
Class C2				-		11		11	11	IN	IN	IN		11	11	IN	11	11	11	
Wholesale, Storage	С	C	N	C	Ν	Ν	Р	N	N	Ν	N	Ν	Р	N	N	N	Ν	Ν	N	N
and Distribution		_	11	_	14	14	'	14	14	11	14	14		14	14	14	14	14	14	11
Class C3																				
Recycling Operations	N	Ν	Ν	N	Ν	Ν	C	Ν	Ν	Ν	Ν	Ν	C	Ν	Ν	Ν	Ν	Ν	Ν	Ν
Class C4																				<u> </u>
Petrol Station	C	C	C	C	Ν	Ν	C	Ν	Ν	Ν	Ν	Ν	Р	Ν	Ν	Ν	Ν	Ν	Ν	Ν
Class C5																				
Motorcycle and	C	C	Ν	C	Ν	Ν	Р	Ν	Ν	Ν	Ν	Ν	Р	Ν	Ν	Ν	Ν	Ν	Ν	Ν
Motorcar Retail																				
Class C6																				
Motorcycle and	N	Ν	Ν	C	Ν	Ν	C	Ν	Ν	Ν	Ν	Ν	Р	Ν	Ν	Ν	Ν	Ν	Ν	Ν
Motorcar Repairs,																				
Service Centres and																				
Workshops	L																			
Class C7																				_
Manufacturing Industry	N	Ν	Ν	N	Ν	Ν	C	Ν	N	N	N	N	Р	N	Ν	Ν	Ν	N	N	N

Class	222	DCC	NCC	U	MXC	MXR	MXCI	R1	R2	R3	ЕНА	ЬН	Ы	TP	P1	P2	051	082	FR	MZ
Class C8 Printing and Publication	N	N	N	N	N	N	С	N	N	N	N	N	Р	N	N	N	N	N	N	N
Class C9 Service Industry	N	N	N	N	N	N	C	N	N	N	N	N	Р	N	N	N	N	N	N	N
Class C10 Technology, Research and Development	Р	Р	N	Р	Р	N	Р	N	N	N	N	N	Р	Р	Р	N	Р	N	N	N
Class	CCC	DCC	NCC	U	MXC	MXR	MXCI	R1	R2	R3	ЕНА	ЬН	Ы	TP	P1	P2	051	052	FR	MZ
Class D1 Medical Facilities Class D2	С	С	N	С	С	С	N	N	N	N	N	N	N	N	Р	Р	N	N	N	<u>P</u>
Religious Facilities Class D3	С	С	С	С	С	С	N	N	С	С	N	Р	С	С	Р	Р	N	С	N	Р
Education	С	С	C	С	Р	C	N	С	С	С	С	С	N	N	Р	Р	N	N	N	Р
Class D4 Civic Use	Р	Р	Р	N	Р	С	N	N	С	С	N	Р	N	N	Р	Р	С	N	N	Р
Class D5 Integrated Community and Cultural Complex	Р	Р	Р	N	С	С	N	N	N	N	N	С	N	N	Р	Р	С	N	N	Р
Class	CCC	DCC	NCC	U	MXC	MXR	MXCI	R1	R2	R3	ЕНА	ЬН	ط	TP	P1	P2	051	052	FR	WZ
Class D6 Sports and Recreation Facilities	С	С	С	С	С	С	N	С	С	С	N	С	С	С	Р	Р	Р	Р	N	С
Class D7 Golf Facilities	N	N	N	С	N	N	N	N	N	N	N	N	N	N	N	N	С	Р	N	N
Class D8 Institution	С	С	С	N	Р	N	N	N	N	N	N	N	N	N	Р	Р	N	N	С	С
Class D9 Secure Residential Institution	N	N	N	С	С	N	N	N	N	N	N	N	N	N	Р	N	Р	N	N	N
Class D10 Infrastructure and Utilities	С	С	С	С	С	С	С	С	С	С	С	С	С	С	С	С	N	С	N	P
Class D11 Emergency and Security	Р	Р	Р	Р	Р	Р	Р	С	С	С	С	С	N	N	N	N	N	N	N	Р
Class	CCC	DCC	NCC	U	MXC	MXR	MXCI	R1	R2	R3	ЕНА	ЬН	В	TP	P1	P2	051	052	FR	WZ
Class E1 Parking	С	С	С	С	С	С	С	N	С	С	N	С	Р	С	C	С	С	С	N	С



#### **KLDCP 2008 ZONING SCHEDULE**

Table ZS 1.0 Zoning Schedule (General)

**Land Use Zone** CITY CENTRE COMMERCIAL ZONE (CCC)

#### 1. **Description of Zone**

Refers to commercial areas generally located within the Kuala Lumpur City Centre and somepart of Damansara Penchala Strategic Zone as identified in the Zoning Plan. It is the highest hierarchy of commercial zone that reflects its function as the main commercial zone within Kuala Lumpur.

#### 2. **Permitted Uses**

2.1	Class A2	Multi Dwelling Units

- Hostels Class A4 2.2
- 2.3 Class A5 **Residential Care Homes**
- Class B1 General Retail and Office 2.4
- 2.5 Class B2 Food and Drink
- Class B3 Entertainment 2.6
- Class B4 Leisure 2.7
- 2.8 Class B6 Hotels
- 2.9 Class B7 **Service Apartments**
- 2.10 Class B8 **Home Based Business**
- 2.11 Class C10 Technology, Research and Development
- Class D4 2.12
- 2.13 Class D5 Integrated Community and Cultural Complex
- 2.14 Class D11 **Emergency and Security**

#### 3. **Prohibited Use**

3.1	Class A1	Single Dwelling Units
3.2	Class A3	Workers quarters
3.3	Class B9	Hypermarket
3.4	Class B10	Funeral Home & Services
3.5	Class C3	Recycling Operations
3.6	Class C6	Motorcycle and Motorcar Repairs, service centres and workshops
3.7	Class C7	Manufacturing Industry
3.8	Class C8	Printing and Publication
3.9	Class C9	Service Industry
3.10	Class D7	Golf Facilities
3.11	Class D9	Secure Residential Institution

#### **Conditional Uses** 4.

#### **Class B5 - Theme Park** 4.1

- Activity shall not be permitted in Heritage Zone.
- Shall only be considered on sites located within Transit Planning Zones where existing transit facilities are already in place.
- Traffic Impact Assessment (TIA) must be conducted and submitted to KLCH during submission of planning approval.
- Sufficient parking spaces must be provided in accordance to the numbers required and to be determined by KLCH.
- Outdoor theme park will not be permitted in areas adjacent to existing residential areas or developments.

#### Land Use Zone **CITY CENTRE COMMERCIAL ZONE (CCC)**

#### 4.2 Class C1 - Showroom

- Only sales and display of related items are allowed. Repair and services are not allowed.
- Open-air display area is not permitted.
- Loading and unloading activities must be undertaken at the back of the building through back lanes, rear service lanes or dedicated service/loading/unloading bay. Provisions for such activities must be made available.
- Only allowed in areas where back lane/rear lane (to allow for loading and unloading activities) is of a minimum of 12m.

#### Class C2 - Wholesale, storage and distribution 4.3

- Not exceeding 2000 square feet (185 sq.m.)
- Storage of goods and material only allowed within buildings. Outdoor storage and storage of non-operating vehicles are not allowed.
- Loading and unloading activities must be undertaken at the back of the building through back lanes, rear service lanes or dedicated service/loading/unloading bay. Provisions for such activities must be made available.
- · Only allowed in areas where back lane/rear lane (to allow for loading and unloading activities) is of a minimum of 12m.
- · No direct access from main road shall be allowed, areas with service roads are highly recommended.
- Accessory office use which is customarily ancillary to wholesaling is allowed be it enclosed or 'open area' but it shall not exceed 10% of the total floor area.

#### **Class C4 - Petrol station** 4.4

- Primary use or activity is the sale of petrol and related items.
- Accessory activities such as repair and services of vehicles including motorcycles are not allowed in the CCC.
- A new petrol station will not be allowed on any site where the range of visibility the road to and from the accesses is inadequate.
- Accessory retail use is not allowed in an enclosed area only but is limited to 20 sqm.

#### 4.5 Class C5 - Motorcycle and Motorcar Retail

- Activities allowable are restricted only to sale and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Body painting, spraying, welding and knocking are not permitted.
- Grease and oil trap must be installed in the premises. Oil and grease must not be discharged into communal drain. Owner or operator must take appropriate measures to discharge oil and grease at designated and appropriate discharge centres.
- Verandahway, public walkway and roadside parking spaces must not be obstructed.
- Allowable only at the second layer of lot from the road.

**Land Use Zone CITY CENTRE COMMERCIAL ZONE (CCC)** 

#### 4.6 **Class D1 – Medical facilities**

- Shall be located within highly accessible area of the city.
- Sufficient access and service areas must be provided to provide for emergency response
- Other facilities such as parking requirement, support facilities and etc must comply with planning requirements of KLCH.

#### 4.7 Class D2 – Religious facilities

Principal use must be for religious activities only and must support appropriate population catchments.

#### Class D3 - Education 4.8

- Shall be located within the denser and highly accessible areas, preferably within the Transit Planning Zones, Mixed Use Residential areas and near Multi Dwelling areas and along major public transport routes.
- Pedestrian linkages must be made available from transit centres and other areas.

#### Class D6 - Sports and recreation facilities 4.9

- · Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion.
- Use should be compatible with adjacent land use.

# 4.10 Class D8 - Institution

Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.

### 4.11 Class D10 – Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

# 4.12 Class E1 - Parking

Parking spaces for cars and motorcycles only. Heavy vehicles and depot use are NOT allowed.

#### 5. **Areas Under Zoning Schedule (Special)**

Nil

**DISTRICT CENTRE COMMERCIAL ZONE (DCC) Land Use Zone** 

#### 1. **Description of Zone**

Refers to commercial land located within the boundary of district growth centres as identified in the Zoning Plan.

The district commercial zones are in:-

- Wangsa Maju
- 2. Dato' Keramat
- Maluri 3.
- 4. Jalan Kepong
- 5. Bandar Menjalara
- 6. Sentul Raya
- 7. Sri Hartamas
- Damansara
- 9. Bangsar
- 10. Bukit Indah
- 11. Bukit Jalil
- 12. Bandar Tun Razak
- 13. Taman Segar

2.	Darm	ittad	Uses
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2.1	Class A2	Multi Dwelling Units
2.2	Class A5	Residential Care Homes
2.3	Class B1	General Retail and Office
2.4	Class B2	Food and Drink
2.5	Class B7	Service Apartments
2.6	Class B8	Home Based Business
2.7	Class C10	Technology, Research and Development
2.8	Class D4	Civic Use
2.9	Class D5	Integrated Community and Cultural Complex
2.10	Class D11	Emergency and Security

# **Prohibited Use**

3.1	Class A1	Single Dwelling Units
3.2	Class A3	Workers quarters
3.3	Class B10	Funeral Home & Services
3.4	Class C3	Recycling Operations
3.5	Class C6	Motorcycle and Motorcar Repairs, Service Centres and Workshops
3.6	Class C7	Manufacturing Industry
3.7	Class C8	Printing and Publication
3.8	Class C9	Service Industry
3.9	Class D7	Golf Facilities
3.10	Class D9	Secure Residential Institution

#### **Land Use Zone DISTRICT CENTRE COMMERCIAL ZONE (DCC)**

#### 4. **Conditional Uses**

#### Class A4 - Hostels 4.1

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian outes.
- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.
- Accessory Retail Use can be allowed on the ground floor and must be of street front.

#### 4.2 Class B3 - Entertainment

- Only allowable at the following district centres: Bukit Jalil, Bukit Indah, Sg Besi, Bandar Tun Razak, Taman Segar, Maluri, Datuk Keramat, Wangsa Maju, Sentul Raya, Sri Utara, Kepong, Sri Hartamas, Damansara and Bangsar.
- No accessory use shall be allowed other than the entertainment license given for a particular premise.
- Shall be located away from any residential use and its activities shall not a nuisance to its surrounding areas.

#### 4.3 Class B4 - Leisure

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion and noise.

#### 4.4 Class B5 - Theme park

- Shall only be considered on sites located within Transit Planning Zones where existing transit facilities are already in place.
- Traffic Impact Assessment (TIA) must be conducted and submitted to KLCH during submission of planning approval.
- Sufficient parking spaces must be provided in accordance to the numbers required and to be determined by KLCH.
- Outdoor theme park will not be permitted in areas adjacent to existing residential development.

#### 4.5 Class B6 - Hotels

- Budget hotels and affordable lodging accommodations only.
- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.

#### 4.6 **Class B9 - Hypermarket**

- Shall be located within the more highly accessible areas.
- The lot or land must be able to place at least 5,000 square meters commercial spaces with the support facilities are required i.e parking and loading and unloading bay.

#### 4.7 Class C1 – Showrooms

- Only sales and display of related items are allowed. Repair and services are not allowed.
- Open-air display area is not permitted. Open-air display area is not permitted. Overspills of activities on public road and parking spaces are not permitted.
- No direct access from main road is allowed, areas with service roads are recommended.
- The scale, intensity and operation of such use must not increase the volume of traffic.

**Land Use Zone DISTRICT CENTRE COMMERCIAL ZONE (DCC)** 

#### 4.8 Class C2 – Wholesale, storage and distribution

- Not exceeding 2000 square feet (185 sq.m.)
- Storage of goods and material only allowed within buildings. Outdoor storage and storage of non-operating vehicles are not allowed.
- Loading and unloading activities must be undertaken at the back of the building through back lanes, rear service lanes or dedicated service/loading/unloading bay. Provisions for such activities must be made available.
- Only allowed in areas where back lane/rear lane (to allow for loading and unloading activities) is of a minimum of 12m.
- No direct access from main road shall be allowed, areas with service roads are highly recommended.
- Accessory office use which is customarily ancillary to wholesaling is allowed be it enclosed or 'open area' but it shall not exceed 10% of the total floor area.

#### 4.9 Class C4 – Petrol station

- Primary use or activity is the sale of petrol and related items.
- Accessory activities such as repair and services of vehicles including motorcycles are limited to car washing, and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Waste oil collection facilities must be provided at all petrol stations and garages where ever possible.
- A new petrol station will not be allowed on any site where the range of visibility along the road to and from the accesses is inadequate. See Streetscape Guidelinesv on 'sight-lines'.
- Accessory retail use is allowed in an enclosed area only but is limited to 100 sq.m. net retail sales area.

# 4.10 Class C5 - Motorcycle and Motorcar Retail

- Activities allowable are restricted only to sale and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Body painting, spraying, welding and knocking are not permitted.
- Grease and oil trap must be installed in the premises. Oil and grease must not be discharged into communal drain. Owner or operator must take appropriate measures to discharge oil and grease at designated and appropriate discharge centres.
- Verandahway, public walkway and roadside parking spaces must not be obstructed.

### 4.11 Class D1 - Medical facilities

- Shall be located within highly accessible area of the city.
- Sufficient access and service areas must be provided to provide for emergency response
- Other facilities such as parking requirement, support facilities and etc must comply to planning requirements of KLCH.

**Land Use Zone DISTRICT CENTRE COMMERCIAL ZONE (DCC)** 

# 4.12 Class D2 – Religious facilities

Principal use must be for religious activities only and must support appropriate population catchments as determined in the Supplementary Planning Guidelines on Community Facilities.

### 4.13 Class D3 - Education

- Shall be located within denser and more highly accessible areas, preferably within the Transit Planning Zones, Mixed Use Residential areas and Multi Dwelling Residential areas and along major public transport routes.
- Pedestrian linkages from transit centres and other areas must be provided.

# 4.14 Class D6 - Sports and recreation facilities

- Shall be located within the denser and highly accessible areas, preferably within the Transit Planning Zones, Mixed Used Residential areas and along major public transport routes.
- Pedestrian linkages from surrounding areas must also be made available.
- The use shall not generate traffic congestion and create nuisance to its surrounding area.

### 4.15 Class D8 - Institution

Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.

### 4.16 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

# 4.17 Class E1 - Parking

Only for car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

#### **Areas Under Zoning Schedule (Special)** 5.

#### **Other Areas with Specific Conditions** 5.1

- Bandar Bukit Jalil (refer to table ZS 2.1)
- Bukit Indah (refer to table ZS 2.2)
- Bandar Tasik Permaisuri (refer to table ZS 2.3)

Table ZS 2.1 Zoning Schedule (Specific)

Land Use Zone DISTRICT CENTRE COMMERCIAL ZONE (DCC)

Location **BANDAR BUKIT JALIL** 

#### 1. **Description of Area**

The DCC is located in the main commercial centre of Bandar Bukit Jalil and the Bandar Bukit Jalil LRT station. It is planned to play an important role in the international zone.

#### 2. **Permitted Uses**

- 2.1 Class A2 Multi Dwelling Units
- Class B1 General Retail and Office 22
- 2.3 Class B2 Food and Drink
- Class B3 Entertainment 24
- 2.5 Class B4 Leisure
- 2.6 Class B6 Hotels
- 2.7 Class B7 Service Apartment
- 2.8 Class B9 Hypermarket
- Class B10 Funeral Home & Services 2.9
- 2.10 Class C1 Showroom
- 2.11 Class C10Technology, Research and Development
- 2.12 Class D4 Civic Use
- 2.13 Class D11 Emergency and Security

#### 3. **Prohibited Use**

- Class A1 Single Dwelling Units 3.1
- Class A3 Workers quarters
- 3.3 Class A5 Residential Care Homes
- 3.4 Class B5 Theme park
- 3.5 Class B8 Home Based Business
- Class C2 Wholesale, storage and distribution 3.6
- 3.7 Class C3 Recycling Operations
- 3.8 Class C5 Motorcycle Retail and Services
- 3.9 Class C6 Motorcar repair, service centres and workshop
- 3.10 Class C7 Manufacturing Industry
- 3.11 Class C9 Service Industry
- 3.12 Class D7 Golf Facilities
- 3.13 Class D9 Secure Residential Institution

#### 4 **Conditional Uses**

#### 4.1 Class A4 – Hostels

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Bandar Bukit Jalil LRT Station) and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.
- Activities on the ground floor must be of those related to retail activities to ensure high pedestrian activities and street retail frontages.



Table ZS 2.1 Zoning Schedule (Specific)

**Land Use Zone** DISTRICT CENTRE COMMERCIAL ZONE (DCC)

Location **BANDAR BUKIT JALIL** 

#### Class C4 - Petrol station 4.2

Only sale of petrol and other related items. Repair and services of vehicles including motorcycles are not allowed.

#### 4.3 Class C8 – Printing and publication

- Only digital printing not involving polluting effluent.
- · Activity shall not generate unreasonable noise.

#### Class D1 - Medical facilities 4.4

- Shall be located within the more highly accessible portion of the area.
- Other facilities such as parking requirement, support facilities and etc must comply with planning requirements of KLCH.

#### Class D2 - Religious facilities 4.5

Activity as ancillary use to main activity and must support appropriate population catchments.

#### Class D3 - Education 4.6

Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Bandar Bukit Jalil LRT Station) and along major public transport routes and pedestrian routes.

#### **Class D5 – Integrated Community and Cultural Complex** 4.7

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Bandar Bukit Jalil LRT Station) and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion.
- Sites should be located so as to maximize compatibility with adjacent land use

#### Class D6 – Sports and recreation facilities 4.8

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Bandar Bukit Jalil LRT Station) and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion.
- Sites should be located so as to maximize compatibility with adjacent land use.

#### Class D8 - Institution 4.9

Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Bandar Bukit Jalil LRT Station) and along major public transport routes and pedestrian routes.

### 4.10 Class D10 - Infrastructure and Utilities

 Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

# 4.10 Class E1 - Parking

Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

Table ZS 2.2 Zoning Schedule (Specific)

Land Use Zone DISTRICT CENTRE COMMERCIAL ZONE (DCC)

Location **BUKIT INDAH** 

#### 1. **Description of Area**

The DCC is located in the commercial areas of Taman United and Taman Yarl. The Bukit Indah LRT station is nearby and will be the focus of development along the TOD concept. This DCC will function as the main commercial component of Seputeh.

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### **Permitted Uses**

- Class A5 Residential Care Homes 2.1
- 2.2 Class B1 General Retail and Office
- 2.3 Class B2 Food and Drink
- Class B6 Hotels 24
- 2.5 Class B7 Service Apartment
- 2.6 Class B9 Hypermarket
- 2.7 Class B10 Funeral Home & Services
- 2.8 Class C1 Showroom
- Class C10Technology, Research and Development 2.9
- 2.10 Class D4 Civic Use
- 2.11 Class D11 Emergency and Security

#### 3. **Prohibited Use**

- 3.1 Class A1 Single Dwelling Units
- 3.2 Class A2 Multi Dwelling Units
- 3.3 Class A3 Workers quarters
- 3.4 Class B4 Entertainment
- Class B6 Theme park 3.5
- 3.6 Class B9 Home Base Business
- 3.7 Class C3 Recycling Operations
- 3.8 Class C7 Manufacturing Industry
- 3.9 Class C9 Service Industry
- 3.10 Class D5 Integrated Community and Cultural Complex
- 3.11 Class D7 Golf Facilities
- 3.12 Class D8 Secure Residential Institution

#### 4. **Conditional Uses**

#### **Class A4 - Hostels** 4.1

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Bukit Indah LRT Station) and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.
- Activities on the ground floor must be of those related to retail activities to ensure high pedestrian activities and street retail frontages.

Table ZS 2.2 Zoning Schedule (Specific)

Land Use Zone **DISTRICT CENTRE COMMERCIAL ZONE (DCC)** 

Location **BUKIT INDAH** 

#### 4.2 Class B4 – Leisure

Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Bukit Indah LRT Station) and along major public transport routes and pedestrian routes.

The scale, intensity and operation of the use shall not generate traffic congestion and noise.

#### 4.3 Class C2 – Wholesale, storage and distribution

- Not exceeding 2000 square feet (185 sqm).
- Only storage of goods and material within buildings. Outdoor storage and storage of nonoperating vehicles are not allowed.
- Loading and unloading activities must be undertaken at the back of the building through back lanes, rear service lanes or dedicated service/loading/unloading bay.

#### Class C4 - Petrol station 4.4

Only sale of petrol and related items. Repair and services of vehicles including motorcycles are not allowed.

#### 4.5 Class C5 – Motorcycle retail and services

- Activities allowable are restricted only to sale and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Body painting, spraying, welding and knocking are not permitted.
- Grease and oil trap must be installed in the premises. Oil and grease must not be discharged into communal drain. Owner or operator must take appropriate measures to discharge oil and grease at designated and appropriate discharge centres.
- Verandahway, public walkway and roadside parking spaces must not be obstructed.

#### Class C6 - Motorcar repair, service centres and workshop 4.6

- Can only be considered for activity within shop house premises.
- Activities allowable are restricted only to sale and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Body painting, spraying, welding and knocking are not permitted.
- Grease and oil trap must be installed in the premises. Oil and grease must not be discharged into communal drain. Owner or operator must take appropriate measures to discharge oil and grease at designated and appropriate discharge centres.
- Verandahway, public walkway and roadside parking spaces must not be obstructed.

#### 4.7 Class C8 – Printing and publication

- Only digital printing not involving polluting effluent.
- Activity shall not generate unreasonable noise.

#### Class D1 - Medical facilities 4.8

- Shall be located within the more highly accessible portion of the area.
- Other facilities such as parking requirement, support facilities and etc must comply to planning requirements of KLCH.

Table ZS 2.2 Zoning Schedule (Specific)

**Land Use Zone DISTRICT CENTRE COMMERCIAL ZONE (DCC)** 

Location **BUKIT INDAH** 

#### 4.9 Class D2 – Religious facilities

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

## 4.10 Class D3 - Education

Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.

# 4.11 Class D6 - Sports and recreation facilities

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Bukit Indah LRT Station) and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion.
- Sites should be located so as to maximize compatibility with adjacent land use.

# 4.12 Class D8 - Institution

Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Bukit Indah LRT Station) and along major public transport routes and pedestrian routes.

# 4.13 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

# 4.13 Class E1 - Parking

Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

Table ZS 2.3 Zoning Schedule (Specific)

Land Use Zone **DISTRICT CENTRE COMMERCIAL ZONE (DCC)** 

Location **BANDAR SERI PERMAISURI** 

#### 1. **Description of Area**

The DCC is made up of the commercial area of Bandar Sri Permaisuri and proposed Tasik Permaisuri LRT station. It is planned to be the main commercial component of Bandar Tun Razak.

#### 2. **Permitted Uses**

- Class A5 Residential Care Homes 2.1
- 2.2 Class B1 General Retail and Office
- Class B2 Food and Drink 2.3
- Class B6 Hotels 24
- Class B7 Service Apartment 2.5
- Class B9 Hypermarket 2.6
- 2.7 Class C10Technology, Research and Development
- 2.8 Class D4 Civic Use
- Class D5 Integrated Community and Cultural Complex 2.9
- 2.10 Class D11 Emergency and Security

#### 3. **Prohibited Use**

- Class A1 Single Dwelling Units 3.1
- Class A2 Multi Dwelling Units 3.2
- Class A3 Workers quarters 3.3
- 3.4 Class B3 Entertainment
- Class B5 Theme park 3.5
- 3.6 Class B8 Home Based Business
- 3.7 Class B10 Funeral Home & Services
- Class C3 Recycling Operations 3.8
- 3.9 Class C7 Manufacturing Industry
- 3.10 Class C9 Service Industry
- Class D7 Golf Facilities 3.11
- 3.12 Class D8 Secure Residential Institution

#### **Conditional Uses** 4.

#### 4.1 Class A4 - Hostels

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Tasik Permaisuri LRT station) and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.
- Activities on the ground floor must be of those related to retail activities to ensure high pedestrian activities and street retail frontages.

#### 4.2 Class B4 - Leisure

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Tasik Permaisuri LRT station) and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion and noise.



Table ZS 2.3 Zoning Schedule (Specific)

**DISTRICT CENTRE COMMERCIAL ZONE (DCC) Land Use Zone** 

Location **BANDAR SERI PERMAISURI** 

#### 4.3 Class C1 - Showrooms

- Only sales of related items are allowed. Repair and services are not allowed.
- · Open-air display area is not permitted. Open-air display area is not permitted. Overspills of activities on public road and parking spaces are not permitted.
- Must be located within the more highly accessible areas.
- The scale, intensity and operation of such use must not increase the volume of traffic.

#### 4.4 Class C2 – Wholesale, storage and distribution

- Not exceeding 2000 square feet (185 sqm).
- · Only storage of goods and material within buildings. Outdoor storage and storage of nonoperating vehicles are not allowed.
- · Loading and unloading activities must be undertaken at the back of the building through back lanes, rear service lanes or dedicated service/loading/unloading bay.

#### 4.5 Class C4 – Petrol station

 Only sale of petrol and related items. Repair and services of vehicles including motorcycles are not allowed.

#### 4.6 Class C5 – Motorcycle retail and services

- Activities allowable are restricted only to sale and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Body painting, spraying, welding and knocking are not permitted.
- · Grease and oil trap must be installed in the premises. Oil and grease must not be discharged into communal drain. Owner or operator must take appropriate measures to discharge oil and grease at designated and appropriate discharge centres.
- Verandahway, public walkway and roadside parking spaces must not be obstructed.

#### 4.7 Class C6 – Motorcar repair, service centres and workshop

- Can only be considered for activity within shop house premises.
- Activities allowable are restricted only to sale and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Body painting, spraying, welding and knocking are not permitted.
- Grease and oil trap must be installed in the premises. Oil and grease must not be discharged into communal drain. Owner or operator must take appropriate measures to discharge oil and grease at designated and appropriate discharge centres.
- Verandahway, public walkway and roadside parking spaces must not be obstructed.

#### 4.8 Class C8 – Printing and publication

- Only digital printing not involving polluting effluent.
- Activity shall not generate unreasonable noise.

#### 4.9 Class D1 – Medical facilities

- Shall be located within the more highly accessible portion of the area.
- Other facilities such as parking requirement, support facilities and etc must comply to planning requirements of KLCH.

Table ZS 2.3 Zoning Schedule (Specific)

**Land Use Zone DISTRICT CENTRE COMMERCIAL ZONE (DCC)** 

Location **BANDAR SERI PERMAISURI** 

### 4.10 Class D2 – Religious facilities

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

### 4.11 Class D3 - Education

Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Tasik Permaisuri LRT station) and along major public transport routes and pedestrian routes.

### 4.12 Class D6 - Sports and recreation facilities

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Tasik Permaisuri LRT station) and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion.
- Sites should be located so as to maximize compatibility with adjacent land use.

### 4.13 Class D8 - Institution

Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Tasik Permaisuri LRT station) and along major public transport routes and pedestrian routes.

# 4.14 Class D11 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the

# 4.15 Class E1 - Parking

Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

**Land Use Zone NEIGHBOURHOOD CENTRE COMMERCIAL ZONE (NCC)** 

## **Description of Zone**

Refers to commercial land located within residential neighbourhood and serves the neighbourhood area to provide small-scale retail and services. Intensity of development and type of activities permissible should be consistent with the character of the neighbourhood area in order to limit adverse impacts on nearby residential lots.

#### 2. **Permitted Uses**

- Class B1 General Retail and Office 2.1
- 2.2 Class B2 Food and Drink
- 2.3 Class D4 Civic Use
- 2.4 Class D5 Integrated Community and Cultural Complex
- 2.5 Class D11Emergency and Security

#### 3. **Prohibited Use**

- 3.1 Class A1 Single Dwelling Units
- Class A2 Multi Dwelling Units 3.2
- 3.3 Class A3 Workers quarters
- 3.4 Class B3 Entertainment
- 3.5 Class B4 Leisure
- Class B5 Theme Park 3.6
- 3.7 Class B7 Service Apartments
- 3.8 Class B8 Home Based Business
- 3.9 Class B9 Hypermarket
- 3.10 Class B10 Funeral Homes and Services
- 3.11 Class C2 Wholesale, Storage and Distribution
- 3.12 Class C3 Recycling Operations
- 3.13 Class C5 Motorcycle and Motocar Retail
- 3.14 Class C6 Motorcar Repairs, Service Centres and Workshop
- 3.15 Class C7 Manufacturing Industry
- 3.16 Class C8 Printing and Publication
- 3.17 Class C9 Service Industry
- 3.18 Class C10Technology, Research and Development
- 3.19 Class D1 Medical Facilities
- 3.20 Class D7 Golf Facilities
- 3.21 Class D9 Secure Residential Institution

#### 4. **Conditional Uses**

### Class A4 - Hostels

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.
- Accessory Retail Use can be allowed on the ground floor and must be of street frontage.

Table ZS 3.0

Zoning Schedule (General)

**Land Use Zone** 

**NEIGHBOURHOOD CENTRE COMMERCIAL ZONE (NCC)** 

#### 4.2 **Class A5 – Residential Care Homes**

- Purpose built residential care homes are allowed within this zone and has to comply with the Supplementary Guidelines of KLCH.
- Such use shall be located within areas accessible to parks and public amenities as well as linked to the other areas of the neighbourhood centre especially public transport route. These linkages shall provide for barrier free environment.
- It shall also be located away from heavy traffic areas.

#### Class B6 - Hotels 4.3

- Budget hotels and affordable lodging accommodations only. Primary activity is confine to providing food and lodgings (more than 10 rooms).
- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- Entertainment activities are not allowed in hotels in a Neighbourhood Centre area.

#### 4.4 Class C1 - Showrooms

- Only display and sales of related items are allowed. Repair and services are not allowed.
- Open-air display area is not permitted. Overspills of activities on verandahway, parking spaces and public road are not permitted.
- Must be located along the main road, but no direct access to main road is allowed.
- · Low scale activities only allowed.
- Accessory office use which is customarily ancillary to wholesaling is allowed be it enclosed or 'open area' but it shall not exceed 10% of the total floor area.

#### 4.5 **Class C4 – Petrol station**

- Primary use or activity is the sale of petrol and related items.
- Accessory activities such as repair and services of vehicles including motorcycles are limited to car washing, and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Waste oil collection facilities must be provided at all petrol stations and garages where ever possible.
- A new petrol station will not be allowed on any site where the range of visibility along the road to and from the accesses is inadequate. See Streetscape Guidelines on 'sight-lines'.
- Accessory retail use is allowed in an enclosed area only but is limited to 100 sq.m. net retail sales area.

#### Class D2 – Religious facilities 4.6

- Only on the upper floors of shop house building.
- Principal use must be for religious activities only and must support appropriate population catchments as determined in the Supplementary Planning Guidelines on Community Facilities.

#### 4.7 Class D3 - Education

- Shall be located within accessible areas, preferably along major public transport routes.
- Pedestrian linkages from transit stops and other areas must be provided.
- For nursery or kindergarten located in NCC, it must be areas that are safe and shall be located within premise that has a service road.

**Land Use Zone NEIGHBOURHOOD CENTRE COMMERCIAL ZONE (NCC)** 

#### 4.8 Class D6 - Sports and recreation facilities

- Shall be located within accessible areas, preferably along major public transport routes.
- Pedestrian linkages from transit stops and other areas must be provided.
- The scale, intensity and operation of the use shall not generate traffic congestion.
- Sites should be located so as to maximize compatibility with adjacent land use.

#### Class D8 – Institution 4.9

- Shall be located within accessible areas, preferably along major public transport routes.
- Pedestrian linkages from transit stops and other areas must be provided.

### 4.10 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

### 4.11 Class E1 – Parking

• Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

#### 5. **Areas Under Zoning Schedule (Special)**

Chan Sow Lin (along Jalan Sg.Besi) (refer to table ZS 3.1)

Table ZS 3.1 Zoning Schedule (Specific)

**Land Use Zone NEIGHBOURHOOD CENTRE COMMERCIAL ZONE (NCC)** 

Location **CHAN SOW LIN (along Jalan Sg. Besi)** 

#### 1. **Description of Area**

The Chan Sow Lin NCC fronts Jalan Sg. Besi and BESRAYA Highway and forms the introduction to the automotive based industries for the city. The area is planned to be the centre for all automotive - related industries and is visible from the KL - Seremban Highway.

#### **Permitted Uses** 2.

- Class B1 General Retail and Office 2.1
- Class B2 Food and Drink 2.2
- Class C1 Showrooms 2.3

#### **Prohibited Use** 3.

- Class A1 Single Dwelling Units 3.1
- 3.2 Class A2 Multi Dwelling Units
- Class A3 Workers quarters 3.3
- Class A4 Hostels 3.4
- Class A5 Residential Care Homes 3.5
- 3.7 Class B5 Leisure
- 3.8 Class B6 Theme Park
- 3.9 Class B7 Hotels
- 3.10 Class B8 Service Apartment
- 3.11 Class B9 Home Base Business
- 3.12 Class B10 Hypermarket
- 3.13 Class B11 Funeral Home & Services
- 3.14 Class C2 Wholesale, Storage and Distribution
- 3.15 Class C3 Recycling Operations
- 3.16 Class C4 Petrol Station
- 3.17 Class C5 Motorcycle Retail and Services
- 3.18 Class C6 Motorcar Repairs, Service Centres and Workshop
- 3.19 Class C7 Manufacturing Industry
- 3.20 Class C9 Service Industry
- 3.21 Class C10Technology, Research and Development
- 3.22 Class D1 Medical Facilities
- 3.23 Class D2 Religious Facilities
- 3.24 Class D3 Education
- 3.25 Class D4 Civic Use
- 3.26 Class D5 Sports and recreation facilities
- Class D6 Golf 3.27
- 3.28 Class D7 Institution
- 3.29 Class D8 Secure Residential Institution

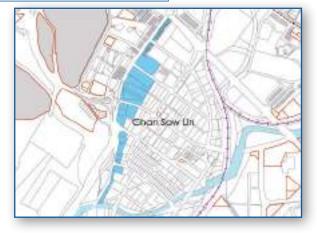


Table ZS 3.1 Zoning Schedule (Specific)

**NEIGHBOURHOOD CENTRE COMMERCIAL ZONE (NCC) Land Use Zone** 

Location **CHAN SOW LIN (along Jalan Sg. Besi)** 

#### **Conditional Uses** 4.

#### 4.1 Class B3 – Entertainment

· Only permitted on the ground and first floors of shop house or commercial buildings subject to licensing conditions.

#### 4.2 Class C8 - Printing and Publication

- Only digital printing, not involving polluting effluent.
- Activity shall not generate unreasonable noise.

#### 4.3 Class E1 – Parking

• Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

Table ZS 4.0 Zoning Schedule (General) **Land Use Zone COMMERCIAL (C)** 

#### **Description of Zone** 1.

Only commercial uses and activities are allowable in this area i.e. 100% commercial.

#### 2. **Permitted Uses**

- Class B1 General Retail and Office 2.1
- Class B2 Food and Drink 2.2
- 2.3 Class C1 Showroom
- Class C10 Technology, Research and Development 2.4
- 2.5 Class D11Emergency and Security

#### 3. **Prohibited Use**

- Class A1 Single Dwelling Units 3.1
- Class A2 Multi Dwelling Units 3.2
- 3.3 Class A3 Workers quarters
- 3.4 Class A5 Residential Care Homes
- Class B8 Home Based Business 3.5
- Class B9 Hypermarket 3.7
- 3.8 Class C3 Recycling Operations
- 3.9 Class C7 Manufacturing Industry
- 3.10 Class C8 Printing and Publication
- Class C9 Service Industry 3.11
- 3.12 Class D4 Civic Use
- 3.13 Class D5 Integrated Community and Cultural Complex
- 3.14 Class D8 Institution

#### **Conditional Uses** 4.

#### 4.1 Hostel

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.
- Accessory Retail Use can be allowed on the ground floor and must be of street frontage.

#### 4.2 Class B3 - Entertainment

- Only permitted on the ground and first floors of shop house or commercial buildings subject to licensing conditions.
- Activity shall not generate unreasonable traffic congestion and noise especially at night.
- No accessory use shall be allowed other than the entertainment license given for a particular premise.
- Accessory Retail Use can be allowed on the ground floor and must be of street frontage.

### Class B4 - Leisure

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion and noise.

Table ZS 4.0 Zoning Schedule (General) **Land Use Zone** COMMERCIAL (C)

#### 4.3 Class B5 – Theme park

- Shall only be considered on sites located within Transit Planning Zones where existing transit facilities are already in place. Activity shall not be permitted in Historic Zone.
- Traffic Impact Assessment (TIA) must be conducted and submitted to KLCH during submission of planning approval.
- Sufficient parking spaces must be provided in accordance to the numbers required and to be determined by KLCH.
- Outdoor theme park will not be permitted in areas adjacent to existing residential development.

#### Class B6 – Hotels 4.4

- Only permissible on purpose built buildings for primary use of a hotel.
- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.

#### 4.5 Class B7 – Service apartments

- Only permissible on purpose built buildings for primary use of a service apartment.
- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- Provision of amenities and facilities such as car parks, parks, pool etc. shall be determined by KLCH Supplementary Guidelines for Service Apartments.

#### Class B10 – Funeral Homes & Services 4.6

The scale, intensity and operation of the use shall not generate traffic congestion, noise and any nuisances.

#### 4.7 Class C2 – Wholesale, storage and distribution

- Not exceeding 2000 square feet (185 sq.m.)
- Storage of goods and material only allowed within buildings. Outdoor storage and storage of non-operating vehicles are not allowed.
- Loading and unloading activities must be undertaken at the back of the building through back lanes, rear service lanes or dedicated service/loading/unloading bay. Provisions for such activities must be made available.
- Only allowed in areas where back lane/rear lane (to allow for loading and unloading activities) is of a minimum of 12m.
- No direct access from main road shall be allowed, areas with service roads are highly recommended.
- Accessory office use which is customarily ancillary to wholesaling is allowed be it enclosed or open area' but it shall not exceed 10% of the total floor area.

#### 4.8 Class C4 – Petrol station

- Primary use or activity is the sale of petrol and related items.
- · Accessory activities such as repair and services of vehicles including motorcycles are limited to car washing, and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Waste oil collection facilities must be provided at all petrol stations and garages where ever possible.

#### Table ZS 4.0 Zoning Schedule (General) Land Use Zone **COMMERCIAL (C)**

- A new petrol station will not be allowed on any site where the range of visibility along the road to and from the accesses is inadequate. See Streetscape Guidelines on 'sight-lines'.
- Accessory retail use is allowed in an enclosed area only but is limited to 100 sq.m. net retail sales area.

#### 4.9 Class C5 - Motorcycle and Motorcar Retail

- Activities allowable are restricted only to sale and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Body painting, spraying, welding and knocking are not permitted.
- Grease and oil trap must be installed in the premises. Oil and grease must not be discharged into communal drain. Owner or operator must take appropriate measures to discharge oil and grease at designated and appropriate discharge centres.
- Verandahway, public walkway and roadside parking spaces must not be obstructed.

### 4.10 Class C6 – Motorcycle and motorcar repair, service centres and workshop

- Activities allowable are restricted only to sale and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Body painting, spraying, welding and knocking are not permitted.
- Grease and oil trap must be installed in the premises. Oil and grease must not be discharged into communal drain. Owner or operator must take appropriate measures to discharge oil and grease at designated and appropriate discharge centres.
- Verandahway, public walkway and roadside parking spaces must not be obstructed.

### 4.11 Class D1 – Medical facilities

- Shall be located within highly accessible area of the city.
- Sufficient access and service areas must be provided to provide for emergency response services. Other facilities such as parking requirement, support facilities and etc must comply to planning requirements of KLCH.

# 4.12 Class D2 - Religious facilities

Ancillary use to the primary use of the building as additional facilities for the buildings

### 4.13 Class D3 - Education

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion and noise.

### 4.14 Class D6 – Sports and recreation facilities

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion and noise.

# 4.15 Class D7 – Golf Facilities

- The scale, intensity and operation of the use shall not generate traffic congestion and noise.
- Only golf driving range is allowed and the fences must be providing with at least 15 meter

Table ZS 4.0 Zoning Schedule (General) Land Use Zone COMMERCIAL (C)

### 4.16 Class D9 – Secure residential institution

Only permissible on purpose built buildings along major roads and public transport routes

# 4.17 Class D10 - Infrastructure and Utilities

• Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

# 4.18 Class E1 - Parking

• Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

#### 5. **Areas Under Zoning Schedule (Special)**

- Kg. Pasir (refer to table ZS 4.1)
- Kg. Kerinchi (refer to table ZS 4.2)
- Jalan Maarof (refer to table ZS 4.3)
- Pekan Sg.Besi (refer to table ZS 4.4)

Table ZS 4.1 Zoning Schedule (Specific) **COMMERCIAL (C) Land Use Zone** 

Location **KG. PASIR** 

#### 1. **Description of Area**

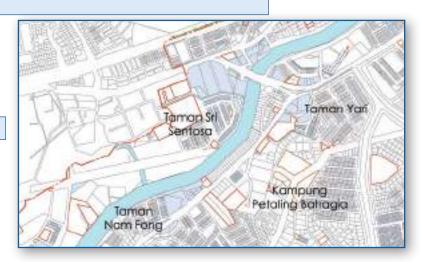
This commercial area is located near to the Petaling LRT station and the Jalan Klang Lama - NPE junction, which separates the area into 2 parts. It is surrounded by residential development.

#### 2. **Permitted Uses**

- Class B1 General Retail and Office 2.1
- 2.2 Class B2 Food and Drink
- 2.3 Class B9 Hypermarket
- 2.4 Class B10 Funeral Home & Services
- Class C1 Showroom 2.5
- 26 Class C4 Petrol Station
- 2.7 Class C10 Technology, Research and

Development

2.8 Class D11 Emergency and Security



#### 3. **Prohibited Use**

- Class A1 Single Dwelling Units 3.1
- 3.2 Class A2 Multi Dwelling Units
- Class A3 Workers quarters 3.3
- Class A4 Hostels 3.4
- 3.5 Class A5 Residential Care Homes
- Class B3 Entertainment 3.7
- Class B4 | Leisure 38
- 3.9 Class B5 Theme Park
- 3.10 Class B6 Hotels
- 3.11 Class B8 Home Base Business
- 3.12 Class C3 Recycling Operations
- 3.13 Class C7 Manufacturing Industry
- 3.14 Class C9 Service Industry
- 3.15 Class D1 Medical Facilities
- 3.16 Class D4 Civic Use
- Class D5 Integrated Community and Cultural Complex 3.17
- Class D7 Golf Facilities 3.18
- 3.19 Class D8 Institution

#### **Conditional Uses** 4.

#### 4.1 Class B7 – Service apartment

Subject to conditions and approvals by KLCH.

#### 4.2 Class C2 - Wholesale, storage and distribution

- Not exceeding 2000 square feet (185 sq m).
- Only storage of goods and material within buildings. Outdoor storage and storage of nonoperating vehicles are not allowed.
- Loading and unloading activities must be undertaken at the back of the building through back lanes, rear service lanes or dedicated service/loading/unloading bay.

Table ZS 4.1 Zoning Schedule (Specific) **Land Use Zone** COMMERCIAL (C)

Location KG. PASIR

#### 4.3 Class C5 – Motorcycle and Motorcar Retail

- Activities allowable are restricted only to sale and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Body painting, spraying, welding and knocking are not permitted.
- Grease and oil trap must be installed in the premises. Oil and grease must not be discharged into communal drain. Owner or operator must take appropriate measures to discharge oil and grease at designated and appropriate discharge centres.
- Verandahway, public walkway and roadside parking spaces must not be obstructed.

#### Class C6 – Motorcycle and Motorcar repair, service centres and workshop 4.4

- Activities allowable are restricted only to sale and service of tyres, batteries, minors ervices such as change of oil filters and wiring services, sale and services of accessories.
- Body painting, spraying, welding and knocking are not permitted.
- Grease and oil trap must be installed in the premises. Oil and grease must not be discharged into communal drain. Owner or operator must take appropriate measures to discharge oil and grease at designated and appropriate discharge centres.
- Verandahway, public walkway and roadside parking spaces must not be obstructed.

#### 4.5 **Class C8 - Printing and Publication**

- Only digital printing not involving polluting effluent.
- Activity shall not generate unreasonable noise.

#### 4.6 Class D2 – Religious facilities

Ancillary use to the primary use of the building as additional facilities for the buildings.

#### 4.7 Class D3 - Education

- Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community
- Only kindergarten/nursery are allowed.

# Class D6 - Sports and recreation facilities

The scale, intensity and operation of the use shall not generate traffic congestion and noise.

#### 4.9 Class D9 - Secure residential institution

Only permissible on purpose built buildings along major roads and public transport routes

# 4.10 Class D10 - Infrastructure and Utilities

The scale, intensity and operation of the use shall not generate traffic congestion and noise.

# 4.10 Class E1 - Parking

• Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

Table ZS 4.2 Zoning Schedule (Specific) **Land Use Zone COMMERCIAL (C)** Location KG. KERINCHI

#### 1. **Description of Area**

This commercial area of Kg. Kerinchi ia partly in a redevelopment area and fronting the Federal Highway. It will be developed into commercial centre not only for the local residents but also since it is facing Menara Telekom, an important landmark to the city.

#### 2. **Permitted Uses**

- 2.1 Class B1 General Retail and Office
- Class B2 Food and Drink 2.2
- 2.3 Class B9 Hypermarket
- Class C1 Showroom 2.4
- 2.5 Class C4 Petrol Station
- 2.6 Class C10 Technology, Research
  - and Development
- Class D11 Emergency and Security 2.7

#### 3. **Prohibited Use**

- Class A1 Single Dwelling Units 3.1
- Class A2 Multi Dwelling Units 3.2
- 3.3 Class A3 Workers quarters
- Class A4 Hostels 3.4
- Class A5 Residential Care Homes 3.5
- 3.6 Class B3 Entertainment
- Class B4 Leisure 3.7
- 3.8 Class B5 Theme park
- Class B6 Hotels 3.9
- 3.10 Class B8 Home Base Business
- Class B10 Funeral Home & Services 3.11
- 3.12 Class C2 Wholesale, storage and distribution
- 3.13 Class C3 Recycling Operations
- 3.14 Class C7 Manufacturing Industry
- 3.15 Class C9 Service Industry
- 3.16 Class D1 Medical Facilities
- Class D4 Civic Use 3.17
- 3.18 Class D5 Integrated Community and Cultural Complex
- 3.19 Class D6 Golf
- 3.20 Class D7 Institution

#### **Conditional Uses** 4.

#### 4.1 Class B8 - Service apartment

Subject to conditions and approvals by KLCH.



Table ZS 4.2 Zoning Schedule (Specific) **Land Use Zone** COMMERCIAL (C) Location **KG. KERINCHI** 

#### 4.2 Class C5 – Motorcycle and Motocar Retail

- Activities allowable are restricted only to sale and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Body painting, spraying, welding and knocking are not permitted.
- Grease and oil trap must be installed in the premises. Oil and grease must not be discharged into communal drain. Owner or operator must take appropriate measures to discharge oil and grease at designated and appropriate discharge centres.
- Verandahway, public walkway and roadside parking spaces must not be obstructed.

#### 4.3 Class C6 – Motorcycle and Motorcar repair, service centres and workshops

- Activities allowable are restricted only to sale and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Body painting, spraying, welding and knocking are not permitted.
- Grease and oil trap must be installed in the premises. Oil and grease must not be discharged into communal drain. Owner or operator must take appropriate measures to discharge oil and grease at designated and appropriate discharge centres.
- Verandahway, public walkway and roadside parking spaces must not be obstructed.

#### 4.4 Class C8 – Printing and publication

- Only digital printing not involving polluting effluent.
- Activity shall not generate unreasonable noise.

#### 4.5 Class D2 – Religious facilities

Ancillary use to the primary use of the building as additional facilities for the buildings.

#### 4.6 Class D3 - Education

- Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community
- Only kindergarden/nursery are allowed.

#### Class D6 - Sports and recreation facilities 4.7

The scale, intensity and operation of the use shall not generate traffic congestion and noise.

#### 4.8 Class D9 – Secure residential institution

Only permissible on purpose built buildings along major roads and public transport routes

#### 4.9 Class D10 - Infrastructure and utilities

Infrastructur and utilities are allowed in accordance to the technical requirements of the utility providers.

# 4.10 Class E1 - Parking

· Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

Table ZS 4.3 Zoning Schedule (Specific) **Land Use Zone** COMMERCIAL (C) Location **JALAN MAAROF** 

#### **Description of Zone** 1.

- 1.1 Bungalow lots along Jalan Maarof between Lot 6155 (Jln. Menaga) and Lot 6255 (beside Water Tank).
- 1.2 Bungalow lots along Jalan Maarof between Lot 28639 (Lrg. Maarof 5) and Lot 14888 (Lrg. Maarof 3).

#### 2. **Permitted Uses**

- 2.1 Class A1 Single Dwelling Units (continuing Use)
- Class A5 Residential Care Homes 2.2
- 2.3 Class B2 Food and Drink
- 2.4 Class B9 Home Based Business
- 2.5 Class D11 Emergency and Security

#### 3. **Prohibited Use**

- 3.1 Class A2 Multi Dwelling Units
- Class A3 Workers quarters 3.2
- Class A4 Hostels 3.3
- 3.4 Class B3 Entertainment
- 3.5 Class B5 Theme Park
- 3.6 Class B6 Hotels
- Class B7 Service Apartment 3.7
- Class B9 Hypermarket 3.8
- 3.9 Class B10 Funeral Home & Services
- 3.10 Class C2 Wholesale, Storage and Distribution
- 3.11 Class C3 Recycling Operations
- 3.12 Class C4 Petrol Station
- 3.13 Class C5 Motorcycle Retail and Services
- 3.14 Class C6 Motorcycle and motorcar Repair, Service Centres and Workshop
- 3.15 Class C7 Manufacturing Industry
- 3.16 Class C8 Printing and Publication
- 3.17 Class C9 Service Industry
- 3.18 Class C10 Technology, Research and Development
- 3.19 Class D3 Education
- 3.20 Class D4 Civic Use
- 3.21 Class D5 Integrated Community and Cultural Complex
- 3.22 Class D6 Sports and Recreation Facilities
- Class D7 Golf 3.23
- 3.24 Class D8 Institution

#### **Conditional Use** 4.

#### Class B1 - General Retail and Office 4.1

- Only several activities under Personal and Household Services are allows. The activities are:
  - Tailoring and dressmaking
  - **Astrologers**
  - Interior decorators (including office and showrooms)
  - Clinic (excludes animal clinic)
  - Spa & Therapy
  - Others activities are also allowable:-
  - Banking, financial institutions, stockbrokers, insurance and real estates.
  - Professional services
  - **Business support services**



Table ZS 4.3 Zoning Schedule (Specific) Land Use Zone COMMERCIAL (C) Location **JALAN MAAROF** 

#### 4.2 Class B4 – Leisure

Leisure activity is limited to art gallery only.

#### 4.3 Class C1 - Showrooms

- Only sales and display of related items are allowed. Repair and services are not allowed.
- Open-air display area is not permitted. No overspill of activities on public road and parking
- The scale, intensity and operation of such use must not cause traffic congestion.

#### 4.4 Class D1 - Medical Facilities

Medical Facilities is limited to medical specialist only.

#### 4.5 Class D2 – Religious Facilities

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

#### 4.7 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

#### 4.8 Class E1 – Parking

Only car and motorcycle parking spaces are permitted. Heavy vehicle and depot use are not allowed.

### **Additional Control**

# Restriction of physical changes to the premise

- No external extension to the existing premise to maintain the essential character of the area.
- In the case of amalgamation of lots, the adjoining buildings shall have a connecting structure which shall be (refer to diagram on right) :- set back from the main façade and rear façade of the building by 3m lower than Existing Buildings
- Internal renovation is permitted Internal renovation is permitted according to existing guidelines.
- Linksque mot ins taction higher than THAN DUTTE NO
- Building height is restricted to a maximum of 2 storeys.
- Building line shall be maintained at 6m front set back and 3m side and back set back from the existing boundary.

#### 5.2 **Restriction on outdoor activities**

- Business operations must take place only within the premise.
- Prohibit outdoor storage activities.

#### 5.3 Restriction on signage

- Size of signage to comply with existing DBKL advertising guidelines.
- Prohibit free standing signage or signage which is illuminated, flashing or moving.

Table ZS 4.4 Zoning Schedule (Specific) **Land Use Zone** COMMERCIAL (C) Location **PEKAN SG. BESI** 

#### 1. **Description of Area**

This commercial area forms the Sg. Besi town. It is located across the road from the Sq. Besi LRT station and is involved in redevelopment.

#### 2. **Permitted Uses**

- Class B1 General Retail and Office 2.1
- 2.3 Class B2 Food and Drink

#### 3. **Prohibited Use**

- Class A1 Single Dwelling Units 3 1
- 3.2 Class A2 Multi Dwelling Units
- Class A3 Workers quarters 3.3
- Class A4 Hostels 3.4
- 3.5 Class A5 Residential Care Homes
- Class B3 Entertainment 3.6
- 3.7 Class B4 Leisure
- Class B5 Theme park 3.8
- 3.9 Class B6 Hotels
- 3.10 Class B7 Service apartment
- 3.11 Class B8 Home Base Business
- 3.12 Class B9 Hypermarket
- 3.13 Class B10 Funeral Home & Services
- 3.14 Class C1 Showroom
- 3.15 Class C2 Wholesale, storage and distribution
- 3.16 Class C3 Recycling Operations
- 3.17 Class C4 Petrol Station
- 3.18 Class C5 Motorcycle retail and services
- 3.19 Class C6 Motorcar repair, service centres and workshop
- 3.20 Class C7 Manufacturing Industry
- 3.21 Class C8 Printing and Publication
- 3.22 Class C9 Service Industry
- 3.23 Class C10Technology, Research and Development
- 3.24 Class D1 Medical Facilities
- 3.25 Class D3 Education
- 3.26 Class D4 Civic Use
- 3.27 Class D5 Integrated Community and Cultural Complex
- 3.28 Class D6 Sports and recreation facilities
- 3.29 Class D7 Golf
- 3.30 Class D8 Institution
- 3.31 Class D9 Secure residential institution

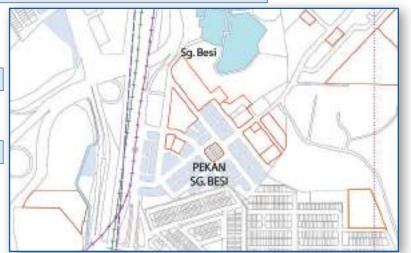
#### 4. **Conditional Uses**

#### 4.3 Class D2 - Religious facilities

Ancillary use to the primary use of the building as additional facilities for the buildings.

#### 4.9 Class E1 - Parking

Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.



**Land Use Zone MIXED USE COMMERCIAL (MXC)** 

# **Description of Zone**

Refers to land zoned for mixed-use commercial development in the Zoning Plan. It is intended to promote residential component in commercial zone and at transit planning zones. The zone is predominantly commercial with minimum 30% residential.

#### 2. **Permitted Uses**

- Class A2 Multi Dwelling Unit 2.1
- Class B1 General Retail and Office 2.2
- Class B2 Food and Drink 2.3
- 2.4 Class B7 Service Apartment
- Class B8 Home Based Business 2.5
- 26 Class C10 Technology, Research and Development
- 2.7 Class D3 Education
- 2.8 Class D4 Civic Use
- 2.9 Class D8 Institution
- 2.10 Class D11 Emergency and Security

#### 3. **Prohibited Use**

- 3.1 Class A1 Single Dwelling Units
- 3.2 Class B9 Hypermarket
- 3.3 Class B10 Funeral Home & Services
- Class C2 Wholesale, Storage and Distribution 3.4
- 3.5 Class C3 Recycling Operations
- 3.6 Class C4 Petrol Station
- 3.7 Class C5 Motorcycle and Motorcar Retail
- 3.8 Class C6 Motorcycle and Motorcar Repairs, Service Centres and Workshop
- 3.9 Class C7 Manufacturing Industry
- 3.10 Class C8 Printing and Publication
- 3.11 Class C9 Service Industry
- 3.12 Class D7 Golf Facilities

#### 4. **Conditional Uses**

#### 4.1 Class A3 – Workers quarters

- On purpose built premises.
- Located along public transport route and pedestrian routes.
- Sufficient buffers eq. Roads, car parking spaces, other uses that are non-conflicting such as shop houses, should be provided to reduce any risk or impact of any uncontrolled level of pollution.
- Community facilities required to be provided in accordance to requirement of the Supplementary Development Control Guidelines of provision of Community Facilities.

#### Class A4 - Hostels 4.2

- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.
- Activities on the ground floor must be of those related to retail activities to ensure high pedestrian activities and street retail frontages.

**Land Use Zone** MIXED USE COMMERCIAL(MXC)

#### 4.3 **Class A5 – Residential Care Homes**

- Appropriate licensing from the respective government department or agencies must be obtained.
- To ensure the safety, the building or area that allocated for these activities must be fully guarded.
- Facilities required to support such activities must be provided within the premises.

#### 4.4 Class B3 - Entertainment

Only permitted on the ground and first floors of shop house or commercial buildings subject to licensing conditions.

#### 4.5 Class B4 – Leisure

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion and noise

#### Class B5 – Theme Park 4.6

- Activity shall not be permitted in Historic Zone.
- Shall only be considered on sites located within Transit Planning Zones where existing transit facilities are already in place.
- Traffic Impact Assessment (TIA) must be conducted and submitted to KLCH during submission of planning approval.
- Sufficient parking spaces must be provided in accordance to the numbers required and to be determined by KLCH.
- Outdoor theme park will not be permitted in areas adjacent to existing residential development.

#### **Class B6 - Hotels** 4.7

- Only permissible on purpose built buildings for primary use of a hotel.
- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.

#### 4.8 Class C1 – Showrooms

- Only sales of related items are allowed. Repair and services are not allowed.
- Open-air display area is not permitted.
- Must be located within the more highly accessible areas.
- The scale, intensity and operation of such use must not increase the volume of traffic.

#### 4.9 Class D1 – Medical Facilities

- Shall be located within the more highly accessible portion of the area.
- Other facilities such as parking requirement, support facilities and etc must comply to planning requirements of KLCH.

**Land Use Zone** MIXED USE COMMERCIAL(MXC)

### 4.10 Class D2 – Religious Facilities

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

### 4.11 Class D5 – Integrated Community and Cultural Complex

- Shall be located within the more highly accessible portion, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- Shall be provided based on the planning standards and planning requirements.

# 4.12 Class D6 - Sports and Recreation Facilities

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion.
- Sites should be located so as to maximize compatibility with adjacent land use.

### 4.13 Class D9 – Secure Residential Institution

For the safety purposes, the area or building that allocated for these activities must be fully guarded to ensure there are no interferences.

### 4.14 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

# 4.15 Class E1 - Parking

Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

# **Areas Under Zoning Schedule (Special)**

- Jalan Raja Muda Abdul Aziz (Auto City Mall refer ZS 5.1)
- Jalan Raja Muda Abdul Aziz Jalan Raja Bot (Medical and Services refer ZS 5.2)

Table ZS 5.1 Zoning Schedule (Specific)

**Land Use Zone MIXED USE COMMERCIAL (MXC)** 

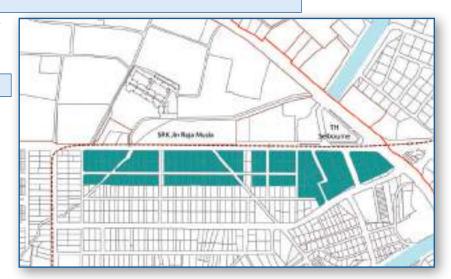
Location **JALAN RAJA MUDA ABDUL AZIZ (AUTO CITY MALL)** 

#### 1. **Description of Area**

This mixed use commercial area forms the Auto City Mall. It is located along Jalan Raja Muda Abdul Aziz

2.	Permitted Uses	
2.1	Class A2	Multi Dwelling Unit
2.2	Class B1	General Retail and Office
2.3	Class B2	Food and Drink
2.4	Class B7	Service Apartment
2.5	Class B8	Home Based Business
2.6	Class C5	Motorcycle and Motorcar
		Retail
2.7	Class C10	Technology, Research and
		Development
2.8	Class D3	Education
2.9	Class D4	Civic Use
2.10	Class D8	Institution

2.11 Class D11 Emergency and Security



#### 3. **Prohibited Use**

3.1	Class A1	Single Dwelling Units
3.2	Class B9	Hypermarket
3.3	Class B5	ThemePark
3.4	Class B10	Funeral Home & Services
3.5	Class C2	Wholesale, Storage and Distribution
3.6	Class C3	Recycling Operations
3.7	Class C4	Petrol Station
3.8	Class C6	Motorcycle and motorcar Repairs, Service Centres and Workshop
3.9	Class C7	Manufacturing Industry
3.10	Class C8	Printing and Publication
3.11	Class C9	Service Industry
3.12	Class D7	Golf Facilities

#### 4. **Conditional Uses**

#### 4.1 Class A3 – Workers quarters

- On purpose built premises.
- Located along public transport route and pedestrian routes.
- Sufficient buffers eg. Roads, car parking spaces, other uses that are non-conflicting such as shop houses, should be provided to reduce any risk or impact of any uncontrolled level of pollution.

Community facilities required to be provided in accordance to requirement of the Supplementary Development Control Guidelines of provision of Community Facilities.

#### 4.2 **Class A4 - Hostels**

- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.
- Activities on the ground floor must be of those related to retail activities to ensure high pedestrian activities and street retail frontages.

Table ZS 5.1 Zoning Schedule (Specific)

**MIXED USE COMMERCIAL (MXC)** Land Use Zone

Location JALAN RAJA MUDA ABDUL AZIZ (AUTO CITY MALL)

#### 4.3 Class A5 - Residential Care Homes

- · Appropriate licensing from the respective government department or agencies must be obtained.
- · To ensure the safety, the building or area that allocated for these activities must be fully guarded.
- Facilities required to support such activities must be provided within the premises.

#### 4.4 Class B3 - Entertainment

· Only permitted on the ground and first floors of shop house or commercial buildings subject to licensing conditions.

#### 4.5 Class B4 – Leisure

- · Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- · The scale, intensity and operation of the use shall not generate traffic congestion and noise.

#### 4.6 **Class B6 - Hotels**

- Only permissible on purpose built buildings for primary use of a hotel.
- · Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.

#### Class C1 - Showrooms 4.7

- Only sales of related items are allowed. Repair and services are not allowed.
- Open-air display area is not permitted.
- Must be located within the more highly accessible areas.
- The scale, intensity and operation of such use must not increase the volume of traffic.

#### Class D1 - Medical facilities 4.8

- Shall be located within the more highly accessible portion of the area.
- · Other facilities such as parking requirement, support facilities and etc must comply to planning requirements of KLCH.

#### 4.9 Class D2 – Religious facilities

 Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

# 4.10 Class D5 – Integrated Community and Cultural Complex

- Shall be located within the more highly accessible portion, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- Shall be provided based on the planning standards and planning requirements.

Table ZS 5.1 Zoning Schedule (Specific)

Land Use Zone **MIXED USE COMMERCIAL (MXC)** 

Location **JALAN RAJA MUDA ABDUL AZIZ (AUTO CITY MALL)** 

# 4.11 Class D6 - Sports and recreation facilities

- · Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion.
- Sites should be located so as to maximize compatibility with adjacent land use.

### 4.12 Class D9 – Secure residential institution

For the safety purposes, the area or building that allocated for these activities must be fully guarded to ensure there are no interferences.

## 4.13 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

# 4.14 Class E1 - Parking

Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

Table ZS 5.2 Zoning Schedule (Specific)

Land Use Zone **MIXED USE COMMERCIAL (MXC)** 

Location JALAN RAJA MUDA ABDUL AZIZ AND JALAN RAJA BOT

[ MEDICAL AND SERVICES ]

### 1. **Description of Area**

This mixed use commercial area forms the Medical and Services. It is located between Jalan Raja Muda Abdul Aziz and Jalan Raja Bot

2.	Permitted	Uses
2.1	Class A2	Multi Dwelling Unit
2.2	Class B1	General Retail and Office
2.3	Class B2	Food and Drink
2.4	Class B7	Service Apartment
2.5	Class B8	Home Based Business
2.6	Class C10	Technology, Research
		and Development
2.7	Class D3	Education
2.8	Class D4	Civic Use
2.9	Class D8	Institution
2.10	Class D1	Medical facilities
2.11	Class D11	Emergency and Security



## 3. **Prohibited Use**

3.1	Class A1	Single Dwelling Units
3.2	Class B9	Hypermarket
3.3	Class B10	Funeral Home & Services
3.4	Class C2	Wholesale, Storage and Distribution
3.5	Class C3	Recycling Operations
3.6	Class C4	Petrol Station
3.7	Class C5	Motorcycle and Motorcar Retail
3.8	Class C6	Motorcycle and motorcar Repairs, Service Centres and Workshop
3.9	Class C7	Manufacturing Industry
3.10	Class C8	Printing and Publication
3.11	Class C9	Service Industry
3.12	Class D7	Golf Facilities

### **Conditional Uses** 4.

## 4.1 Class A3 – Workers quarters

- On purpose built premises.
- Located along public transport route and pedestrian routes.
- · Sufficient buffers eg. Roads, car parking spaces, other uses that are non-conflicting such as shop houses, should be provided to reduce any risk or impact of any uncontrolled level of pollution.
- · Community facilities required to be provided in accordance to requirement of the Supplementary Development Control Guidelines of provision of Community Facilities.

Table ZS 5.2 Zoning Schedule (Specific)

Land Use Zone MIXED USE COMMERCIAL(MXC)

Location JALAN RAJA MUDA ABDUL AZIZ AND JALAN RAJA BOT

[ MEDICAL AND SERVICES ]

#### 4.2 **Class A4 - Hostels**

· The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.

Activities on the ground floor must be of those related to retail activities to ensure high pedestrian activities and street retail frontages.

#### **Class A5 – Residential Care Homes** 4.3

- · Appropriate licensing from the respective government department or agencies must be
- To ensure the safety, the building or area that allocated for these activities must be fully guarded.
- Facilities required to support such activities must be provided within the premises.

#### 4.4 Class B3 - Entertainment

· Only permitted on the ground and first floors of shop house or commercial buildings subject to licensing conditions.

#### 4.5 Class B4 – Leisure

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- · The scale, intensity and operation of the use shall not generate traffic congestion and noise.

#### 4.6 Class B6 - Hotels

- Only permissible on purpose built buildings for primary use of a hotel.
- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.

#### 4.7 Class C1 - Showrooms

- Only sales of related items are allowed. Repair and services are not allowed.
- · Open-air display area is not permitted.
- Must be located within the more highly accessible areas.
- The scale, intensity and operation of such use must not increase the volume of traffic.

#### 4.8 **Class D1 – Medical facilities**

- Shall be located within the more highly accessible portion of the area.
- Other facilities such as parking requirement, support facilities and etc must comply to planning requirements of KLCH.

### Class D2 - Religious facilities 4.9

 Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

Table ZS 5.2 Zoning Schedule (Specific)

MIXED USE COMMERCIAL(MXC) Land Use Zone

Location JALAN RAJA MUDA ABDUL AZIZ AND JALAN RAJA BOT

[ MEDICAL AND SERVICES ]

## 4.10 Class D5 – Integrated Community and Cultural Complex

- Shall be located within the more highly accessible portion, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- Shall be provided based on the planning standards and planning requirements.

## 4.11 Class D6 – Sports and recreation facilities

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion.
- Sites should be located so as to maximize compatibility with adjacent land use.

## 4.12 Class D9 – Secure residential institution

For the safety purposes, the area or building that allocated for these activities must be fully guarded to ensure there are no interferences.

## 4.13 Class D10 – Infrastructure and Utilities

· Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## 4.14 Class E1 - Parking

Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

Table ZS 6.0 Zoning Schedule (General)

**Land Use Zone MIXED USE RESIDENTIAL (MXR)** 

## **Description of Zone** 1.

Refers to land zoned for mixed-use development within general residential area as identified in the Zoning Plan. It is intended to promote commercial component in residential zone and at transit planning zone. This zone is generally located within transit planning zone to encourage street level activities. This zone is predominantly residential where the commercial activity allowed is not more than 30% of the allowable gross floor area.

#### 2. **Permitted Uses**

- Class A2 Multi Dwelling Units 2.1
- Class A3 Workers Quarters 2.2
- Class B8 Home Based Business 2.3
- Class D11Emergency and Security 2.4

#### 3. **Prohibited Use**

- 3.1 Class A1 Single dwelling units
- 3.2 Class B3 Entertainment
- 3.3 Class B5 Theme Park
- Class B9 Hypermarket 3.4
- 3.5 Class B10 Funeral Home & Services
- Class C1 Showroom 3.6
- 3.7 Class C2 Wholesale, Storage and Distribution
- Class C3 Recycling Operations 3.8
- 3.9 Class C4 Petrol Station
- 3.10 Class C5 Motorcycle and Motorcar Retail
- 3.11 Class C6 Motorcar Repairs, Service Centres and Workshop
- 3.12 Class C7 Manufacturing Industry
- 3.13 Class C8 Printing and Publication
- 3.14 Class C9 Service Industry
- 3.15 Class C10 Technology, Research and Development
- 3.16 Class D7 Golf Facilities
- Class D8 Institution 3 17
- 3.18 Class D9 Secure Residential Institution

## 4. **Conditional Uses**

### Class A4 - Hostels 4.1

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.
- Accessory Retail Use can be allowed on the ground floor and must be of street frontage.

Table ZS 6.0 Zoning Schedule (General)

MIXED USE RESIDENTIAL(MXR) Land Use Zone

#### 4.2 **Class A5 – Residential Care Homes**

- Residential care homes within this zone have to comply with the Supplementary Guidelines of KLCH. Design of such premise must be barrier free and cater for the needs of the dwellers of the residential care homes.
- Such use shall be located within areas accessible to parks, public amenities and medical facilities as well as linked to the other areas by public transport route.
- It shall also be located away from heavy traffic areas.

### Class B1 – General Retail and Office 4.3

- Commercial floor space shall not exceed 30% of the total gross floor areas.
- General Retail Sales are to be located at ground level of building to ensure high pedestrian activities and street retail frontages.
- Service zones for commercial activities must not be in conflict with the residential use activities.
- Loading and unloading activities must be controlled and limited to certain hours of the
- Segregated entrances to be provided for residential units and commercial units.
- Activities such as hair saloon, health centre, and gymnasium can be allowed on the upper floors of the premise; however access to these services must be segregated from the residential use.

## Class B2 - Food And Drink

Located on the ground level only.

### 4.5 Class B4 - Leisure

- Shall be allowed if premise is located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- Leisure activities allowed is limited to more of passive leisure activities such as art galleries.
- Use allowed shall not cause nuisance to the residents of the premise.

#### Class B6- Hotels 4.6

- Component to make up part of commercial development not exceeding maximum 30% of the total commercial floor space.
- The provision of facilities and services for the hotel shall comply with the requirements of KLCH.
- Hotel activities are confining to lodging and providing of food and beverages.
- Segregated entrances to be provided for residential and hotel.

## **Class B7 - Service Apartment**

- Component to make up part of commercial development not exceeding maximum 30% of the total commercial floor space.
- The provision of facilities and services for the service apartments shall comply with the requirements of KLCH.

Table ZS 6.0 Zoning Schedule (General)

**Land Use Zone** MIXED USE RESIDENTIAL (MXR)

#### 4.8 **Class D1 – Medical Facilities**

- Shall be located within the more highly accessible portion of the premise.
- Other facilities such as parking requirement, support facilities and etc must comply with planning requirements of KLCH.

### 4.9 Class D2 - Religious Facilities

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

## 4.10 Class D3 - Education

- Shall be located within accessible areas, preferably along major public transport routes.
- Pedestrian linkages from transit stops and other areas must be provided.
- For nursery or kindergarten, it must be areas that are safe and shall be located within the premise.

## 4.11 Class D4 – Civic use

- Shall be located within the more highly accessible portion, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The activities shall unreasonably increase traffic on local residential streets.
- Other requirement for example parking spaces and etc. must be based on the planning standards and planning requirements.

# 4.12 Class D5 – Integrated Community and Cultural Complex

- Shall be located within the more highly accessible portion, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- Shall be provided based on the planning standards and planning requirements.

## 4.13 Class D6 – Sports And Recreation Facilities

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion and noise.

## 4.14 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## 4.15 Class E1 – Parking

Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

#### 5. **Areas Under Zoning Schedule (Special)**

- Bukit Indah (refer to table ZS 6.1)
- Chan Sow Lin (refer to table ZS 6.2)
- Bandar Tasik Permaisuri (refer to table ZS 6.3)

Table ZS 6.1 Zoning Schedule (General)

Land Use Zone **MIXED USE RESIDENTIAL (MXR)** 

Location **BUKIT INDAH** 

### 1. **Description of Area**

The MXR is located in the Bukit Indah transit planning zone. It shall provide the residents with residential facilities. Commercial facilities will also be provided to enhance the TOD concept.

#### 2. **Permitted Uses**

- 2.1 Class A2 Multi Dwelling Units
- Class A3 Workers quarters 2.2
- 2.3 Class D11 Emergency and Security

#### 3. **Prohibited Use**

- 3.1 Class A1 Single Dwelling Units
- 3.2 Class A6 Resort Homes
- 3.3 Class B3 Entertainment
- 3 4 Class B5 Theme Park
- 3.5 Class B7 Service Apartment
- Class B9 Hypermarket 3.6
- 3.7 Class B10 Funeral Home & Services
- Class C1 Showroom
- 3.9 Class C2 Wholesale, Storage and Distribution
- 3.10 Class C3 Recycling Operations
- 3.11 Class C4 Petrol Station
- 3.12 Class C5 Motorcycle Retail and Services
- 3.13 Class C6 Motorcar Repairs, Service Centres and Workshop
- 3.14 Class C7 Manufacturing Industry
- 3.15 Class C8 Printing and Publication
- 3.16 Class C9 Service Industry
- 3.17 Class D7 Golf Facilities
- 3.18 Class D8 Institution
- 3.19 Class D9 Secure Residential Institution

#### 4. **Conditional Uses**

#### 4.1 Class A4 - Hostels

- · Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances to residents.
- Activities on the ground floor must be of those related to retail activities to ensure high pedestrian activities and street retail frontages.

## **Class A5 – Residential Care Homes** 4.2

- Appropriate licensing from the respective government department or agencies must be
- Only daycare/child care centre/nursery are allowed



Table ZS 6.1 Zoning Schedule (General)

**Land Use Zone** MIXED USE RESIDENTIAL(MXR)

Location **BUKIT INDAH** 

#### 4.3 Class B1 – General Retail and Office

Commercial floor space shall not exceed 30% of the total gross floor areas.

- Commercial spaces are to be located at ground level of building to ensure high pedestrian activities and street retail frontages.
- Segregated entrances to be provided for residential units and commercial units.

#### 4.6 Class B2 - Food And Drink

Located on the ground level only.

#### 4.7 Class B4 – Leisure

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion.
- Sites should be located so as to maximize compatibility with adjacent land use.

#### Class B6 - Hotels 4.8

Component to make up part of commercial development not exceeding maximum 30% of the total commercial floor space.

#### Class B8 - Home Base Business 4.9

- Not more than 5 employees allowed.
- Tuition activity and activity involving preparation of food and drinks are not allowed.

## 4.10 Class D1 – Medical Facilities

- Shall be located within the more highly accessible portion of the area.
- Other facilities such as parking requirement, support facilities and etc must comply to planning requirements of KLCH.

## 4.11 Class D2 – Religious Facilities

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

## 4.12 Class D3 - Education

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- Only kindergarden/nursery are allowed.

## 4.13 Class D4 - Civic Use

- Shall be located within the more highly accessible portion, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The activities shall unreasonably increase traffic on local residential streets.
- Other requirement for example parking spaces and etc. must be based on the planning standards and planning requirements.

## 4.14 Class D5 - Integrated Community and Cultural Complex

- Shall be located within the more highly accessible portion, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The activities shall unreasonably increase traffic on local residential streets.
- Other requirement for example parking spaces and etc. must be based on the planning standards and planning requirements.

## 4.14 Class E1 – Parking

Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

Table ZS 6.2 Zoning Schedule (General)

Land Use Zone **MIXED USE RESIDENTIAL (MXR)** 

Location **CHAN SOW LIN** 

### 1. **Description of Area**

The MXR area is situated in the Cheras LRT station transit planning zone. It is planned to provide residential facilities to workers of nearby Chan Sow Lin, besides commercial facilities which are vital for the success of TOD planning.



- Class A2 Multi Dwelling Units 2.1
- 2.2 Class D11 Emergency and Security

#### 3. **Prohibited Use**

- Class A1 Single Dwelling Units 3.1
- 3.2 Class A3 Workers quarters
- 3.3 Class B3 Entertainment
- 3.4 Class B5 Theme Park
- Class B7 Service Apartment 3.5
- 3.6 Class B8 Home Base Business
- 3.7 Class B9 Hypermarket
- 3.8 Class B10 Funeral Home & Services
- 3.9 Class C1 Showroom
- 3.10 Class C2 Wholesale, Storage and Distribution
- 3.11 Class C3 Recycling Operations
- 3.12 Class C4 Petrol Station
- 3.13 Class C5 Motorcycle Retail and Services
- 3.14 Class C6 Motorcar Repairs, Service Centres and Workshop
- 3.15 Class C7 Manufacturing Industry
- 3.16 Class C8 Printing and Publication
- 3.17 Class C9 Service Industry
- 3.18 Class D7 Golf Facilities
- 3.19 Class D8 Institution
- 3.20 Class D9 Secure Residential Institution

## 4. **Conditional Uses**

### 4.1 Class A4 - Hostels

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Cheras LRT station) and along major public transport routes and pedestrian routes.
- · The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances to residents.
- · Activities on the ground floor must be of those related to retail activities to ensure high pedestrian activities and street retail frontages.



Table ZS 6.2 Zoning Schedule (General)

**Land Use Zone MIXED USE RESIDENTIAL (MXR)** 

Location **CHAN SOW LIN** 

#### 4.2 Class A5 – Residential Care Homes

Appropriate licensing from the respective government department or agencies must be

Only daycare/child care centre/nursery is allowed.

#### 4.3 Class B1 – General Retail and Office

- Commercial floor space shall not exceed 30% of the total gross floor areas.
- Commercial spaces are to be located at ground level of building to ensure high pedestrian activities and street retail frontages.
- Segregated entrances to be provided for residential units and commercial units.

#### 4.4 Class B2 - Food And Drink

· Located on the ground level only.

#### Class B4 – Leisure 4.5

- The scale, intensity and operation of the use shall not generate traffic congestion.
- Sites should be located so as to maximize compatibility with adjacent land use.

#### 4.6 Class B6 – Hotels

Component to make up part of commercial development not exceeding maximum 30% of the total commercial floor space.

### 4.7 **Class D1 – Medical Facilities**

- Shall be located within the more highly accessible portion of the area.
- · Other facilities such as parking requirement, support facilities and etc must comply to planning requirements of KLCH.

### 4.8 **Class D2 – Religious Facilities**

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

#### 4.9 Class D3 - Education

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Cheras LRT station) and along major public transport routes and pedestrian routes.
- Only for kindergarden/nursery are allowed.

## 4.10 Class D4 – Civic Use

- Shall be located within the more highly accessible portion, preferably within the Transit Planning Zones (around Cheras LRT station) and along major public transport routes and pedestrian routes.
- The activities shall unreasonably increase traffic on local residential streets.
- Other requirement for example parking spaces and etc. must be based on the planning standards and planning requirements.

Table ZS 6.2 Zoning Schedule (General)

**Land Use Zone MIXED USE RESIDENTIAL (MXR)** 

Location **CHAN SOW LIN** 

# 4.11 Class D5 – Integrated Community Facilites

- Shall be located within the more highly accessible portion, preferably within the Transit Planning Zones (around Cheras LRT station) and along major public transport routes and pedestrian routes.
- The activities shall unreasonably increase traffic on local residential streets.
- Other requirement for example parking spaces and etc. must be based on the planning standards and planning requirements.

## 4.12 Class D10 - Infrastructure and Utilities

· Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## 4.13 Class E1 – Parking

• Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

Table ZS 6.3 Zoning Schedule (General)

**Land Use Zone** MIXED USE RESIDENTIAL (MXR)

Location **BANDAR SRI PERMAISURI** 

### 1. **Description of Area**

The MXR is located in the transit planning zone (Tasik Permaisuri LRT station) and is planned to provide residential facilities for the residents. Commercial facilities will also be built to further enhance the TOD concept.

#### 2. **Permitted Uses**

- 2.1 Class A2 Multi Dwelling Units
- 2.2 Class D11 Emergency and Security

#### 3. **Prohibited Use**

- Class A1 Single Dwelling Units 3.1
- 3.2 Class A3 Workers quarters
- 3.3 Class A6 Resort Homes
- Class B3 Entertainment 3 4
- 3.5 Class B5 Theme Park
- Class B7 Service Apartment 3.6
- 3.7 Class B9 Hypermarket
- Class B10 Funeral Home & Services 3.8
- Class C1 Showroom 3.9
- 3.10 Class C2 Wholesale, Storage and Distribution
- 3.11 Class C3 Recycling Operations
- 3.12 Class C4 Petrol Station
- 3.13 Class C5 Motorcycle Retail and Services
- 3.14 Class C6 Motorcar Repairs, Service Centres and Workshop
- 3.15 Class C7 Manufacturing Industry
- 3.16 Class C8 Printing and Publication
- 3.17 Class C9 Service Industry
- 3.18 Class D7 Golf Facilities
- Class D8 Institution 3.19
- 3.20 Class D9 Secure Residential Institution
- 3.21 Class E1 Agriculture

### 4. **Conditional Uses**

### Class A4 - Hostels 4.2

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Tasik Permaisuri LRT station) and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances to residents.
- Activities on the ground floor must be of those related to retail activities to ensure high pedestrian activities and street retail frontages.

## 4.3 **Class A5 – Residential Care Homes**

- Appropriate licensing from the respective government department or agencies must be obtained.
- Only daycare/child care centre/nursery are allowed.



Table ZS 6.3 Zoning Schedule (General)

**MIXED USE RESIDENTIAL (MXR) Land Use Zone** 

Location **BANDAR SRI PERMAISURI** 

#### Class B1 - General Retail and Office 4.4

- Commercial floor space shall not exceed 30% of the total gross floor areas.
- Commercial spaces are to be located at ground level of building to ensure high pedestrian activities and street retail frontages.
- Segregated entrances to be provided for residential units and commercial units.

#### 4.5 Class B2 – Food And Drink

Located on the ground level only.

#### Class B4 – Leisure 4.6

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Tasik Permaisuri LRT station) and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion.
- Sites should be located so as to maximize compatibility with adjacent land use.

#### Class B6 - Hotels 4.7

Component to make up part of commercial development not exceeding maximum 30% of the total commercial floor space.

## Class B8 - Home Base Business

- Not more than 5 employees allowed.
- Tuition activity and activity involving preparation of food and drinks are not allowed.

#### 4.9 Class D1 – Medical Facilities

- Shall be located within the more highly accessible portion of the area.
- Other facilities such as parking requirement, support facilities and etc must comply to planning requirements of KLCH.

## 4.10 Class D2 – Religious Facilities

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

## 4.11 Class D3 - Education

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Tasik Permaisuri LRT station) and along major public transport routes and pedestrian routes.
- Only kindergartens and nurseries are allowed.

Table ZS 6.3 Zoning Schedule (General)

**Land Use Zone MIXED USE RESIDENTIAL (MXR)** 

Location **BANDAR SRI PERMAISURI** 

## 4.12 Class D4 - Civic Use

- Shall be located within the more highly accessible portion, preferably within the Transit Planning Zones (around Tasik Permaisuri LRT station) and along major public transport routes and pedestrian routes.
- The activities shall unreasonably increase traffic on local residential streets.
- Other requirement for example parking spaces and etc. must be based on the planning standards and planning requirements.

## 4.13 Class D10 - Infrastructure and Utilities

Infrastructure and Utilities are allowed in accordance to the technical requirements of the utility providers.

## 4.14 Class E1 - Parking

Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

Table ZS 7.0 Zoning Schedule (General)

**Land Use Zone MIXED USE COMMERCIAL & INDUSTRY (MXCI)** 

### 1. **Description of Zone**

Refers to land zoned for mixed-use industry and commercial development as identified in the Zoning Plan. It is intended to promote clustering of similar activities incorporating compatible and non-polluting industrial activities with support services such as storage, distributive trade, and service industries. This zone is predominantly industrial with maximum 30% commercial use.

#### 2. **Permitted Uses**

- 2.1 Class B9 Hypermarket
- 2.2 Class C2 Wholesale, Storage and Distribution
- 2.3 Class C5 Motorcycle and Motorcar Retail
- 2.4 Class C10 Technology, Research and Development
- 2.5 Class D11Emergency and Security

#### 3. **Prohibited Use**

- 3.1 Class A1 Single Dwelling Units
- Class A2 Multi Dwelling Units
- 3.3 Class A4 Hostels
- 3.4 Class A5 Residential Care Homes
- 3.5 Class B3 Entertainment
- 3.6 Class B4 Leisure
- Class B5 Theme Park 3.7
- 3.8 Class B6 Hotels
- 3.9 Class B7 Service Apartment
- 3.10 Class B8 Home Base Business
- 3.11 Class B10 Funeral Home & Services
- 3.12 Class D1 Medical Facilities
- 3.13 Class D2 Religious Facilities
- 3.14 Class D3 Education
- 3.15 Class D4 Civic Use
- 3.16 Class D5 Integrated Community and Cultural Complex
- 3.17 Class D6 Sports and Recreational Facilities
- 3.18 Class D7 Golf Facilities
- 3.19 Class D8 Institution
- 3.20 Class D9 Secure Residential Institution

#### 4. **Conditional Uses**

### 4.1 **Class A3-Workers quarters**

- On purpose built premises.
- Located along public transport route and pedestrian routes.
- Sufficient buffers eg. Roads, car parking spaces, other uses that are non-conflicting such as shop houses, should be provided to reduce any risk or impact of any uncontrolled level of pollution.
- Community facilities required to be provided in accordance to requirement of the Supplementary Development Control Guidelines of provision of Community Facilities.

Table ZS 7.0 Zoning Schedule (General)

**Land Use Zone** MIXED USE COMMERCIAL & INDUSTRY (MXCI)

#### 4.2 Class B1 – General Retail and Office

- Must not exceed maximum 30% of the development total floor area.
- General Retail Sales are to be located at ground level of building to ensure high pedestrian activities and street retail frontages.
- Public entrance for commercial activities must not be in conflict with the industrial use activities.

#### 4.6 Class B2 – Food and Drink

- Must not exceed maximum 30% of the development total floor area.
- Commercial spaces are to be located at ground level of building to ensure high pedestrian activities and street retail frontages. However it shall be confined to indoor or in purpose built buildings. Solid waste disposal must comply to requirements of KLCH.
- Public entrance for commercial activities must not be in conflict with the industrial use activities.

#### 4.7 Class C1 – Showrooms

- Must not exceed maximum 30% of the development total floor space.
- Display and sales of related items are allowed. Repair and services are also allowed as a backroom operation for the premise.
- Public entrance for commercial activities must not be in conflict with the industrial use activities
- Accessory use for retail is to be located at ground level of building to ensure high pedestrian activities and street retail frontages.
- Must be located along the main road, but no direct access to main road is allowed.

### 4.8 Class C3 – Recycling Operations

- Only recycling collection centres allowable. Automotive recycling yard are not allowed.
- Environmental Impact Assessment (EIA) has to be submitted to KLCH before planning permission is granted for such activities.

#### 4.9 **Class C4 – Petrol Station**

- Primary use or activity is the sale of petrol and related items.
- Accessory activities such as repair and services of vehicles including motorcycles are limited to car washing, and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Waste oil collection facilities must be provided at all petrol stations and garages where
- A new petrol station will not be allowed on any site where the range of visibility along the road to and from the accesses is inadequate.
- Accessory retail use is allowed in an enclosed area only but is limited to 100 sq.m. net retail sales area.

# 4.10 Class C6 – Motorcycle and Motorcar Repairs, Service Centres and Workshops

- Activities allowable are restricted only to sale and service of tyres, batteries, minor services such as change of oil filters and wiring services, sale and services of accessories.
- Body painting, spraying, welding and knocking are not permitted.
- Grease and oil trap must be installed in the premises. Oil and grease must not be discharged into communal drain. Owner or operator must take appropriate measures to discharge oil and grease at designated and appropriate discharge centres.
- Verandahway, public walkway and roadside parking spaces must not be obstructed.

Table ZS 7.0 Zoning Schedule (General)

**Land Use Zone** MIXED USE COMMERCIAL & INDUSTRY (MXCI)

## 4.10 Class C7 – Manufacturing Industry

- Only non-polluting activities that does not generate waste and effluent. Activities using high technology are encouraged.
- Equipments and materials must not be stored within buildings and away from public view.
- Loading and unloading activities must be undertaken at the back of the building through back lanes, rear service lanes or dedicated service/loading/unloading bay.
- Accessory office use in a manufacturing industry can be allowed for general administration etc. floor space shall not exceed 10% of the total floor area.
- Environmental Impact Assessment (EIA) has to be submitted to KLCH before planning permission is granted for such activities.

# 4.11 Class C8 – Printing and publication

- Only digital printing not involving polluting effluent.
- Activity shall not generate unreasonable noise.

## 4.12 Class C9 – Service industry

- This activity shall not generate unreasonable noise.
- Activity must be free from danger of fire, explosions, toxic and noxious matter, vibration and other objectionable influences.
- Equipments and materials must not be stored within buildings and away from public view.
- Loading and unloading activities must be undertaken at the back of the building through back lanes, rear service lanes or dedicated service/loading/unloading bay.

# 4.15 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## 4.16 Class E1 - Parking

Car and motorcycle parking spaces area allowed. Heavy vehicle and depot use are allowed for use of vehicles within the zone.

## **Areas Under Zoning Schedule (Special)**

- Chan Sow Lin (refer to table ZS 7.1)
- Chan Sow Lin (refer to table ZS 7.2)

Table ZS 7.1 Zoning Schedule (General)

**Land Use Zone MIXED USE COMMERCIAL INDUSTRY (MXCI)** 

Location **CHAN SOW LIN** 

### 1. **Description of Area**

The MXCI is located within Jalan Chan Sow Lin, Jalan 6 and the new Pandan Corridor Highway. The land use designted for this area is commercial and industrial in the same area, and still emphasised on the automotive – based activities.

#### 2. **Permitted Uses**

- 2.1 Class B9 Hypermarket
- 2.2 Class C2 Wholesale, Storage and Distribution
- 2.3 Class C5 Motorcycle Retail and Services
- Class C6 Motorcar Repairs, Service Centres and Workshop 2.4
- 2.5 Class C10 Technology, Research and Development
- Class D11 Emergency and Security 2.6



#### 3. **Prohibited Use**

- Class A1 Single Dwelling Units 3.1
- Class A2 Multi Dwelling Units 3.2
- 3.3 Class A3 Workers Quarters
- 3.4 Class A5 Residential Care Homes
- 3.5 Class B3 Entertainment
- Class B4 Leisure 3.6
- 3.7 Class B5 Theme Park
- Class B6 Hotels 3.8
- 3.9 Class B7 Service Apartment
- 3.10 Class B8 Home Base Business
- Class B10 Funeral Home & Services 3.11
- 3.12 Class C7 Manufacturing Industry
- 3.13 Class D1 Medical Facilities
- 3.14 Class D3 Education
- 3.15 Class D4 Civic Use
- 3.16 Class D5 Integrated Community and Cultural Complex
- Class D6 Sports and Recreational Facilities 3.17
- 3.18 Class D7 Golf Facilities
- 3.19 Class D8 Institution
- 3.20 Class D9 Secure Residential Institution
- 3.21 Class E1 Parking

### 4. **Conditional Uses**

### **Class A4- Hostels** 4.1

- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.
- Activities on the ground floor must be of those related to retail activities to ensure high pedestrian activities and street retail frontages.

Table ZS 7.1 Zoning Schedule (General)

**Land Use Zone** MIXED USE COMMERCIAL INDUSTRY (MXCI)

Location **CHAN SOW LIN** 

#### Class B1 - General Retails and Office 4.2

- Located on the ground level or along high pedestrian route.
- Only retail trade industrial, business and construction materials and Personal and Household services are allowed.

#### Class B2 – Food and drink 4.4

- Must not exceed maximum 30% of the development total floor space.
- Located on the ground level or along high pedestrian route.

## Class C1 – Showrooms

Must not exceed maximum 30% of the development total floor space.

## 4.6 Class C3 – Recycling Operations

Only recycling collection centres allowable. Automotive recycling yard are not allowed.

#### 4.7 Class C8 – Printing and Publication

- Only digital printing not involving polluting effluent.
- Activity shall not generate unreasonable noise.

### Class C9 - Service Industry 4.8

- This activity shall not generate unreasonable noise.
- Activity must be free from danger of fire, explosions, toxic and noxious matter, vibration and other objectionable influences.
- Equipments and materials must not be stored within buildings and away from public view.
- Loading and unloading activities must be undertaken at the back of the building through back lanes, rear service lanes or dedicated service/loading/unloading bay.

### Class D2 - Religious Facilities 4.9

Activity as ancillary use to main activity and must support appropriate catchments as required in the Supplementary Planning Guidelines on population Community Facilities.

## 4.10 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

Table ZS 8.0 Zoning Schedule (General) **Land Use Zone RESIDENTIAL 1 (R1)** 

### **Description of Zone** 1.

1.1 This residential zone refers to low density residential of 4 –40 person per acre.

#### 2. **Permitted Uses**

Class A1 Single Dwelling Units 2.1

#### 3. **Prohibited Use**

- 3.1 Class A4 Hostels
- Class B1 General Retail and Office 3.2
- 3.3 Class B2 Food and Drink
- Class B3 Entertainment 3.4
- 3.5 Class B4 Leisure
- 3.6 Class B5 Theme Park
- Class B6 Hotels 3.7
- 3.8 Class B7 Service Apartment
- 3.9 Class B9 Hypermarket
- 3.10 Class B10 Funeral Home & Services
- 3.11 Class C1 Showroom
- 3.12 Class C2 Wholesale, Storage and Distribution
- 3.13 Class C3 Recycling Operations
- 3.14 Class C4 Petrol Station
- 3.15 Class C5 Motorcycle and Motorcar Retail
- 3.16 Class C6 Motorcycle and Motorcar Repairs, Service Centres and Workshop
- 3.17 Class C7 Manufacturing Industry
- 3.18 Class C8 Printing and Publication
- 3.19 Class C9 Service Industry
- 3.20 Class C10Technology, Research and Development
- 3.21 Class D1 Medical Facilities
- 3.22 Class D2 Religious Facilities
- 3.23 Class D4 Civic Use
- 3.24 Class D5 Integrated Community and Cultural Complex
- 3.25 Class D7 Golf Facilities
- 3.26 Class D8 Institution
- Class D9 Secure Residential Institution 3.27
- 3.28 Class E1 Parking

### 4. **Conditional Uses**

### Class A2 - Multi Dwelling Unit 4.1

Density must not exceed than allowable (maximum 40 person per acre)

#### 4.2 **Class A3 - Workers Quarters**

- On purpose built premises.
- Located along public transport route and pedestrian routes.
- Sufficient buffers eg. Roads, car parking spaces, other uses that are non-conflicting such as shop houses, should be provided to reduce any risk or impact of any uncontrolled level of pollution.
- Community facilities required to be provided in accordance to requirement of the Supplementary Development Control Guidelines of provision of Community Facilities.

Table ZS 8.0 Zoning Schedule (General) **Land Use Zone RESIDENTIAL 1 (R1)** 

#### **Class A5 – Residential Care Homes** 4.3

- As accessory use within the zone.
- Purpose built building is preferred.
- Residential care homes within this zone have to comply with the Supplementary Guidelines of KLCH. Design of such premise must be barrier free and cater for the needs of the dwellers of the residential care homes.
- Such use shall be located within areas accessible to parks, public amenities and medical facilities as well as linked to the other areas by public transport route.
- It shall also be located away from heavy traffic areas.

#### Class B8 – Home Based Business 4.4

- Not more than 3 employees allowed.
- Activities must not create discomfort or nuisance to surrounding residential areas eg. Parking of vehicles within the neighbourhood areas. Façade and character of building must be consistent with surrounding residential buildings.

#### 4.5 Class D3 – Education

Activity as accessory use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

## Class D6 - Sports and Recreation Facilities

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

#### 4.7 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## 4.8 Class D11 - Emergency and Security

Activities as ancillary use and only police beat and neighbourhood watch allowable.

### 5. **Areas Under Zoning Schedule (Special)**

Nil

Table ZS 9.0 Zoning Schedule (General) **Land Use Zone RESIDENTIAL 2 (R2)** 

### **Description of Zone** 1.

1.1 This residential zone refers to medium density residential of 48 – 120 person per acre.

#### 2. **Permitted Uses**

- 2.1 Class A1 Single Dwelling Units
- 2.2 Class A2 Multi Dwelling Units
- Class A3 Workers Quarters 2.3

### 3. **Prohibited Use**

- Class A4 Hostels 3.1
- 3.2 Class B1 General Retail and Office
- Class B3 Entertainment 3.3
- 3.4 Class B4 Leisure
- Class B5 Theme Park 3.5
- Class B6 Hotels 3.6
- 3.7 Class B7 Service Apartment
- Class B9 Hypermarket 3.8
- Class B10 Funeral Home & Services 3.9
- 3.10 Class C1 Showroom
- 3.11 Class C2 Wholesale, Storage and Distribution
- 3.12 Class C3 Recycling Operations
- 3.13 Class C4 Petrol Station
- 3.14 Class C5 Motorcycle Retail and Services
- 3.15 Class C6 Motorcycle and Motorcar Repairs, Service Centres and Workshops
- 3.16 Class C7 Manufacturing Industry
- 3.17 Class C8 Printing and Publication
- 3.18 Class C9 Service Industry
- 3.19 Class C10Technology, Research and Development
- 3.20 Class D1 Medical Facilities
- 3.21 Class D5 Integrated Community and Cultural Complex
- 3.22 Class D7 Golf Facilities
- 3.23 Class D8 Institution
- 3.24 Class D9 Secure Residential Institution

#### 4. **Conditional Uses**

### **Class A5 - Residential Care Homes** 4.1

- As ancillary activity within the zone and development proposal.
- Purpose built building is preferred. Internal arrangement must be tailored to suit the intended activity.
- Residential care homes within this zone have to comply with the Supplementary Guidelines of KLCH. Design of such premise must be barrier free and cater for the needs of the dwellers of the residential care homes.
- Such use shall be located within areas accessible to parks, public amenities and medical facilities as well as linked to the other areas by public transport route.
- It shall also be located away from heavy traffic areas.

Table ZS 9.0 Zoning Schedule (General) **Land Use Zone RESIDENTIAL 2 (R2)** 

#### Class B2 - Food and Drink 4.2

Business operations must take place only within the premises and not create serious nuisance impacts on the residential neighborhood.

#### 4.3 Class B8 – Home base business

- Not more than 5 employees allowed.
- Activities must not create discomfort or nuisance to surrounding residential areas especially parking along the roads. Façade and character of building must be consistent with surrounding residential buildings.
- · Tuition activity and activities involving preparation of food and drinks are not allowed within high rise buildings.

### 4.4 Class D2 – Religious facilities

Activity as ancillary use to main activity and must support appropriate population catchmentsas required in the Supplementary Planning Guidelines on Community Facilities.

#### 4.5 Class D3 – Education

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

#### Class D4 - Civic Use 4.6

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

## 4.7 Class D6 - Sports and Recreation Facilities

• Only futsal and courts for games or spaces for recreational and sport are allowed.

#### Class D10 - Infrastructure and Utilities 4.8

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

### 4.9 **Class D11 - Emergency and Security**

Activities as ancillary use and only police beat and neighbourhood watch allowable.

# 4.10 Class E1 - Parking

Car and motorcycle parking spaces area allowed in accordance to KLCH requirements. Heavy vehicle and depot are NOT allowed.

## 5. **Areas Under Zoning Schedule (Special)**

Nil

Table ZS 10.0 Zoning Schedule (General) **Land Use Zone RESIDENTIAL 3 (R3)** 

## **Description of Zone** 1.

1.1 This residential zone refers to high density residential of 160 - 400 person per acre

#### 2. **Permitted Uses**

- Class A2 Multi Dwelling Units 2.1
- Class A3 Workers Quarters 2.2

#### 3. **Prohibited Use**

- Class A1 Single Dwelling Units 3.1
- Class B3 Entertainment 3.2
- 3.3 Class B4 Leisure
- 3.4 Class B5 Theme Park
- 3.5 Class B6 Hotels
- Class B7 Service Apartment 3.6
- 3.7 Class B9 Hypermarket
- 3.8 Class B10 Funeral Home & Services
- Class C1 Showroom 3.9
- 3.10 Class C2 Wholesale, Storage and Distribution
- 3.11 Class C3 Recycling Operations
- 3.12 Class C4 Petrol Station
- 3.13 Class C5 Motorcycle and Motorcar Retail
- 3.14 Class C6 Motorcycle and Motorcar Repairs, Service Centres and Workshop
- 3.15 Class C7 Manufacturing Industry
- 3.16 Class C8 Printing and Publication
- 3.17 Class C9 Service Industry
- 3.18 Class C10Technology, Research and Development
- 3.19 Class D1 Medical Facilities
- 3.20 Class D5 Integrated Community and Cultural Complex
- Class D7 Golf Facilities 3.21
- 3.22 Class D8 Institution
- 3.23 Class D9 Secure Residential Institution

#### **Conditional Uses** 4.

#### **Class A4- Hostels** 4.1

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.
- Accessory Retail Use can be allowed on the ground floor and must be of street frontage.

#### 4.2 **Class A5 – Residential Care Homes**

- As ancillary activity within the zone and development proposal.
- Purpose built building is preferred.
- Residential care homes within this zone have to comply with the Supplementary Guidelines of KLCH. Design of such premise must be barrier free and cater for the needs of the dwellers of the residential care homes.
- Such use shall be located within areas accessible to parks, public amenities and medical facilities as well as linked to the other areas by public transport route.
- It shall also be located away from heavy traffic areas.
- If activity occurs at high rise buildings, preferred location is on the ground or first floors only.

Table ZS 10.0 Zoning Schedule (General) Land Use Zone RESIDENTIAL 3 (R3)

### 4.3 Class B1 – General Retail and Office

- Only items a, b, c, d, e, f and g are allowed (refer to schedule 3:Use Classes)
- As accessory use within the zone, area shall not exceed 10% of the total floor area.
- If activity occurs at high rise buildings, preferred location is on the ground floor only to ensure high pedestrian activities and street retail frontages.
- Service zones for commercial activities must not be in conflict with the residential use activities.
- Loading and unloading activities must be controlled and limited to certain hours of the
- Segregated entrances to be provided for residential units and commercial units.
- · Any activities which related with personal and household services absolutely are not allowed.

#### 4.4 Class B2 - Food and Drink

Business operations must take place only within the premises and not create serious nuisance impacts on the residential neighborhood.

## 4.5 Class B8 – Home Based Business

- Not more than 5 employees allowed.
- Activities must not create discomfort or nuisance to surrounding residential areas. Façade and character of building must be consistent with surrounding residential buildings.
- Tuition activity and activities involving preparation of food and drinks are not allowed within high rise buildings.

## 4.6 Class D2 – Religious Facilities

- Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.
- Purpose built building is proffered especially within high rise development.

#### 4.7 Class D3 – Education

- Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.
- Pedestrian linkages from residential areas must be provided.
- For nursery or kindergarten, it must be areas that are safe and shall be located within the premise.

#### 4.8 Class D4 – Civic Use

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

Table ZS 10.0 Zoning Schedule (General) **Land Use Zone** RESIDENTIAL 3 (R3)

#### **Class D6 – Sports and Recreation Facilities** 4.9

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

# 4.10 Class D10 - Infrastructure and Utilities

· Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

# 4.11 Class D11 - Emergency and Security

· Activities as ancillary use and only police beat and neighbourhood watch allowable.

# 4.11 Class E1 - Parking

· Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

## 5. **Areas Under Zoning Schedule (Special)**

- Pantai Dalam (refer to table ZS 10.1)
- Chan Sow Lin (refer to table ZS 10.2)
- Kg. Baru Salak Selatan (refer to table ZS 10.3)
- Rumah Murah Bandar Tun Razak (refer to table ZS 10.4)
- Razak Mansion (refer to table ZS 10.5)

Table ZS 10.1 Zoning Schedule (Specific) Land Use Zone **RESIDENTIAL 3 (R3)** Location **PANTAI DALAM** 

### 1. **Description of Area**

The areas on both sides of the New Pantai Expressway will be redeveloped into highdensity housing areas. They are accessible via the NPE and Jalan Pantai Dalam.

#### 2. **Permitted Uses**

- Class A2 Multi Dwelling Units 2.1
- 2.2 Class A3 Workers Quarters

#### 3. **Prohibited Use**

- 3.1 Class A1 Single Dwelling Units
- Class B3 Entertainment 3.2
- Class B4 Leisure 3.3
- 3.4 Class B5 Theme Park
- 3.5 Class B6 Hotels
- 3.6 Class B7 Service Apartment
- 3.7 Class B9 Hypermarket
- 3.8 Class B10 Funeral Home & Services
- 3.9 Class C1 Showroom
- 3.10 Class C2 Wholesale, Storage and Distribution
- 3.11 Class C3 Recycling Operations
- 3.12 Class C4 Petrol Station
- 3.13 Class C5 Motorcycle Retail and Services
- 3.14 Class C6 Motorcar Repairs, Service Centres and Workshop
- 3.15 Class C7 Manufacturing Industry
- 3.16 Class C8 Printing and Publication
- 3.17 Class C9 Service Industry
- 3.18 Class C10Technology, Research and Development
- 3.19 Class D1 Medical Facilities
- 3.20 Class D7 Golf Facilities
- 3.21 Class D8 Institution
- 3.22 Class D9 Secure Residential Institution

## 4. **Conditional Uses**

### 4.1 **Class A4- Hostels**

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Pantai Dalam commuter station) and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.

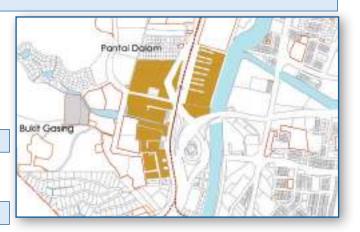


Table ZS 10.1 Zoning Schedule (Specific) **Land Use Zone RESIDENTIAL 3 (R3)** Location **PANTAI DALAM** 

#### Class A5 - Residential Care Homes 4.2

- As ancillary activity within the zone and development proposal.
- Purpose built building is preferred. Internal arrangement must be tailored to suit the intended activity.
- If activity occurs at high rise buildings, preferred location is on the ground or first floors only.

### Class B1 - General retail and Office 4.3

- As ancillary activity within the zone and development proposal.
- Purpose built building is preferred.
- If activity occurs at high rise buildings, preferred location is on the ground floor only.
- Any activities which related with personal and household services totally are not allowed.
- Activities below are also NOT allowable
  - Banking, financial institutions, stockbrokers, insurance and real estate
  - Professional services
  - **Business support services**

## Class B2 – Food and drink

Located on the ground level only.

#### 4.5 Class B8 - Home base business

- Not more than 5 employees allowed.
- Activities must not create discomfort or nuisance to surrounding residential areas. Façade and character of building must be consistent with surrounding residential buildings.
- Tuition activity and activities involving preparation of food and drinks are not allowed within high rise buildings.

### Class D2 – Religious facilities 4.6

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

#### Class D3 - Education 4.7

- Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.
- Only kindergarten/nursery is allowed.

#### 4.8 Class D4 – Civic Use

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

Table ZS 10.1 Zoning Schedule (Specific) Land Use Zone **RESIDENTIAL 3 (R3)** Location **PANTAI DALAM** 

## **Class D5 – Integrated Community and Cultural Complex**

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

# 4.10 Class D6 - Sports and Recreational Facilities

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

## 4.11 Class D10 - Infrastructure and Utilities

· Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## 4.12 Class D11 - Emergency and Security

• Activities as ancillary use and only police beat and neighbourhood watch allowable.

## 4.11 Class E1 - Parking

• Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

Table ZS 10.2 Zoning Schedule (Specific) **Land Use Zone** RESIDENTIAL 3 (R3) Location **CHAN SOW LIN** 

## 1. **Description of Area**

This is the residential section of Chan Sow Lin and is located next to the LRT reserves / KL – Putrajaya Dedicated Highway. Adjacent to this area are the MCI and MXR land uses. Density is high as the area is planned for TOD and emphasises on walking distance to the transit station as well as optimising the use of (high-cost) land.

### 2. **Permitted Uses**

- 2.1 Class A2 Multi Dwelling Units
- Class A3 Workers Quarters 2.2

#### **Prohibited Use** 3.

- Class A1 Single Dwelling Units 3.1
- 3.2 Class B3 Entertainment
- 3.3 Class B4 Leisure
- 3.4 Class B5 Theme Park
- Class B6 Hotels 3.5
- Class B7 Service Apartment 3.6
- 3.7 Class B9 Hypermarket
- Class B10 Funeral Home & Services 3.8
- 3.9 Class C1 Showroom
- 3.10 Class C2 Wholesale, Storage and Distribution
- 3.11 Class C3 Recycling Operations
- 3.12 Class C4 Petrol Station
- 3.13 Class C5 Motorcycle Retail and Services
- 3.14 Class C6 Motorcar Repairs, Service Centres and Workshop
- 3.15 Class C7 Manufacturing Industry
- 3.16 Class C8 Printing and Publication
- 3.17 Class C9 Service Industry
- 3.18 Class C10Technology, Research and Development
- 3.19 Class D1 Medical Facilities
- 3.20 Class D6 Sports and Recreational Facilities
- 3.21 Class D7 Golf Facilities
- 3.22 Class D8 Institution
- 3.23 Class D9 Secure Residential Institution

## **Conditional Uses** 4.

### **Class A4- Hostels** 4.1

- Shall be located within the more highly accessible areas, preferably within the Transit Planning Zones (around Cheras LRT station) and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.



Table ZS 10.2 Zoning Schedule (Specific) Land Use Zone RESIDENTIAL 3 (R3)

Location **CHAN SOW LIN** 

#### Class A5 – Residential Care Homes 4.2

- As ancillary activity within the zone and development proposal.
- Purpose built building is preferred. Internal arrangement must be tailored to suit the intended activity.
- If activity occurs at high rise buildings, preferred location is on the ground or first floors only.

## 4.3 Class B1 - General retail and Office

- As ancillary activity within the zone and development proposal.
- Purpose built building is preferred.
- If activity occurs at high rise buildings, preferred location is on the ground floor only.
- Any activities which related with personal and household services absolutely are not allowed.
- Activities below are also NOT allowable
  - Banking, financial institutions, stockbrokers, insurance and real estate
  - Professional services
  - **Business support services**

## Class B2 - Food and drink

Located on the ground level only.

## 4.5 Class B8 – Home base business

- Not more than 5 employees allowed.
- Activities must not create discomfort or nuisance to surrounding residential areas. Façade and character of building must be consistent with surrounding residential buildings.
- · Tuition activity and activities involving preparation of food and drinks are not allowed within high rise buildings.

### 4.6 Class D2 – Religious Facilities

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

#### Class D3 – Education 4.7

- Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.
- Only kindergarden/nursery are allowed.

### 4.8 Class D4 – Civic Use

· Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

### 4.9 **Class D5 – Integrated Community and Cultural Complex**

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

Table ZS 10.2 Zoning Schedule (Specific) **RESIDENTIAL 3 (R3)** Land Use Zone

**CHAN SOW LIN** Location

## 4.10 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## 4.11 Class D11 - Emergency and Security

• Activities as ancillary use and only police beat and neighbourhood watch allowable.

# 4.10 Class E1 - Parking

• Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

Table ZS 10.3 Zoning Schedule (Specific) **Land Use Zone RESIDENTIAL 3 (R3)** Location **KG. BARU SALAK SELATAN** 

### 1. **Description of Area**

This high density residential development lies next to

commercial centre of Kg. Baru Salak Selatan and the KTM reserves.

#### 2. **Permitted Uses**

- Class A2 Multi Dwelling Units 2.1
- 2.2 Class A3 Workers Quarters

### 3. **Prohibited Use**

- 3.1 Class A1 Single Dwelling Units
- 3.2 Class B3 Entertainment
- 3.3 Class B4 Leisure
- Class B5 Theme Park 3 4
- 3.5 Class B6 Hotels
- 3.6 Class B7 Service Apartment
- 3.7 Class B9 Hypermarket
- Class B10 Funeral Home & Services 3.8
- Class C1 Showroom 3.9
- 3.10 Class C2 Wholesale, Storage and Distribution
- 3.11 Class C3 Recycling Operations
- 3.12 Class C4 Petrol Station
- 3.13 Class C5 Motorcycle Retail and Services
- 3.14 Class C6 Motorcar Repairs, Service Centres and Workshop
- 3.15 Class C7 Manufacturing Industry
- 3.16 Class C8 Printing and Publication
- 3.17 Class C9 Service Industry
- 3.18 Class C10Technology, Research and Development
- 3.19 Class D1 Medical Facilities
- 3.20 Class D6 Sports and Recreational Facilities
- 3.21 Class D7 Golf Facilities
- 3.22 Class D8 Institution
- 3.23 Class D9 Secure Residential Institution

### 4. **Conditional Uses**

#### 4.1 **Class A4- Hostels**

· The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.

#### **Class A5 - Residential Care Homes** 4.2

- As ancillary activity within the zone and development proposal.
- Purpose built building is preferred. Internal arrangement must be tailored to suit the intended activity.
- If activity occurs at high rise buildings, preferred location is on the ground or first floors only.



Table ZS 10.3 Zoning Schedule (Specific) **Land Use Zone** RESIDENTIAL 3 (R3) Location **KG. BARU SALAK TINGGI** 

#### 4.3 Class B1 – General retail and Office

- As ancillary activity within the zone and development proposal.
- Preferred location is on the ground floor only.
- Activities below are NOT allowable
  - Banking, financial institutions, stockbrokers, insurance and real estate
  - Professional services
  - **Business support services**

## Class B2 – Food and Drink

Located on the ground level only.

#### 4.5 Class B9 - Home Base Business

- Not more than 5 employees allowed.
- Activities must not create discomfort or nuisance to surrounding residential areas. Façade and character of building must be consistent with surrounding residential buildings.
- Tuition activity and activities involving preparation of food and drinks are not allowed within high rise buildings.

### **Class D2 – Religious Facilities** 4.6

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

## 4.7 Class D3 - Education

- Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community
- Only kindergarten/nursery and schools in a high-density area are allowed.

## Class D4 - Civic Use 4.8

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

### 4.9 **Class D5 – Integrated Community and Cultural Complex**

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

## 4.10 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## 4.11 Class D11 - Emergency and Security

Activities as ancillary use and only police beat and neighbourhood watch allowable.

## 4.12 Class E1 - Parking

Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

Table ZS 10.4 Zoning Schedule (Specific) Land Use Zone **RESIDENTIAL 3 (R3)** 

Location **RUMAH MURAH BANDAR TUN RAZAK** 

### 1. **Description of Area**

The area is located north of the Lebuhraya Hubungan Timur-Barat (Salak Expressway). The dilapidated singke-storey cluster houses will be redeveloped into high-density residential areas.

#### 2. **Permitted Uses**

- 2.1 Class A2 Multi Dwelling Units
- Class A3 Workers Quarters 2.2

#### 3. **Prohibited Use**

- 3.1 Class A1 Single Dwelling Units
- 3.2 Class B3 Entertainment
- 33 Class B4 Leisure
- 3.4 Class B5 Theme Park
- Class B6 Hotels 3.5
- 3.6 Class B7 Service Apartment
- 3.7 Class B9 Hypermarket
- 3.8 Class B10 Funeral Home & Services
- Class C1 Showroom
- 3.10 Class C2 Wholesale, Storage and Distribution
- 3.11 Class C3 Recycling Operations
- 3.12 Class C4 Petrol Station
- 3.13 Class C5 Motorcycle Retail and Services
- 3.14 Class C6 Motorcar Repairs, Service Centres and Workshop
- 3.15 Class C7 Manufacturing Industry
- 3.16 Class C8 Printing and Publication
- 3.17 Class C9 Service Industry
- 3.18 Class C10Technology, Research and Development
- 3.19 Class D1 Medical Facilities
- 3.20 Class D6 Sports and Recreational Facilities
- 3.21 Class D7 Golf Facilities
- 3.22 Class D8 Institution
- 3.23 Class D9 Secure Residential Institution

#### 4. **Conditional Uses**

## **Class A4- Hostels**

The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.

# 4.2 Class A5 – Residential Care Homes

- As ancillary activity within the zone and development proposal.
- · Purpose built building is preferred. Internal arrangement must be tailored to suit the intended activity.
- If activity occurs at high rise buildings, preferred location is on the ground or first floors only.

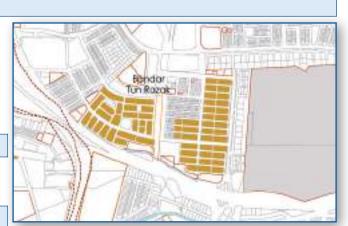


Table ZS 10.4 Zoning Schedule (Specific) **Land Use Zone RESIDENTIAL 3 (R3)** 

Location **RUMAH MURAH BANDAR TUN RAZAK** 

#### Class B1 - General Retail and Office 4.3

- As ancillary activity within the zone and development proposal.
- Preferred location is on the ground floor only.
- Activities below are also NOT allowable
- Banking, financial institutions, stockbrokers, insurance and real estate
  - Professional services
  - **Business support services**

## Class B2 - Food and Drink

Located on the ground level only.

### Class B8 – Home Base Business 4.5

- Not more than 5 employees allowed.
- Activities must not create discomfort or nuisance to surrounding residential areas. Façadeandcharacter of building must be consistent with surrounding residential buildings.
- Tuition activity and activities involving preparation of food and drinks are not allowed within high-rise buildings.

#### 4.6 Class D2 – Religious Facilities

- Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.
- Preferred location is on the ground floor only.

#### 4.7 **Class D3 – Education**

- Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.
- Only kindergarden/nursery are allowed.
- Preferred location is on the ground floor only.

#### 4.8 Class D4 – Civic Use

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

#### 4.9 **Class D5 – Integrated Community and Cultural Complex**

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

# 4.10 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## 4.11 Class D11 - Emergency and Security

Activities as ancillary use and only police beat and neighbourhood watch allowable.

## 4.12 Class E1 - Parking

Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

Table ZS 10.5 Zoning Schedule (Specific) Land Use Zone **RESIDENTIAL 3 (R3)** Location **RAZAK MANSION** 

## 1. **Description of Area**

One of KLCH's public housing, this area is assessible via **BESRAYA** Highway. redevelopment area will be turned into high-density residential and some parts of it will be made workers workers quarters for those working nearby.



## 2. **Permitted Uses**

- Class A2 Multi Dwelling Units 2.1
- 2.2 Class A3 Workers quarters

## 3. **Prohibited Use**

- Class A1 Single Dwelling Units 3.1
- 3.2 Class B3 Entertainment
- 3.3 Class B4 Leisure
- 3 4 Class B5 Theme Park
- Class B6 Hotels 3.5
- 3.6 Class B7 Service Apartment
- 3.7 Class B9 Hypermarket
- 3.8 Class B10 Funeral Home & Services
- 3.9 Class C1 Showroom
- 3.10 Class C2 Wholesale, Storage and Distribution
- 3.11 Class C3 Recycling Operations
- 3.12 Class C4 Petrol Station
- 3.13 Class C5 Motorcycle and Motorcar Retail
- 3.14 Class C6 Motorcycle and Motorcar Repairs, Service Centres and Workshop
- 3.15 Class C7 Manufacturing Industry
- 3.16 Class C8 Printing and Publication
- 3.17 Class C9 Service Industry
- 3.18 Class C10Technology, Research and Development
- 3.19 Class D1 Medical Facilities
- 3.20 Class D5 Integrated Community and Cultural Complex
- 3.20 Class D6 Sports and Recreational Facilities
- 3.21 Class D7 Golf Facilities
- 3.22 Class D8 Institution
- 3.23 Class D9 Secure Residential Institution

## 4. **Conditional Uses**

### 4.1 Class A4- Hostels

 The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.

Table ZS 10.5 Zoning Schedule (Specific) **Land Use Zone RESIDENTIAL 3 (R3)** Location **RAZAK MANSION** 

## Class A5 - Residential Care Homes 4.2

- As ancillary activity within the zone and development proposal.
- Purpose built building is preferred. Internal arrangement must be tailored to suit the intended activity.
- If activity occurs at high rise buildings, preferred location is on the ground or first floors only.

#### 4.3 Class B1 - General Retail and Office

- As ancillary activity within the zone and development proposal.
- Preferred location is on the ground floor only.
- Activities below are also NOT allowable
  - Banking, financial institutions, stockbrokers, insurance and real estate
  - Professional services
  - **Business support services**

## Class B2 - Food and Drink

Located on the ground level only.

#### 4.5 Class B8 - Home Base Business

- Not more than 5 employees allowed.
- Activities must not create discomfort or nuisance to surrounding residential areas. Façade and character of building must be consistent with surrounding residential buildings.
- Tuition activity and activities involving preparation of food and drinks are not allowed within high rise buildings.

## **Class D2 – Religious Facilities** 4.6

- Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.
- Preferred location is on the ground floor only.

## 4.7 **Class D3 – Education**

- Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community
- Only kindergartens and nurseries are allowed.

#### 4.8 Class D4 - Civic Use

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

## **Class D10 - Infrastructure and Utilities** 4.9

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## 4.10 Class D11 - Emergency and Security

Activities as ancillary use and only police beat and neighbourhood watch allowable.

## 4.11 Class E1 - Parking

Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

Table ZS 11.0 Zoning Schedule (General)

Land Use Zone **ESTABLISHED HOUSING AREA (EHA)** 

### 1. **Description of Zone**

1.1 Areas which consist of housing development (old or new) that are in existence and well planned and is expected to remain residential within the planning period of the KL City Plan.

#### 2. **Permitted Uses**

### 3. **Prohibited Use**

- 3.1 Class A1 Single Dwelling Units
- 3.2 Class A2 Multi Dwelling Units
- 3.3 Class A3 Workers quarters
- Class A4 Hostels 3.4
- Class A5 Residential Care Homes 3 5
- Class B1 General Retail and Office 3.6
- Class B2 Food and Drink 3 7
- Class B3 Entertainment 3.8
- Class B4 Leisure 3.9
- 3.10 Class B5 Theme Park
- 3.11 Class B6 Hotels
- 3.12 Class B7 Service Apartment
- 3.13 Class B8 Home Based Business
- 3.14 Class B9 Hypermarket
- 3.15 Class B10 Funeral Home & Services
- 3.16 Class C1 Showroom
- 3.17 Class C2 Wholesale, Storage and Distribution
- 3.18 Class C3 Recycling Operations
- 3.19 Class C4 Petrol Station
- 3.20 Class C5 Motorcycle and Motorcar Retail
- 3.21 Class C6 Motorcycle and Motorcar Repairs, Service Centres and Workshop
- 3.22 Class C7 Manufacturing Industry
- 3.23 Class C8 Printing and Publication
- 3.24 Class C9 Service Industry
- 3.25 Class C10Technology, Research and Development
- 3.26 Class D1 Medical Facilities
- 3.27 Class D2 Religious Facilities
- 3.28 Class D4 Civic Use
- 3.29 Class D5 Integrated Community and Cultural Complex
- 3.30 Class D6 Sports and Recreational Facilities
- 3.31 Class D7 Golf Facilities
- 3.32 Class D8 Institution
- 3.33 Class D9 Secure Residential Institution
- 3.34 Class E1 Parking

## **Conditional Uses** 4.

## Class D3 - Education 4.1

- Only Kindergarten and nursery are allowable.
- Within catchments area there is no others a such facilities.

Table ZS 11.0 Zoning Schedule (General)

**Land Use Zone ESTABLISHED HOUSING AREA (EHA)** 

## 4.2 **Class D10 – Infrastructure and Utilities**

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## 4.3 Class D11 – Emergency and Security

• Activities as ancillary use and only police beat and neighbourhood watch allowable.

Table ZS 12.0 Zoning Schedule (General) Land Use Zone **PUBLIC HOUSING (PH)** 

## 1. **Description of Zone**

1.1  $Refers \, to \, land \, designated \, for \, public \, housing \, only \, and \, these \, includes \, all \, KLCH's \, and \, government's \, and \, government's \, all \,$ public housing areas.

### 2. **Permitted Uses**

- 2.1 Class A2 Multi Dwelling Units
- Class D2 Religious Facilities
- Class D4 Civic Use 2.3

#### 3. **Prohibited Use**

- 3.1 Class A1 Single Dwelling Units
- 3.2 Class A3 Workers Quarters
- 3.3 Class B3 Entertainment
- Class B4 Leisure 3 4
- Class B5 Theme Park 3.5
- Class B6 Hotels 3.6
- 3.7 Class B7 Service Apartment
- Class B9 Hypermarket 3.8
- 3.9 Class C1 Showroom
- 3.10 Class C2 Wholesale, Storage and Distribution
- 3.11 Class C3 Recycling Operations
- 3.12 Class C4 Petrol Station
- 3.13 Class C5 Motorcycle and Motorcar Retail
- 3.14 Class C6 Motorcycle and Motorcar Repairs, Service Centres and Workshops
- 3.15 Class C7 Manufacturing Industry
- 3.16 Class C8 Printing and Publication
- 3.17 Class C9 Service Industry
- 3.18 Class C10Technology, Research and Development
- 3.19 Class D1 Medical Facilities
- 3.20 Class D5 Integrated Community and Cultural Complex
- 3.21 Class D7 Golf Facilities
- 3.22 Class D8 Institution
- 3.23 Class D9 Secure Residential Institution

#### 4. **Conditional Uses**

## 4.1 Class A4 - Hostels

- Shall be located within the more highly accessible areas in same compound and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.
- Accessory Retail Use can be allowed on the ground floor and must be of street frontage.

## Class A5 – Residential Care Homes

- As ancillary activity within the zone and development proposal.
- Residential care homes within this zone have to comply with the Supplementary Guidelines of KLCH. Design of such premise must be barrier free and cater for the needs of the dwellers of the residential care homes.
- It shall also be located away from heavy traffic areas.

Table ZS 12.0 Land Use Zone Zoning Schedule (General) **PUBLIC HOUSING (PH)** 

## 4.3 Class B1 - General retail and Office

- Commercial floor space shall not exceed 30% of the total gross floor areas.
- General Retail Sales are to be located at ground level of building to ensure high pedestrian activities and street retail frontages.
- Service zones for commercial activities must not be in conflict with the residential use activities.
- Loading and unloading activities must be controlled and limited to certain hours of the day.
- Segregated entrances to be provided for residential units and commercial units.
- Activities such as hair saloon, health centre, and gymnasium can be allowed on the upper floors of the premise; however access to these services must be segregated the residential use.

### 4.4 Class B2 - Food and Drink

Business operations must take place only within the premises and not create serious nuisance impacts on the residential neighborhood.

### Class B8 - Homed Based Business 4.5

- Not more than 5 employees allowed.
- Activities must not create discomfort or nuisance to surrounding residential areas especially parking along the roads. Façade and character of building must be consistent with surrounding residential buildings.
- Tuition activity and activities involving preparation of food and drinks are not allowed within high rise buildings.

#### Class B10 – Funeral Homes & Services 4.6

The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.

## Class D3 - Education 4.7

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

## **Class D5 - Integrated Community and Cultural Complex** 4.8

Activities as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

# **Class D6 – Sports and Recreational Facilities**

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

## 4.10 Class D10 – Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## 4.11 Class D11 – Emergency and Security

Activities as ancillary use and only police beat and neighbourhood watch allowable.

# 4.12 Class E1 – Parking

Car and motorcycle parking spaces area allowed in accordance to KLCH requirements. Heavy vehicle and depot are NOT allowed.

Table ZS 13.0 Zoning Schedule (General) **Land Use Zone INDUSTRIAL (IP)** 

## 1. **Description of Zone**

1.1 Area designated and zoned for manufacturing and its associated activities in the Zoning

### 2. **Permitted Uses**

- 2.1 Class C1 Showroom
- 2.2 Class C2 Wholesale, Storage and Distribution
- 2.3 Class C4 Petrol Station
- 2.4 Class C5 Motorcycle and Motorcar Retail
- Class C6 Motorcycle and Motorcar Repairs, Service Centres and Workshop 2.5
- 2.6 Class C7 Manufacturing Industry
- 2.7 Class C8 Printing and Publication
- 2.8 Class C9 Service Industry
- Class C10Technology, Research and Development 2.9
- 2.10 Class E1 Parking

### 3. **Prohibited Use**

- 3.1 Class A1 Single Dwelling Units
- 3.2 Class A2 Multi Dwelling Units
- 3.3 Class A4 Hostels
- 3.4 Class A5 Residential Care Homes
- 3 5 Class B1 General Retail and Office
- 3.6 Class B3 Entertainment
- Class B4 Leisure 3 7
- Class B5 Theme Park 3.8
- Class B6 Hotels 3.9
- 3.10 Class B7 Service Apartment
- 3.11 Class B8 Home Based Business
- 3.12 Class B9 Hypermarket
- 3.13 Class D1 Medical Facilities
- 3.14 Class D3 Education
- 3.15 Class D4 Civic Use
- 3.16 Class D5 Integrated Community and Cultural Complex
- 3.17 Class D7 Golf Facilities
- 3.18 Class D8 Institution
- 3.19 Class D9 Secure Residential Institution
- 3.20 Class D11Emergency and Security

## 4. **Conditional Uses**

## Class A3 – Workers Quarters 4.1

- On purpose built premises.
- Located along public transport route and pedestrian routes.
- Sufficient buffers eg. Roads, car parking spaces, other uses that are non-conflicting such as shop houses, should be provided to reduce any risk or impact of any uncontrolled level of pollution.
- · Community facilities required to be provided in accordance to requirement of the Supplementary Development Control Guidelines of provision of Community Facilities.

Table ZS 13.0 Zoning Schedule (General) **Land Use Zone INDUSTRIAL (IP)** 

## 4.2 Class B2 - Food and Drink

- Ancillary activity to serve workers and visitors of the Industrial Area.
- To be located at ground level of building to ensure high pedestrian activities and street retail frontages. However it shall be confined to indoor or in purpose built buildings. Solid waste disposal must comply to requirements of KLCH.
- Public entrance for activities must not be in conflict with the main use activities.

## 4.3 Class B10 - Funeral Homes & Services

The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances

## 4.4 **Class C3 – Recycling Operations**

Environmental Impact Assessment (EIA) has to be submitted to KLCH before planning permission is granted for such activities.

### 4.5 **Class D2 - Religious Facilities**

- Ancillary activity to serve workers and visitors of the Technology Park Zone.
- Activity as ancillary use to main activity and must support appropriate workers catchments within the Industrial zone.

## **Class D6 – Sports and Recreational Facilities** 4.6

Activity as ancillary use to main activity and must support appropriate workers catchments within the Industrial zone.

## Class D10 - Infrastructure and Utilities 4.7

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## 5. **Areas Under Zoning Schedule (Specific)**

Chan Sow Lin (refer to table ZS 13.1)

Taman Sungai Besi (refer to table ZS 13.2)

Table ZS 13.1 Zoning Schedule (Specific) **Land Use Zone INDUSTRIAL (IP)** Location **CHAN SOW LIN** 

## 1. **Description of Area**

Located mainly in the second layer from Sg. Besi Highway, the industrial area forms the biggest land use in the area. It is surrounded by the NCC, MCI, MXR and residential land uses. The uses are primarily automotive - related activites.





### **Prohibited Use** 3.

3.1	Class A1	Single Dwelling Units
3.2	Class A2	Multi Dwelling Units
3.3	Class A4	Hostels
3.4	Class A5	Residential Care Homes
3.5	Class B2	Food and Drink
3.6	Class B3	Entertainment
3.7	Class B4	Leisure
3.8	Class B5	Theme Park
3.9	Class B6	Hotels
3.10	Class B7	Service Apartment
3.11	Class B9	Hypermarket
3.12	Class B10	Funeral Home & Services
3.13	Class C8	Printing and Publication
3.14	Class D1	Medical Facilities
3.15	Class D2	Religious facilities
3.16	Class D3	Education
3.17	Class D4	Civic Use
3.18	Class D5	Integrated Community and Cultural Complex
3.19	Class D6	Sports and Recreational Facilities
3.20	Class D7	Golf Facilities
3.21	Class D8	Institution
3.22	Class D9	Secure Residential Institution

#### 4. **Conditional Uses**

## 4.1 **Class A3 Workers Quarters**

- On purpose built premises.
- Community facilities required to be provided in accordance to requirement of the Supplementary Development Control Guidelines of provision of Community Facilities.

## 4.2 **Class B1 General Retail and Office**

- Located on the ground level or along high pedestrian route.
- Only retail trade industrial, business and construction materials are allowed.

Table ZS 13.2 Zoning Schedule (Specific) **Land Use Zone INDUSTRIAL (IP)** Location **TAMAN SG. BESI** 

## **Description of Area** 1.

This light industrial area takes up most of Taman Sq. Besi. It is surrounded by residential and commercial areas on either side of the BESRAYA Highway.

## 2. **Permitted Uses**

- 2.1 Class C2 Wholesale, Storage and Distribution
- Class C3 Recycling Operations 2.2
- Class C4 Petrol Station 2.3
- 2.4 Class C5 Motorcycle Retail and Services
- 2.5 Class C6 Motorcar Repairs, Service Centres and Workshop
- Class C7 Manufacturing Industry 2.6
- 2.7 Class C9 Service Industry
- Class E1 Parking 2.8

## 3. **Prohibited Use**

- Class A1 Single Dwelling Units 3.1
- Class A2 Multi Dwelling Units 3.2
- Class A4 Hostels 3.3
- Class A5 Residential Care Homes 3.4
- 3.5 Class B2 Food and Drink
- Class B3 Entertainment 3.6
- 3.7 Class B4 Leisure
- 3.8 Class B5 Theme Park
- 3.9 Class B6 Hotels
- 3.10 Class B7 Service Apartment
- 3.11 Class B8 Home Based Business
- 3.12 Class B9 Hypermarket
- 3.13 Class B10 Funeral Home & Services
- 3.14 Class C1 Showroom
- 3.15 Class C8 Printing and Publication
- 3.16 Class C10Technology, Research and Development
- 3.17 Class D1 Medical Facilities
- 3.18 Class D2 Religious facilities
- 3.19 Class D3 Education
- 3.20 Class D4 Civic Use
- 3.21 Class D5 Integrated Community and Cultural Complex
- 3.22 Class D7 Golf
- Class D8 Institution 3.23
- 3.24 Class D9 Secure Residential Institution
- 3.25 Class D11Emergency and Security



Table ZS 13.2 Zoning Schedule (Specific) Land Use Zone **INDUSTRIAL (IP)** Location **TAMAN SG. BESI** 

## **Conditional Uses** 4.

## 4.1 Class A3 – Workers quarters

- On purpose built premises.
- Community facilities required to be provided in accordance to requirement of the Supplementary Development Control Guidelines of provision of Community Facilities.
- Meant to provide workers' accommodation.
- · Buildings to be compatible with surroundings.

# 4.2 Class B1 - General Retail and Office

- Located on the ground level or along high pedestrian route.
- Only retail trade industrial, business and construction materials are allowed.

### 4.3 **Class D6 - Sports and Recreation Facilities**

• Activity as ancillary use to main activity and must support appropriate workers catchments within the industrial zone.

#### Class D10 - Infrastructure and Utilities 4.4

· Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

Table ZS 14.0 Zoning Schedule (General) **Land Use Zone TECHNOLOGY PARK (TP)** 

## 1. **Description of Zone**

1.1 This zone is intended to concentrate on high technology industry and industry with a significant amount of research development activity in line with the cluster strategy proposed. Professional offices, financial institutions, and other similar uses may be ancillary activities that are appropriate when they provide services to the technology park.

## **Permitted Uses** 2.

2.1 Class C10Technology, Research and Development

#### 3. **Prohibited Use**

- 3.1 Class A1 Single Dwelling Units
- 3.2 Class A2 Multi Dwelling Units
- Class A4 Hostels 3.3
- Class A5 Residential Care Homes 3.4
- Class B3 Entertainment 3.5
- Class B4 Leisure 3.6
- 3.7 Class B5 Theme Park
- Class B6 Hotels 3.8
- 3.9 Class B7 Service Apartment
- 3.10 Class B8 Home Base Business
- 3.11 Class B9 Hypermarket
- 3.12 Class B10 Funeral Home & Services
- 3.13 Class C1 Showroom
- 3.14 Class C2 Wholesale, Storage and Distribution
- 3.15 Class C3 Recycling Operations
- 3.16 Class C4 Petrol Station
- 3.17 Class C5 Motorcycle and Motorcar Retail
- 3.18 Class C6 Motorcycle and Motorcar Repairs, Service Centres and Workshop
- 3.19 Class C7 Manufacturing Industry Workshops
- 3.20 Class C8 Printing and Publication
- 3.21 Class C9 Service Industry
- 3.22 Class D1 Medical Facilities
- 3.23 Class D3 Education
- 3.24 Class D4 Civic Use
- 3.25 Class D5 Integrated Community and Cultural Complex
- 3.26 Class D7 Golf Facilities
- Class D8 Institution 3.27
- 3.28 Class D9 Secure Residential Institution
- 3.29 Class D11Emergency and Security

## 4. **Conditional Uses**

## 4.1 **Class A3 - Workers quarters**

- On purpose built premises.
- Located along public transport route and pedestrian routes.
- Sufficient buffers e.g. Roads and car parking spaces
- Activity is only for cater accommodations requirement for workers.
- Community facilities required to be provided in accordance to requirement of the Supplementary Development Control Guidelines of provision of Community Facilities.

Table ZS 14.0 Zoning Schedule (General) **Land Use Zone TECHNOLOGY PARK (TP)** 

## Class B1 - General Retail and Office

- Accessory use to Technology Park activities and to serve workers and visitors of the Zone
- Activities preferred to be indoor in a purpose built building. Solid waste disposal facilities shall be in accordance to the KLCH requirements.

#### 4.3 Class B2 – Food And Drink

- Ancillary activity to serve workers and visitors of the Technology Park Zone.
- To be located at ground level of building to ensure high pedestrian activities and street retail frontages. However it shall be confined to indoor or in purpose built buildings. Solid waste disposal must comply to requirements of KLCH.
- Public entrance for activities must not be in conflict with the main use activities.

## 4.4 Class D2 - Religious Facilities

Activity as ancillary use to main activity and must support appropriate workers catchments within the Technology Park Zone.

### 4.5 **Class D5 – Integrated Community and Cultural Complex**

- Activity as ancillary use to main activity and must support appropriate workers requirements and population catchments as required in the Supplementary Planning Guidelines on Community Facilities.
- Purpose built building is preferred and only indoor activities shall be allowed.

### 4.6 **Class D6 – Sports and Recreational Facilities**

Activity as ancillary use to main activity and must support appropriate workers catchments within the Technology Park Zone.

## 4.7 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## 4.8 Class E1 – Parking

Only car and motorcycle parking spaces. Heavy vehicle parking are to be provided for vehicles servicing this zone.

## Areas Under Zoning Schedule (Specific)

Table ZS 15.0 Zoning Schedule (General)

**Land Use Zone PUBLIC INSTITUTIONAL (P1)** 

## 1. **Description of Zone**

1.1 Major institutional and civic uses such as art galleries, museum, government offices, Istana reserve, military reserve, health & emergency such as police headworkers quarters, hospitals, universities, and other civic use.

## 2. **Permitted Uses**

- 2.1 Class A3 Workers Quarters
- Class A4 Hostels 2.2
- 2.3 Class A5 Residential Care Homes
- Class C10Technology, Research and Development 2.4
- 2.5 Class D1 Medical Facilities
- 2.6 Class D2 Religious Facility
- Class D3 Education 2.7
- Class D4 Civic Use 2.8
- Class D5 Integrated Community and Cultural Complex 2.9
- 2.10 Class D6 Sports and Recreational Facilities
- Class D8 Institution 2.11
- 2.12 Class D9 Secure Residential Institution

## 3. **Prohibited Use**

- 3.1 Class A1 Single Dwelling Units
- 3.2 Class B7 Service Apartment
- 3.3 Class B3 Entertainment
- Class B4 Leisure 3.4
- 3.5 Class B5 Theme Park
- 3.6 Class B8 Home Based Business
- Class B9 Hypermarket 3.7
- Class B10 Funeral Home & Services 3.8
- 3.9 Class C1 Showroom
- 3.10 Class C2 Wholesale, Storage and Distribution
- 3.11 Class C3 Recycling Operations
- 3.12 Class C4 Petrol Station
- 3.13 Class C5 Motorcycle and Motorcar Retail
- 3.14 Class C6 Motorcycle and Motorcar Repairs, Service Centres and Workshops
- 3.15 Class C7 Manufacturing Industry
- 3.16 Class C8 Printing and Publication
- 3.17 Class C9 Service Industry
- 3.18 Class D7 Golf Facilities
- 3.19 Class D11Emergency and Security

## 4. **Conditional Uses**

## 4.1 Class B1 - General Retail and Office

- Accessory use to Institutional activities and to serve workers and visitors of the Zone only. Only 10% of the total floor area is allowed for such use or activity.
- Activities preferred to be indoor in a purpose built building. Solid waste disposal facilities shall be in accordance to the KLCH requirements.

Table ZS 15.0 Zoning Schedule (General)

Land Use Zone **PUBLIC INSTITUTIONAL (P1)** 

#### 4.2 Class B2 - Food and Drink

- Accessory use to Institutional main activities and to serve workers and visitors of this Institutional Zone. Total floor space allowed shall not exceed 10% of the total floor area.
- To be located at ground level of building to ensure high pedestrian activities and street retail frontages. However it shall be confined to indoor or in purpose built buildings. Solid waste disposal must comply with requirements of KLCH.
- Public entrance for activities must not be in conflict with the main use activities.

#### 4.3 Class B6 - Hotels

• Budget hotels and affordable lodging accommodations only.

## Class A6 - Multi Dwelling Unit 4.4

- Activity as ancillary use and only for caters student/workers accommodation requirements.
- The provision of facilities and services for the service apartments shall comply with the requirements of KLCH.

### 4.5 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

# Class E1 – Parking

Only car and motorcycle parking spaces. Heavy vehicle parking are to be provided for vehicles servicing this zone.

## 5. **Areas Under Zoning Schedule (Specific)**

Table ZS 16.0 Zoning Schedule (General)

**Land Use Zone PRIVATE INSTITUTIONAL (P2)** 

#### 1. **Description of Zone**

1.1 Institutional uses such as private art galleries, private museum, health services etc.

### 2. **Permitted Uses**

- Class A3 Workers Ouarters 2.1
- 2.2 Class A4 Hostels
- Class A5 Residential Care Homes 2.3
- 2.4 Class C10Technology, Research and Development
- 2.5 Class D1 Medical Facilities
- 2.6 Class D2 Religious Facility
- 2.7 Class D3 Education
- 2.8 Class D4 Civic Use
- 2.9 Class D5 Integrated Community and Cultural Complex
- 2.10 Class D6 Sports and Recreational Facilities
- Class D8 Institution 2.11
- 2.12 Class D9 Secure Residential Institution

## **Prohibited Use** 3.

- Class A1 Single Dwelling Units 3.1
- Class A2 Multi Dwelling Units 3.2
- 3.3 Class B3 Entertainment
- Class B4 Leisure 3.4
- 3.5 Class B5 Theme Park
- Class B8 Home Based Business 3.6
- 3.7 Class B9 Hypermarket
- 3.8 Class B10 Funeral Home & Services
- Class C1 Showroom 3.9
- 3.10 Class C2 Wholesale, Storage and Distribution
- 3.11 Class C3 Recycling Operations
- 3.12 Class C4 Petrol Station
- 3.13 Class C5 Motorcycle and Motorcar Retail
- 3.14 Class C6 Motorcycle and Motorcar Repairs, Service Centres and Workshops
- 3.15 Class C7 Manufacturing Industry
- 3.16 Class C8 Printing and Publication
- 3.17 Class C9 Service Industry
- 3.18 Class D7 Golf Facilities
- 3.19 Class D11Emergency and Security

### 4. **Conditional Uses**

## 4.1 Class B1 - General Retail and Office

- Accessory use to Institutional activities and to serve workers and visitors of the Zone only. Only 10% of the total floor area is allowed for such use or activity.
- Activities preferred to be indoor in a purpose built building. Solid waste disposal facilities shall be in accordance to the KLCH requirements.

Table ZS 16.0 Zoning Schedule (General)

PRIVATE INSTITUTIONAL (P2) **Land Use Zone** 

#### 4.2 Class B2 – Food and Drink

- Accessory use to Institutional main activities and to serve workers and visitors of this Institutional Zone. Total floor space allowed shall not exceed 10% of the total floor area.
- To be located at ground level of building to ensure high pedestrian activities and street retail frontages. However it shall be confined to indoor or in purpose built buildings. Solid waste disposal must comply with requirements of KLCH.

### 4.3 Class B6 - Hotels

Budget hotels and affordable lodging accommodations only.

## 4.4 Class B7 – Service Apartment

- · Activity as ancillary use and only for caters student/workers accommodation requirements.
- · The provision of facilities and services for the service apartments shall comply with the requirements of KLCH.

### 4.5 Class D10 - Infrastructure and Utilities

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## Class E1 - Parking

Only car and motorcycle parking spaces. Heavy vehicle parking are to be provided for vehicles servicing this zone.

# **Areas Under Zoning Schedule (Specific)**

Table ZS 17.0 Zoning Schedule (General) **Land Use Zone PUBLIC OPEN SPACE (OS1)** 

## 1. **Description of Zone**

1.1 Parks and open spaces are public spaces that provides for a variety of active and passive needs of ages of the community and local residents.

#### 2. **Permitted Uses**

2.1 Class D6 Sports and Recreation Facilities

## 3. **Prohibited Use**

- 3.1 Class A1 Single Dwelling Units
- 3.2 Class A2 Multi Dwelling Units
- 3.3 Class A3 Workers quarters
- Class A4 Hostels 3.4
- Class A5 Residential Care Homes 3.5
- Class B3 Entertainment 3.6
- Class B5 Theme Park 3.7
- Class B6 Hotels 3.8
- Class B7 Service Apartments 3.9
- 3.10 Class B8 Home Based Business
- 3.11 Class B9 Hypermarket
- 3.12 Class B10 Funeral Home & Services
- 3.13 Class C1 Showroom
- 3.14 Class C2 Wholesale, Storage and Distribution
- 3.15 Class C3 Recycling Operations
- 3.16 Class C4 Petrol Station
- 3.17 Class C5 Motorcycle and Motorcar Retail
- 3.18 Class C6 Motorcycle and Motorcar Repairs, Service Centres and Workshop
- 3.19 Class C7 Manufacturing Industry
- 3.20 Class C8 Printing and Publication
- Class C9 Service Industry 3.21
- 3.22 Class C10Technology, Research and Development
- 3.23 Class D1 Medical Facilities
- 3.24 Class D2 Religious Facilities
- 3.25 Class D3 Education
- 3.26 Class D8 Institution
- Class D9 Secure Residential Institution 3.27
- 3.28 Class D10Infrastructure and Utilities
- 3.29 Class D11Emergency and security

## **Conditional Uses** 4.

## **Class B1- General Retail and Office**

For general retail, this activity is allowable and must at underground with selected areas. The areas are Dataran Merdeka and public open space at KLCC.

Table ZS 17.0 Zoning Schedule (General) **Land Use Zone PUBLIC OPEN SPACE (OS1)** 

## Class B2 - Food and Drink 4.2

- Accessory use to support main activities of the parks or open space and to serve users and visitors of the areas.
- Premise should be purpose built and located within well accessible areas i.e. by pedestrian and visitors. Pedestrian routes from parking areas must be provided.
- Solid waste disposal facilities must be well designed and located within easy access for storage and collection.
- · Activities should be indoors and any outdoor activities should be well planned and designed to cater for the sale of food and drink.

### 4.3 Class B4 - Leisure

Leisure activity is limited to art gallery only.

#### 4.4 Class D4 - Civic Use

 Ancillary activity to support main activities of the parks or open space and to serve users and visitors of the areas.

## 4.5 **Class D5 - Integrated Community and Cultural Complex**

Activity as ancillary use to main activity and must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.

# Class D7 - Golf Facilities

· This activities must open for public user.

## 4.7 Class E1 - Parking

· Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

## 5. **Areas Under Zoning Schedule (Specific)**

Table ZS 18.0 Zoning Schedule (General)

Land Use Zone PRIVATE OPEN SPACE (OS2)

## 1. Description of Zone

1.1 Private open space refers to parks and green areas that is owned by individuals or businesses and are used at the discretion of the owner. Private parks are privately maintained and usage to the public is at the discretion of the owners. One example of such park in Kuala Lumpur is the golf courses.

# 2. Permitted Uses

- 2.1 Class B5 Theme Park
- 2.2 Class D6 Sports and Recreation Facilities
- 2.3 Class D7 Golf Facilities

# 3. Prohibited Use

- 3.1 Class A1 Single Dwelling Units
- 3.2 Class A2 Multi Dwelling Units
- 3.3 Class A4 Hostels
- 3.4 Class A5 Residential Care Homes
- 3.5 Class B1 General Retail and Office
- 3.6 Class B3 Entertainment
- 3.7 Class B7 Service Apartment
- 3.8 Class B8 Home Based Business
- 3.9 Class B9 Hypermarket
- 3.10 Class B10 Funeral Homes & Services
- 3.11 Class C1 Showroom
- 3.12 Class C2 Wholesale, Storage and Distribution
- 3.13 Class C3 Recycling Operations
- 3.14 Class C4 Petrol Station
- 3.15 Class C5 Motorcycle and Motorcar Retail
- 3.16 Class C6 Motorcycle and Motorcar Repairs, Service Centres and Workshop
- 3.17 Class C7 Manufacturing Industry
- 3.18 Class C8 Printing and Publication
- 3.19 Class C9 Service Industry
- 3.20 Class C10Technology, Research and Development
- 3.21 Class D1 Medical Facilities
- 3.22 Class D3 Education
- 3.23 Class D4 Civic Use
- 3.24 Class D5 Integrated Community and Cultural Complex
- 3.25 Class D8 Institution
- 3.26 Class D9 Secure Residential Institution
- 3.27 Class D11Emergency and Security

# 4. Conditional Uses

## 4.1 Class A3 - Workers Quarters

- Community facilities required to be provided in accordance to requirement of the Supplementary Development Control Guidelines of provision of Community Facilities.
- Meant to provide workers' accommodation.
- Buildings to be compatible with surroundings.

Table ZS 18.0 Zoning Schedule (General) Land Use Zone **PRIVATE OPEN SPACE (OS2)** 

#### 4.2 Class B2 - Food and Drink

- Premise should be purpose built and located within well accessible areas i.e. by pedestrian and visitors. Pedestrian routes from parking areas must be provided.
- Solid waste disposal facilities must be well designed and located within easy access for storage and collection.
- Activities should be indoors and any outdoor activities should be well planned and designed to cater for the sale of food and drink.

#### 4.3 Class B4 - Leisure

· Only permissible on purpose built buildings.

## 4.4 Class B6 - Hotels

• Only permissible on purpose built buildings for primary use of a hotel.

### Class D2 - Religious Facilities 4.5

Activity as ancillary use to main activity and must comply with the Supplementary Planning Guidelines on Community Facilities.

## **Class D10 – Infrastructure and Utilities** 4.6

Infrastructure and utilities are allowed in accordance to the technical requirements of the utility providers.

## 4.7 Class E1 – Parking

· Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

# **Areas Under Zoning Schedule (Specific)**

Table ZS 19.0 Zoning Schedule (General)

Land Use Zone FOREST RESERVE]

## 1. Description of Zone

1.1 Bukit Nanas Forest Reserve, Bukit Sungai Besi Forest Reserve and Bukit Sungai Puteh Forest Reserve.

## 2. Permitted Uses

No other activities allowed except those conditionally permitted as specified below.

## 3. Prohibited Use

- 3.1 Class A1 Single Dwelling Units
- 3.2 Class A2 Multi Dwelling Units
- 3.3 Class A3 Workers Quarters
- 3.4 Class A4 Hostels
- 3.5 Class A5 Residential Care Homes
- 3.6 Class B1 General Retail and Office
- 3.7 Class B2 Food and Drink
- 3.8 Class B3 Entertainment
- 3.9 Class B4 Leisure
- 3.10 Class B5 Theme Park
- 3.11 Class B6 Hotel
- 3.12 Class B7 Service Apartment
- 3.13 Class B8 Home Based Business
- 3.14 Class B9 Hypermarket
- 3.15 Class B10 Funeral Homes & Services
- 3.16 Class C1 Showroom
- 3.17 Class C2 Wholesale, Storage and Distribution
- 3.18 Class C3 Recycling Operations
- 3.19 Class C4 Petrol Station
- 3.20 Class C5 Motorcycle and Motorcar Retail
- 3.21 Class C6 Motorcycle and Motorcar Repairs, Service Centres and Workshop
- 3.22 Class C7 Manufacturing Industry
- 3.23 Class C8 Printing and Publication
- 3.24 Class C9 Service Industry
- 3.25 Class C10Technology, Research and Development
- 3.26 Class D1 Medical Facilities
- 3.27 Class D2 Religious Facilities
- 3.28 Class D3 Education
- 3.29 Class D4 Civic Use
- 3.30 Class D5 Integrated Community and Cultural Complex
- 3.31 Class D6 Sports and Recreation Facilities
- 3.32 Class D7 Golf Facilities
- 3.33 Class D9 Secure Residential Institution
- 3.33 Class D10Infrastructure and Utilities
- 3.34 Class D11Emergency and Security
- 3.35 Class E1 Parking

## 4. Conditional Uses

## 4.1 Class D8 – Institution

· Government use only.

# 5. Areas Under Zoning Schedule (Specific)

Table ZS 20.0 Zoning Schedule (General)

Land Use Zone WHITE ZONE

## 1. **Description of Zone**

1.1 Refer to public facilities, infrastructure and utilities services.

## 2. **Permitted Uses**

- 2.1 Class D1 Medical Facilities
- 2.2 Class D2 Religious Facilities
- 2.3 Class D3 Education
- 2.4 Class D4 Civic Use
- 2.5 Class D5 Integrated Community and Cultural Complex
- 2.6 Class D10Infrastructure and Utilities
- 2.7 Class D11Emergency & Security

### 3. **Prohibited Use**

- Class A1 Single Dwelling Units 3.1
- Class A2 Multi Dwelling Units 3.2
- 3.3 Class B1 General Retail and Office
- 3.4 Class B3 Entertainment
- 3.5 Class B4 Leisure
- 3.6 Class B5 Theme Park
- 3.7 Class B6 Hotels
- 3.8 Class B7 Service Apartment
- 3.9 Class B8 Home Based Business
- 3.10 Class B9 Hypermarket
- 3.11 Class C1 Showroom
- 3.12 Class C2 Wholesale, Storage and Distribution
- 3.13 Class C3 Recycling Operations
- 3.14 Class C4 Petrol Station
- 3.15 Class C5 Motorcycle and Motorcar Retail
- 3.16 Class C6 Motorcycle and Motorcar Repairs, Service Centres and Workshop
- 3.17 Class C7 Manufacturing Industry
- 3.18 Class C8 Printing and Publication
- 3.19 Class C9 Service Industry
- 3.20 Class C10 Technology, Research and Development
- 3.21 Class D7 Golf Facilities
- 3.22 Class D9 Secure Residential Institution

### 4. **Conditional Uses**

#### 4.1 Class A3 – Workers Quarters

- On purpose built premises.
- Located along public transport route and pedestrian routes.
- Sufficient buffers e.g. Roads and car parking spaces
- Activity is only for cater accommodations requirement for workers.
- Community facilities required to be provided in accordance to requirement of the Supplementary Development Control Guidelines of provision of Community Facilities.

Table ZS 20.0 Zoning Schedule (General)

Land Use Zone WHITE ZONE

## 4.2 Class A4 - Hostels

- Shall be located within the more highly accessible areas in same compound and along major public transport routes and pedestrian routes.
- The scale, intensity and operation of the use shall not generate traffic congestion or other potential nuisances.
- Accessory Retail Use can be allowed on the ground floor and must be of street frontage.

## 4.3 Class A5 – Residential Care Homes

- As ancillary activity within the zone and development proposal.
- Residential care homes within this zone have to comply with the Supplementary Guidelines
  of KLCH. Design of such premise must be barrier free and cater for the needs of the dwellers
  of the residential care homes.
- It shall also be located away from heavy traffic areas.

## 4.4 Class B2 – Food and Drink

 Business operations must take place only within the premises and not create serious nuisance impacts on the area.

## 4.5 Class B10 – Funeral Homes and Services

Only allowed for Funeral Parlours and Undertakers within the cemetery sites only.

## 4.6 Class D6 – Sports and Recreational Facilities

- Must support appropriate population catchments as required in the Supplementary Planning Guidelines on Community Facilities.
- Purpose built building is preferred and only indoor or outdoor activities shall be allowed.
- Must be accessible via public transport and pedestrian route.

## 4.7 Class D8 – Institutions

- Institutions except for embassies shall be allowed in the white zone. Provisions of parking and other amenities should comply with KLCH Supplementary Guidelines.
- Must be located along main public transport and pedestrian routes. Should also be located within Transit Planning Zone.

## 4.8 Class E1 - Parking

Only car and motorcycle parking spaces. Heavy vehicle and depot use are not allowed.

# **KLDCP 2008 USE CLASSES**

Column I	Column II	Column III
Class	Activities	Description
Class A1 Single dwelling units	<ul><li>a. Terrace Houses</li><li>b. Bungalows</li><li>c Semi-detached Houses</li><li>d. Zero end lot</li></ul>	Family houses or houses occupied by residents living together as a single household.
Class A2 Multi Dwelling Units	a. Apartments b. Town houses c. Condominiums	Building or any part thereof which is planned or designed for use for two or more dwelling units.
Class A3 Workers quarters	<ul> <li>a. Government Workers Quarters</li> <li>b. Military Workers Quarters</li> <li>c. Police Workers Quarters</li> <li>d. Workers Workers As The Army, Police, Fire Department And Hospitals And Other Organisations Intended For The Use By Their Workers.</li> </ul>	Accommodations for employees of an organisation typically of government and quasi Quarters government Bodies Such
Class A4 Hostels	a. Student Hostel b. Youth Hostel c. Transient Homes	Use for the provision of accommodation normally supervised to provide lodgings at a much less than usual commercial or rental rate on a short to medium term basis.
Class A5 Residential Care Homes	<ul> <li>a. Welfare Homes/Foster Care Homes/Halfway Homes/ Orphanages</li> <li>b. Homes/Centres for the Disabled</li> <li>c. Old Folks Homes</li> <li>d. Day Care/Child Care Centre/Nursery</li> </ul>	Use for provision of a facility licensed to provide residential care to physically disabled, older people, children and other groups of people requiring care, attention or supervision.
Class B1 General Retail and Office	<ul> <li>a. Provision Store</li> <li>Groceries Store</li> <li>Mini Market</li> <li>Convenience Store</li> <li>supermarket</li> </ul>	Shops or commercial spaces for the general retail sale of goods (food and household) and for provision of personal and household services.

Column I	Column II	Column III
Class	Activities	Description
		Supermarket refers to a self- service distribution store with sales floor area of 2,000 square meters to less than 4,500 square meters retailing a wide variety of mainly consumer goods, comprising a mx of food and non-food products.
	b. Retail trade in household and personal goods including:-	
	<ul> <li>electrical appliances,</li> <li>furniture (2800sqft)     (excluding showroom),</li> <li>textile and shoes,</li> <li>pharmacies,</li> <li>Plastic goods,</li> <li>Musical instrument     (sales only),</li> <li>cosmetic and perfumery ,</li> <li>book and stationary shops,     newsagent,</li> <li>optical products,</li> <li>toys,</li> <li>florist,</li> <li>sports equipments,</li> <li>goldsmiths and jewellery     (sales only),</li> <li>hardware (small scale up to     a maximum of 2,000sq ft)     and DIY shop,</li> <li>sanitary ware and fittings,</li> <li>other household and     personal goods not listed     above.</li> <li>Souvenir</li> </ul>	
	<ul> <li>c. Retail trade in industrial, agricultural and business (office) equipment and materials,</li> <li>Office equipment,</li> <li>Machines, material and other equipments not requiring large external storage areas (excluding heavy machineries and large equipments).</li> </ul>	

Column I	Column II	Column III
Class	Activities	Description
	Other industrial, agricultural and business (office) equipments and materials not listed above.	
	d. Retail trade in construction materials	
	e. Personal and household services including:-  Laundromats/drycleaners, tailoring and dressmaking, barber, beauty shops, health centres, massage parlour, spa, photographic studios, photo frame makers, funeral parlours and undertakers, astrologers, pawnshops, interior decorators (including office and showrooms), postal and courier services clinics (including dental clinics and animal clinic) Pet shop, aquarium Internet cafe, Boutique Money Changer Beauty salon and treatment Reflexology and massage treatment other personal and household services not listed above.	Activities are primarily engaged in activities concerned with the operation of private households
	f. Private educational services including:-	

Column I	Column II	Column III
Class	Activities	Description
	g. Household goods repair services to individual and households (excluding automotive and large equipment repair services).  Typical uses include:  • electrical appliance repair shop (air conditioning, TV, fridge, washing machine, etc.)  • computer repair services  • bicycle repair services  • musical instrument repair services.	
	h. Banking, financial institutions, stockbrokers, insurance, and real estates	Use for the provision of financial services, professional services (other than medical services), or any other services to the public and business entities.
	<ul> <li>i. Professional services</li> <li>legal</li> <li>accounting, auditing, book-keeping</li> <li>consultants (town planning, architectural, engineering, valuation etc)</li> </ul>	
	<ul> <li>j. Business support services</li> <li>clerical</li> <li>secretarial services</li> <li>computer services</li> <li>Photostatting</li> </ul>	
Class B2 Food and Drink	<ul><li>a. Restaurants/Cafes/Bistro</li><li>b. Kopitiams/coffee shops</li><li>c. Fast food</li><li>d. Food court</li><li>e. Food stalls</li></ul>	Sale of food and beverages prepared either on or off the premises for on and off premises consumption where no significant element of entertainment (loud music) exists.
Class B3 Entertainment	<ul><li>a. Discotheques</li><li>b. Night clubs/Pubs/Bars</li><li>c. Karaoke outlets</li><li>d. Slot machines</li><li>e. Snooker/billiards</li></ul>	An establishment that stays open late at night and provides food, drink, entertainment, and music for dancing and singing including life music and other form of entertainment.

Column I	Column II	Column III
Class	Activities	Description
Class B4 Leisure	<ul> <li>a. Cinema, theatres, auditorium, concert halls</li> <li>b. Art galleries</li> <li>c. Amusement outlets/arcade games</li> <li>d. Bowling alley</li> <li>e. Skating rink</li> </ul>	Commercially operated venues for leisure related activities
Class B5 Theme Park	a. Indoor theme park b. Outdoor theme park	Indoor or outdoor commercially operated park offering various forms of entertainment such as arcade games, carousels, roller coasters, as well as food, drink and souvenirs.
Class B6 Hotels	a. Hotels b. Chalets c. Budget Hotel	Includes hotel, resort cum hotel and chalets with 10 rooms and above. Also include budget hotel.
Class B7 Service Apartment	a. Service apartments b. Service suites c. Service residences	Accommodations built on commercial land, normally managed by a third party and includes business or hotel like facilities normally not found in residential apartments
Class B8 Home Based Business	<ul> <li>a. Financial services such as accounting, auditing, taxation and investment consultants</li> <li>b. Company secretary services;</li> <li>c. Property management consultants;</li> <li>d. Translation services;</li> <li>e. Writing;</li> <li>f. IT services and web page providers and designers;</li> <li>g. Training consultants;</li> <li>h. Plan drawings and graphics;</li> <li>i. Insurance consultants;</li> <li>j. Professionals (e.g. town planners, engineers, accountants, surveyors etc)</li> <li>k. Will writing services;</li> <li>l. Tuition services not exceeding 8 students;</li> <li>m. Preparation of food and drinks that do not involve heavy machinery;</li> </ul>	A business activity operated by a self-employed individual from a house, either on his / her own or employing not more than 5 employees, and provides a service or product from that workplace.

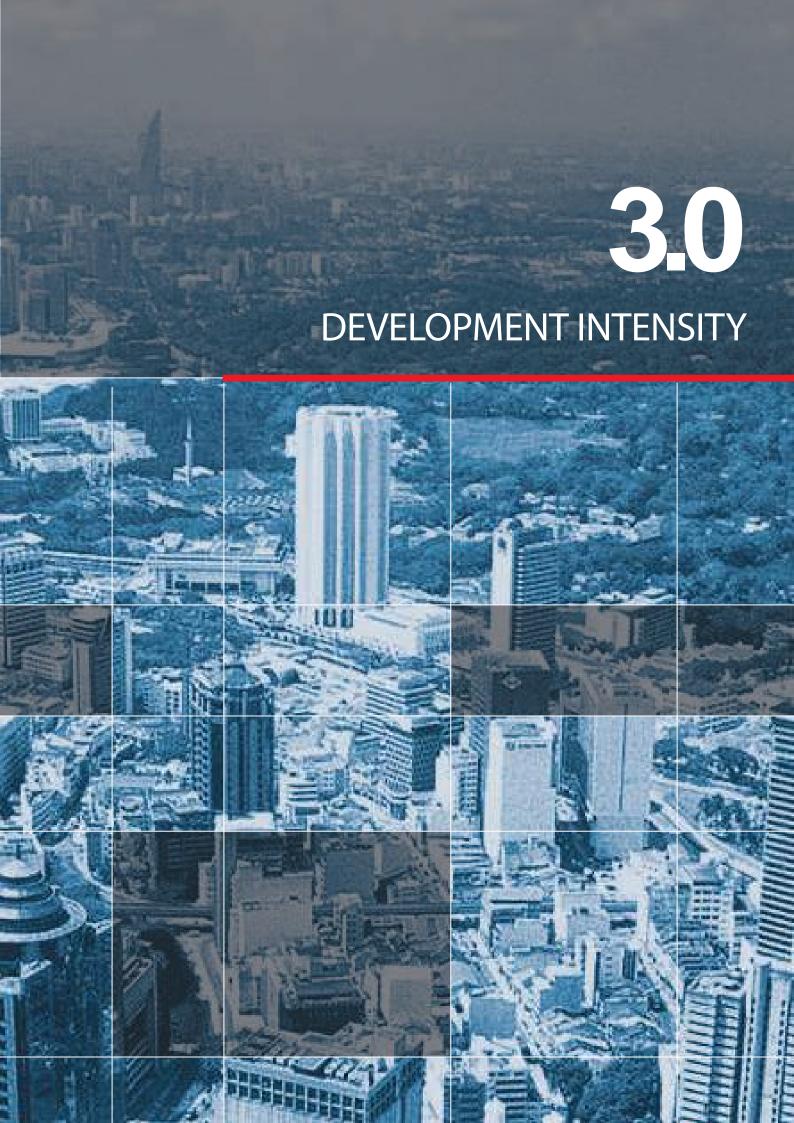
Column I	Column II	Column III
Class	Activities	Description
	<ul><li>n. Event management;</li><li>o. Dating services;</li><li>p. Other activities that do not create general disturbance to the public</li></ul>	
Class B9 Hypermarket	a. Hypermarket	A self-service distribution store with sales floor area of 5,000 square meters or more, selling a very wide variety of mainly consumer goods, comprising a mix of food and non-food products, in a range of transaction sizes or quantities and in different forms of packaging.
Class B10 Funeral Homes and Services	a. Crematorium b. Columbaria	A facility that provides a number of functions dealing with the deceased person and their families.
Class C1 Showroom	<ul><li>a. Car (new and used)</li><li>b. Furniture</li><li>c. Other products</li></ul>	Space used to display products for sale, such as automobiles, furniture, carpet or apparel.
Class C2 Wholesale, Storage and Distribution	<ul> <li>a. Wholesale trade</li> <li>b. Storage and warehousing of goods, raw materials and equipments</li> <li>c. Storage of non-operating vehicles</li> </ul>	A building, buildings or premises that relate to, or engaged in storage of goods and products, the sale and distribution of goods in large quantities.
Class C3 Recycling Operation	<ul><li>a. Automotive recycling yard</li><li>b. Paper recycling centre</li><li>c. Recycling facilities</li></ul>	Storage, sale, dismantling, or other processing of used, source separated, or waste materials not intended to be used in their original form and for recycling.
Class C4 Petrol Station	<ul> <li>a. Sale of petrol</li> <li>b. Sale of auto accessories and parts</li> <li>c. Service of motor vehicles</li> <li>d. Car wash</li> <li>e. Convenience store</li> <li>f. Food and Beverages</li> <li>g. Banking Facilities</li> </ul>	A retail establishment, at which motor vehicles are refuelled, serviced and sometimes repaired.

Column I	Column II	Column III
Class	Activities	Description
Class C5 Motorcycle and Motorcar Retail (sale only)	a. Sale of motorcycles. b. Sale of motorcars	Retail establishment where motorcycle and motorcars are sold (sale only).
Class C6 Motorcycle and Motorcars repairs, service centre and workshop	<ul> <li>a. Sale, service and repair of motorcycles</li> <li>b. Sales, service and repair of motorcars</li> <li>c. Sales, service and repair of lorries and trucks</li> <li>d. Sales, service and repair of buses and vans</li> </ul>	A building or premise where motorcycle and motorcars are sold and serviced or repaired either together on the same premise on only serviced/repaired.
Class C7 Manufacturing Industry	All manufacturing activities	Industrial establishment or premises that involves production of goods i.e. making products from raw materials by the use of manual labour or machines and that is usually carried out systematically. This includes the fabrication or assembly of components into finished products on a fairly large scale.
Class C8 Printing and Publication	<ul> <li>a. Plans</li> <li>b. Books, magazines and binding on a large quantities</li> <li>c. Newspaper</li> <li>d. Banner and other advertising materials (excluding shop signs)</li> </ul>	A building, or buildings or premises that process, or produce printed material by means of inked type and a printing press or by similar means
Class C9 Service Industry	<ul> <li>a. Handicraft</li> <li>b. Foundries and small metal workshops</li> <li>c. Furniture</li> <li>d. Small scale tailoring. (with more than 10 machines)</li> </ul>	Industrial establishments or premises that provides services rather than goods
Class C10 Technology, Research and Development	<ul> <li>a. Research and development (R&amp;D)</li> <li>b. Bio-technology</li> <li>c. Nano-technology</li> <li>d. Information technology and multimedia</li> </ul>	Activities using high-end equipment or ICT oriented.

Column I	Column II	Column III
Class	Activities	Description
	e. Science park f. Research centre	
Class D1 Medical Facilities	a. Hospitals (public or private) b. Medical specialists c. Sanatorium	A building/ buildings or premises that houses organization or activities dedicated to the practice of giving treatment by medicine maintenance of health and the prevention, alleviation, or cure of disease.
Class D2 Religious Facilities	<ul> <li>a. Mosque, Surau</li> <li>b. Chapel</li> <li>c. Church</li> <li>d. Temple</li> <li>e. Religious associations</li> <li>f. Religious centre</li> <li>g. Others religious facilities</li> </ul>	A building/ buildings or premises that houses organization or activities dedicated to religion. Religious activities are related to teachings of a beliefs and values.
Class D3 Education	<ul><li>a. University</li><li>b. College</li><li>c. Training centre</li><li>d. School</li><li>e. Kindergarten</li><li>f. nursery</li></ul>	A building/ buildings or premises that houses organization or activities dedicated to education.
Class D4 Civic Use	<ul><li>a. Community hall</li><li>b. Public library</li><li>c. Museum</li><li>d. Cultural centre/complex</li><li>e. Community centre</li></ul>	A building/ buildings or premises that houses organization or activities dedicated to public service and culture.
Class D5 Integrated Community and Cultural Complex	a. Integrated Community and Cultural Complex	Land or building that provides variety type of community facilities

Column I	Column II	Column III
Class	Activities	Description
Class D6 Sports and Recreation Facilities	<ul> <li>a. Stadiums</li> <li>b. Futsal</li> <li>c. Courts for games or spaces     for recreation and sport</li> <li>d. Race Courses</li> <li>e. Swimming pool</li> <li>f. Polo Grounds</li> <li>g. Club House</li> <li>h. Go-Kart</li> </ul>	A building/ buildings or premises that houses organization or activities dedicated to sports and recreation.
Class D7 Golf Facilities	a. Golf course b. Driving range	A land or building that provides golf related activities and facilities.
Class D8 Institution	<ul><li>a. Government offices</li><li>b. Embassies</li><li>c. Archives</li><li>d. Courts</li><li>e. Army Camp</li></ul>	A building/ buildings or premises that houses organization or activities dedicated to administration and management by the government.
Class D9 Secure Residential Institution	<ul><li>a. Young offender institution</li><li>b. Drug rehabilitation centre</li><li>c. Short term holding centre</li><li>d. Detention centre</li><li>e. Convalescent home</li></ul>	Use for a provision of accommodation, care and rehabilitation of persons with problems deemed requiring care and rehabilitation by relevant authorities and bodies
Class D10 Infrastructure and Utilities	a. Water tank b. Water treatment plant c. PPU d. PMU e. Sub-station f. Feeder pillar g. Pump station/Pump house h. Sewerage treatment plant i. Telecommunication tower j. Drainage k. Gross Pollutant Traps (GPT) l. Refuse Derive Fuel plant (RDF) or Waste to Energy plant (WTE). m. Retention Pond	Land or buildings that provides facilities, services, and installations needed for the functioning of a community or society, such as water and power lines, telecommunication, and sewerage,

Column I	Column II	Column III
Class	Activities	Description
Class D11 Emergency & Security	<ul><li>a. Police Station</li><li>b. Police Beat</li><li>c. Fire Station/Bridged</li><li>d. Neighbourhood Watch</li></ul>	Land or building designated for emergency and security services involving facilities services for the police, fire bridged, and neighbourhood watch.
Class E1 Parking	<ul><li>a. Cars and motorcycles parking</li><li>b. Heavy vehicles parking</li><li>c. Depot</li><li>d. Terminal</li><li>e. Park and Ride</li></ul>	Space in which to park vehicles or a vehicle. Parking facilities include indoor and outdoor private the side of the road, a parking lot or car park, and indoor and outdoor multi-level structures.



# **Development Intensity Planning Control**

The need to control the development intensity of urban activities within Kuala Lumpur is essential in attaining a sustainable built environment for the city. To strengthen the land use zoning control and use classes control, the development intensity of Kuala Lumpur will also be regulated in order to ensure developments supports infrastructure development as well as protects the urban amenities.

Development intensity will be control in two forms:-

- Density control for residential development (except for areas in the City Centre).
- (b) Plot ratio control for residential development in City Centre area, commercial, mixed-use and industrial developments.

## **Density Control** 3.2

Density is used in planning for new residential developments to measure the amount of new housing schemes to be built on a piece of land. Density control also avoids sprawling of development to areas outside the designated zones and concentrates development within areas covered by public transportation and infrastructure.

The need to identify density is base on the need to maintain and regulate:-

- (a) The character of the site itself; or
- (b) The character of the surrounding area; or
- (c) The need to preserve the amenities of existing or future residents, including providing satisfactory on-site provision of public open space, landscaping, and car parking; or
- (d) The need to generate a critical mass of people to support urban services such as public transportation, local shops and schools; or
- (e) The need to provide a variety of housing types in Kuala Lumpur so as to meet the needs of local residents and future communities of the city.





# 3.2.1 Definition for Density Control

The Federal Territory (Planning) Act 1982 (Act 267) defines **Density** as the intensity of use of land reckoned or expressed in terms of the number of person, dwelling units, or habitable rooms, or any combination of those factors, per unit area of land.

The use of maximum persons per acre of residential density has been adopted in this plan, where DCP 2 - Density Map of the KLDCP 2008 indicates the allowable densities for residential development in Kuala Lumpur.

# 3.2.2 Application

The application of Density Control shall be applied on land zoned for:-

- (a) Residential 1
- (b) Residential 2
- (c) Residential 3
- (d) Public Housing

The density shall be designated to each plot of land/lot basis in these zones other than the City Centre area.

# 3.2.3 Guideline for Density Control in Kuala Lumpur

The guideline for Density Control refers to the DCP 2 – Density Map for Kuala Lumpur, where there are four main categories of residential densities that shall be the control measure for residential development intensity in the city.

Table 3.1- Density Control in Residential Zones

Land Use Zone	Maximum Density Allowable		
Land 03e 20ne	Person/ac (ppa)	Unit/ac	
R1	4	1	
	12	3	
	24	6	
	40	10	
R2	48	12	
	80	20	
	120	30	
R3	160	40	
	240	60	
	320	80	
	400	100	
Public Housing	400	100	

The R1 is intended specifically for Low Density Residential. Here the densities vary from 4 persons per acre to 40 persons per acre and residential units shall vary from the 1 unit/acre homes in the hillside areas of the city to conventional housing that is compatible with single-family neighbourhoods.

R2 are Medium Density Residential where the densities vary from 48 persons per acre to 120 persons per acre. Residential units here vary from landed conventional link homes to low-rise multi-dwellings units or landed strata homes.

R3 are High Density Residential where the intended developments are multidwelling units. Here the densities allowed vary from 160 persons per acre to 400 persons per acre. The higher densities are focussed on urban areas such as the District Commercial Centre, Neighbourhood Commercial Centre and around Transit Stations.

The **Public Housing** has a maximum allowable density of **400 persons per acre**. The high density public housing allows for better integrated social housing where amenities will be well served and the development provides economies of infrastructure.

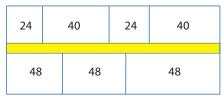
With the above density guidelines for Kuala Lumpur, any area zoned for RI cannot be increase to R2 or R3 and vice-versa. Increase in density will only be allowed within the same land use zone; however application for Planning Permission for Increase of Density is required and approvals is subject to decisions made on best use for land and quality of life is not compromised.

For the purpose of any conversion required from persons per acre to the number of residential units or vice-versa; the conversion factor of 4.0 persons per household shall be used. Thus column 3 of Table 3.1 shows the maximum number of units per acre allowed for each type of residential zone.

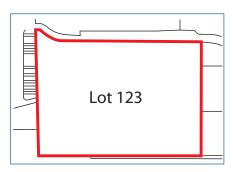


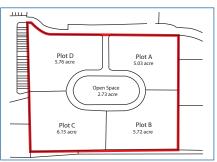
\*The Household size for Kuala Lumpur in 2000 is 4.2. However the household size is anticipated to decrease to 4.0 in 2000 and subsequently to 3.8 in 2015 and 2020. Thus the average household size of 4.0 is to be used for the KLDCP 2008.

### Example



Density asssigned by plot/lot





Plot A =600 units Plot B = 600 units Plot C = 500 units 500 units Plot D =2,200 units

## **UNDERSTANDING DENSITIES**

### What is the Density for my land?

Each plot of land zoned for Residential in Kuala Lumpur has been identified with a Density.

Refer to **DCP 2 - Density Map** Kuala Lumpur for your maximum allowable density.

## Does Density refers to Gross or Net Allowable?

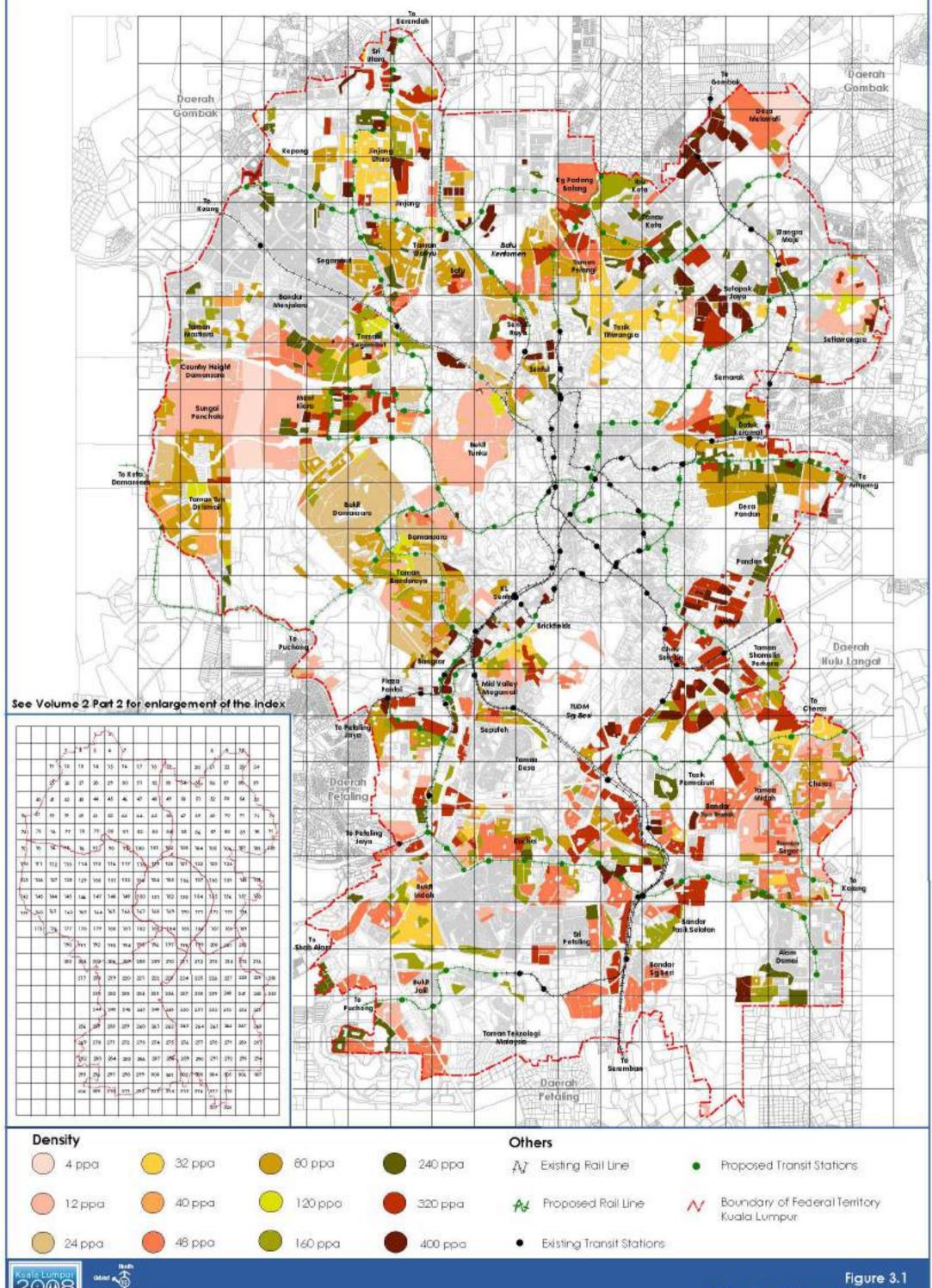
Density is calculated in gross. In other words, all the surrendered land such as open space, road reserves as well as infrastructures and utilities areas will also be included in the density calculation for a plot of land identified for development.

## Example:

- Based on the acreage and density that has been assigned for Lot 123, the maximum allowable density is 8800 persons which is equivalent to a maximum allowable of 2200 units.
- When Lot 123 has been approved for sub division and in order to obtain the Development Order (DO), the number of units must be based on the gross (not exceeding 8800 persons/2,200 units) inclusive of the open space, roads as well as infrastructures and utilities.

Land Use Zoning	Residential 3
Development Area	27.5 acre
Assigned Density Densities	320 persons per acre
Max Persons / Max Allowable Number of Units for Land	8800 person/2,200 units

- Should the land owner decide to develop his plots of land in phases, the density allowed for each plot of land is base on the approved layout plan and as stated in the Development Order given.
- Should the land owner sells any of the plots to another party the latter can only develop the purchased plot of land based on the approved layout plan and its allowable densities and as stated in the Development Order given.





#### 3.3 **Plot Ratio Control**

Floor Area Requirements (FAR) limitations will be imposed for commercial development as well as industrial and mixed-use developments within all designated zones. Such limitations on FAR will be controlled by means of the plot ratio.

Plot ratio standards express the relationship between the gross area of a site, and the total floor area of a building. The need to designate Plot Ratio to land in Kuala Lumpur so as to achieve:-

- (a) Sustainable development intensity that would prevent adverse effects of over development;
- (b) The desired functions and character of commercial zones in Kuala Lumpur;
- (c) A critical mass that can support the urban services such as Public Transportation, urban amenities etc; and
- (d) The overall urban design objectives of Kuala Lumpur

The use of Maximum Permissible Plot Ratio has been adopted in this plan, where DCP 3 - Plot Ratio Map of the KLDCP 2008 indicates the maximum allowable/permissible plot ratio for development in Kuala Lumpur.

### 3.3.1 Definition

"Plot ratio means the ratio between the total floor area of a building and the area of the building plot" as defined in the Federal Territory Planning Act 1982 (Act 267) (Part I, Section 2).

### 3.3.2 Application

The application of Plot Ratio as a Planning Control measure shall be applied to land zoned as:

- City Centre Commercial
- **District Centre Commercial**
- Neighbourhood Centre Commercial
- Commercial
- Residential zone in City Centre
- Mixed Use Commercial
- Mixed Use Residential
- Mixed Use Commercial Industrial Zone
- **Industrial Park**
- Technology Park

The plot ratio shall be designated to each plot of land/lot basis in the above zones.



# 3.3.3 Guidelines for Plot Ratio Control in Kuala Lumpur

The guidelines for the Plot Ratio Control in Kuala Lumpur are prescribed in the DCP 3 - Plot Ratio Map and the table below (Table 3.2) reflects the maximum permissible plot ratio for the various land use zoning.

Table 3.2 - Plot Ratio Control

Land Use Zoning		Maximum Permissible Plot Ratio
City Centre Commercial	ccc	1:0.5
		1:1
		1:2
		1:3
		1: 4
		1:5
		1:6
		1:7
		1:8
		1:9
		1:10
District Centre Commercial	DCC	1:1
		1:2
		1:3
		1:4
Neighbourhood Centre Commercial	NCC	1:1
		1:2
		1:3
Commercial	С	1:2
		1:4
Mixed Use Commercial	MXC	1:4
		1:6
		1:8
Mixed Use Residential	MXR	1:4
		1:6
		1:8
Mixed Use Commercial and Industry	MXCI	1:2
Industrial	IP	1:2
Technology Park	TP	1:2



This guideline applies both to new development and to existing development where building extension is proposed. Any existing development that has a higher plot ratio that the guidelines provided will retain its existing plot ratio approved.

The distribution of plot ratio is also related to the land use strategies identified in the Draft KL City Plan 2020. The land use strategy calls for sustainable use of land, where optimum land development will be encouraged to support the vision for a World Class City. Thus plot ratio assigned to land plots in Kuala Lumpur in general refers to the following guide:-

### Plot Ratio and hierarchy of Commercial Zones

The three main hierarchies of commercial zones i.e. the CCC, DCC and NCC all have different range of plot ratio as illustrated above. The City Centre Commercial has the highest range of permissible Plot Ratio especially in its premier business district which will enhance the role of Kuala Lumpur as a World Class Business Centre.

Other urban centres, District Commercial Centres and Neighbourhood Commercial Centres are given lesser intensity of development; depending on the threshold of area it serves as well as the physical setting of the area.

## (b) Plot Ratio for Urban Redevelopment Areas

The Plan encourage development of Brownfield sites in the city; where infill sites, areas identified for urban redevelopment will be given incentives through enhanced economic use of land as well as higher intensity of development. Such sites are given higher plot ratios to ensure of its economic feasibility without compromising the benefits to local community and contribute towards quality development and built environment.

Most land identified as urban redevelopment areas are within the inner city of Kuala Lumpur. These areas includes at Jalan Davis, Jalan Duta and Kampong Bharu; where most of these lands are zone as City Centre Commercial, Mixed Use Commercial or Mixed Use Residential. Permissible plot ratio varies from 4 to 8 depending on its physical setting and carrying capacity of the area (especially with regards to traffic generation).

# **Plot Ratio and Industrial Parks**

The Plan encourage the development of properly planned industrial sites that is clean, modern and complements the economic needs of the city. Hence plot ratios are assigned to support and regulate these activities.

### (d) Plot Ratio in the Transit Planning Zone (TPZ)

The Plan encourage transit oriented development that can provide a critical mass for the public transportation system. Hence higher plot ratios are given as incentives for plots of land near transit stations. The general guide for areas in TPZ is an additional 0.5 ratio added to the existing prescribed plot ratio. (Refer to Chapter 7.0, Transit Planning Zone)

## (e) Plot Ratio for Comprehensive Development Areas (CDA's)

The KLSP 2020 has identified two areas as Comprehensive Development Area i.e. areas within Kampung Bharu as well as government complex in Jalan Duta (within Jalan Dutamas and Jalan Hikmat Usaha). In these two areas most land is zone as Mixed Use Commercial except for CDA Kampung Baru which allows for City Centre Commercial and District Centre Commercial zoning.

### **UNDERSTANDING PLOT RATIO**

# What is the Plot Ratio for my land?

Each plot of land zone for City Centre Commercial/ District centre Commercial/ Neighbourhood Commercial/ Commercial/ Mixed Use Commercial/Mixed Use Residential/ Mixed Use Commercial Industrial/ Industrial Park/ Technology Park in Kuala Lumpur has been identified with a Plot Ratio.

Refer to **DCP 3 - Plot Ratio Map** Kuala Lumpur for your maximum permissible Plot Ratio.

### **Plot Ratio and Floor Area Requirement**

The Floor Area Requirement (FAR) shall be the total floor area allowed to be built on the said site based on the maximum permissible plot ratio for the said land/ site.

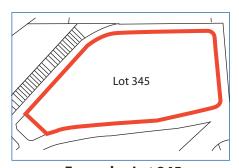
## Plot ratio and the Development Approvals

Upon the approvals for planning permission, a plot of land that has requested for sub-division will then be given Development Order base on the new sub-divided plots of land. Such development order includes the new approved plot ratio for the said lands.

### Example:

- When Lot 345 has been approved for sub division, the land now has 5 new plots with new approved Plot Ratio (as per table).
- · Should the land owner develops in phases or sells one of the plots to another party the latter could only develop the purchased plot based on the approved layout plan allowable plot ratio as stated in the Development Order.

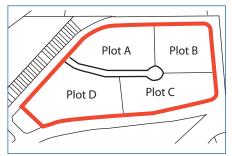
Plot	Land (acre)	Land (sq ft)	FAR	Approved PR
Plot A	3.00	130,679.00	513,964.44	3.93
Plot B	2.82	122,838.20	685,285.92	5.58
Plot C	2.72	118,482.30	342,642.96	2.89
Plot D	3.89	169,447.10	171,321.48	1.01
Total	12.43	541,446.60	1,713,214.80	



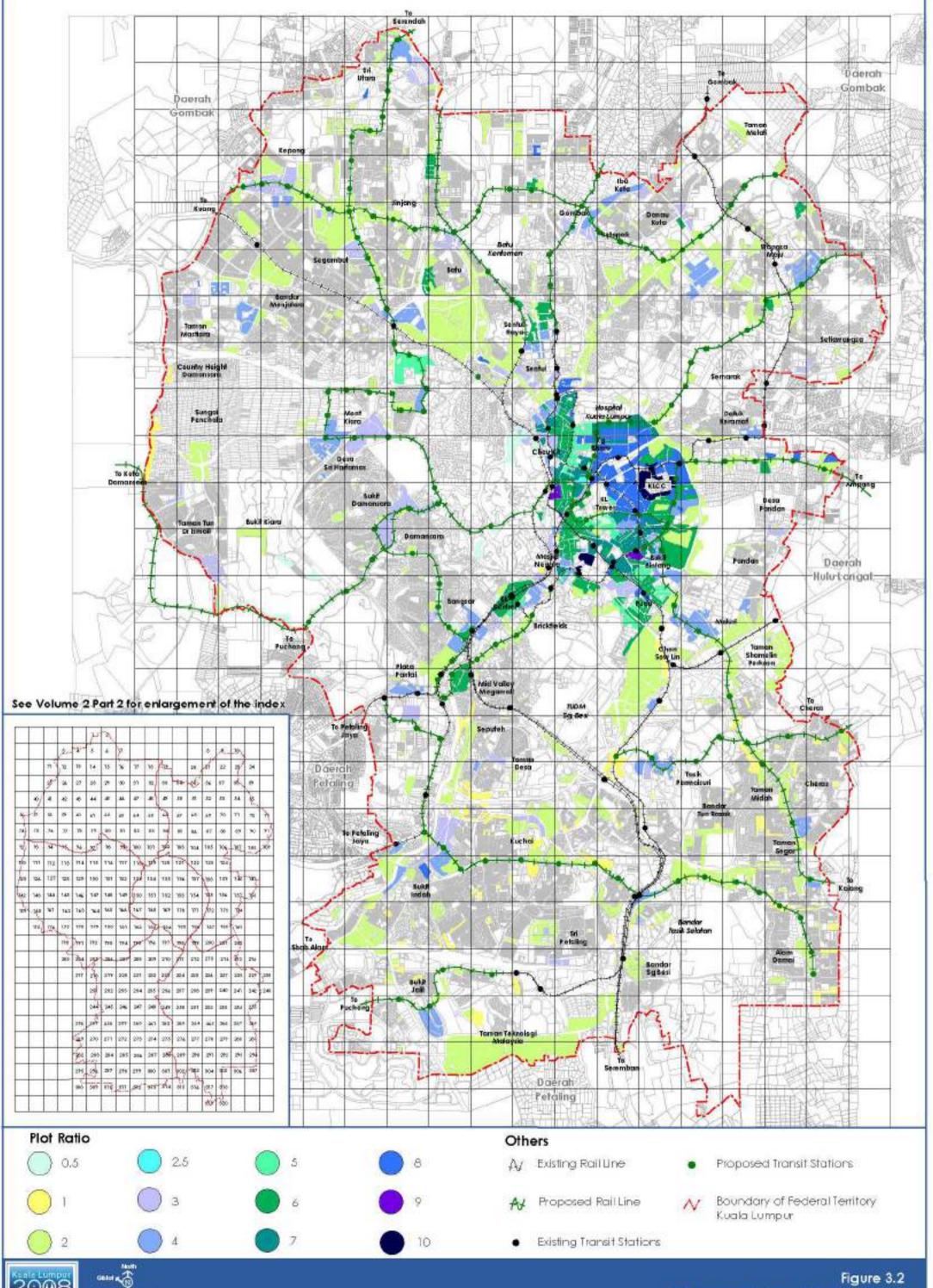
Example: Lot 345 **Zoning** = District Commercial Centre **Maximum Permissible Plot Ratio** = 3:1

**Site Area** = 13.11 acres

Floor Area Requirement (FAR):  $3 \times 13.11 \text{ acres } \times 43560 \text{ sq.ft} =$ 1,713,214.80 sq.ft









## 3.3.4 Plot Ratio for Residential Zone within City Centre Area

The Floor Area Requirement (FAR) for Residential within the City Centre area is also controlled by Plot Ratio. This is due to dynamic rate of development in the city centre area and the trend of high rise living within the city centre i.e. condominiums and apartments; and plot ratio allows for an immediate representation of allowable space to be built on the designated plot of residential lands.

## a. Distribution of Residential Area in the City Centre

The DCP 1-Land Use Zoning Map of Kuala Lumpur has designate areas in Jalan Davis, around Jalan Kemuning, Bukit Bintang, Dang Wangi, Jalan Yap Kwan Seng and part of Bukit Tunku as areas zone for residential development. This is to encourage inner city living in Kuala Lumpur and to reduce out-migration of population to neighbouring cities and states.

## b. Floor Area Distribution for Residential in the City Centre

Base the plot ratio designated for residential zone in the city centre, the total FAR generated for Kuala Lumpur by the year 2020 shall be 40,910,463.00 sq.ft.

Table 3.3 - Plot Ratio Control for Residential at the City Centre, 2020

Zoning	Plot Ratio	Acreage	Floor Area Requirement (sq.ft.)
Residential	1:0.5	63.99	1,393,702.20
	1:2	114.70	9,992,664.00
	1:3	38.50	5,031,180.00
	1:4	49.15	8,563,896.00
	1:5	27.69	6,030,882.00
	1:6	18.26	4,772,433.60
	1:7	3.37	1,027,580.40
	1:8	11.76	4,098,124.80
	Total	331.92	40,910,463.00

Note:  $GFA = Plot Ratio \times Acreage \times 43,560 (1 acre = 43,560 sq. ft.)$ 



# c. Projecting Needs for Community Facilities and Infrastructure

The Residential development in the City Centre will require the provisions of community facilities and infrastructure to support its population. Thus for this purpose, the conversion factor for converting floor area to population is necessary.

Factors influencing conversion factor are:

- i. Plot Ratio as per assigned for residential zoning and mixed use zoning at the city centre.
- ii. Floor Area Requirement (FAR) generated by allowable plot ratio that had already been assigned and the areas for each zoning.
- iii. Residential Unit Size by Cost average size of residential units by cost; high cost, medium cost and low cost.
- iv. Household size the average household size of 4.0 persons

## POPULATION EQUIVALENT FOR RESIDENTIAL IN CITY CENTRE

# **Example:**

Zoning : Residential in City Centre Commercial

Land Acreage : 5.5 acres

Plot Ratio : 1:5

Floor Area Requirement : 5.5 acres X 5 (Plot Ratio) x 43560 = 1,197,900 sq.ft.

Residential Type

Residential Cost : Medium Cost

Residential Unit Size : 950 sq.ft.

Population Equivalent

Total FAR/Residential Unit Size X Household Size

1,197,900 sq.ft./950 x 4 = 5,044 persons

For the purpose of this conversion, the residential units by cost shall be as follows:-

Low Cost = min. 700 sq.ft

Low Medium Cost = min. 800 sq.ft.

Medium Cost = min. 950 sq.ft

Medium High Cost = min. 1000 sq.ft.

High Cost = min. 1500 sq.ft



## **GROSS FLOOR AREA**

The FAR for Kuala Lumpur refers to the total Gross Floor Area (GFA) of a development. All covered floor areas of a building, except otherwise exempted and uncovered areas for commercial uses are deemed as the Gross Floor Area of the building. Gross floor area is measured on the overall dimensions of the building or part of the building on each floor below and above ground and includes internal and external walls.

# It includes:

- (a) Stairs and lifts shafts.
- (b) Corridors and covered passages by which there is no public right of
- (c) Conference rooms, board rooms, director rooms, assembly rooms and libraries.
- (d) Computer rooms.
- (e) Changing rooms and lavatories.
- (f) Canteens, restaurants, cafeterias, kitchens, restrooms, recreation rooms,
- (g) Basements with activity generating uses.

### It excludes:

- (a) Basement car and cycle parking accommodation.
- (b) Car parking above ground (no car parking allowed at ground level to allow for continuous street level activities).
- (c) Space for commercial vehicles awaiting loading or unloading.
- (d) Roof top garden.
- (e) Pedestrian linkage to Transit Stations (direct subterranean/underground pedestrian linkage).

Kuala Lumpur City Hall reserves the right to decide on GFA matters based on the specific design of a development proposal on a case-by-case basis.





## **Plinth Area**

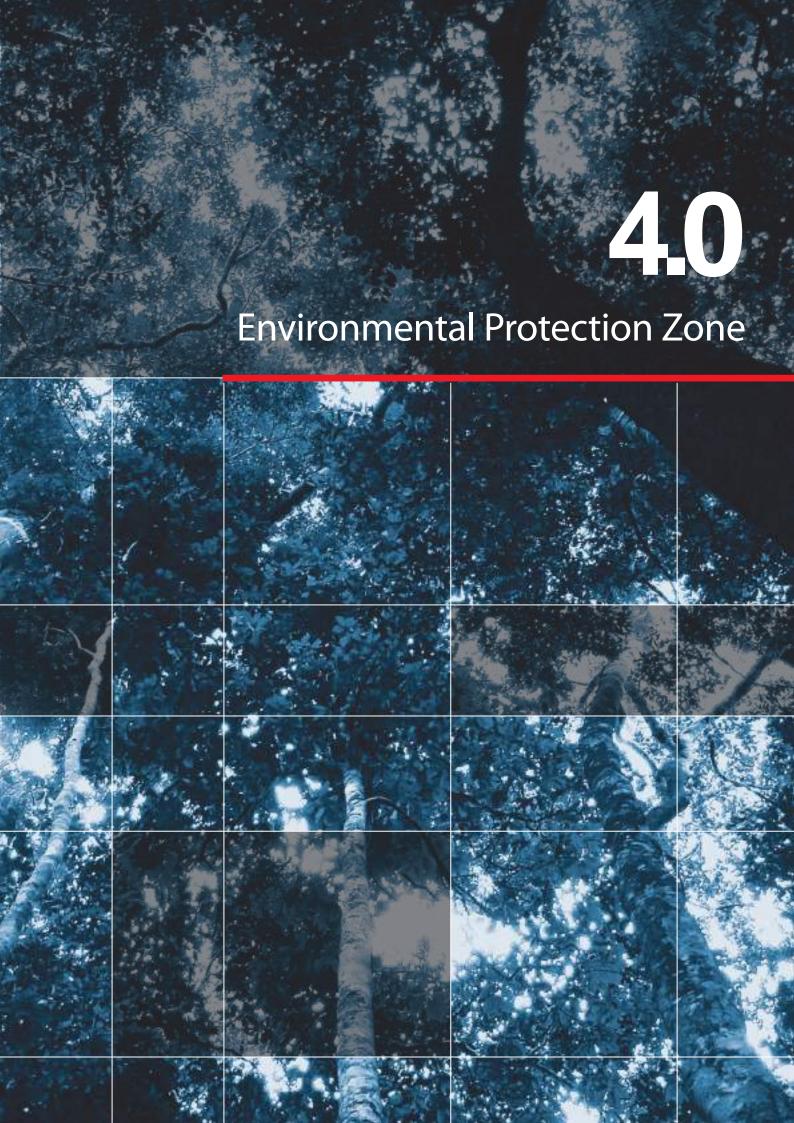
Determining the plinth area of urban development will further contribute towards the urban design objectives of an attractive built environment for the City of Kuala Lumpur. Plinth area controls allows more spaces for public realms and gree areas and helps reduce the large blocks of develment that will create barriers to pedestrian as well as a wall-like street frontage.

Plinth area is defined as the portion of a plot of land that is covered by a building or structure having a solid roof. It includes balconies or other projections from the building but does not include eaves up to a maximum of 600mm in width. It is measured as a proportion of building area over land area.



Zoning	Maximum Recommended Plinth Area
Commercial	70%
Residential	60%
Industrial	80%
Mixed Uses	70%





#### 4.1 Introduction

Environmental Protection Zone (EPZ) refers to an area where additional control and guide with regards to protecting the environment is imposed and specified on development and use of land. The EPZ is an additional layer of development control plan over the zoning control (DCP 1) that needs to be adhered to by any land identified to be within the environmental protection zone.

Areas identified as EPZ are highly sensitive to development or changes in land use and needs to be conserved for their limited biodiversity value, to be maintained and enhanced for their life support and recreational function and/or to be protected to mitigate the hazard risks to the surrounding areas. Developments within the EPZ are to be carried out sensitively in accordance to the controls and guidelines specified in this section.

This EPZ control is imposed onto all lands located in the following areas:-

- (a) Sungai Penchala
- (b) University Malaya
- (c) Bukit Gasing
- (d) Sungai Besi Army Camp
- (e) Bukit Nanas
- (f) Bukit Persekutuan
- (g) Bukit Dinding, Setiawangsa
- (h) Bukit Wangsa, Wangsa Maju
- (i) Dusun Ranjau
- (j) Areas along major rivers as identified in this guideline

#### 4.2 Objective

Key objectives of the EPZ are:

- i. To protect limited natural assets such as forest reserves, rivers, ponds, and residual forested areas from encroachment of developments into the areas.
- ii. To provide protection for sensitive areas and areas surrounding it from hazard risks such as erosion and landslide.

## 4.3 EPZ Categories

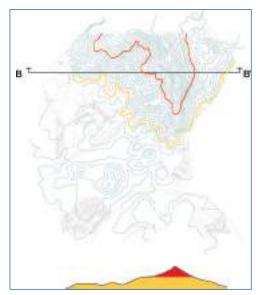
EPZ are categorized into four categories as follows:-

- Residual Forested Area
- ii. Hill Land
- iii. Hillside
- iv. Riverside Corridor

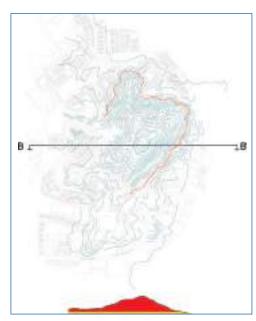




Cross section of Bukit Arang



Cross section of Bukit Batu Tabor



Cross section of Bukit Dinding

Categories of Environmental Protection Zone		
Description		
Existing forested areas bordering forest reserves in Kuala Lumpur. The residual forested areas serve as buffer area protecting the forest reserves from encroachment of urban developments on land located adjoining the forest reserves.		
Forest reserve in Kuala Lumpur consists of:- (a) Bukit Nanas Forest Reserve (b) Bukit Sungai Besi Forest Reserve (c) Bukit Sungai Air Puteh Forest Reserve		
Refers to areas located between the top of the identified hills in Kuala Lumpur to specified elevation for the respective hills as follows:  (a) Bukit Kiara  • From elevation of 200m to the top of the hill.  (b) Bukit Arang  • From elevation of 100m to the top of the hill.  (c) Bukit Gasing  • From elevation of 100m to the top of the hill.		

- (d) Bukit Sg.Besi
  - From elevation of 200m to the top of the hill.
- (e) Bukit Dinding
  - From elevation of 200m to the top of the hill.
- (f) Bukit Wangsa Maju
  - From elevation of 150m to the top of the hill.
- (g) Part of Bukit Batu Tabor (Dusun Ranjau)
  - From elevation of 200m to the top of the hill.

Protection and conservation of the hills are important for Kuala Lumpur in order to achieve the following objectives:-

- i. To protect the hill land area from disruptions cause by the uncontrolled urban development.
- ii. To preserve the natural environment, its ecosystem and biodiversity of the hill land.
- iii. To maintain the aesthetic and scenic qualities of Kuala Lumpur hill land areas.

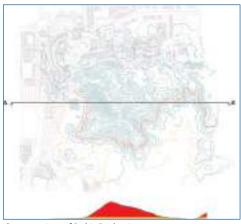
EPZ	Description
Hillside	Refers to areas located within the region specified for the respective
Tillisiac	hills below:
	(a) Bukit Kiara
	From the foothill to the elevation of about 200m
	(b) Bukit Lanjan
	From the foothill to top of the hill
	(c) Bukit Arang
	<ul> <li>From the foothill to the elevation of about 100m</li> </ul>
	(d) Bukit Gasing
	<ul> <li>From foothill to the elevation of 100m</li> </ul>
	(e) Bukit Sg.Besi
	<ul> <li>From foothill to the elevation of 200m</li> </ul>
	(f) Bukit Dinding
	From foothill to the elevation of 200m
	(g) Bukit Wangsa Maju
	• From foothill to the elevation of 150m
	(h) Bukit Batu Tabor (Dusun Ranjau)
	From the foothill to the elevation of about 200m
	<ul><li>(i) Bukit Persekutuan</li><li>From the foothill to the top of the hill</li></ul>
	• From the foothill to the top of the filli
	The purpose of the hillside areas are to ensure that developments
	in the hillside areas occurs in such a manner as to:
	i. Minimise the potential for geologic failures and resultant
	hazards to life and property,
	ii. Conserve the natural features of the site such as topography,
	natural drainage, vegetation and other physical features,
	iii. Minimise vegetation removal in slope areas,
	iv. Maintain the natural character of the hillside,
	v. Ensure that development does not dominate, but rather
	visually blends and achieves harmony between the natural and built environment.
	and built environment.
Riverside	Land located within 50m from the following major river reserves
Corridor	in Kuala Lumpur:
	(a) Sungai Klang
	(b) Sungai Gombak
	(c) Sungai Jinjang
	(d) Sungai Batu
	(e) Sungai Bunus
	(f) Sungai Penchala
	(g) Sungai Kayu Ara
	(h) Sungai Kerayong
	(i) Sungai Kuyoh
	Designation of areas along both side of the above rivers as an
	EPZ is to ensure that:
	i. The amenity value of rivers can be enhanced for enjoyment
	of the general public,
	ii. Greater awareness on importance of rivers in the city,
	iii. Water quality of major rivers can, in the long term, be improved



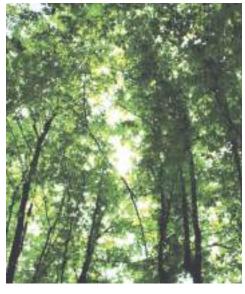
Cross section of Bukit Gasing



Cross section of Bukit Kiara & Bukit Lanjan

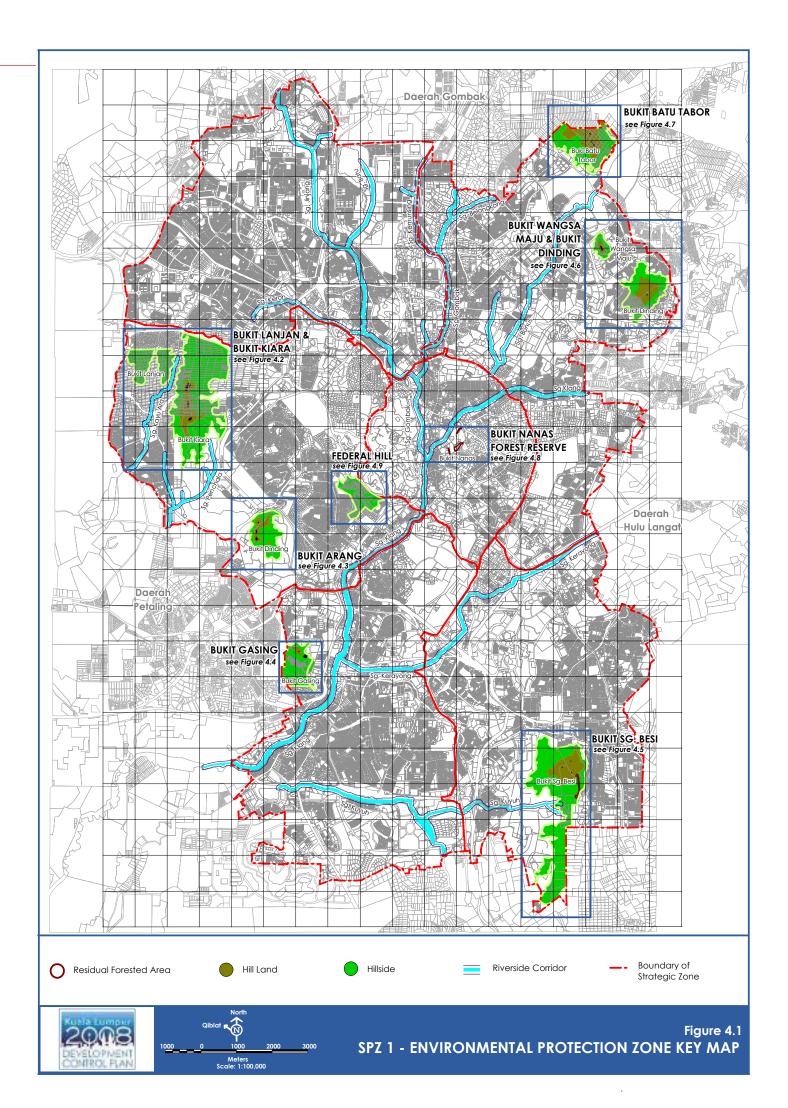


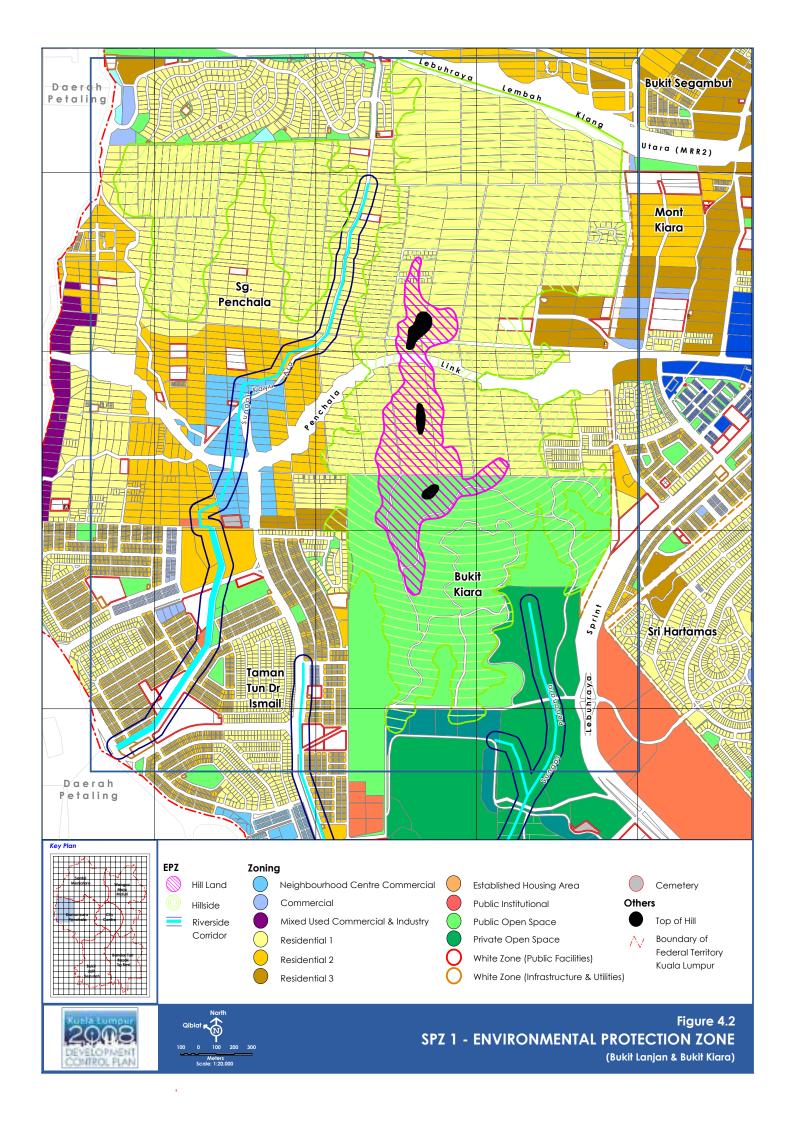
Cross section of Bukit Sg. besi

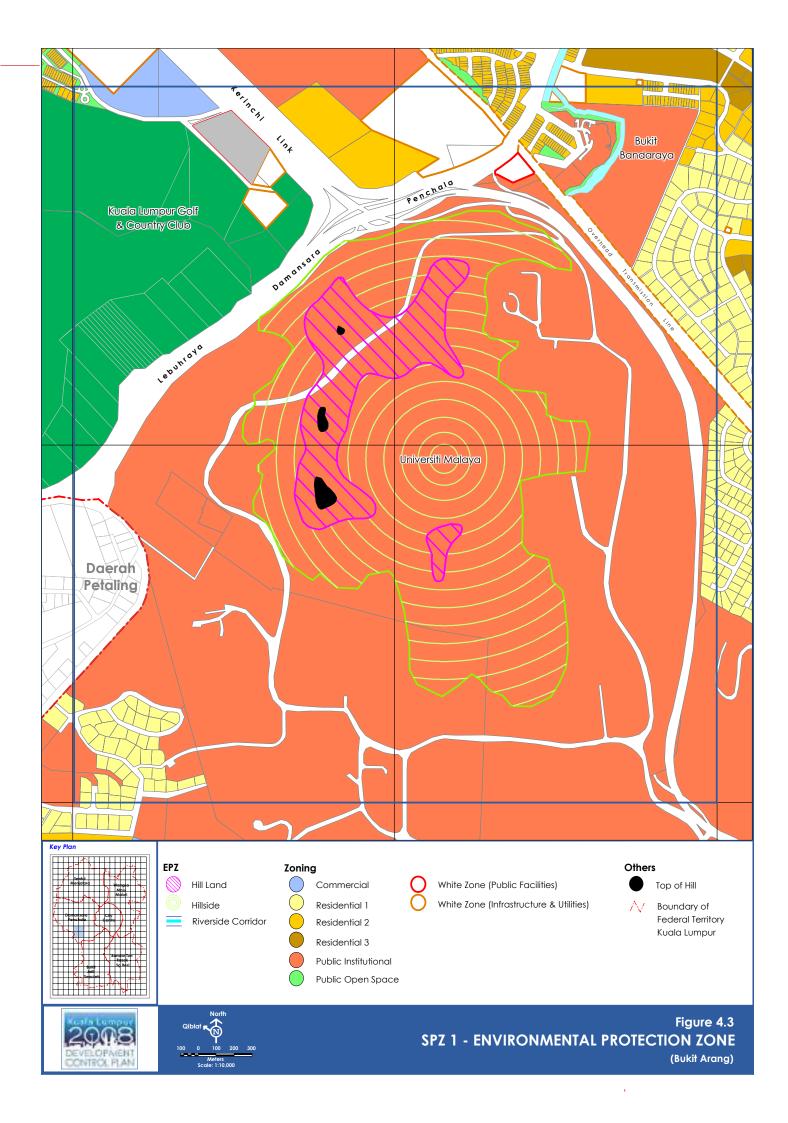


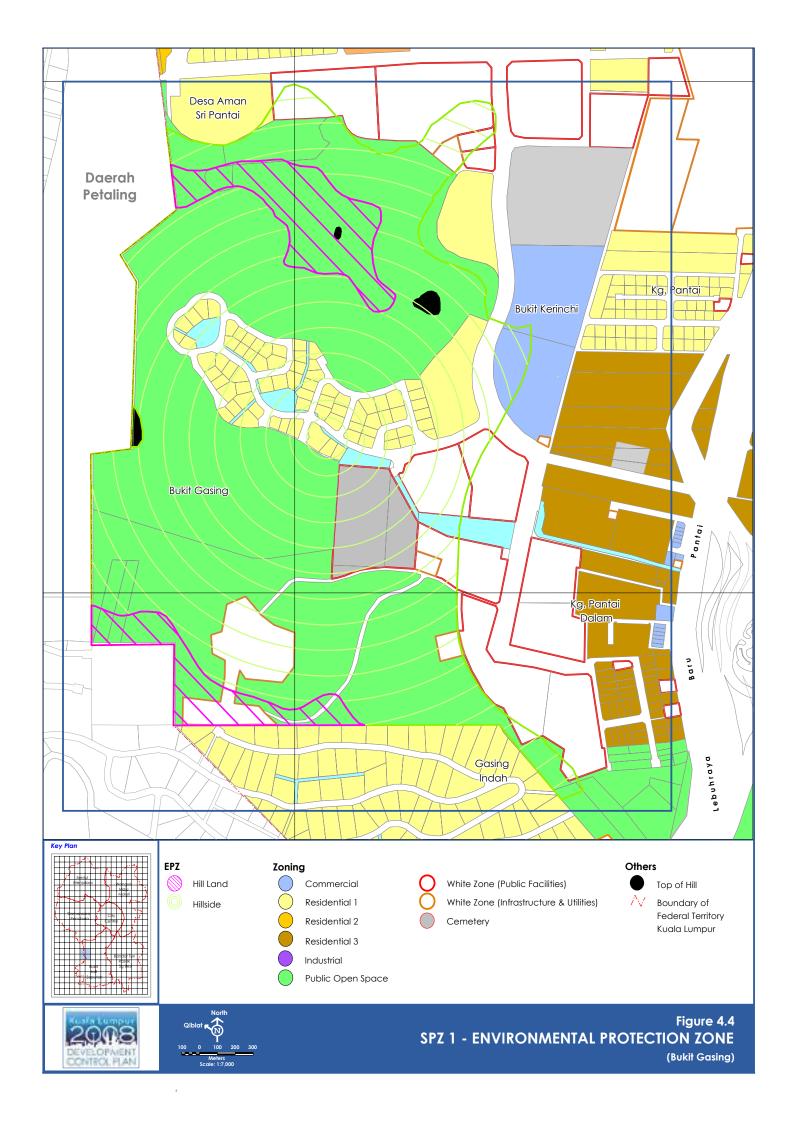
rivers.

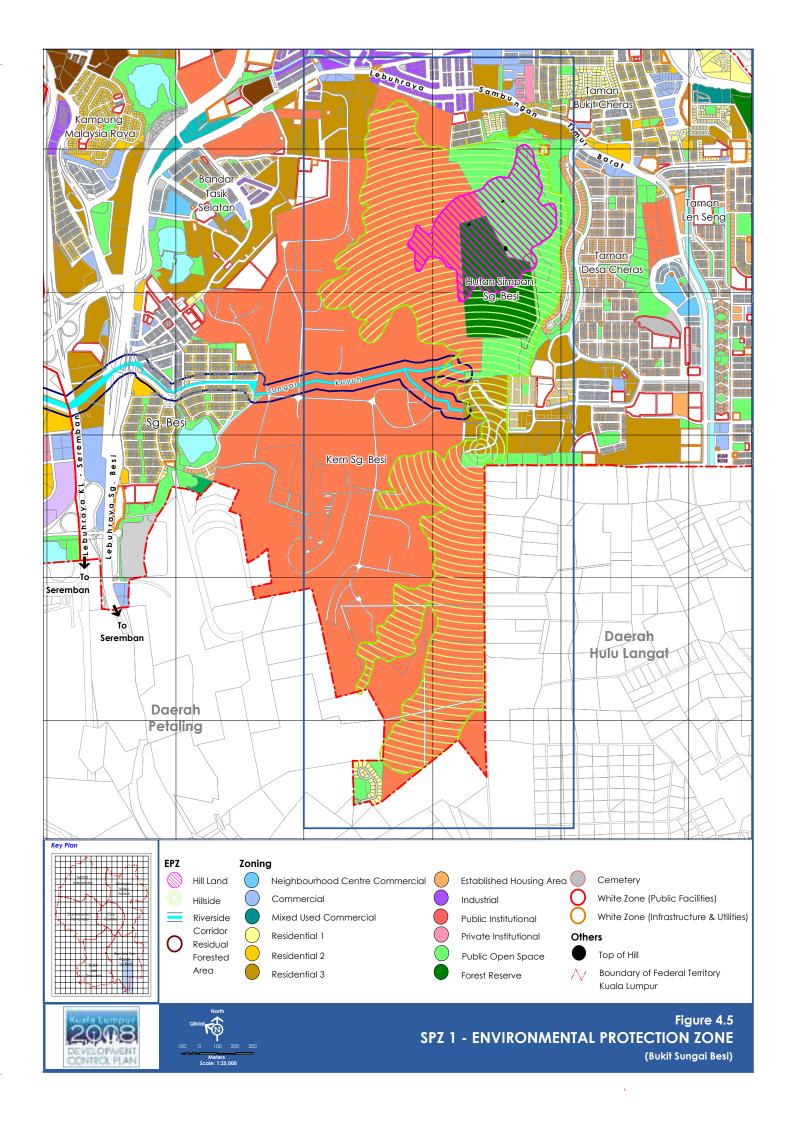
by eliminating potential polluting activities directly abutting

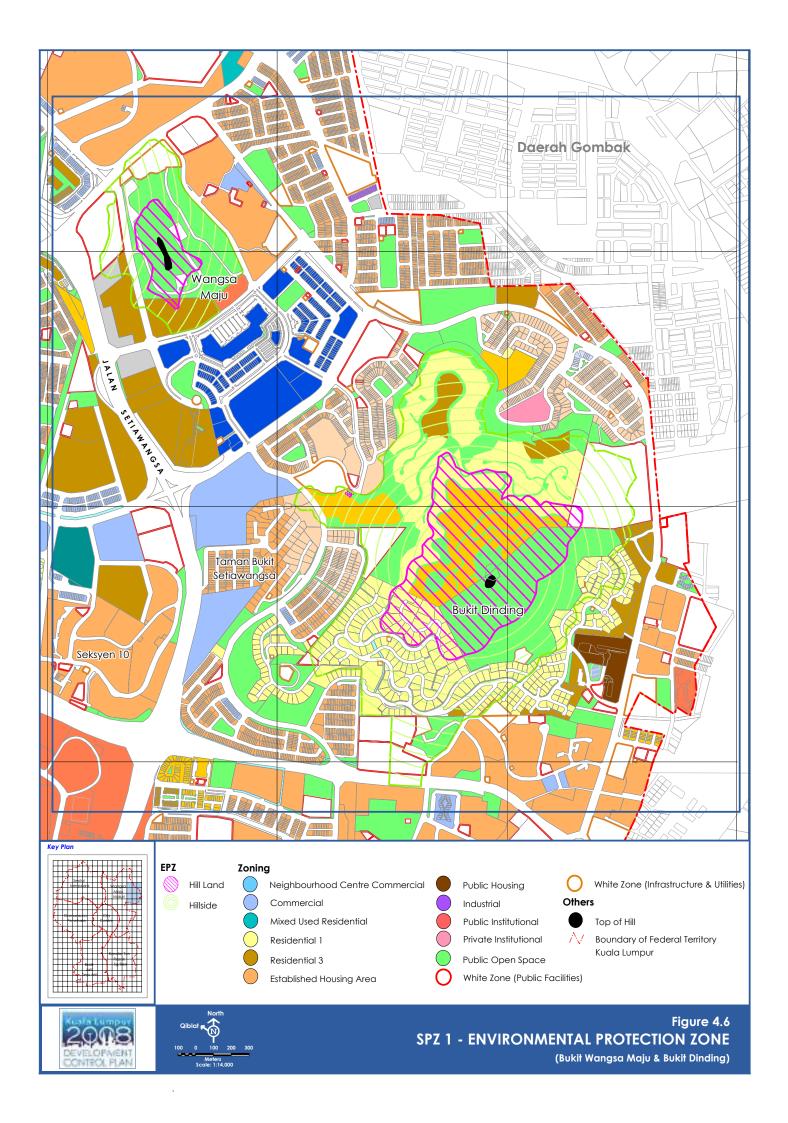


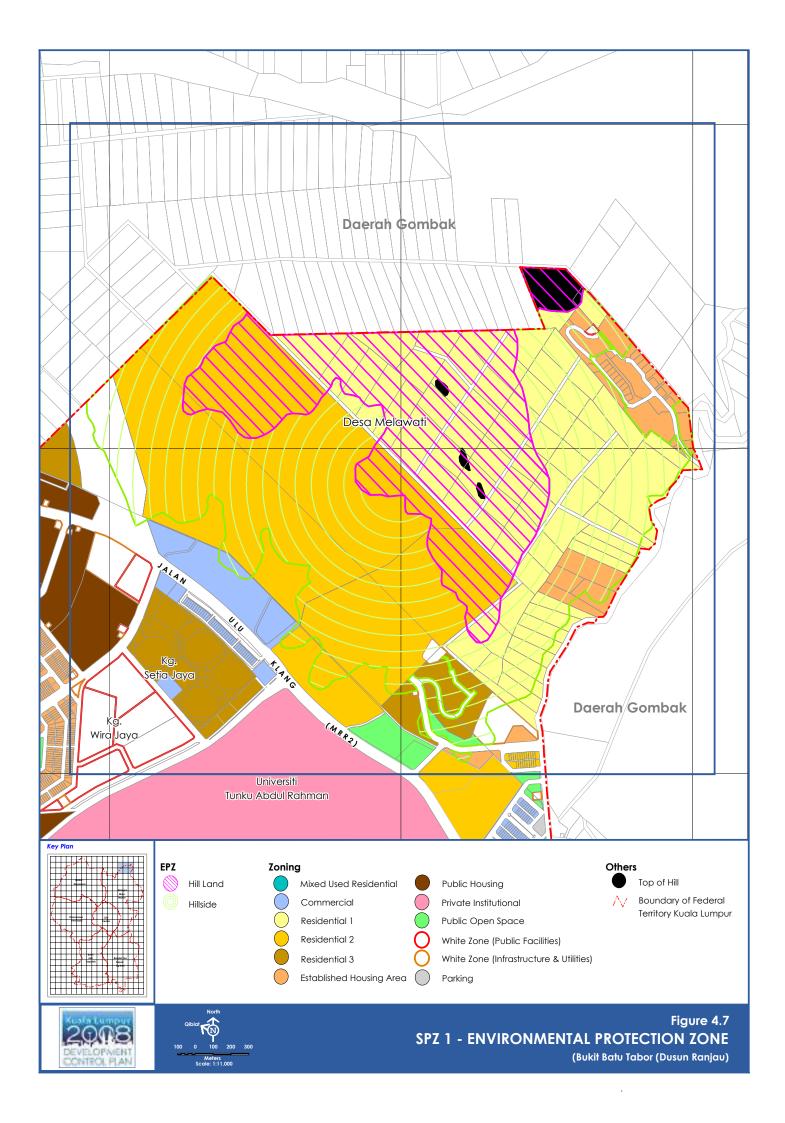


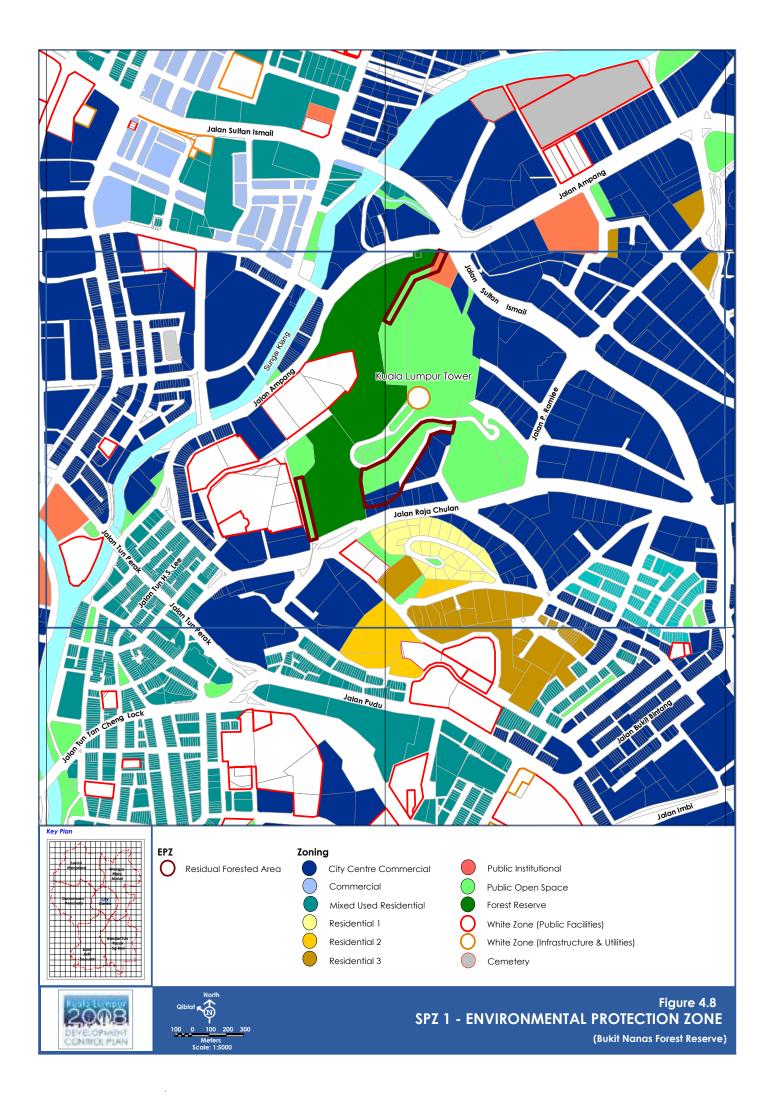


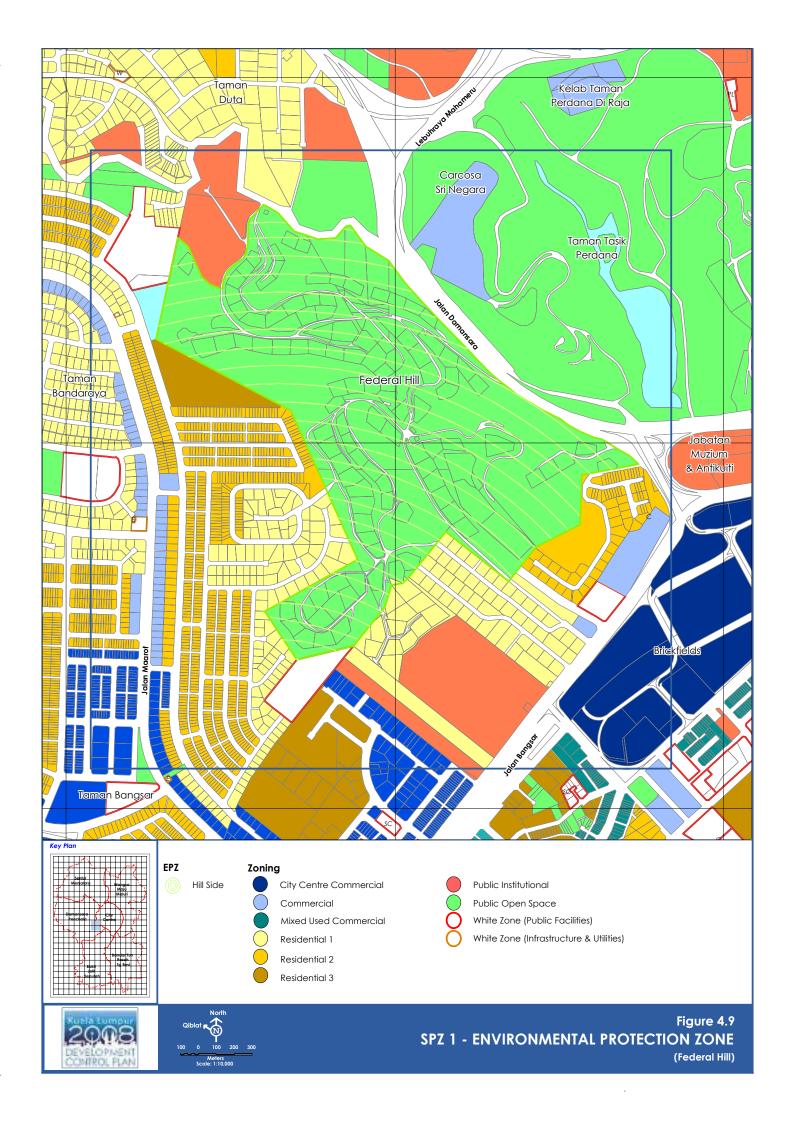












## **Activities Allowable in Environmental Protection Zone**

Planning and development within the EPZ requires special care and attention. The types of activities allowed in this zone are limited to passive and low impact activities. Properties surrounding these areas should also take extra care and be more sensitive towards the significance of the EPZ. Opportunities to create natural green areas, parks and recreational areas can add value to surrounding land area and thus EPZ should be viewed as prime land and not land constraints for development. EPZ adds to the natural built environment of the city and this would contribute towards the quality of life of Kuala Lumpur.

Table 4.2 - EPZ Allowable Activities

EPZ	Development or activity allowed in EPZ	Development or activity not allowed in EPZ
Residual Forested Area	<ul> <li>Low impact recreational activities         (Trekking, nature walks, camping,         mountain biking, picnics)</li> <li>Eco-tourism         [Refer EPZ guideline on Residual Forested Area         in Section 4.5]</li> </ul>	Urban development (residential, commercial, industrial)
Hill Land	Ecotourism     Low impact recreational activities     (Trekking, nature walks, camping,     mountain biking, picnics) (Trekking,     nature walks, educational tours)  [Refer EPZ guideline on Hill Land in Section 4.6]	Urban development (residential, commercial, industrial)
Hillside	Urban development (residential, commercial, institutional, educational, industrial) (subject to compliance to - Zoning, Intensity, Other technical [Refer EPZ guideline on Hillside in Section 4.7]	
Riverside Corridor	<ul> <li>Within river reserve:</li> <li>Low impact recreational activities (fishing, jogging, biking, picnics)</li> <li>River maintenance and improvement works</li> <li>River landscaping and riparian buffer</li> <li>Within 50m from river reserve</li> <li>Urban development (commercial, residential) subject to compliance to: <ul> <li>Zoning Control,</li> <li>Intensity Control,</li> <li>Other technical requirements,</li> </ul> </li> <li>[Refer EPZ guideline on Riverside Corridor in Section 4.8]</li> </ul>	<ul> <li>Urban development (residential, commercial, industrial)</li> <li>Permanent building structures</li> <li>Industrial</li> <li>Other polluting activities</li> </ul>



# **Guidelines Relating to Residual Forested Area**

## 4.5.1 Application

- This guideline applies to residual forested areas surrounding Bukit Nanas and Bukit Sungai Besi which are still not developed and still in their natural conditions.
- For Bukit Sungai Puteh, the natural character of the adjoining land has been lost by development occurring right to the of the forest reserve; hence this guideline does not apply to adjoining land of the Bukit Sungai Puteh Forest Reserve.

### 4.5.2 Demarcation of Residual Forested Area

20m from boundaries of forest reserves.

### 4.5.3 Allowable Activities

### Activities Allowable

- Low impact recreational activities such as trekking, nature walks, camping, picnic areas and other low impact recreational facilities
- Eco-tourism related to forest reserve activities.

### Activities Not Allowable

- Urban development (residential, commercial, industrial).
- Mass land clearing and grading.

# 4.5.4 Trees and Vegetation

- Removal of existing trees and vegetation cover permitted only for trails, camping site, picnic areas and for light and small building structures.
- Existing trees with diameter of 0.8m and other important native forest species (regardless of size) must be identified and inventoried on all plans. They are not to be removed or damaged.

# **Guidelines Relating to Hill Land**

#### 4.6.1 **Application**

This guideline applies to all land located within hill land zone of the identified hills in Kuala Lumpur as indicated in Table 4.1

### 4.6.2 Allowable Activities

Environmental Protection Zone	Development or activity allowed in EPZ	Development or activity not allowed in EPZ
Hill Land	<ul> <li>Ecotourism</li> <li>Low impact recreational activities (Trekking, nature walks, camping, mountain biking, picnics)</li> <li>Structured allowed are only related to provision of related facilities for eco tourism sush as look out tower, shelters, facility centres such as rest areas, rest room and small kiosk.</li> <li>Any access ways must be limited to service roads that lead to any form of facility centrefor the eco tourism area. Such road must be designed and aligned to follow the natural terrain and with minimum earthworks.</li> </ul>	Urban development (residential, commercial, industrial)

#### **Ecotourism Development within Hill Land** 4.6.3

- Ecotourism development within Hill Land must be carried out with the following sustainable development principles:
  - a. Strive to improve the environment of a site through appropriate design and integration of all elements of a development,
  - b. Focus on minimal site disturbance with a requirement for design to reflect and not alter the natural existing landscape,
  - Reduce the footprints of development components to the minimum required for development to proceed,
  - d. Integration of waste minimization and energy efficiency within the design and the operation of development.

#### **Additional Submittal Requirements** 4.6.4

- Apart from normal documents required for submission of planning approval, development proposals on both Hill Land and Hillside Area must accompanied by the followings:
  - a. Geotechnical and Soil Investigation Report
  - b. Approved Environmental Impact Assessment Report
  - Layout plan, building footprint and road layout to be superimposed on Terrain Analysis Map.
  - d. Proposed earthwork plan clearly showing all areas to be cut, filled or cleared for:
    - Roads and sidewalks
    - ii) Utilities
    - iii) Building envelopes or footprints
    - iv) Driveways and all other accessways
    - Other improvements





# **Guidelines Relating to Hillside**

### 4.7.1 Application

This guideline applies to all land located within hillside zone of the identified hills in Kuala Lumpur as indicated in Table 4.1 ahove

### 4.7.2 Allowable Activities

In accordance to allowable land use zoning as indicated in Land Use Zoning Map (DCP 1) and Zoning Schedules subject to compliance to development intensity requirements as specified in either the Density Map (DCP 2) and/or Plot Ratio Map (DCP 3).

## 4.7.3 Areas with Slopes Greater Than 35° in Hillside

Development is not allowed on areas exhibiting slopes of 350 or greater (before development).

# 4.7.4 Areas with Slopes 25° to 35° in Hillside

For areas with slope 25° - 35°, development can be permitted subject to soil stability and geologic conditions, natural and man-made drainage and other technical requirements of the Jawatankuasa Penyelarasan Pembangunan Kawasan Sensitif Alam Sekitar and EIA requirements.

### 4.7.5 Additional Submittal Requirements

- Apart from normal documents required for submission of planning approval, development proposals on both Hill Land and Hillside must be accompanied by the followings:
  - a. Geotechnical and Soil Investigation Report
  - b. Approved Environmental Impact Assessment Report
  - c. Layout plan, building footprint and road layout to be superimposed on Terrain Analysis Map.
  - d. Proposed earthwork plan clearly showing all areas to be cut, filled or cleared for:
    - Roads and sidewalks i)
    - Utilities ii)
    - iii) Building envelopes or footprints
    - Driveways and all other accessways
    - Other improvements
  - e. Submitted for approval by the Jawatankuasa Penyelarasan Pembangunan Kawasan Sensitif Alam Sekitar at the Ministry of the Federal Territory.

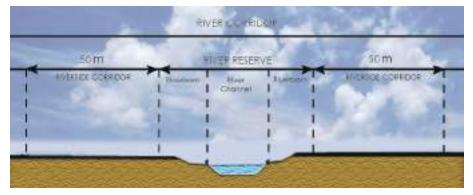
## 4.7.6 General Guidelines

- Development in Hillside must observe the following general guide:
  - a) To preserve the existing character of areas with mature
  - To keep the cutting of existing mature trees to a minimum, and ensure adequate replacement of trees that are removed.
  - c) To keep the cutting of the existing natural topography to a minimum
  - d) To maintain the low density detached residential character of the area
  - e) To maintain existing road widths and road network
  - f) To control building heights such that they do not exceed the existing tree heights.

# **Guidelines Relating to Riverside Corridor**

## 4.8.1 Application

This guideline applies to all private and public lands located within 50m or the major river reserves specified in Table 4.1 above.

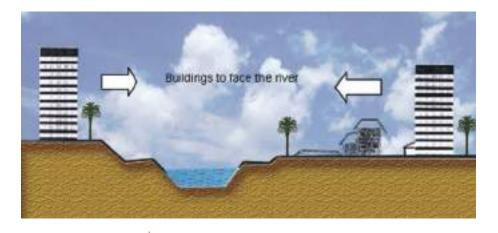


### 4.8.2 Activities Within Riverside Corridor

- In accordance to allowable land use zoning as indicated in Land Use Zoning Map (DCP 1) and Zoning Schedules subject to compliance to development intensity requirements as specified in either the Density Map (DCP 2) and/or Plot Ratio Map (DCP 3). However, industrial and other polluting activities are not permitted within the riverside corridor although may generally permitted in the Zoning Schedules.
- Direct discharge of polluting effluents and sewerage into the river is not permitted. Discharge from development or activities within the riverside corridor must go back into sewerage system.
- Garbage collection areas and service loading entrances will not be permitted to front the river and within the setback areas specified below in Section 4.8.4.

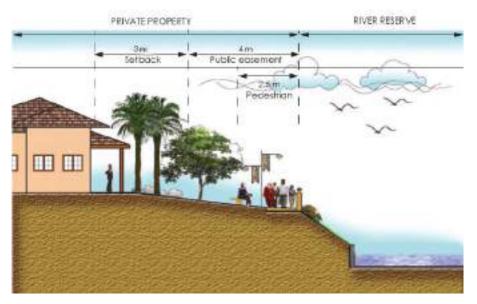
# 4.8.3 Orientation of Buildings Within Riverside Corridor

Buildings on riverfront sites will have their primary facades facing the river, and will orient their required open space to the waterfront.



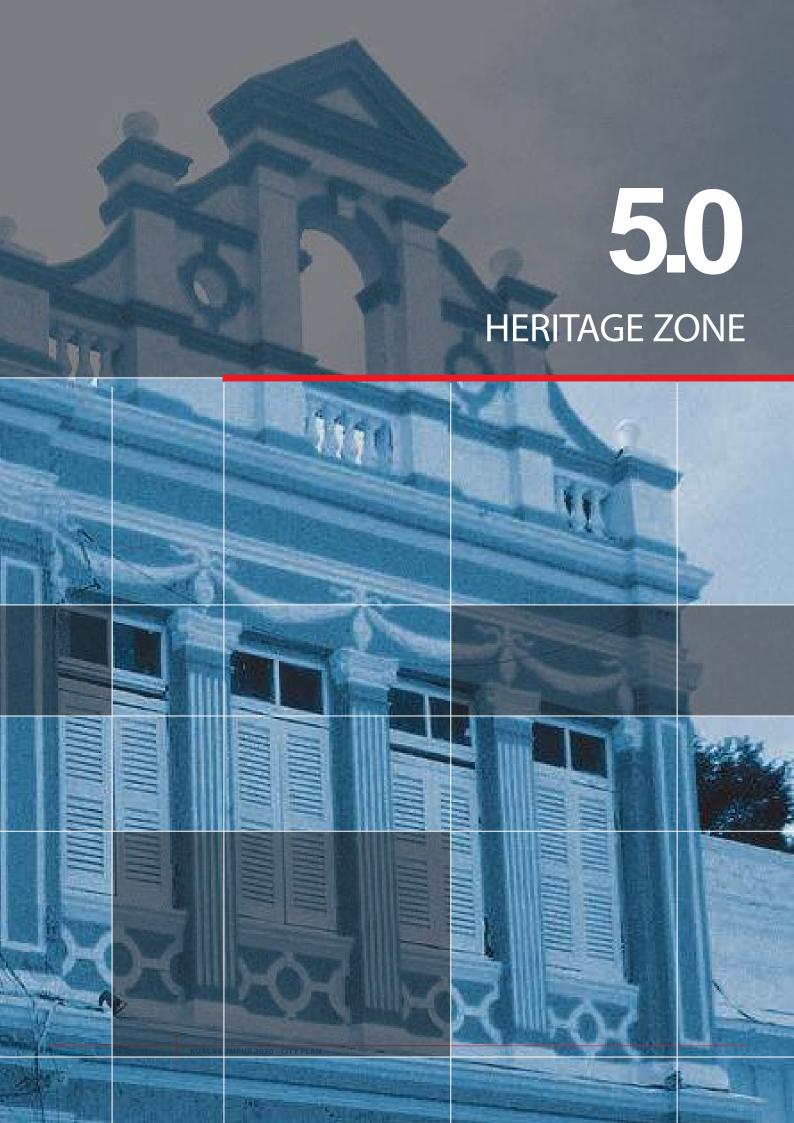
# 4.8.4 Building Setbacks

- All buildings within the riverside corridor must provide minimum building setback of 7m from the river reserve.
- Within the setback area, 4m from the river reserve must be kept clear of any buildings or permanent structures. At least 2.5m within the 4m public easement is to be allocated for pedestrian footpath or cycling track that is interconnected to the overall green spine of the river.
- Outdoor alfresco activities can be allowed within the public easement subject to approval by KLCH and compliance to guidelines stated in Section 4.8.2 above.



### 4.8.5 Street and Public Access

· Streets that connect the riverfront will be designed to be pedestrian scale with a high level of amenity.



# 5.1 Application

Heritage Zone is identified by the Draft Kuala Lumpur City Plan 2020 as an area of special architectural or historic interest and character of which it is desirable to preserve or conserve. The purpose of designating such a zone is to ensure that the areas within it maintain the essential historic character. or ambience and the development within the area is undertaken with care and sympathy to form, character and scale of existing heritage sites and buildings.



# 5.2 Objectives

The objectives of this zone are: -

- i. To preserve and conserve buildings or sites of historical, architectural and social significance and to ensure that they area not adversely affected by any new development;
- ii. To conserve and enhance the character, scale and quality of urban landscape of Kuala Lumpur; and
- iii. To provide guidelines for of building owners, planners, architects and other concerned professionals, developers and general public with regard to the demolition, protection, conservation or rebuilding of, and alterations and additions to existing buildings that have been designated to be conserved or which fall within designated heritage areas.

# 5.3 Designation of Heritage Zone and Heritage Buildings in Kuala Lumpur

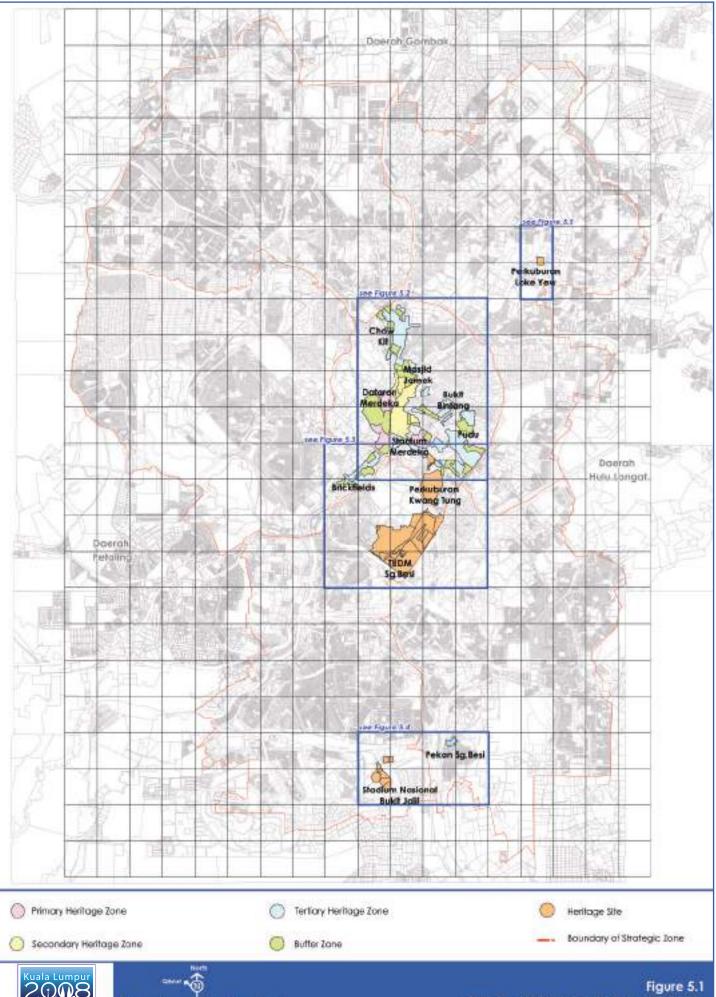
The KLSP 2020 has designated a Heritage Zone in the City Centre which covers most of the older part of the City Centre. This has been refined and expanded to include other areas as follows:-

- i. Other shophouse areas in the City Centre which have a distinctive character:
- ii. Parts of the shophouse areas in Brickfields;
- iii. Parts of the shophouse areas in Sungai Besi town;

Apart from heritage zones, buildings and sites which are significant to the city are also indentified and to be conserved (see Attachment 1 for full list):-

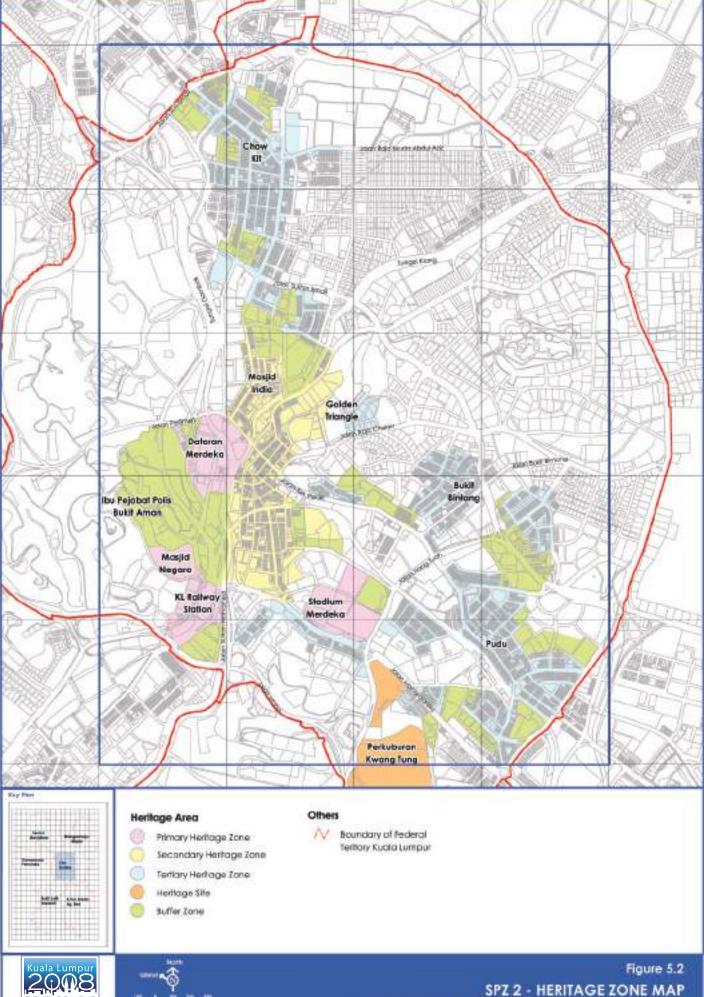
- i. Site of the Sungai Besi Airport Base;
- ii. Bukit Jalil Sports Complex site;
- iii. Kwang Tung and Loke Yew Cemeteries;
- iv. Buildings within the PULAPOL site;
- v. Pustaka Peringatan P.Ramlee in Taman P.Ramlee, Setapak; and
- vi. Masjid Jami'ul Ehsan at Jalan Pahang.















# **Buildings in Primary Heritage Zones**



Bangunan Sultan Abdul Samad



Old KL Railway Station



Stadium Negara



Masjid Jamek

# **5.4 Categories of Heritage Zones**

Heritage zones are categorized into 5 categories:-

- (a) Primary Heritage zone
  - A core area for heritage which is contiguous and contains groups of buildings gazetted under the National Heritage Act.
- (b) Secondary Heritage Zone Area that is less contiguous and contains a mixture of newer and older buildings with significant historicmerit.
- (c) Tertiary Heritage Zone More recently developed shophouse areas that has buildings of little or no historic significance.
- (d) Buffer Zone Areas that adjoin or connect heritage areas to others and has no intrinsic existing character which requires conservation.
- (e) Heritage Site

## 5.4.1 Primary Heritage Zone

These are relatively contiguous zones containing groups of buildings gazetted under the Antiquities Act, now covered by the National Heritage Act and include the historic precincts around Merdeka Square, the Old KL Railway Station and Complex.

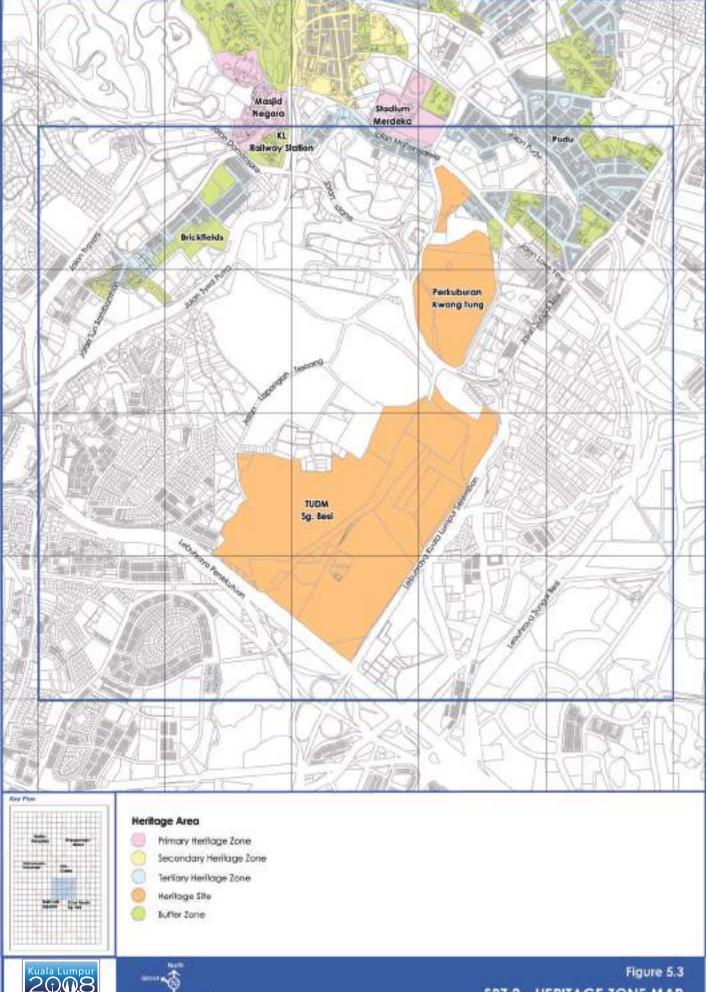
Development in these areas will be strictly controlled so as to maintain their existing ambience and to ensure that any alterations or additions to non gazetted buildings within the areas or new developments do not adversely affect the appearance of the existing buildings or public open spaces.

Due to the unique character of these areas, no specific set of guidelines are provided. All new development or proposals for alterations and additions to existing buildings will be referred to a Design Review Panel.

# 5.4.2 Secondary Heritage Zone

These are less contiguous areas containing a mixture of newer and older buildings some of which are of significant historic or architectural merit. These areas cover most of the original historic shophouse areas of the old city and also include the school and cathedral precinct around St. John's Cathedral.

Alterations and additions to designated heritage buildings as well new developments in Secondary Heritage Zone which are not shophouse areas will be subject to review by the Design Review Panel.







# **Buildings in Secondary Heritage Zones**



Medan Pasar



Jalan Tun H.S.Lee



Jalan Tuanku Abdul Rahman



St. John's Cathedral

# 5.4.3 Tertiary Heritage Zone

These are in the more recently developed shophouse areas such as PWTC/ Chow Kit, Pudu Brickfields and Sungai Besi that mainly contain buildings of little or no historic significance. Requirements in these areas will be the least stringent and aimed primarily at maintaining the shophouse ambience, scale and urban continuity.

Alterations and additions to designated heritage buildings as well as new developments in Tertiary Heritage Zone which are not shophouse areas will be subject to review by the Design Review Panel.

#### 5.4.4 Buffer Zone

Buffer zones are those which have no inherent existing character which requires heritage but which adjoin or connect other heritage areas. Their function is to maintain a degree of urban continuity between or around heritage areas and to avoid the sudden juxtaposition of development that is out of scale. Buffer Zones may be in the form of low intensity or predominantly green areas adjoining Primary Heritage Zones where development should maintain the existing character.

Alternatively, Buffer Zones may be redevelopment areas between shophouse heritage areas where urban continuity in the form of pedestrian linkages, covered walkways and active frontages should be maintained.

# 5.5 Heritage Buildings Categories

Buildings identified to be conserved in heritage area are categorised into 4 categories as follows:-

- (a) Category 1 Heritage Buildings
- (b) Category 2 Heritage Buildings
- (c) Category 3 Heritage Buildings
- (d) Category 4 Heritage Buildings

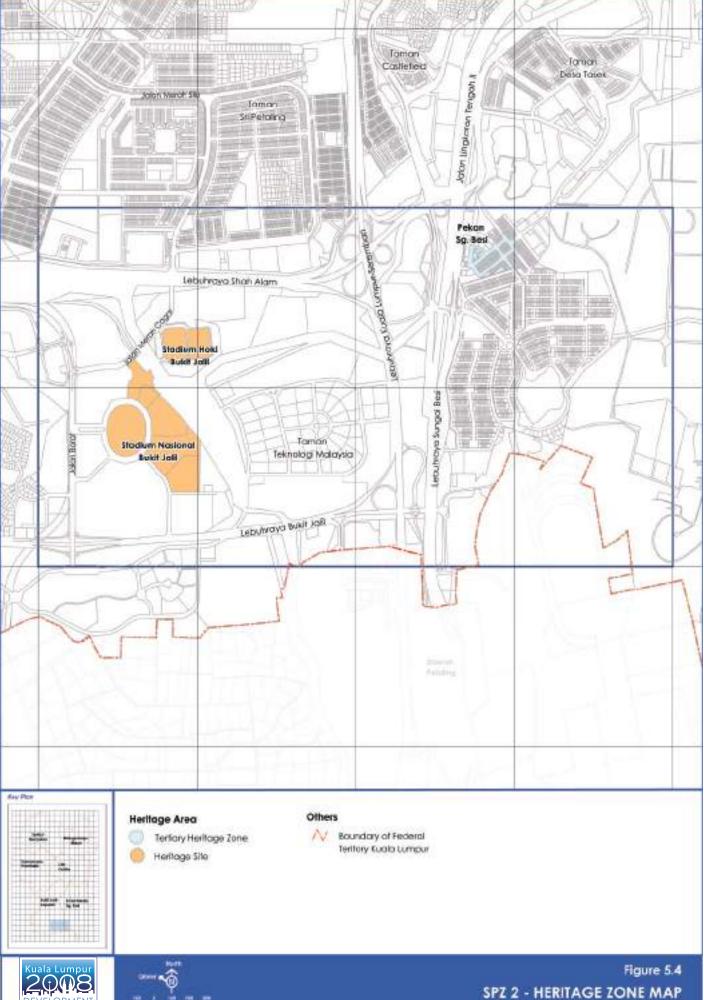
# 5.5.1 Category 1 Heritage Buildings

#### a) General

These are buildings previously gazetted under the Antiquities Act and now covered by the National Heritage Act 2005 and fall under the jurisdiction of the Commissioner for Heritage (Refer to Table 1 in Attachment 1).

### b) Heritage Schedule

Category 1 Heritage Buildings may acquire their designation either as a consequence of their architectural qualities, or their historical or cultural significance. Certain aspects or elements of these buildings may be of greater significance than others requiring preservation and/ or restoration while other aspects may be less critical. In view of the wide range of considerations for buildings in this category, there are no fixed guidelines. Instead each building or group of buildings that is identified will be accompanied by a schedule outlining which are the







# **Buildings in Tertiary Heritage Zones**



Jalan Ipoh



Jalan Tuanku Abdul Rahman



Lorong Merbau



Jalan Brunei Barat

characteristics or elements that are to be preserved or conserved and which aspects may enjoy some latitude.

# c) Design Review Panel

Notwithstanding the schedule accompanying each Category 1 listed building, any application for alterations and additions to these buildings will be assessed by a Design Review Panel to ensure that the essential character and importance of the building or buildings is not compromised.

# 5.5.2 Category 2 Heritage Buildings

These include buildings of significant historical and/or architectural importance which have not previously been gazetted principally because they are or were not more than 100 years old and therefore fell outside the provisions of the Antiquities Act. However, it is likely that most if not all of these buildings will be gazetted in the future under the National Heritage Act.

Heritage guidelines for this category are the most stringent and decisions relating to major alterations and additions will be referred to the Design Review Panel. (Refer to Table 2 in Attachment 2).

## 5.5.3 Category 3 Heritage Buildings

These include other buildings which contain elements or characteristics of some historical or architectural significance which are recommended to be conserved (Refer to Table 3 in Attachment 2).

#### 5.5.4 **Category 4 Heritage Buildings**

These include other shophouse buildings which are of purely contextual value. (Refer to Table 4 in Attachment 2).

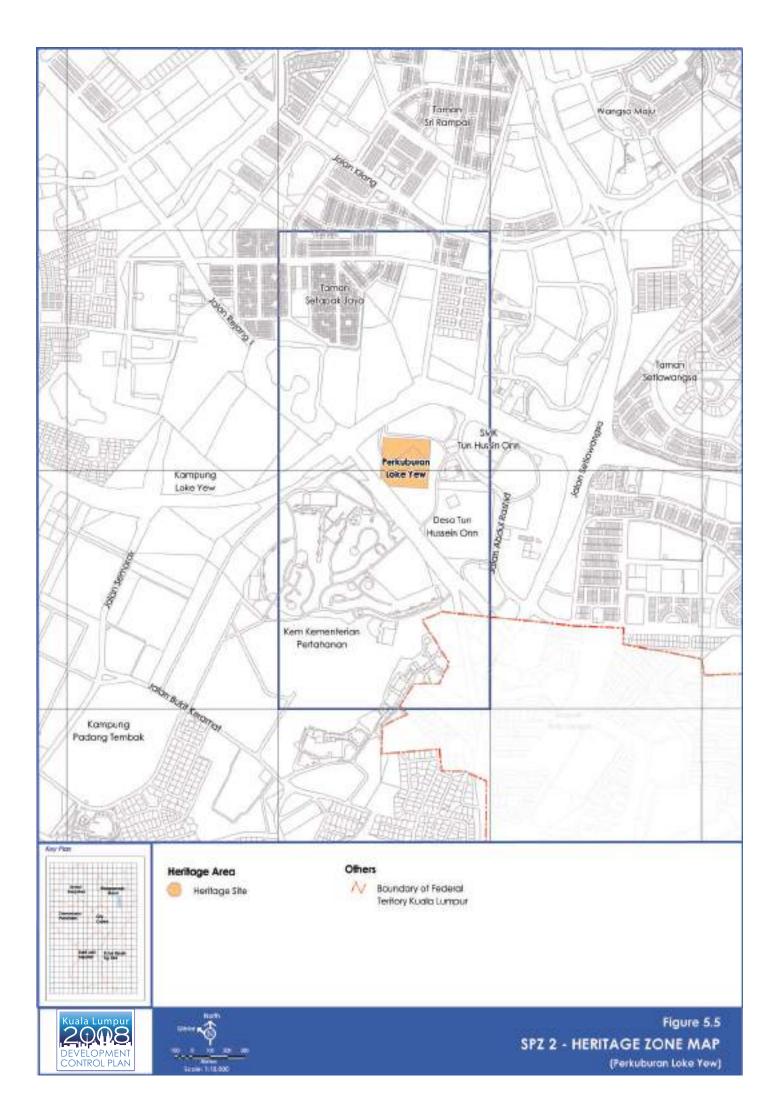
# 5.6 Other Buildings within Heritage Zones

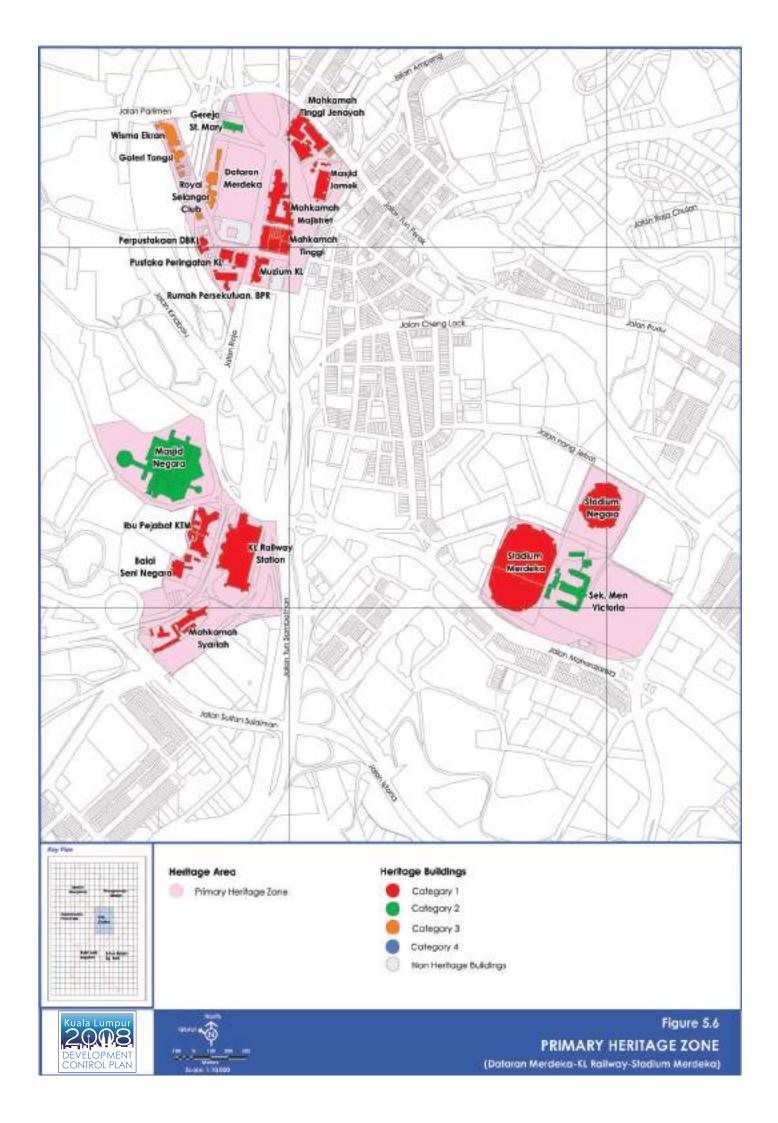
## 5.6.1 General

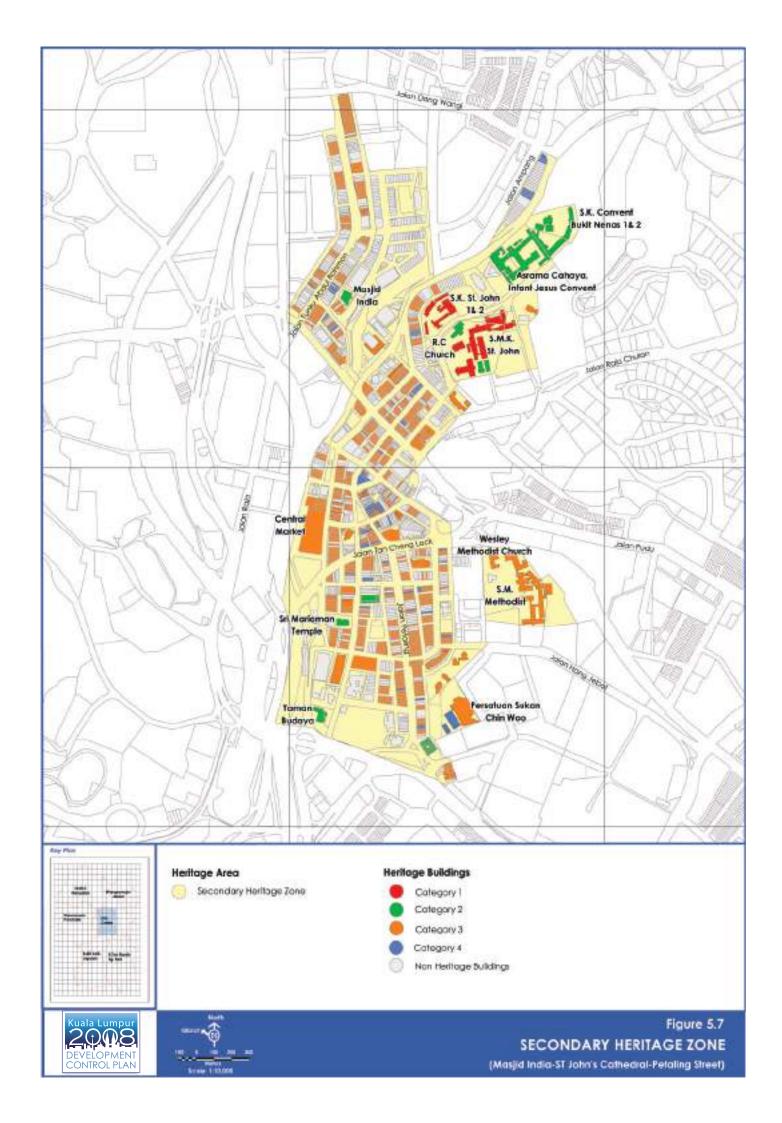
In addition to designated heritage buildings, heritage zones contain other buildings including existing buildings without any intrinsic heritage value and vacant sites. These buildings are also to a greater or lesser extent covered by the guidelines.

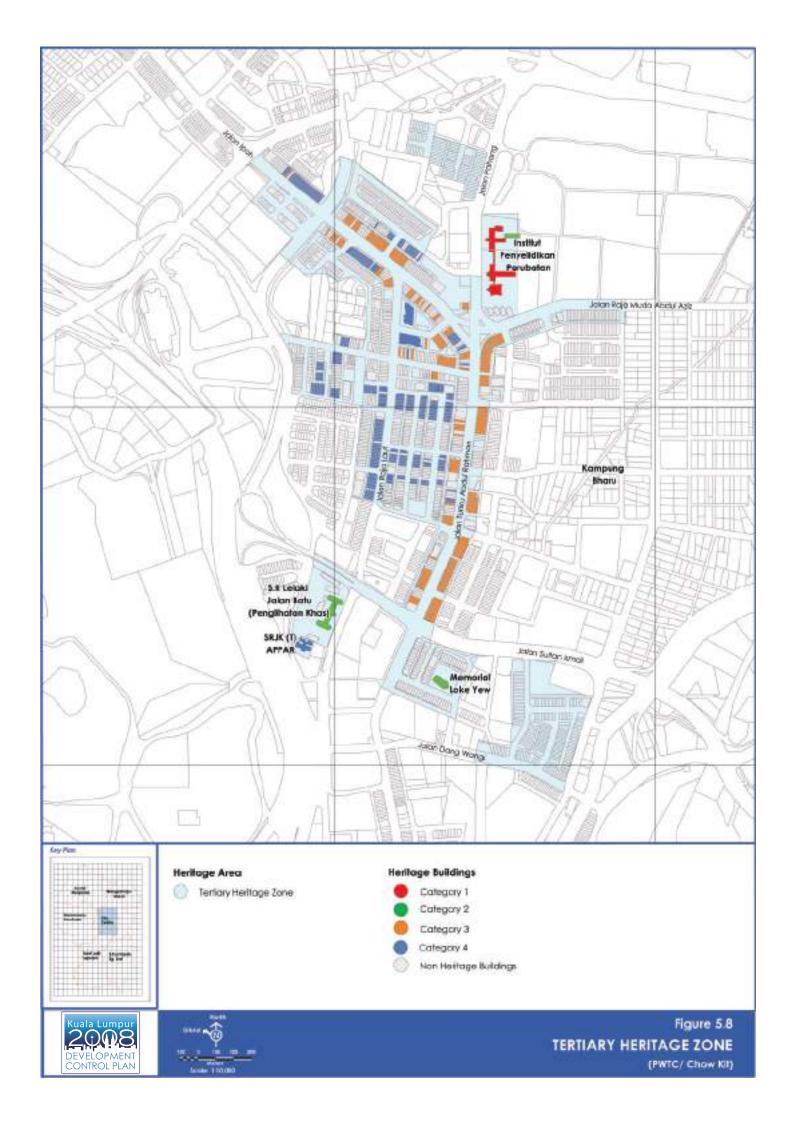
# 5.6.2 Compliant Existing Buildings

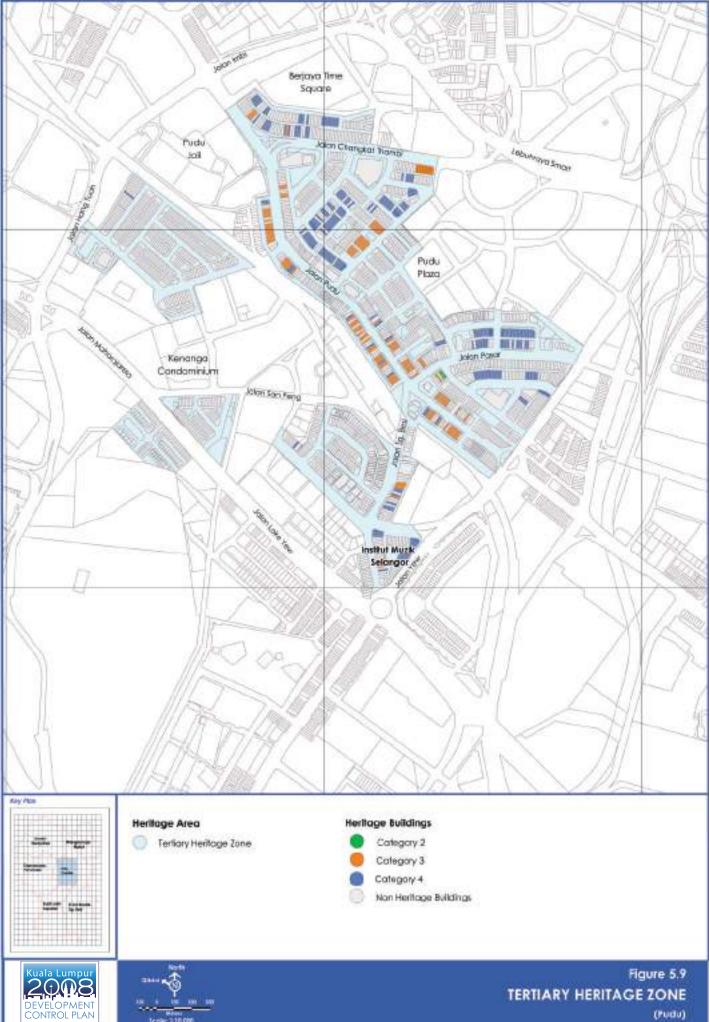
These are existing buildings which are not designated as buildings to be conserved but which comply with heritage area guidelines. Additions, alterations and rebuilding are permitted provided that the works comply with the guidelines and other pertinent development control requirements.





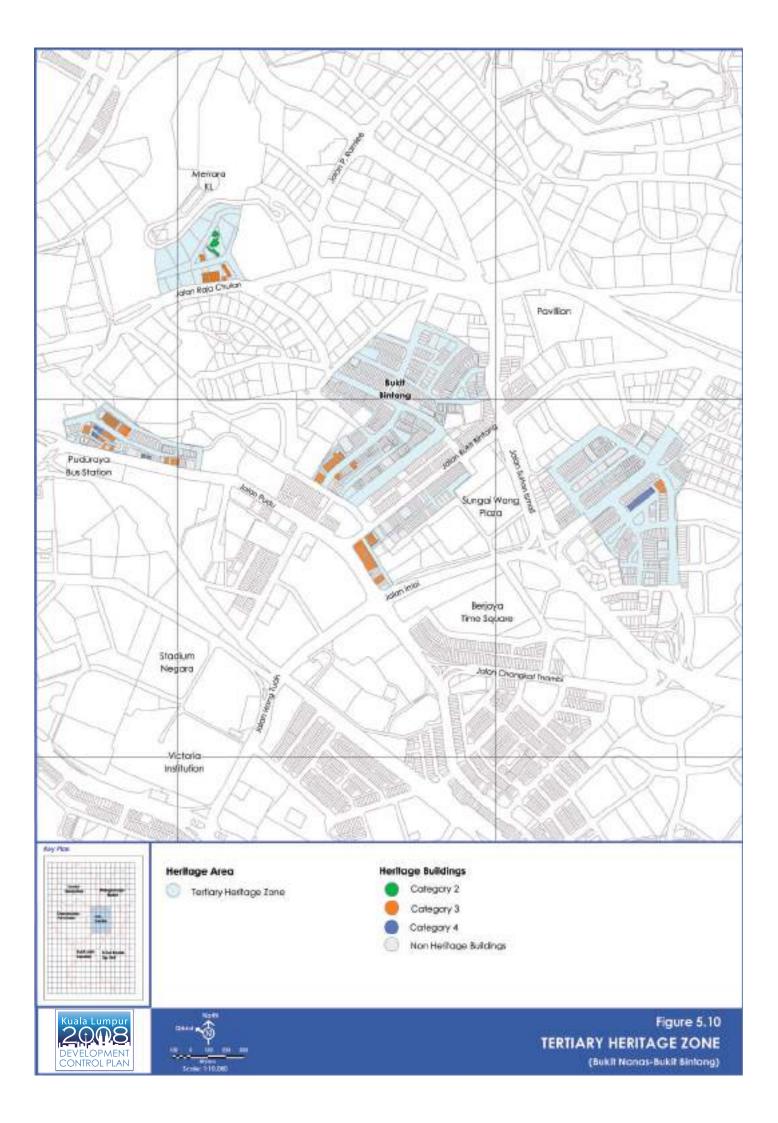


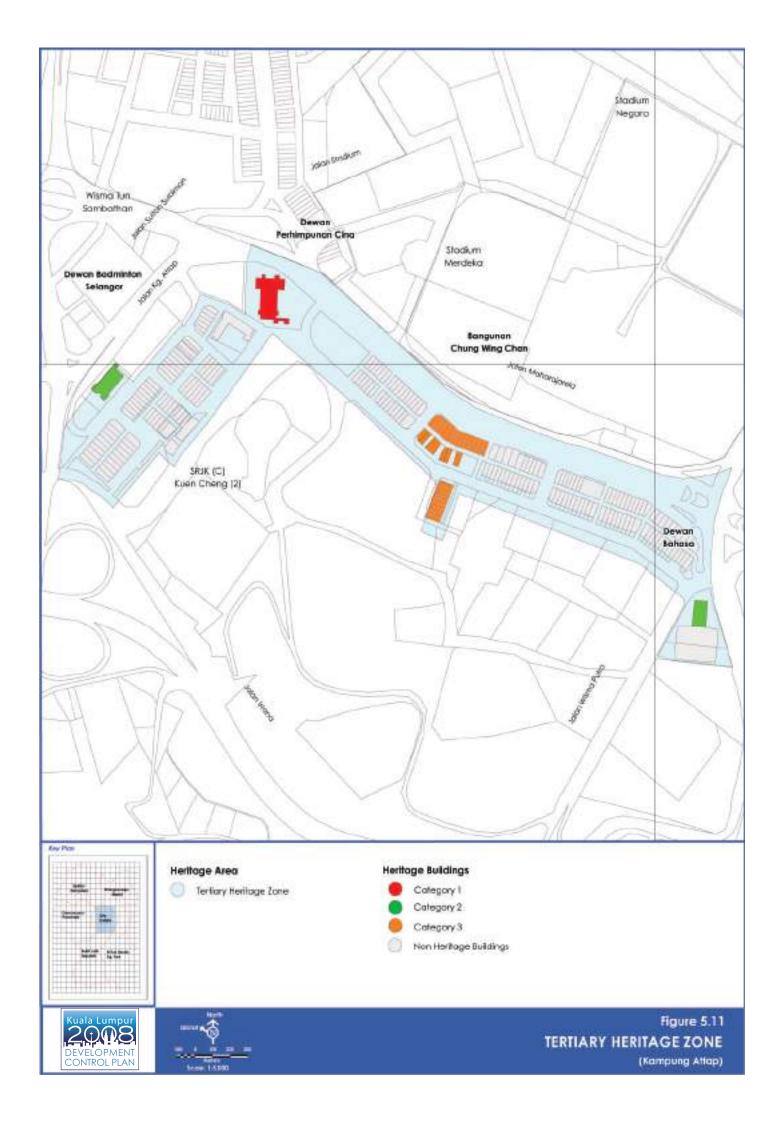


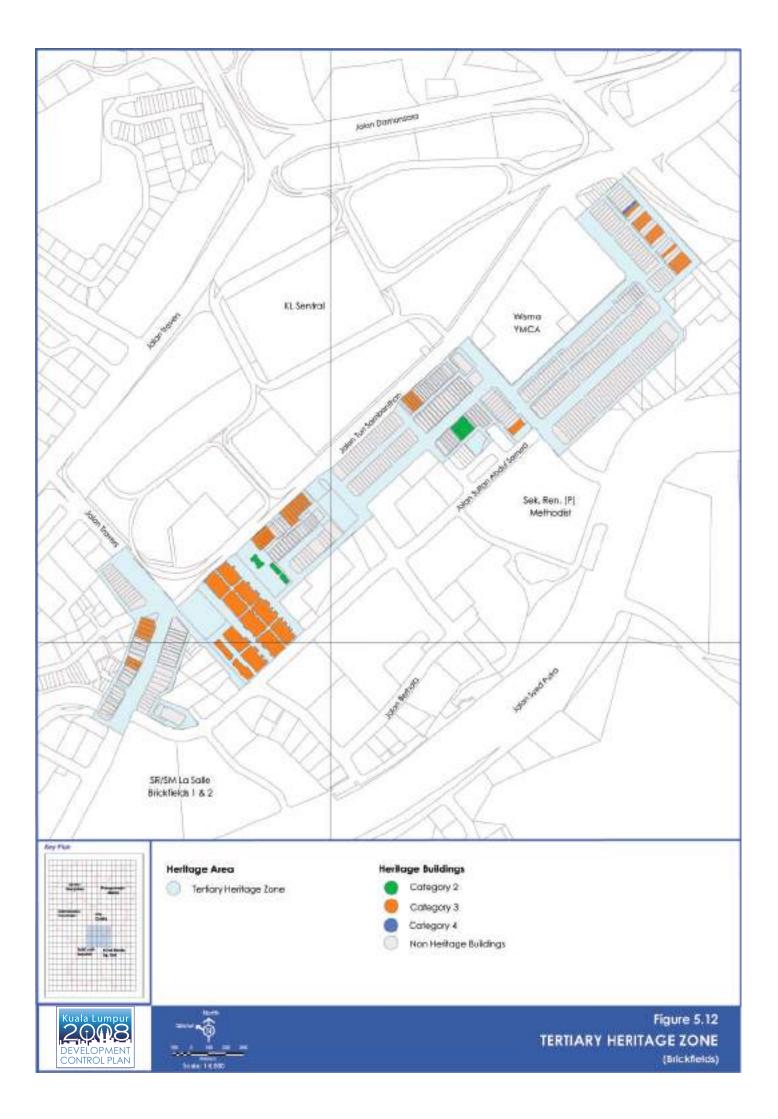














# 5.6.3 Non Compliant Existing Buildings

Non compliant existing buildings are those in Heritage Zones which have been redeveloped in such a way that they do not conform to the Heritage Guidelines. When a planning application for these buildings is made, the works must be such that the building will comply with the guidelines. Under certain exceptional circumstances KLCH may waive compliance with some of the heritage guidelines.

# 5.6.4 Infill Development

New buildings on vacant sites must comply with the guidelines and other development control requirements.

# 5.7 Guidelines Relating to Heritage Zones

## 5.7.1 General

In addition to any specific guidelines which apply to designated heritage buildings in the heritage zones, guidelines will apply to alterations or additions and infill developments of non designated heritage buildings. The guidelines cover envelope controls, building elements and materials, verandah ways, car and motorcycle parking and advertising and signage.

## **5.7.2 Envelope Controls**

These controls apply to the shophouse heritage areas and buffer zones. Envelope control relates to the three dimensional envelope within which development can take place as defined by the following factors:

- **Height Control**
- **Build to Line**
- Setbacks
- Parapet Line
- Nominal Sightline
- Main Roof

Figure 5.9 indicates the factors affecting envelope control in shophouse heritage areas.

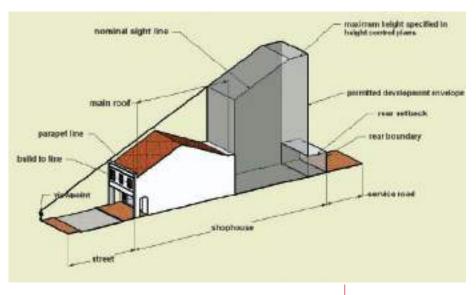


Figure 5.9 - Factors Affecting Envelope Controls in Shophouse Areas

# a. Height Control

Height Control is determined in the Building Control Plans and is expressed as the maximum permissible number of storeys above pavement level. In some areas where the overall height of buildings needs to be controlled for specific reasons such as to maintain views of important buildings, there are special height controls which are expressed in terms of AMSL. These controls determine the absolute overall height of any development.

#### **Build to Line** b.

New developments and alterations and additions to existing buildings must adhere to the build to lines which, for the lower floors of shophouses are the front boundary of the site and include, in the case of corner units, the side site boundary.

More detailed build to lines will be determined in streetblock plans which will take into account the need for public open space.

## **Roof Parapet Line**

The roof parapet line is the main height controlling line at the front (or side) build to lines and is determined by the existing parapet line in heritage buildings. New buildings or existing non heritage buildings will follow the parapet line of the adjacent heritage units or the predominant parapet line in the block.

In the case of a heritage building with an irregular parapet (such as a pediment) the parapet level is taken as the lower part of the parapet. Where there is no parapet, then the parapet of the adjoining or nearest heritage building is assumed.

## d. Main Roofs

In order to retain and enhance the roofscape in heritage areas, the existing main roofs of heritage buildings are to be maintained as per guidelines set out below, and new developments or alterations and additions to existing buildings should follow these guidelines.

A setback of 600mm from the main roof is applied before the other envelope parameters (nominal sight lines and overall height control) are applied. Secondary architectural features such as aesthetic articulation, sun shading devices, condenser unit shelves or other decorative elements may be located within the 600mm setback area.

# e. Nominal Sightlines

Nominal sightlines relate to the projected line between the designated viewing point of a hypothetical pedestrian on the street and the top of the shophouse roof parapet on the front elevation. No additions or developments may protrude above these nominal sightlines.

Figure 5.10 - Determination of Parapet Line



For roofs with irregular parapets the lower parapet line shall apply



Where there is no parapet the parapet of the nearest conservation building is assumed



The purpose of this control is to control the visibility of new development from the street so as to maintain the street scale and will be more stringent in heritage areas than in Tertiary Heritage Zone and Buffer Zones.

In so far as there are a number of variables and differing situations even with respect to adjacent shophouse properties, standards have been set which fix the controlling "nominal sightlines" depending on the Heritage Zone Category.

Nominal sightlines only apply to the front, i.e. main street elevations, and not to the side or corner elevations for corner lots where the side setbacks apply.

#### f. Setbacks

Setbacks apply to the rear boundary and the upper levels of corner units above the parapet line.

# **Envelope Controls for Secondary Heritage Zones**

Table 5.1 - Envelope Controls for Secondary Heritage Zones

ASPECT	CONTROL				
Build to Line	Front property line at main street and side street unless otherwise determined in detailed street block plans.				
Roof Parapet Line	To follow existing adjacent heritage building predominant parapet line in block or as determine in detailed street block plans.				
Main Roof	Main pitched roofs to be retained up to rear eaves line for heritage buildings.				
	Other buildings to follow predominant pitched roof profile up to rear eaves line to a minimum of 12m back from front build to line.				
Nominal Sightline	Viewpoint is set at 1500mm above pavement level at 11m from the front build to line* .Nominal sightline is projected from the viewpoint to the parapet line.				
Front Setbacks	600mm articulation zone behind main roof eaves (for storeys above parapet line Beyond this zone, envelope is determined by line and behind main roof) sight line up to maximum height permitted in Building Control Plans.				
Side Setback at side road	Parapet line maintained around corner. 3m setback for storeys above parapet line.				
Side setback at adjacent lot Rear Setback	None None at first storey. 3m from rear site boundary for upper storeys.				

<sup>\*</sup> Note: This is the notional distance from the face of the building to a viewing point on the opposite pavement assuming a 7m road and 2m pavements either side.

Figure 5.12 - Envelope Controls for Secondary Heritage Zones

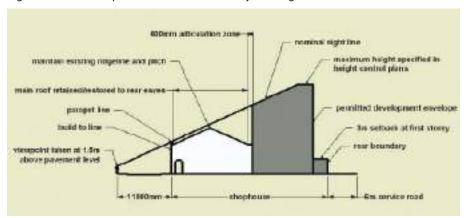


Figure 5.13 - Typical Applications of Envelope Controls for Secondary Heritage Zones

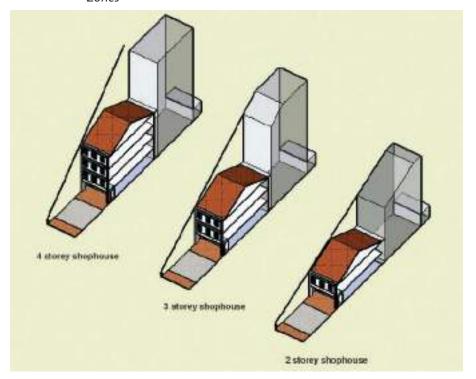
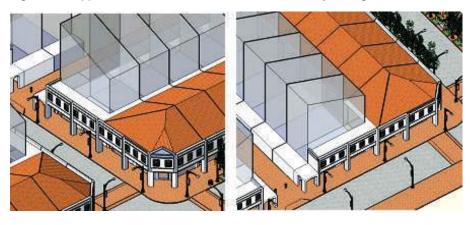


Figure 5.14 - Applications of Side and Rear Setbacks in Secondary Heritage Zones





# **Envelope Controls for Tertiary Heritage Zones**

Table 5.2 - Envelope Controls for Tertiary Heritage Zones

ASPECT	CONTROL			
Build to Line	Front property line at main street and side street and as determined in detailed street block plans.			
Parapet Line	To follow existing adjacent heritage buildings, or the predominant parapet line within the block whichever is the lower, or as determined in detailed street block plans.			
Main Roof	Main pitched roofs to be retained up to ridge line for heritage buildings.			
	Other buildings to follow predominant pitched roof profile up to ridge line to a minimum of 6m back from front build to line.			
Nominal Sightline	Viewpoint is set at 1500mm above pavement level at 5.5m from the front build to line. * Nominal sightline is projected from the view point to the parapet line.			
Front Setbacks	600mm articulation zone behind main roof ridge line. Beyond this zone, envelope is determined by nominal sight line taken as indicated up to maximum height permitted in Building Control Plans.			
Side Setback at side road	Parapet line maintained around corner. 3m setback for storeys above parapet line.			
Side setback at adjacent lot Rear Setback	None None at first and second storey. 3m from rear site boundary for upper storeys. Setbacks to allow for widening of existing service roads to 8m. Setbacks should be measured from the resultant new site boundary.			

Note: This is the notional distance from the face of the building to a viewing point in the middle of the road assuming a 7m road and 2m pavements either side.

Figure 5.15 - Envelope Controls for Tertiary Heritage Zone

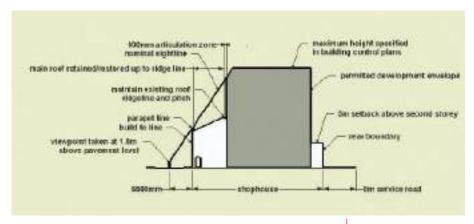


Figure 5.16 - Typical Applications of Envelope Controls for Tertiary Heritage Zone

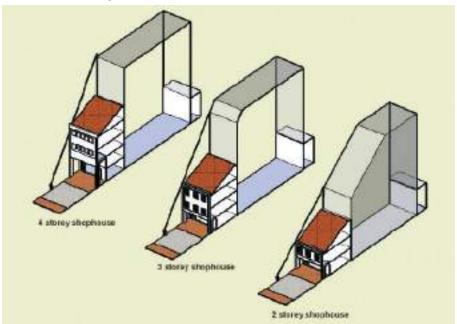
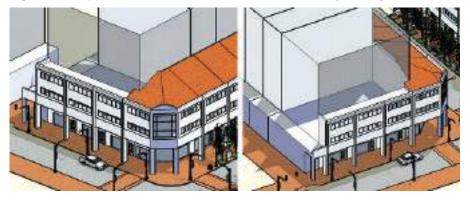


Figure 5.17 - Application of Side and Rear Setbacks in Tertiary Heritage Zone

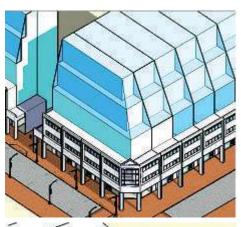


# **Envelope Controls for Buffer Zones**

Table 5.3 - Envelope Controls for Buffer Zones

ASPECT	CONTROL			
Build to Line	Front property line at main street and side street and as determined in detailed street block plans.			
Parapet Line	4 storeys or as determined in detailed street ble plans.			
Front Elevation Setbacks	3m setback every 6m increase in height.			
Side Setback at side road	3m setback at 5th storey or as determined in detailed street block plans.			
	Upper floors above 6th storey 1.5m setback every 2 storey increase in height.			
Side setback at adjacent lot	No setback for 6m back from front build to line or as determined in streetblock plans. 3m for remainder of side boundary.			
Rear Setback	3m from rear site boundary			

Figure 5.19 - Applications of Side and Rear Setbacks in Buffer Zones



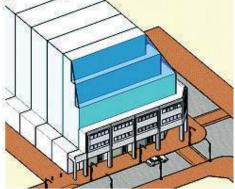
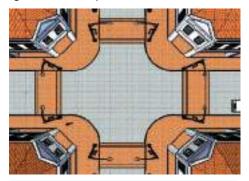


Figure 5.20 - Wrap Round Corner Treatment

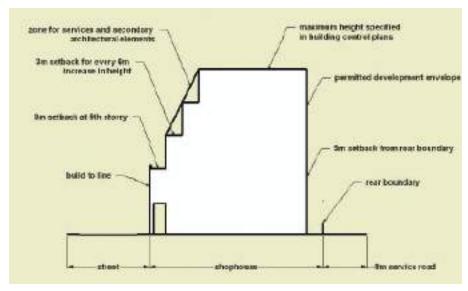


Wrap round corner treatment in Secondary Heritage Zones



Typical wrap round corner treatment in **Tertiary Conservation Zones** 

Figure 5.18 - Envelope Controls for Buffer Zones



#### **Corner Units** 5.7.3

# a. Wrap Round Corner Units

Corner units should address the corner in a manner that is consistent with traditional shophouse areas. The wrap round treatment with a chamfered frontage and some emphasis on the corner façade elements is preferred as it gives continuity to the façade as well as the verandah way, and creates more pavement space at road junctions. This approach is to be adopted in all heritage zones except in instances where there is an existing end of terrace heritage building or where particular circumstances such as a narrow plot width make such treatment impractical.

# b. Other Corner Units

In circumstances where a heritage unit which was previously a mid terrace unit but has subsequently become an end of terrace unit due to road widening or other reasons then, provided space permits, a covered walkway will be provided alongside the side wall and the building owner will be encouraged to provide active frontages along the side wall at ground level. Refer to Figure 5.21.

#### 5.7.4 **Facade Treatment**

#### a. General

Components of facade treatment include ground floor elevational treatment, upper level fenestration, and controlling horizontal or vertical lines such as parapet and fascia lines or party wall pilasters. In order to maintain their historic character façade treatment guidelines are more stringent in Secondary Heritage Zone than in Tertiary Heritage Zone.

# b. Primary Heritage Zones Facade Guidelines

The main facades of heritage buildings must be preserved or reinstated to their original state. Other facades may, depending on their architectural significance, be altered but any alterations must be sympathetic to the existing.

For new buildings the main facades should be sympathetic in form, scale and disposition of fenestration to the existing heritage buildings.

# **Secondary Heritage Zones Facade Guidelines**

Upper level fenestration for infill and alterations & additions to existing non heritage buildings should follow the rhythm of the adjoining or nearby heritage buildings and maintain the solid void ratio as well as the fenestration controlling lines for cill and lintol.

Other controlling lines such as the parapet line and fascia lines are also to be maintained. Where these elements differ, either side of a development, the more dominant or traditional are to be maintained. Where there are discontinuous controlling or decorative lines either side of a development, these are to be reinstated. Party wall plasters should be consistent with the adjoining in terms of width and depth.

In certain instances to restore continuity in a row of units, KLCH may require that the new facade should replicate those either side.

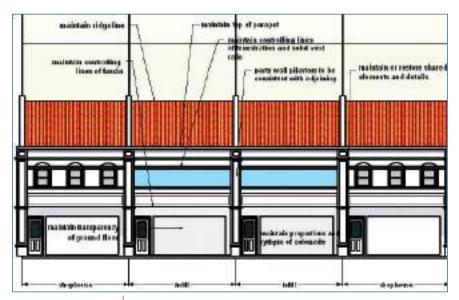


Figure 5.21 - Lean To Covered Walkways At Side Roads



Lean to covered walkways provided along side roads



Building owners to provide active frontages along side wall covered walkways

Figure 5.22 - Main Facade Controls for Secondary Heritage Zones

Other considerations include the need to maintain active frontages at ground floor level which is an essential feature of shophouse areas. Ground level elevational treatment should therefore promote views of internal activities and where possible encourage activity at street level. Acceptable arrangements are indicated in Figure 5.23 below.

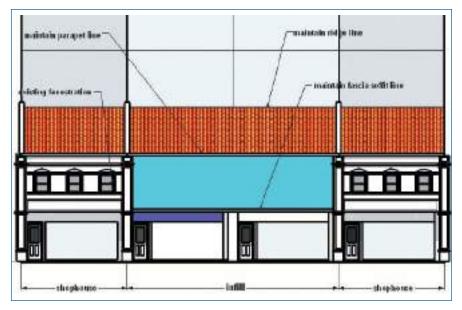
Figure 5.23 - Alternative Arrangements For Ground Floor Shophouse Units



# **Tertiary Heritage Zones Facade Guidelines**

The main controlling lines of the parapet and fascia are to be maintained. Refer to Figure 5.24 below.

Figure 5.24 - Main Facade Controls for Tertiary Heritage Zones



#### 5.7.5 **Verandah Ways and Colonnades**

#### General a.

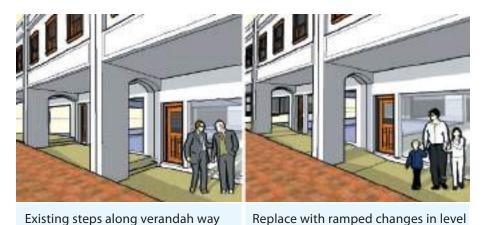
The verandah way is the most significant element of shophouse areas and what gives them their distinctive, traditional street life character. Key elements in the verandah way include the colonnade, the pavement and the ceiling or soffit.

In Secondary Heritage Zone the emphasis is on maintaining the character of the historic shophouse areas, whereas in Tertiary Heritage Zones the emphasis is more on maintaining comfort, consistency and continuity in the covered walkways and encouraging street activity.

#### b. **Changes in Level**

Along many of the verandah ways particularly in the Historic Shophouse Areas, there are frequent changes in level which makes pedestrian movement difficult especially for those with any physical handicap. There are also changes of level between the verandah way footpath and the pavement which has resulted from the gradual increase in the street level over time as new surfaces have been laid over the existing.

Figure 5.25 - Changes In Level Along Verandah Ways



Steps in verandah ways are no longer permitted and must be replaced with ramps of no greater gradient than 1:10. This may involve encroaching onto the verandah ways of adjacent property or properties in which case the works should be carried out jointly. Differences in level between the verandah way and pavement should not exceed 25mm.

As part of the streetscape improvement programmes in certain areas KLCH will reduce or eliminate differences in level between pavements and verandah ways and will also replace steps along the verandah ways with ramps. This may lead to discrepancies between the verandah way levels and the inside of the shophouse units. These differences in level will have to be taken up within the curtelage of the shophouse unit.



Figure 5.26 - Differences In Level Between Verandah Way And Unit To Be Taken **Up Within The Unit** 

#### **Secondary Heritage Zone** c.

Verandah way widths for all buildings in Secondary Heritage Zone are to be retained so as to maintain their existing scale and character. Colonnades should match the existing in terms of spacing, rhythm, size and, where appropriate, detail. Common elements such as archways should be maintained.

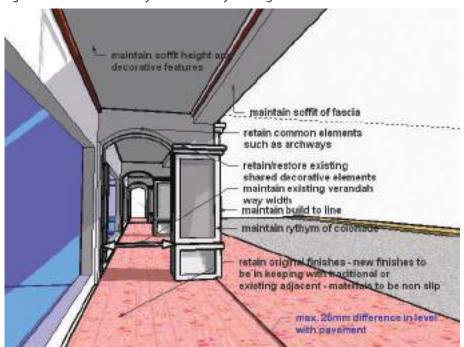


Figure 5.27 - Verandah Ways In Secondary Heritage Zone

#### d. **Tertiary Heritage Zones**

Verandah way widths should be a minimum of 3m for all new buildings and alterations and additions to existing buildings except designated heritage buildings.

There are no specific requirements regarding maintaining aspects of the colonnade or of common features except for buildings which are adjacent to groups of designated heritage buildings or where colonnades are part of a contiguous group of buildings and form part of the character of the area.

#### **Pavement Materials**

Finishing materials for verandah ways should be non slip. Traditional materials include cement screed finishes and terracotta tiles. Other more modern materials include non slip ceramic or homogenous tiles and flamed (not polished) granite tiles.

In Secondary Heritage Zone, certain areas will be themed and materials and finishes for verandah way pavements will be determined by KLCH.

#### **Activities** f.

The primary function of the verandah ways is to provide protection for the pedestrian from the sun and rain and it is important therefore that ground level activities in shophouse units do not compromise this function.

For this reason car repair and motorcycle repair workshops are not permitted in any of the heritage areas and shop owners and eating establishments will be required to ensure that activities generated by their businesses do not directly or indirectly obstruct the verandah ways.

In certain areas where the verandah way is sufficiently wide or pedestrian traffic is relatively light, activities maybe allowed to spill out into the verandah way provided that a minimum passageway is maintained along which a wheelchair may pass.

# **Buildings following previous Setback Requirements**

In the case of buildings that have been set back from the build to line to conform with previous set back requirements, KLCH may use its discretion as to whether the building owner will be permitted or required to extend the upper floors of their properties up to the build to lines. Where such properties are extended to the build to lines, the verandah way must be maintained and be continuous and consistent with the adjoining properties. The remainder of the alterations and additions must conform to the guidelines.

Figure 5.28 below indicates some of the alternative acceptable methods of extending up to the street build to line.



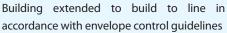
Maintain verandah way features which form part of the character of an area



Verandah ways to be a minimum of 3m width in Tertiary Heritage Zone

Figure 5.28 - Alternative Methods Of Extending Buildings That Have Been Setback To Build To Line



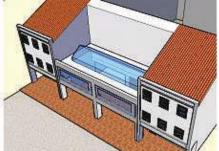




Lean to roof over area to provide covered outdoor area



Covered walkway linking existing verandah way. External al fresco or landscaped area retained external



Roofed over external or internal area to provide double height internal or space



# Service zone in

# **Building Services**

Building services such as satellite dishes, condenser units and air conditioners unit are not permitted on the main street elevations in Secondary or Tertiary Heritage Zone. They may be placed within the setback zone of the side and rear elevations or in the articulation zone behind the main roofs.

Figure 5.29 - Services Zone In Secondary Heritage Zone

#### 5.7.6 Al Fresco Areas

#### a. General

In order to encourage greater liveliness in certain areas, zones may be demarcated for al fresco activities. These areas remain in the public domain and the authority will determine guidelines for their usage and may carry out any works in the areas at any time.

## b. Permitted activities

The primary intention is for these areas to be used for outdoor refreshment associated with adjacent premises but they may not be used for the preparation of food.

## c. Advertising, Signage and Furniture

No fixed advertising, signage or street furniture is permitted in these areas and designated verandah ways and other public passageways must not be obstructed at any time.

## d. Double Frontage Shophouses

In certain areas shophouses will be encourage to create double frontages in order to enliven the service lanes or other areas behind them. Guidelines for al fresco areas relating to signage, advertisements, furniture and other relevant guidelines will apply.

## 5.7.7 Materials

### a. General

Table 5.4 sets out the requirements for materials for buildings in the various categories of Heritage Zones according to building category.

# b. Walls

Of prime concern are the main elevations facing streets or major public open spaces.

External wall finishes in Primary and Secondary Heritage Zones should follow the traditional materials of painted rendered brickwork or fair faced brickwork as applicable.

Wall finishes in Tertiary Heritage Zones are not controlled although consideration should be given to new developments that adjoin designated heritage buildings.



# c. Windows, Door and Shutters

The traditional material for windows, doors and shutters is timber but this can be difficult to maintain and a dark brown or black anodized or powder coated aluminium framing may be used in Secondary Heritage Zone.

Table 5.4 - Materials for Building Elements in Heritage Zones

			Heritage Zone Categories			
ltem	Building Element	Building Category	Primary (Note: All materials to be approved by the Design Panel)	Secondary	Tertiary	
1. Roofs	Main Roof and other pitched roofs	Alterations and Additions to Heritage Buildings	To maintain or restore original profile and materials. Only internal non visible alterations to roofs such as additional or replaced structural members or roof insulation.	Profiled Terracotta tiles	Profiled Terracotta tiles	
		Infill and existing non Heritage	Profiled Terracotta Tiles or to match existing nearby heritage building main roofs.	Profiled Terracotta tiles	Clay or concrete tiles for main roofs for infill or other existing buildings	
	Flat Roofs	All	No requirement	No requirement	No requirement	
Walls (i.e. fa or oth open	Main Facades (i.e. facing streets or other public open spaces)	Alterations and Additions to Heritage Buildings	To maintain or restore original materials and finishes.	To maintain or restore original materials and finishes.	To maintain or restore original materials and finishes.	
		Infill and existing non heritage buildings	To match existing nearby heritage buildings. Note: fair faced brickwork to match existing where applicable.	Painted rendered brickwork or fair faced brickwork to match existing adjacent heritage buildings	No restrictions.	
	 	Alterations and Additions to Heritage Buildings	To maintain or restore original materials and finishes. Alterations to facades should match existing materials	No restrictions	No restrictions.	
		Infill and existing non heritage buildings	To match existing nearby heritage buildings.	No restrictions.	No restrictions.	
3. Fenestration	Windows, Doors and Shutters	Alterations and Additions to Heritage Buildings	To maintain or restore original. In order to promote active frontages, shutters may be replaced by clear glazing at ground floor level in selected buildings.	Street facing elevations must use original materials or black/ dark brown powder coated or anodized aluminium. In order to	Street facing elevations must use original materials or black/ dark brown powder coated or anodized aluminium. In order to	

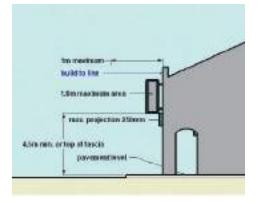
ltem		Building Category	Heritage Zone Categories		
	Building Element		Primary (Note: All materials to be approved by the Design Panel)	Secondary	Tertiary
				promote active frontages, shutters may be replaced by clear glazing at ground floor level.	promote active frontages, shutters may be replaced by clear glazing at ground floor level.
		Infill and existing non heritage buildings	To match nearby ceritage buildings or framing and shutter materials may be powder coated or anodized aluminium. Colour to match existing nearby heritage buildings.	For street facing elevations framing to be black/ dark brown powder coated or anodized aluminium.	No restrictions.
4. Verandahways	Pavements	All buildings	As determined by DBKL	Cement screed, non slip ceramic, granite or terracotta tiles. Or as determined by KLCH in specific areas.	Non slip finish.
	Verandah Way Soffits	Alterations and Additions to Heritage Buildings	To match existing.	Painted rendered concrete or external quality inert board.	Painted rendered concrete or external quality inert board.
		Infill and existing non heritage buildings	To match nearby heritage buildings.	Painted rendered concrete or external quality inert board.	No restrictions
5. Others	Rainwater Goods	Alterations and Additions to Heritage Buildings	Profiles and materials to match existing	Black square section UPVC gutters and downpipes.	Black square section UPVC gutters and downpipes.
		Infill and existing non heritage buildings non heritage buildings	Black square section UPVC gutters and downpipes.	Black square section UPVC gutters and downpipes.	No restrictions

Figure 5.30 - Signage on Shophouses in Secondary Heritage Zones

- · Signage at first storey fascia and ground floor shopfront panel only
- Signage to project no more than 250mm from face of building
- · Projecting Signage only on party walls to project no more than 1m from build to line
- Maximum size of projecting sign 1.8m
- Minimum mounting height of projecting signage 4.5m above pavement level or to top of first floor fascia panel







#### 5.7.8 **Building Signage and Advertisements**

#### a. General

In order not to create visual confusion in heritage zones, signage is controlled in a manner which strikes a balance between avoiding compromising the historic ambience of heritage areas and the need to be flexible enough to maintain a certain vitality especially in shophouse areas.

# b. Heritage Buildings

For designated heritage buildings, signage must be contained within any decorative panels or controlling lines. The mounting of signage should not obscure or damage important architectural decorative elements.

## c. Primary Heritage Zones

Signage and advertising will be strictly controlled so that the historic character of these areas is not compromised. All proposed external signage and advertising is to be reviewed by the Design Review Panel.

## d. Secondary Heritage Zones

Signage will be confined to the ground floor shopfront panel and the first storey fascia panel. Such signage should not project more than 250mm from the face of the building.

Vertical projecting signage may only be mounted on the party walls and may not be more than 1.8m in area on either face. The signage should not project more than 1m from the face of the building or the width of the pavement whichever is the lesser and should be no thicker than 250mm. The signage should not be mounted in such a way that special features such as pediments or decorative elements such as pilasters or spandrels are covered over or so that support brackets damage decorative elements.

## e. Tertiary Heritage Zones

Signage control in Tertiary Heritage Zones is far less stringent and requires only that signage on the face of the building fits within the spandrel panels, does not cover any part of any fenestration, does not project above the parapet line and projects no more than 300mm from the face of the building. Vertical signage should be confined to the party walls and should project no more than 1.2m from the face of the building or the width of the pavement whichever is the lesser.

# f. On Street Signage and Advertisements

No on street signage or advertising boards either fixed or free standing are permitted along covered walkways or other areas in the public realm except for free standing signage in designated al fresco areas.

## g. Signage Lighting

Illumination of building signage should be means of backlighting only in Primary Heritage Zones. Neon lit signage is permitted in Secondary and Tertiary Heritage Zones.

#### 5.7.9 Car Parking and Motorcycle Parking

#### General a.

Car parking provision in heritage zones and shophouse areas in particular is often problematic as car parking spaces are not allowed for within the building curtelage and rear service lanes are narrow making access to car parking in new developments difficult. Most car parking has therefore been accommodated in the form of on street car parking or on vacant surface lots or along service lanes.

In keeping with the KLSP 2020 objective to encourage the usage of public transportation and reduce reliance on private transportation, parking standards for developments within the City Centre are reduced.

Furthermore, in order to encourage more on street pedestrian activity in shophouse heritage areas and reduce traffic congestion, on street car parking will be phased out or limited to short term parking.

There will still however be the need to provide adequate public parking spaces for cars and motorcycles in order to meet the needs of residents and sustain local businesses. This requirement will be met in part by KLCH and in part by requiring certain designated developments to provide public parking spaces.

#### b. **Primary and Secondary Heritage Zones**

Buildings in Primary and Secondary Heritage Zones are exempt from general car parking and motorcycle parking requirements.

Certain new or infill developments will be required to provide additional public parking facilities.

## **Tertiary Heritage Zones**

Heritage buildings will not be required to meet standard car and motorcycle parking requirements.

Other non heritage buildings will be required to meet the car and motorcycle car parking requirement or pay a contribution towards the cost of providing public parking.

Certain new or infill developments will be required to provide additional public parking facilities.

#### d. **Buffer Zones**

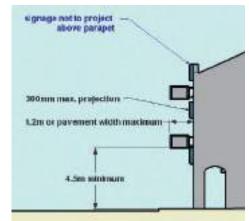
Normal parking standards are to be followed.

Figure 5.31 - Signage on Shophouses in Tertiary Heritage Zones

- Wall mounted signage not to project more than 300mm from face of building
- Signage not to cover over any fenestration or project above the parapet line
- Vertical signage confined to party walls and not to project more than 1.2m from face of building or width of pavement whichever is less









# 5.8 Guidelines Relating to Heritage Zones

#### 5.8.1 General

## **Compliance with Guidelines Relating to Heritage Zones**

Designated Heritage Buildings that lie within designated Heritage Zones will be bound by the Heritage Zone Guidelines in addition to those outlined below.

## **Demolition**

Demolition of any designated heritage building is not permitted without the express consent of the Authority. This consent will not be given except under exceptional circumstances such as in the case of buildings which are in a dangerous condition.

Prior to demolition complete records in the form of measured drawings and photographs should be submitted to KLCH.

## Reinstatement of damaged or demolished features

The authority may require that certain elements or decorative items of buildings to be conserved that have been replaced, removed or otherwise altered should be reinstated using either original materials or similar approved. This provision would normally apply only to Category 1 buildings or to Category 2 buildings in shophouse areas where there is a group of similar building sharing identical or very similar characteristics.

#### d. **Dangerous Structures**

Structures that have been neglected or damaged such that they are in danger of collapse or are a danger either to the general public or other nearby structures may have temporary remedial works carried out provided that any damaged or removed elements are reinstated at a later date. In exceptional circumstances, KLCH may, provided that the building owner is unable or unwilling to do so, carry out immediate remedial works to prevent the deterioration or collapse of a designated heritage building.

## **Adaptive Reuse**

Adaptive reuse of heritage buildings is permitted subject to the authority's approval. No specific guidelines apply but the usage should be in line with the building use and should not degrade its historic significance.

#### 5.8.2 **External Elements**

#### **Elevations** a.

The main street elevations of designated Category 1,2 or 3 Heritage Buildings including all decorative elements such as arches, corbels, pilasters, spandrel and fascia panels, mouldings, vents, arches, balconies etc must be retained or restored to their original condition using the original materials.

In Category 1 Heritage Buildings other elevations may be required to be retained or restored to their original state.

Gable end walls of Category 3 Heritage Buildings may have new fenestration added provided that the form and proportions of the fenestration is in keeping with that of the existing main facades and that the main controlling lines are retained.

#### b. Roofs

#### i. **Main Roofs and Other Pitched Roofs**

Main roof pitches, ridge lines and eaves projections for Categories 1, 2 and 3 Heritage Buildings must be retained or restored to their original condition.

All other pitched roofs for Categories 1 and 2 Heritage Buildings should be retained or restored subject to the requirements of the Design Review Panel.

#### ii. **Flat Roofs**

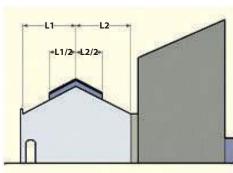
Flat roofs may be landscaped but not covered over by temporary or permanent structures except for Category 1 and 2 Heritage Buildings where approved by the Design Review Panel and for Category 3 and 4 Heritage Buildings provided that the structures are within the envelope controls.

#### iii. Jack Roofs

Jack Roofs over the main roofs are permitted in Category 3 and 4 Heritage Buildings provided that the following conditions are met: (Refer to Figure 5.32)

- The ridge is in line with the main ridge line
- The jack roof is within the relevant envelope controls
- The roofs do not extend more than 50% down the slope of either the front or rear of the main roof
- The sides of the roof are set back 600mm from the party wall lines.

Figure 5.32 - Jack Roof Guidelines



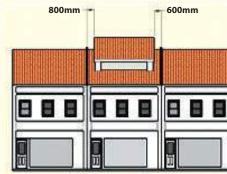




Figure 5.33 - Additional Floors Under Jack Roofs



#### Rooflights iv.

Rooflights are permitted for Category 3 Heritage Buildings on the rear main

#### **Fenestration** V.

Existing or original fenestration for Categories 1, 2 and 3 Heritage Buildings is to be retained or restored to the original condition including all fanlights, shutters, framing, balustrades etc.

Shutters at the ground floor facing the covered verandah way in Category 2 or 3 shophouse units may be replaced with clear glazing.

#### **Other Elements** iv.

Any other external elements that KLCH deems to be of significance such as external staircase or flues should be retained or restored to the original condition.

#### 5.8.3 Internal Elements and Alterations

## a. Alterations to Internal Spaces

Rearrangement of internal spaces in Category 1 and 2 Heritage Buildings is subject to the approval of the Design Review Panel.

There are no restrictions on the rearrangement of internal spaces in other heritage category buildings.

## b. Basements

New basements are not permitted in heritage buildings.

### c. Additional Floors

Additional floors may be permitted in Category 1 and Category 2 Heritage Buildings where this does not impact the external appearance of the building or affect important internal elements to be conserved subject to the approval of the Design Review Panel.

Where jack roofs are permitted in Category 2 and 3 shophouse units, additional floors within the roof space of existing shophouses are permitted up to a maximum of 50% of the area of the floor immediately below the main roof. The space of the access stair is not computed as part of this area.

Additional floors under jack roofs are not to exceed 50% of the floor area of the floor beneath the main roof excluding the area of the stair.

## d. Staircases

Where specified in Category 1 and 2 Heritage Buildings, existing internal staircases are to be retained and restored to their original condition using original materials including balustrades and handrails and other decorative items.

## e. Floors and Ceilings

Where specified in Category 1 and 2 Heritage Buildings, timber floors are to be retained including any exposed timber structures subject to the requirement to comply with building regulations. Any original mouldings and cornices are to be retained or restored.

Ground floors of timber construction may be replaced with concrete floors in all heritage buildings. Existing upper level timber floors maybe replaced by concrete floors in Category 3 Heritage Buildings, but existing floor to floor heights must be maintained.

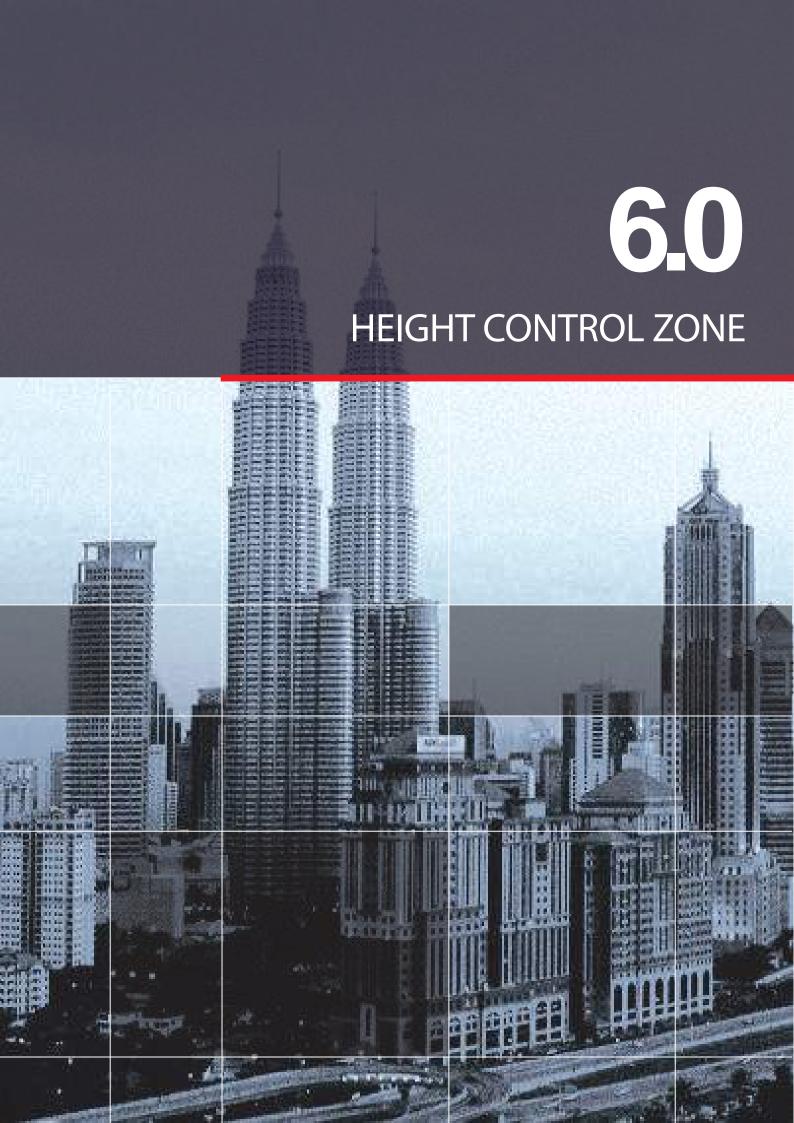
## g. Air Wells

Where specified in Category 1 and 2 Heritage Buildings, existing air wells are to be retained for the proper functioning of the internal spaces that may be covered over with clear glazing.

## h. Other Elements

Where specified in Category 1 and 2 Heritage Buildings, special features such as decorative floor tiles, internal paneling, mouldings, skirtings or other unique building elements are to be retained and restored to their original condition.





# 6.1 Application

Height Control Zones refers to additional overlays of control on heights of new buildings that falls within the designated height control zones in Kuala Lumpur and are imposed on the following areas:

- (a) Areas located within Petronas Twin Tower and KL Tower view corridor.
- (b) Areas located within selected hills backdrop.
- (c) Areas surrounding the Istana Damansara reserve.
- (d) Areas located within designated historical zones.
- (e) Areas surrounding Sungai Besi Airport.

The Height Control is applied as an overlay control over density and plot ratio provisions stated in DCP 2 and DCP 3.

## 6.2 Objectives

Objectives of the Height Control Zones are:

- (a) To ensure visual primacy of Kuala Lumpur's two major landmark buildings, the Petronas Twin Towers and the KL Tower.
- (b) To protect views towards the two landmark buildings from major entrances to the city, from major road corridors and from various public open spaces.
- (c) To protect views towards selected hill that provides visual back drops at the edge of the city.
- (d) To ensure that new buildings in historical zones are built in a scale that is harmonious with their surroundings.
- (e) To ensure safety to aircrafts and buildings located within flight path and in close proximity to Sungai Besi Airport.

# 6.3 Height Control Zones in Kuala Lumpur

The Height Control Zone in Kuala Lumpur is indicated in the Special Planning Zone Map 3: Height Control Plan (SPZ 3 - HCZ) (Figure 6.1) which shows all the designated height control zones for Kuala Lumpur. The plan identifies the boundaries or areas affected by these height control zones. The height control zones are:

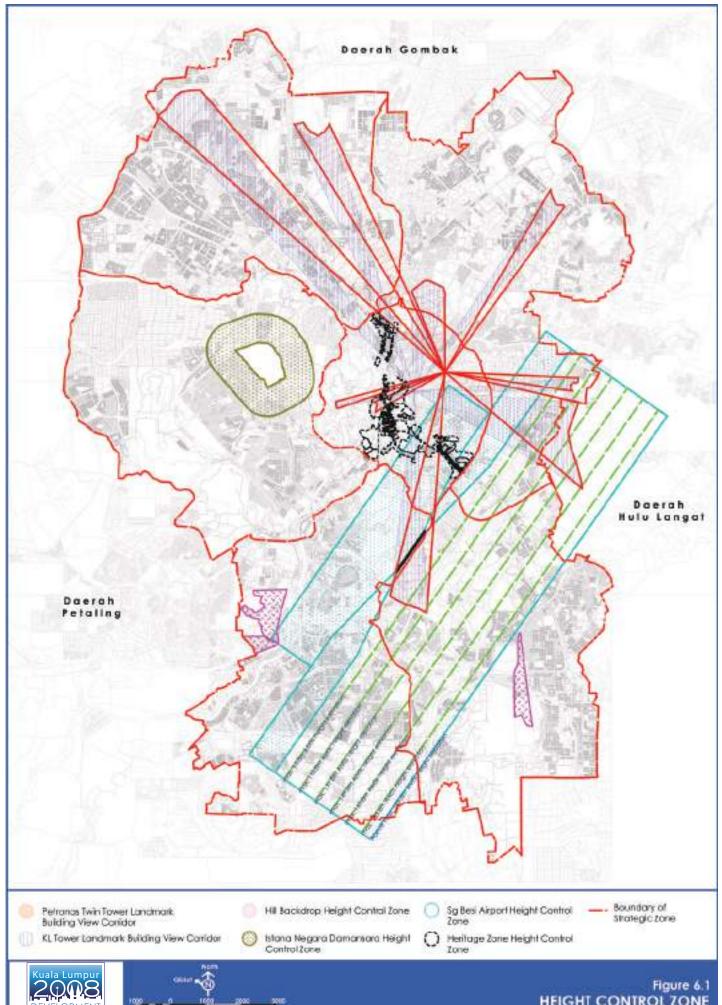
- (a) Landmark View Corridor Height Control Zone.
- (b) Hill Backdrop Height Control Zone.
- (c) Istana Negara Damansara (Royal Palace) Height Control Zone.
- (d) Heritage Area Height Control Zone.
- (e) Sungai Besi Airport Height Control Zone.



Table 6.1 - Kuala Lumpur Height Control Zone

Height Control Zone (HCZ)	Area Affected
Landmark View Control Zone	Applies to areas within view corridors of the city's landmark Corridor Height buildings (Petronas Twin Towers and KL Towers) from the following areas:  (a) View from Taman Tasik Titiwangsa; (b) View from Tasik Ampang Hilir; (c) View from Dataran Merdeka; (d) View from Tugu Negara; (e) View from Batu Metropolitan Park; (f) View from Kepong Metropolitan Park; (g) View from Wangsa Maju Park; (h) View from Sungai Besi Airport; (i) View from MRR2 Kepong; and (j) View from MRR2 Kg. Pandan.
Hill Backdrop Height Control Zone	Applies to areas within view corridors of hill backdrop of Bukit Gasing and Bukit Sungai Besi identified from the following locations:  (a) View from New Pantai Expressway facing Bukit Gasing; and  (b) View from KESAS Highway facing Bukit Sungai Besi.
Istana Negara Damansara (Palace) Height Control Zone	Applies to areas surrounding and within approximately 800m from the property boundary of the Palace.
Heritage Zone Height Control Zone	Applies to buildings and land located within the following designated heritage zones:  (a) City Centre Historical areas  (b) Sungai Besi Heritage Area.
Sungai Besi Airport	Applies to areas located within flight path approach and obstacle limitation surfaces of the airport as identified by the Department of Civil Aviation.

There will be instances where an area will fall under two or more height control zones. In the event where an area falls under two or more height control zones, the more stringent limitation in terms of height shall govern and prevail.







## 6.4 General Allowable Heights for Kuala Lumpur

In general, the allowable heights for all areas in Kuala Lumpur are guided by a General Allowable Height Plan (GAHP) (Figure 6.2). The GAHP serves as guide for developments within Kuala Lumpur with the intentions of:

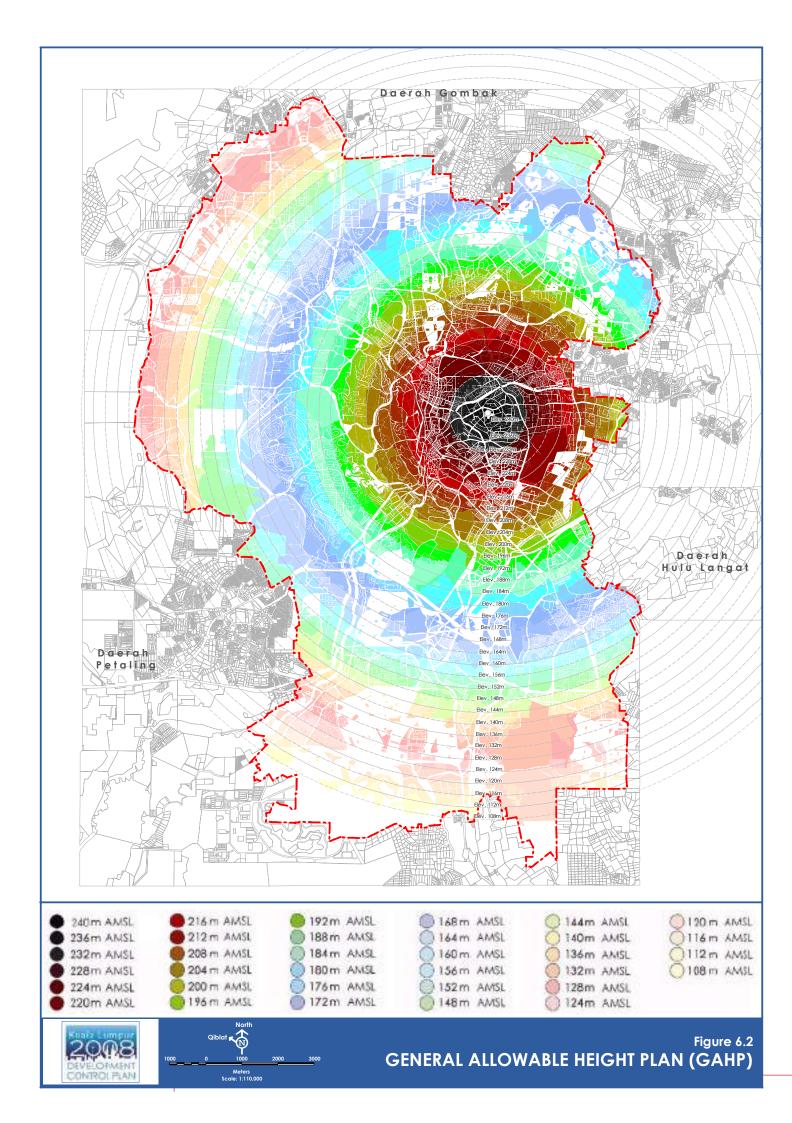
- (a) Developing a city skyline and profiles that accentuate the city centre as central area for Kuala Lumpur and the surrounding areas as edges to the city with major landmark buildings in the city centre being the focus of vistas and views.
- (b) Promoting innovative building designs in terms of massing and heights within the allowable plot ratio and height where strategically located landmark buildings can be created as local vistas or landmark to their locality.

#### 6.4.1 **Application of GAHP**

- The GAHP shall be used as reference and guide in determining indication of maximum heights of buildings subject to compliance to allowable plot ratio or density as defined in the DCP 2 - Density Map and DCP 3 - Plot Ratio Map.
- Variation in heights between several towers and podium within a development can be allowed as long as they do not exceed the allowable plot ratio and the maximum allowable heights indicated in the GAHP.
- For buildings located within the designated height control zones indicated in Height Control Plan, guidelines regarding the respective height control zones must be observed and followed.
- For buildings or development where higher plot ratios are allowed (CDA area, urban regenerations areas and inner city centre area) which results in local peaking situations (higher building heights than GAHP) then such situation shall prevail against the GAHP.

However, such development must develop and submit to KLCH the detail Urban Design Guidelines (DUD) that should indicate how it relates to the overall city's skyline and physical profile.

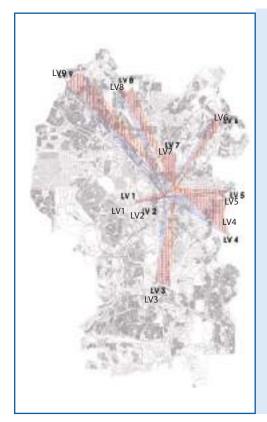




# 6.5 Guideline for Landmark View Corridor Height Control Zone

The two landmarks of Kuala Lumpur are the Petronas Twin Tower in Kuala Lumpur City Centre and the KL Tower in Bukit Nanas. These two landmark buildings have become a symbol that is synonymous to the city of Kuala Lumpur.

Views towards the two landmark buildings are categorized into two categories as follows:



## **Landmark View Corridor**

View looking directly towards either one of the landmark buildings from designated locations.

- (a) LV1 view from Tugu Negara
- (b) LV2 view from Dataran Merdeka
- (c) LV3 view from Sungai Besi Airport
- (d) LV4 view from MRR2 Kg Pandan
- (e) LV5 view from Tasik Ampang Hilir
- (f) LV6 view from Wangsa Maju Park
- (g) LV7 -view from Taman Tasik Titiwangsa
- (h) LV8 view from Taman Metropolitan Batu
- (i) LV9 view from MRR2/Taman Metropolitan Kepong



Landmark View at Petronas Twin Tower from Tasik Titiwangsa



Panaromic View of KL Tower And Petronas Twin Tower from MMR2

#### 6.5.1 **Maximum Allowable Heights**

- Any part of a structure that is within the cone of the two landmark building is subject to the limitation of the building height.
- No part of a structure within the view corridor shall exceed the allowable height elevation in MSL. Whenever a structure lies partially outside and partially inside of the view corridor, the provisions of this guideline shall apply only to that part of the structure that lies within the zone.
- The highest point of the building shall be either the top of the parapet or coping of a flat-roofed building, or the ridge of a sloping roof. No occupied part of any building shall be constructed above the permitted height and this includes unoccupied building features projecting above the highest point of the building.
- Actual building heights would vary depending on the final grade of the site on which the buildings are located, and no part can project up into the defined view cone. This applies even in cases where a building straddles an elevation.
- For buildings located 1000m to the said landmarks, the position of the tower is critical and a tower location analysis needs to be prepared by the developer/land owner (see Section 6.5.3)

#### 6.5.2 **Heights Circulation**

- The building height limitations depend on the vertical measurement to the top of the landmark as a reference point, usually with increased height allowed as the building is located farther and below the elevation of the landmarks. The vertical measurement is measured as elevation above mean sea level and in measurement of feet.
- The vertical measurement to the top of the landmark building i.e. Petronas Twin Tower and KL Tower. Height elevation of any buildings must not exceed the maximum height elevation of these two (2) landmarks (Table 6.2).

Petronas Twin Tower shall mean the two towers located on top of the podium block of Suria KLCC i.e. from the bottom of Link Bridge to the top most of the Tower Antenna.

**KL Tower** shall mean view to at least 75% of the total vertical height.

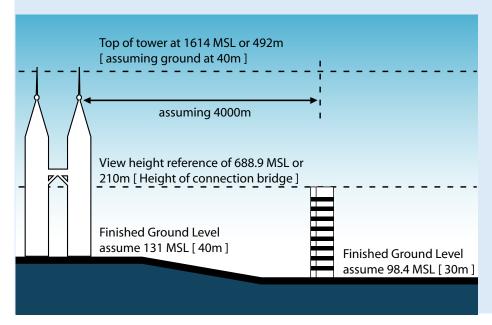
Table 6.2 - Maximum Height Elevation for Respective Landmark Buildings

View	RelativeMax height elevation allowable in MSL(ft)	Meter
Petronas Twin Tower	688.97 ft (taken at bottom of skybridge)	210 m
KL Tower	452.00 ft (taken at 75% of total heights)	138 m

## **Example of Calculation:**

# Height Relative to Petronas Twin Tower Landmark View Corridor

- i. The maximum height elevation of Petronas Twin Tower is 688.97 MSL or 210 m height elevation.
- ii. If the finished ground level of the proposed building is 98.4 MSL and islocated 4000 meter away from the tower, then the height is: 688.97 MSL - 98.4 MSL = 590.57 feet or 45 stories. (at 4.0 meter per level).
- iii. The submitting person then needs to refer to the 'General Allowable Height Plan' (GAHP) to verify the final allowable height for the building.
- iv. If the calculated height elevation as above is higher than GAHP, then the lower allowable height in this case the GAHP prevails. Thus in this case if the allowable height as GAHP is 38 stories, therefore the final allowable height is 38 stories and not 45.
- v. If in any case, the calculated height elevation is lower than GAHP, than the lower allowable height in this case the View Corridor HCZ calculation shall take precedent.



# 6.5.3 Height Calculation for Building within Short Distance of **Landmark Buildings Description**

In general there are many short views from major parksand major development within Kuala Lumpur city centre. There are two (2) means of control:

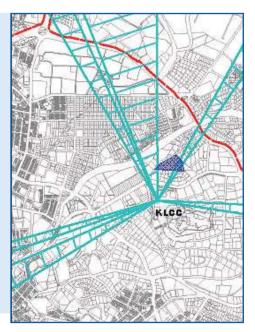
- · Heights as AMSL; and
- · Control over building above podium level. (Positioning of towers must not block visual corridor to the landmark buildings).

# Example of Calculation for Building within Short Distance of Landmark **Buildings**

- i. Part of the site falls onto the view corridor towards KLCC (control at 688.97ft).
- ii. If the finished ground level is 196ft AMSL or 60m, then the allowable height is:
  - = 6.88.77ft 196ft
  - = 492.97 feet (150m AMSL).
- iii. Therefore, allowable heights for building located within the view corridor is 130m (MSL) whilst for part of building located outside the corridor must not exceed 240m (MSL) as determined through the GAHP.
- iv. However, the position of the tower at 240m needs to be in position with narrower width facing towards KLCC.

# Buildings within short distance of the landmark buildings refers to buildings

located within 1000 meter of either Petronas Twin Towers or KL Tower.



#### **Guideline for Hill Backdrop Height Control Zone** 6.6

## 6.6.1 Area

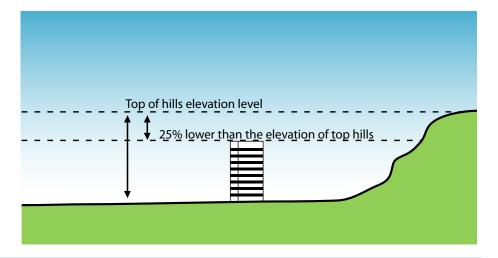
- There are two (2) areas identified for this category as indicated in the Special Planning Zone Map 3: Height Control Plan (SPZ 3 - HCZ):
  - (a) From KESAS Highway facing the Bukit Sungai Besi forest reserve; and
  - (b) From Pantai Expressway facing Bukit Gasing.

## 6.6.2 Allowable Building Heights

- The allowable building heights shall be less than 75% of the top of hill located behind the building.
- Allowable Building Height shall mean the height of a building or a structure measured at the vertical distance from the highest point of the building to the average elevation of the corners of a building at the finished grade.

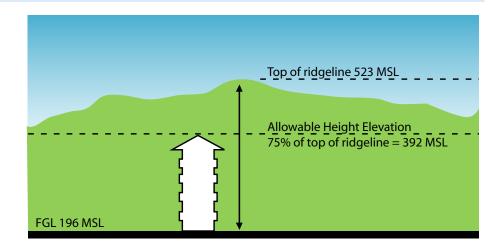
# 6.6.3 Height Calculation

• The building height limitations depend on the vertical measurement to the top of the hill measured in MSL. The buildings allowable heights shall be the reduction of 25% of the hills located behind the building.



# **Example of calculation**

- i. The need to establish the height elevation of the hills located behind the development (by Licensed Land Surveyor)
- ii. The height elevation of the hill for example is 523 MSL or 159m height elevation.
- iii. If the finished ground level is 196 MSL or 60m height elevation, then the allowable heights are 392 MSL – 196 MSL = 196 feet or 14 stories (assuming 4.0 meter per level).



# 6.7 Guideline For City Centre Heritage Height Control Zone

## 6.7.1 Area

· Areas affected by this zone are land located in Heritage Zone at City Centre for example along Jalan Tunku Abdul Rahman, Petaling Street, Jalan Pasar and Jalan Changkat Thambi (Figure 6.3).

# 6.7.1 Allowable Building Heights

• Buildings located within primary heritage zone, secondary heritage and tertiary heritage zone and the allowable height as indicated on the Figure 6.3 - Height Control in Heritage Zone.

# 6.8 Guideline for Istana Negara Damansara Height Control Zone

### 6.8.1 Area

· Areas affected by this zone are land located as far as 2.4km from the property boundary of the Istana/Royal Palace located at Jalan Duta, Bukit Damansara.

## 6.8.2 Allowable Building Heights

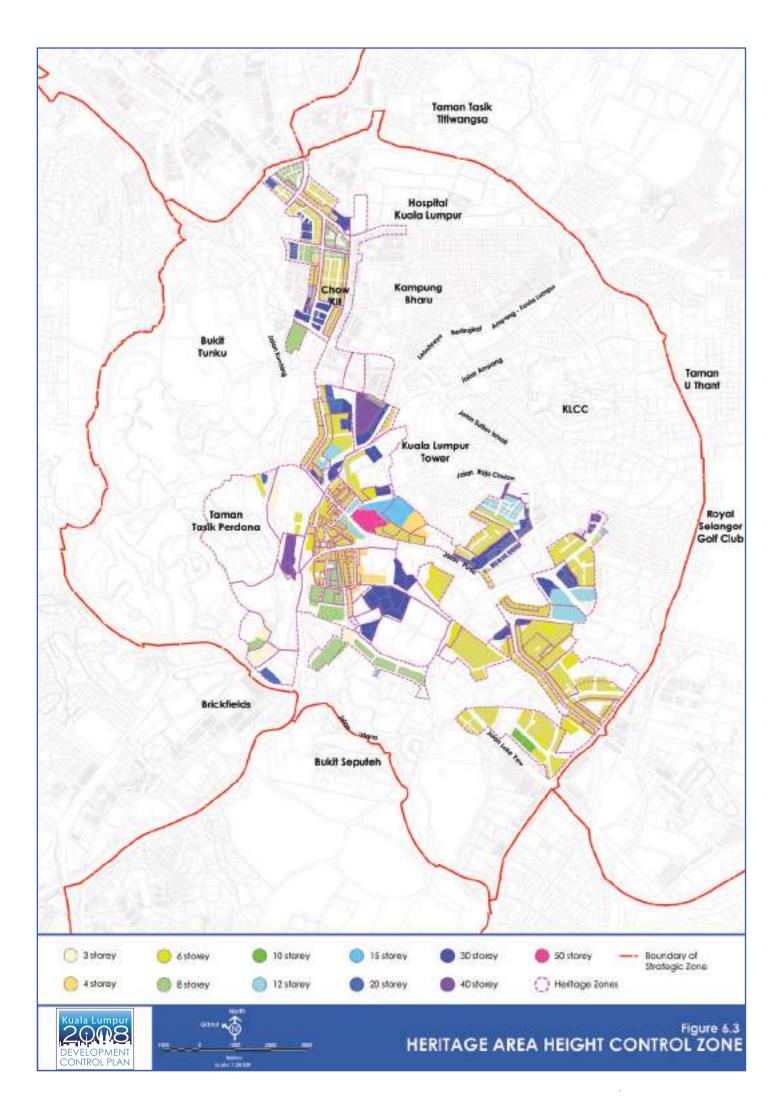
 Buildings located 800m distance to Istana/Royal Palace boundary No part of a structure shall exceed the 400 feet MSL as indicated on the Special Planning Zone Map 3: Height Control Zone (SPZ 3 - HCZ). Whenever a structure lies partially outside and partially inside of the area on the map indicated, the provisions of this guide line shall apply only to that part of the structure that lies within the area indicated on the map.

# 6.9 Guideline for Sungai Besi Height Control Zone

# 6.9.1 Area and Application

- Areas affected by this zone are lands located near the Sungai Besi Airport and land underneath the landing and take off path of the airport as indicated in the Special Planning Zone Map 3: Height Control Plan (SPZ 3 - HCP).
- The actual location of the property in reference to the airport shall be verified in coordinates by a certified land surveyor.
- The airport runway location is as the following coordinates:
- This guideline is to be used as reference point when designing any development/building located within the Sungai Besi Airport Height Control Zone. The final decision with regards to allowable heights within this height control zone shall rest with the Department of Civil Aviation. Any development located within the zone shall be referred to the Department before any decision is made.



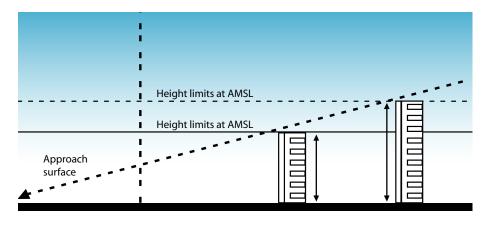


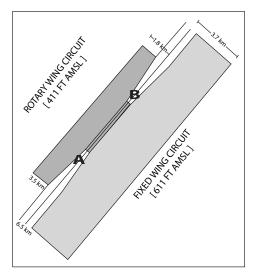
# 6.9.2 Allowable Building Heights

· No part of a structure within the areas indicated on the map shall exceed the heights elevation in MSL as shown in Special Planning Zone Map 3: Height Control Plan (SPZ 3 - HCZ) (see also Figure 6.4):

# 6.9.3 Exception

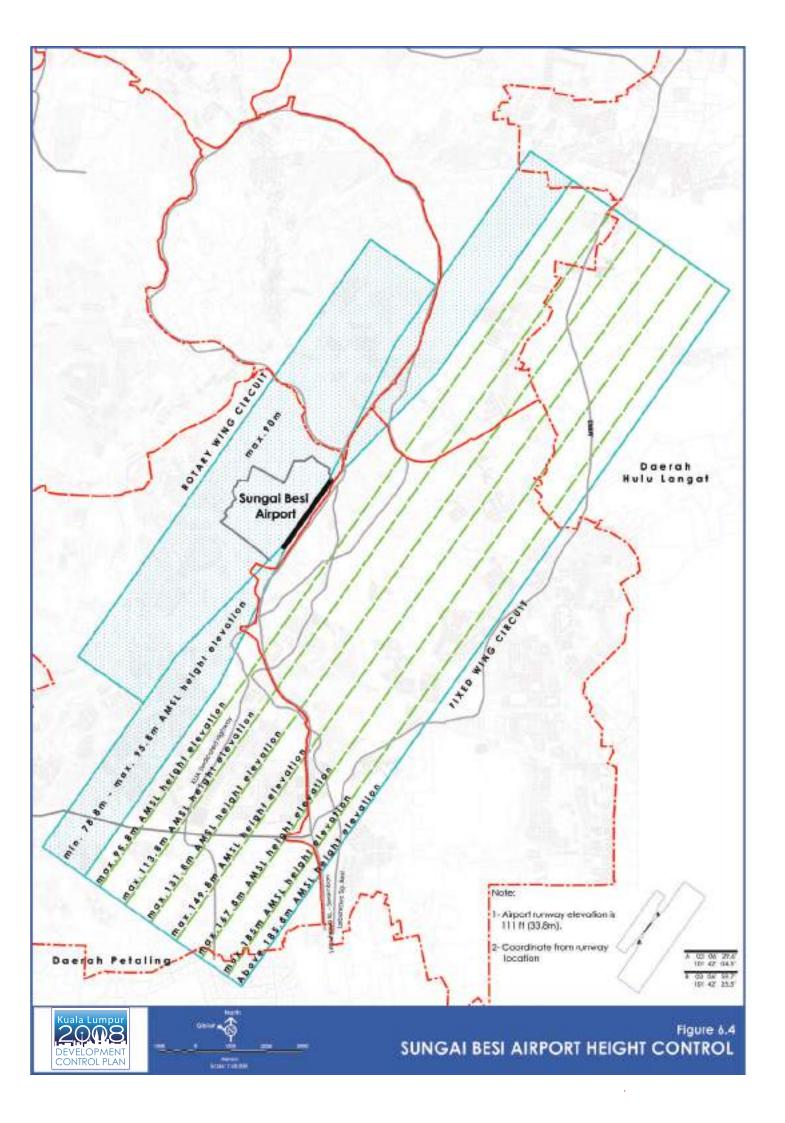
- a) Existing structures. Any structure within the height control zone which was erected prior to the effective date of this documents and which is not in compliance with the terms of this article may be altered or replaced as necessary to the height existing on the effective date of this article.
- b) Flagpoles. Transmittal tower will be considered as structure.

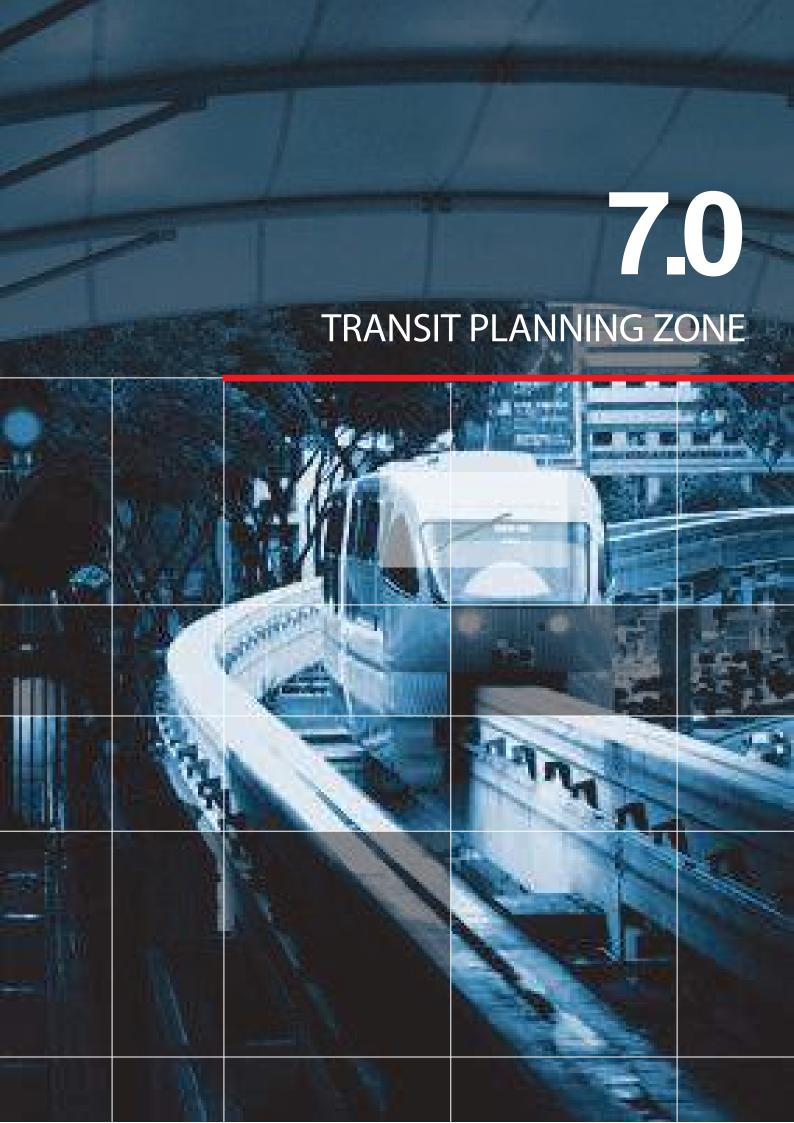




## Coordinates for runway location

Α	03°	06′	29.6"
	101°	42′	04.5"
В	03°	06′	59.6"
	101°	42′	25.5"





#### 7.1 Introduction

Transit Planning Zone (TPZ) refers to areas located within 400m of radius of a Transit Station – a Light Rail Transit (LRT), KTM Commuter, Monorail, or Bus Rapid Transit station. Intensification of development shall be promoted within this area to make transit convenient for more people and to support the transit system.

Land within a TPZ must be specifically designed to integrate with the surrounding development to support the transit system. The merging of various land uses at the transit station creates a volume of pedestrian and thus TPZ must be walkable, pedestrian friendly and compact to help minimise social and environmental costs.

The Draft KL City Plan 2020 has identified a comprehensive rail transit network that is expected to contribute significantly to reducing urban sprawl, traffic congestions and air pollution for Kuala Lumpur. This rail transit network has identified potential areas as Transit Stations and this Special Planning Zone 4 – Transit Planning Zone guidelines is to assist in determining the directions towards the planning and development within the TPZ.

#### 7.2 **Objectives**

The objectives of the Transit Planning Zone are:

- To implement a more sustainable approach to urban planning by integrating land use with transportation planning and development;
- ii. To promote the development of transit supportive uses around transit stations:
- iii. To create a safe and walkable environment within the TPZ; and
- To enhance and support the urban transportation system as part of the city's infrastructure and public transportation network.

#### 7.3 **Transit Planning Zones in Kuala Lumpur**

As a general guide, a Transit Planning Zone is an of 400m around a Transit Station. Within this 400m radius, the immediate 250m radius is the Priority TPZ. This area is calculated from the point of the Transit Station itself. Thus the TPZ shall depend on the varying size of the Transit Station.

The proposed rail network and transit stations have been identified by Draft KL City Plan 2020 and this shall be used as the guiding principle to determine the Transit Planning Zone as well as the Priority Transit Planning Zone. However not all areas with Transit Stations are TPZ, this is especially so for Established Housing Areas and Conservation Areas where land for new development are limited or not available.

There are 66 Transit Stations identified for TPZ in Kuala Lumpur. These stations are mainly within the Kuala Lumpur city centre area, in identified District Commercial Centres and areas that are key employment centres. The 66 numbers of Transit Stations are:



Table 7.1 - Transit Planning Zones (TPZ)

1.	Desa Bakti	29.	Jalan Kia Peng	60.	Bandar Tun Razak
2.	Intan Baiduri	30.	Raja Chulan	61.	Taman Tayton View
3.	Metropolitan Kepong	31.	Bangunan LTAT	62.	Seri Permaisuri
4.	Taman Kepong	32.	Bukit Bintang	63.	Cheras
5.	Fadason	33.	Wisma SPS	64.	Taman Bukit Ria
		34.	Imbi Plaza	65.	Cochrane
6.	Kampung Batu	35.	Hang Tuah	66.	Masjid Alam Shah
7.	Batu Kantomen	36.	Pudu		·
8.	Taman Pelangi	37.	Kuala Lumpur		
9.	Bandar Dalam	38.	Jalan Parlimen		
10.	Padang Balang	39.	Ampang Park		
11.	Melati	40.	Taman U Thant		
12.	Taman Tasik Danau Kota	41.	Datok Keramat		
13.	Pusat Bandar Wangsa Maju	42.	Ampang Hilir		
14.	Taman Setapak Jaya	43.	KL Sentral		
15.	Taman Rainbow	44.	Brickfields		
		45.	Syed Putra		
16.	SentulTimur	46.	Bangsar		
17.	Sentul	47.	Abdullah Hukum		
18.	Matrade	48.	Kerinchi		
19.	Segambut Jaya	49.	Universiti		
20.	Sunrise	50.	Taman Bukit Angkasa		
21.	Taman Sri Hartamas	51.	Pantai Dalam		
22.	Jalan Dungun	52.	Petaling		
23.	Taman Bukit Damansara	53.	Bukit indah		
24.	Titiwangsa	54.	Salak Selatan		
25.	•	55.	Bandar Baru Bukit Jalil		
	Raja Muda Abd Aziz	56.	Desa Petaling		
26.	Kg. Baru	57.	Bandar Tasik Selatan		
27.	KLCC	58.	Desa Tun Razak		
28.	Jalan Stonor	59.	Desa Cheras		





TPZ is determined through the following steps:

- 1. Located around 400m radius of atransit station.
- 2. However, not all stations are TPZ.
  - Determiningwhetheranareaaround a station is a TPZ is dependent on a set of pre determined criteria.
  - In Kuala Lumpur, the TPZ are as listed in Table 7.1.
- 3. At the station, 250m from edge or end of platform (both side) are a TPZ priority zone.



TPZ Priority Zone (250m from end of platform)

#### **Planning Guide For Transit Planning Zone** 7.4

Land within the Transit Planning Zone shall take extra effort in the planning and design of its uses and activities as well as adopt good urban design in its development. The TPZ is a special zone where development shall be encouraged to be more intensified and supports the function of transit

All TPZ's shall be developed with the following key guidance:-

- Land use activities should be transit supportive;
- Mixed use activities are encouraged within TPZ with street level (b) activities to promote vibrancy and safety in the TPZ;
- Higher intensity development to support the transit system; and (c)
- (d) Good urban design that is pedestrian priority.

#### 7.4.1 **Transit Supportive Land Uses and Activities**

Transit supportive land uses and activities encourage transit use and increase the transportation network efficiency. Transit supportive land uses are employment generation uses (commercial, office, and industry), residential uses (high density), retail activities (pedestrian oriented street retail and shopping retail with strong pedestrian connections), civic use areas, entertainment, recreational and cultural facilities.

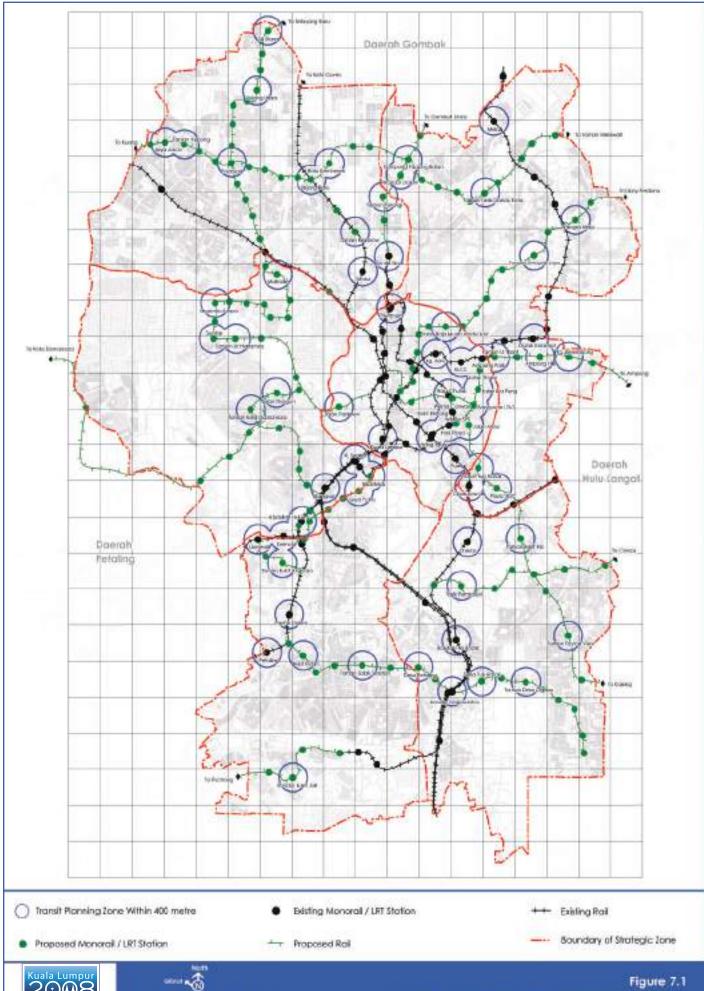
## **Characteristics Of Land Use Activities Around Transit Stations**

- 1. Permitted land use activities within 400m radius are activities that:
  - Produces high employment and high population;
  - · Generate high pedestrian volume;
  - High transit passengers;
  - · Generate reverse-flow movement;
  - Encourage and activate pedestrian and cycling;
  - Allow late night activities on working days and week ends.
- Land use activities that can generate off-peak travel movement must be given priority in order to generate passenger all the time and at the same time creates a safer built environment.

Land use activities that do not support transit are not encouraged in TPZ such as:

- Activities that generate high traffic volume but has low employment
- Utilise large land plots/size but has low intensity development such as Industrial areas; and
- Require a lot of parking spaces and high dependency on vehicles such as Hypermarket.









# 7.4.2 TPZ and the Land Use Zoning

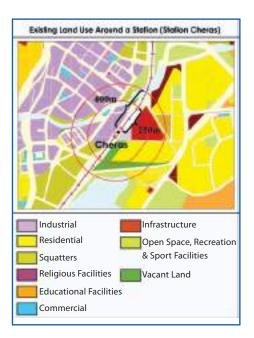
The DCP 1 - Land Use Zoning Map has prescribed activities in the TPZ that support the transit stations. These land use zones and activities are:-

Transit Support Characteristics	TPZ Supportive Land Use Zones
Employment Generation	City Centre Commercial
	District Centre Commercial
	Mixed Use Commercial
Residential with High Population	Residential 3
	Public Housing
Retail and Shopping	City Centre Commercial
	Commercial
	Mixed Use Residential
Public Recreational and	Public Institution
Cultural Facilities	Public Parks



Core Activity	Use Classes		
Residential	1.	Class A2 – Multiple dwelling units	
	2.	Class A4 – Hostel	
Commercial	1.	Class B1 – General Retail and Office	
	2.	Class B2 – Food and Drinks	
	3.	Class B3 – Entertainment	
	4. Class B5 – Theme Park		
	5.	Class B6 – Hotels	
Institutional	1.	Class D3 – Education	
	2.	Class D6 – Sports and Recreation	
	3.	Class D8 - Institution	









# 7.4.3 Development Intensity in TPZ

Development intensity within the TPZ is also enhanced and optimised to support the transit system. Hence densities and plot ratio permissible in these areas shall be higher than areas outside the TPZ so as to sustain high frequent transit services as well as to encourage the population to walk to the transit station.

## **MAXIMUM ALLOWABLE DENSITY IN TPZ**

- 1. Density within the TPZ is shown in the Development Control Plan 2: Density Map.
- 2. Allowable density in a TPZ is as follows:

Residential Zoning	Maximum Density Allowable
• R3	400 persons per acre
PUBLIC HOUSING	400 persons per acre



- Maluri District Centre Commercial
- Station Taman Miharja
- 3. Generally high density residential has been prescribed for the Residential Zone within the TPZ i.e. 400m radius of the transit station.

Zoning	Intensity		
	Density (ppa)	Plot Ratio	
District Centre Commercial	-	4	
Mixed Use Residential	-	4	
Residential 3	320	-	
Commercial	-	2	

## **MAXIMUM PERMISSIBLE PLOT RATIO IN TPZ**

## Plot Ratio Incentive In The TPZ

Any plot of land/part of land that falls within the 250m Priority TPZ and zoned for commercial, industrial or mixed development will be given an additional plot ratio of 0.5 to what its has been prescribed upon. This is to encourage such development to generate activities that support the transit stations, provide direct pedestrian linkages from the station to these activities and provide good urban design that promotes integrated planning and development.

Land Use	Incentive
Commercial	Additional 0.5 plot ratio for any land use either for commercial, industrial or mixed development in the TPZ
Industrial	
Mixed Development	

## Guide:

- (a) Refer to DCP 3 – Plot Ratio Map for permissible plot ratio of any land zone for commercial, industrial and mixed development as well residential land in City Centre area.
- (b) Refer against the SPZ 4 Transit Planning Zone to see, if land falls within the identified 66 TPZ in Kuala Lumpur.
- If land falls 100% within the 250m Priority TPZ, then (c) it can add the 0.5 additional plot ratio to its already prescribed plot ratio.
- (d) However, if only portion of the plot is in the Priority TPZ, then the plot ratio is only applicable to the portion of land that falls within the Priority TPZ

## Example:

Total Land Area = 4 acres. Permissible Plot Ratio = 1:4Land within Priority TPZ = 2.5 acres

Α [plot ratio 1:4] В [ plot ratio 1:4 + 0.5 (bonus)] = 1:4.5 [area A X 4] + [area B X 5] = new allowable plot area

Therefore Total Allowable FAR

Land outside Priority TPZ = 1.5 acres X 43560 X 4= 261,360 sq.ftLand within Priority TPZ = 2.5 acres X 43560 X 4.5 = 490,050 sq.ftTotal = 751,410 sq.ft.

Allocation of plot ratio incentive is not applicable for the following cases:

- Existing approved developments with higher plot ratio than permissible plot ratio
- New approved developments with higher plot ratio than permissible plot ratio + incentive.

# **Gazetted Plans** Attachment

# **Gazetted Plans**

2 Tamar	ng Baru n Kepong n Ibu Kota	4023 , 4001, 4001-1 4002, 4002-1
	lbu Kota	
3 Tamar		
		4003, 4003-1
4 Tamar	n Setapak	4004, 4004-1
5 Tamar	n Sri Segambut	4005
6 Tamar	n City & Perumahan PKNS	4006
7 Tamar	n Kok Doh	4007
8 Tamar	n Overseas Union	4008, 4008-1, 4008-2
9 Tamar	n Mutiara	4009
10 Tamar	n Cheras	4010
11 Tamar	n Taynton	4011, 4011-1
12 Tamar	n Salak South	4012
13 Tamar	n Salak South Timur	4012-1
14 Tamar	n Lian Hoe	4013
15 Tamar	n Midah	4014, 4014-1
16 Tamar	n Lee Yan Lian & Bangsar Park	4016
17 Tamar	n Kok Lian	4017, 4017-1
18 Tamar	n Bunga Raya	4018, 4018-1
19 Susun	atur bagi Kawasan Ampang Hilir	4019
20 Jalan T	「un Mohd Fuad	4022
21 Tamar	n Tun Dr. Ismail	4022-1, 4022-2, 4022-5
22 Sunga	i Penchala di Utara	4022-3, 4022-6
23 Jalan I	Burhanuddin Helmi di Utara	4022-4
24 Bekala	n Lektrik di Utara	4023-1
25 Bukit I	Maluri	4024-1
26 Sebah	agian dari Peringkat 1A dan kawasan perusahaan	4024-2
27 Bukit I	Bangsar	4025
28 Jalan <i>i</i>	Ara di Utara	4025-1, 4025-2
29 Jalan I	Maarof di Utara	4025-3, 4025-5
30 Tamar	n Maluri	4027, 4027-1
31 Tamar	n Melewar	4028
32 Lot 75	44 di Utara Lebuhraya KL-Karak di Timur	4029
33 Tamar	n Sentul Jaya (Datuk Senu)	4029
34 Tamar	n Yarl	4032, 4032-1
35 Tamar	n Goodwood, Taman Gembira, Taman Centimental	
&Tmn	Bukit Indah	4034, 4034-1, 4034-2, 4034-3
36 Tamar	n Rampai	4035, 4035-1
37 Tamar	n Eastern	4036

# **Gazetted Plans**

No	List	Plan No	
38	Taman Batu	4037	
39	Taman Cuepacs	4038	
40	Taman Setapak Jaya	4039	
41	Taman Batu View	4040	
42	Taman Wahyu	4042	
43	Taman Beringin	4043	
44	Taman Nanyang	4044-1	
45	Taman Petaling	4045	
46	Taman Rowther	4046	
47	Taman Pelangi	4047	
48	Sykt Kerjasama Polis	4048	

Source:- Kuala Lumpur City Hall



Table 1 - Category 1 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
1	-	Bukit Nanas	49	S.K. ST John 1 & 2	School
2	-	Bukit Nanas	49	S.M.K. ST. John	School
3	-	Bukit Nanas	49	R.C. Church	Church
4	-	Damansara	55	Mahkamah Syariah	Persekutuan
				Wilayah	Court
5	-	Damansara	59	Muzium Negara	Museum
6	-	Hang Jebat	56	Stadium Negara	Stadium
7	-	Maharajalela	69	Dewan Perhimpunan Cina	Assembly Hall
8	-	Raja	59	Pustaka Peringatan KL	Institution
9	-	Raja	04	Mahkamah Tinggi	
				Jenayah	High Court
10	-	Raja	01	Bangunan Sultan	
				Abdul Samad	High Court
11	29	Raja	59	Muzium K. Lumpur	Museum
12	-	Raja	03	Mahkamah Tinggi	High Court
13	-	Raja Muda Abdul	42	Institut Penyelidikan	
		Aziz		Perubatan	Institution
14	-	Stadium	56	Stadium Merdeka	Stadium
15	-	Sultan Hishamuddin	59	Balai Seni Negara	Vacant
16	-	Sultan Hishamuddin	55	Bangunan KTMB	Railway
					Station
17	-	Sultan Hishamuddin	59	Rumah Persekutuan, BPR,	
				J. Tanah & Galian	Land Office
18	-	Sultan Hishamuddin	55	Mahkamah Tinggi KL	High Court
19	-	Sultan Hishamuddin	59	Hotel Keretapi	
				Tanah Melayu	Hotel
20	-	Tun Perak	04	Masjid Jamek	Mosque
21	-	Parlimen	-	Bangunan Parlimen	Institution
22	-	Persiaran Mahameru	-	Bangunan Carcosa Dan	
				Seri Negara (King's House)	Hotel
23	-	Persiaran Sultan			
		Salahuddin	-	Tugu Negara	Monumen
24	-	Raja	-	Tapak Tiang Bendera	
				Negara Dinaikkan Dan	
				Union Jack Diturunkan	
				Di Padang Kelab Selangor	
				(Dataran Merdeka)	Square

Table 2 - Category 2 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
1	-	Rozario	-	Asrama Vivekananda	Hostel
2	235	TAR	37	-	Shophouse
3	357	TAR	37	-	Shophouse
4	355	TAR	37	-	Shophouse
5	353	TAR	37	-	Shophouse
6	351	TAR	37	-	Shophouse
7	349	TAR	37	-	Shophouse
8	347	TAR	37	-	Shophouse
9	197	TAR	37	-	Shophouse
10	237	TAR	37	-	Shophouse
11	229 & 331	TAR	37	-	Shophouse
12	-	TAR	37	-	Shophouse
13	-	TAR	37	-	Shophouse
14	-	TAR	37	-	Shophouse
15	362	TAR	37	-	Shophouse
16	360	TAR	37	-	Shophouse
17	359	TAR	37	-	Shophouse
18	13	TAR	06	-	Shophouse
19	15	TAR	06	-	Shophouse
20	17	TAR	06	-	Shophouse
21	19	TAR	06	-	Shophouse
22	1	TAR	06	-	Shophouse
23	5	TAR	06	-	Shophouse
24	-	Masjid India	40	Masjid India	Mosque
25	7	TAR	06	-	Shophouse
26	9	TAR	06	-	Shophouse
27	11	TAR	06	-	Shophouse
28	-	Bukit Nanas	49	Asrama Cahaya,	
				Infant Jesus Convent	Hostel
29	-	Bukit Nanas	49	S.K. Convent Bkt. Nenas 1 & 2	School
30	-	Bukit Nanas	49	SMK Convent Bukit Nanas	School
31	-	Raja Chulan	49	Muzium Telekom	Museum
32	-	Hang Kasturi	31	Central Market	Handicraft
					Centre
33	-	Tun H. S. Lee	30	-	Temple
34	-	Medan Pasar	14	-	Monument
35	-	Raja	51	Gereja ST. Mary	Church
36	-	Raja	59	Royal Selangor Club	Private Club

Table 2 - Category 2 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
37	-	Perdana	59	Masjid Negara	Mosque
38	-	Tun Sambathan	55	Arkib Negara Malaysia	Institution
39	-	Tun Sambathan	55	Gereja Katolik Holy Rosary	Church
40	6	Balai Polis	26	Gudwara Sahib Polis	Police Station
41	-	Tun H. S. Lee	27	Taman Budaya	
				(Old Victorian Institute)	Theatre
42	-	Maharajalela	56	-	Temple
43	-	Tun H. S. Lee	22	-	Temple
44	163	Tun H. S. Lee	29	Sri Mariamman Temple	Temple
45	-	Hang Tuah	56	Sekolah Victoria	School
46	-	Changkat Stadium	56	Persatuan Sukan Chin Woo	Stadium
47	3	Hang Jebat	56	Gereja Gospel Hall	Church
48	-	Maharajalela	56	-	Temple
49	-	Raja Laut	46	SK Lelaki Jln Batu /	
				SRK Pendidikan Khas	
				(Penglihan)	School
50	-	Persiaran Raja Chulan	19	Istana Pahang	Residential
51	-	Persiaran Raja Chulan	-	Gereja St. Andrew	Church
52	-	Robertson	56	-	Church
53	-	Lorong Raja Chulan 1	-	-	Residential
54	5	Wisma Putra	69	Dewan Bahasa	Institution
55	-	Kg Attap	69	Dewan Badminton	Badminton
				Selangor	Stadium
56	10	Thambipillay	-	-	Temple
57	-	Wesley	56	Sekolah Menengah	
				Methodist	School
58	-	Sultan Abdul Samad	-	S.R.K (P) Methodist 1 & 2	School
59	-	Berhala	-	Buddhist Maha Vihara	Temple
60	-	Scott	-		Temple
61	-	Sultan Abdul Samad	-	Gereja Lutheran	Church

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
1	87 - 89	lpoh	47	-	Shophouse
2	85	lpoh	47	-	Shophouse
3	83	lpoh	47	-	Shophouse
4	81	lpoh	47	-	Shophouse
5	79	lpoh	47	-	Shophouse
6	77	lpoh	47	-	Shophouse
7	75	lpoh	47	-	Shophouse
8	73	lpoh	47	-	Shophouse
9	71	lpoh	47	-	Shophouse
10	69	lpoh	47	-	Shophouse
11	67	lpoh	47	-	Shophouse
12	65	lpoh	47	-	Shophouse
13	63	lpoh	47	-	Shophouse
14	61	lpoh	47	-	Shophouse
15	59	lpoh	47	-	Shophouse
16	57	lpoh	47	-	Shophouse
17	115-119	lpoh	47	-	Shophouse
18	55	lpoh	47	-	Shophouse
19	109	lpoh	47	-	Shophouse
20	1	Pahang	47	-	Shophouse
21	2&3	Pahang	47	-	Shophouse
22	107	lpoh	47	-	Shophouse
23	103	lpoh	47	-	Shophouse
24	101	lpoh	47	-	Shophouse
25	99	lpoh	47	-	Shophouse
26	181	TAR	37	-	Shophouse
27	163	TAR	37	-	Shophouse
28	-	TAR	36	-	Shophouse
29	153	TAR	36	-	Shophouse
30	151	TAR	36	-	Shophouse
31	-	TAR	36	-	Shophouse
32	147	TAR	36	-	Shophouse
33	145	TAR	36	-	Shophouse
34	143	TAR	36	-	Shophouse
35	141	TAR	36	-	Shophouse
36	129	TAR	36	-	Shophouse
37	-	TAR	36	-	Shophouse
38	-	TAR	36	-	Shophouse
					-

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
39	-	TAR	36	-	Shophouse
40	-	Dang Wangi	40	-	Shophouse
41	-	Dang Wangi	40	-	Shophouse
42	-	Dang Wangi	40	-	Shophouse
43	20	Dang Wangi	40	-	Shophouse
44	-	Dang Wangi	40	-	Shophouse
45	-	Dang Wangi	40	-	Shophouse
46	-	Dang Wangi	40	-	Shophouse
47	38	Dang Wangi	40	WISMA MGIC	Shophouse
48	47	TAR	33	-	Shophouse
49	49	TAR	33	-	Shophouse
50	51	TAR	33	-	Shophouse
51	95	TAR	33	-	Shophouse
52	97	TAR	33	-	Shophouse
53	99	TAR	33	-	Shophouse
54	-	TAR	33	-	Shophouse
55	-	Tun Perak	04	-	Shophouse
56	-	Tun Perak	04	-	Shophouse
57	-	Melayu	06	-	Shophouse
58	-	Melayu	06	-	Shophouse
59	-	Melayu	06	-	Shophouse
60	-	Melayu	06	-	Shophouse
61	-	Melayu	33	-	Shophouse
62	69,71,73	Ampang	49	-	Shophouse
63	61	Ampang	49	-	Shophouse
64	55	Ampang	49	Wisma Eng Choon	Shophouse
65	53	Ampang	49	-	Shophouse
66	51	Ampang	49	-	Shophouse
67	49	Ampang	49	-	Shophouse
68	37	Ampang	49	-	Shophouse
69	35	AMPANG	49	-	Shophouse
70	17,19,21	Gereja	49	-	Residential
71	528	Bukit Nanas	49	-	Institution
72	58	Tun Perak	10	-	Shophouse
73	32	Lebuh Ampang	08	-	Shophouse
74	34	Lebuh Ampang	08	-	Shophouse
75	36	Lebuh Ampang	08	-	Shophouse

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
76	42	Lebuh Ampang	08	-	Shophouse
77	44	Lebuh Ampang	08	-	Shophouse
78	46	Lebuh Ampang	08	-	Shophouse
79	48	Lebuh Ampang	08	-	Shophouse
80	50	Lebuh Ampang	08	-	Shophouse
81	60	Tun Perak	10	-	Shophouse
82	10	Lebuh Ampang	08	-	Shophouse
83	12	Lebuh Ampang	08	-	Shophouse
84	-	Tun Perak	08	ORIENTAL BUILDING	Shophouse
85	62	Tun Perak	10	-	Shophouse
86	64	Tun Perak	10	-	Shophouse
87	66	Tun Perak	10	-	Shophouse
88	68	Tun Perak	10	-	Shophouse
89	36	Hang Lekiu	10	-	Shophouse
90	38	Hang Lekiu	10	-	Shophouse
91	40	Hang Lekiu	10	-	Shophouse
92	42	Hang Lekiu	10	-	Shophouse
93	44	Hang Lekiu	10	-	Shophouse
94	46	Hang Lekiu	10	-	Shophouse
95	48	Hang Lekiu	10	-	Shophouse
96	50	Hang Lekiu	10	BANGUNAN PUBLIC	Shophouse
97	58	Hang Lekiu	10	-	Shophouse
98	60	Hang Lekiu	10	-	Shophouse
99	2&4	Tun H. S. Lee	10	-	Shophouse
100	6	Tun H. S. Lee	10	-	Shophouse
101	8	Tun H. S. Lee	10	-	Shophouse
102	16A	Tun H. S. Lee	10	-	Shophouse
103	20,22,24	Tun H. S. Lee	10	-	Shophouse
104	26	Tun H. S. Lee	10	-	Shophouse
105	28	Tun H. S. Lee	10	-	Shophouse
106	41	Hang Lekiu	50	BGN PERS. HOKKIEN	
				SELANGOR	Shophouse
107	30-36	Tun H. S. Lee	10	-	Shophouse
108	38-40	Tun H. S. Lee	10	-	Shophouse
109	42	Tun H. S. Lee	10	-	Shophouse
110	-	Tun H. S. Lee	09	-	Shophouse
111	-	Tun H. S. Lee	09	-	Shophouse
112	37	Tun H. S. Lee	09	-	Shophouse
113	37	Hang Lekiu	50	-	Shophouse

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
114	33	Tun H. S. Lee	09	-	Shophouse
115	31	Tun H. S. Lee	09	-	Shophouse
116	29	Tun H. S. Lee	09	-	Shophouse
117	25	Tun H. S. Lee	09	-	Shophouse
118	23	Tun H. S. Lee	09	-	Shophouse
119	21	Tun H. S. Lee	09	-	Shophouse
120	13	Tun H. S. Lee	09	-	Shophouse
121	89B	Lebuh Ampang	09	-	Shophouse
122	87	Lebuh Ampang	09	-	Shophouse
123	83	Lebuh Ampang	09	-	Shophouse
124	81	Lebuh Ampang	09	-	Shophouse
125	77-79	Lebuh Ampang	09	-	Shophouse
126	75	Lebuh Ampang	09	-	Residential
127	73	Lebuh Ampang	09	-	Shophouse
128	71	Lebuh Ampang	09	-	Shophouse
129	65	Lebuh Ampang	09	WISMA KOPATHA	Shophouse
130	54	Tun Perak	10	-	Shophouse
131	57	Lebuh Ampang	09	-	Shophouse
132	55	Lebuh Ampang	09	-	Shophouse
133	51-49	Lebuh Ampang	09	WISMA MARIAMMAN	Shophouse
134	47	Lebuh Ampang	09	-	Shophouse
135	45-44	Tun Perak	09	-	Shophouse
136	44	Tun Perak	09	-	Shophouse
137	56	Tun Perak	10	-	Shophouse
138	16	Lebuh Ampang	08	-	Shophouse
139	18	Lebuh Ampang	08	-	Shophouse
140	20	Lebuh Ampang	08	-	Shophouse
141	22	Lebuh Ampang	08	-	Shophouse
142	24	Lebuh Ampang	08	-	Shophouse
143	26	Lebuh Ampang	08	-	Shophouse
144	28	Lebuh Ampang	08	-	Shophouse
145	30	Lebuh Ampang	08	-	Shophouse
146	-	Pudu	19	-	Shophouse
147	-	Pudu	19	-	Shophouse
148	-	Pudu	19	-	Shophouse
149	-	Pudu	19	WISMA AVS	Shophouse
150	-	Pudu	19	-	Shophouse
151	-	Pudu	19	-	Shophouse

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
152	-	Pudu	19	-	Shophouse
153	-	Pudu	19	-	Shophouse
154	-	Pudu Lama	19	-	Shophouse
155	-	Pudu Lama	19	-	Shophouse
156	-	Pudu Lama	19	-	Shophouse
157	-	Pudu Lama	19	-	Residential
158	-	Pudu Lama	19	-	Residential
159	-	Pudu Lama	19	-	Shophouse
160	-	Pudu Lama	19	-	Shophouse
161	-	Pudu Lama	19	-	Shophouse
162	-	Pudu Lama	19	-	Residential
163	18	Pudu Lama	19	-	Shophouse
164	20	Pudu Lama	19	-	Shophouse
165	22-24-26	Pudu Lama	19	-	Residential
166	28-30	Pudu Lama	19	-	Shophouse
167	32	Pudu Lama	19	-	Residential
168	36,38,40	Pudu Lama	19	-	Residential
169	42	Pudu Lama	19	-	Residential
170	-	Pudu	19	-	Shophouse
171	38	Hang Kasturi	30	-	Shophouse
172	-	Tun Perak	11	-	Shophouse
173	-	Tun Perak	11	WISMA LAU CHONG	Shophouse
174	34	Hang Kasturi	30	-	Shophouse
175	34	Hang Kasturi	30	-	Shophouse
176	24	Hang Kasturi	30	-	Shophouse
177	20	Lebuh Pudu	30	-	Shophouse
178	5 & 7	Lebuh Pudu	30	-	Shophouse
179	9	Lebuh Pudu	30	-	Shophouse
180	11 - 13	Leboh Pudu	30	-	Shophouse
181	134	Tun H. S. Lee	21	-	Shophouse
182	50	Hang Kasturi	30	-	Shophouse
183	130	Tun H. S. Lee	21	-	Shophouse
184	17	Leboh Pudu	21	-	Shophouse
185	3	Petaling	21	-	Shophouse
186	50	Hang Kasturi	30	-	Shophouse
187	9 - 11	Petaling	21	-	Shophouse
188	13	Petaling	21	-	Shophouse
189	17	Petaling	21	-	Shophouse

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
190	21	Petaling	21	-	Shophouse
191	101	Tun H. S. Lee	17	-	Shophouse
192	99	Tun H. S. Lee	17	-	Shophouse
193	48	Hang Kasturi	30	-	Shophouse
194	93	Tun H. S. Lee	17	-	Shophouse
195	89	Tun H. S. Lee	17	-	Shophouse
196	87	Tun H. S. Lee	15	-	Shophouse
197	85	Tun H. S. Lee	15	-	Shophouse
198	83	Tun H. S. Lee	15	-	Shophouse
199	81	Tun H. S. Lee	15	-	Shophouse
200	79	Tun H. S. Lee	15	-	Shophouse
201	77	Tun H. S. Lee	15	-	Shophouse
202	46	Hang Kasturi	30	-	Shophouse
203	25	Medan Pasar	15	-	Shophouse
204	23	Medan Pasar	15	-	Shophouse
205	15	Medan Pasar	15	-	Shophouse
206	13	Medan Pasar	15	-	Shophouse
207	11	Medan Pasar	15	-	Shophouse
208	9	Medan Pasar	15	-	Shophouse
209	7	Medan Pasar	15	-	Shophouse
210	44	Hang Kasturi	30	-	Shophouse
211	5	Medan Pasar	15	-	Shophouse
212	3	Medan Pasar	15	-	Shophouse
213	1	Medan Pasar	15	-	Shophouse
214	-	Hang Kasturi	17	-	Shophouse
215	-	Hang Kasturi	17	-	Shophouse
216	-	Hang Kasturi	17	-	Shophouse
217	12	Hang Kasturi	17	-	Shophouse
218	14 & 16	Hang Kasturi	17	-	Shophouse
219	42	Hang Kasturi	30	-	Shophouse
220	18	Hang Kasturi	17	-	Shophouse
221	20	Hang Kasturi	17	-	Shophouse
222	22	Hang Kasturi	17	-	Shophouse
223	-	Hang Kasturi	31	CENTRAL MARKET ANNEXE	Shophouse
224	2	Medan Pasar	14	-	Shophouse
225	4	Medan Pasar	14	-	Shophouse
226	6	Medan Pasar	14	-	Shophouse
227	8	Medan Pasar	14	-	Shophouse
228	12	Medan Pasar	14	-	Shophouse

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
229	14	Medan Pasar	14	KOLEJ GEMILANG	Shophouse
230	-	Lebuh Ampang	11	-	Shophouse
231	Lot 13	Lebuh Ampang	12	WISMA MBSB	Shophouse
232	39	Silang	20	-	Shophouse
233	33	Silang	20	-	Shophouse
234	3 - 5	Lrg Pudu Lane	20	-	Shophouse
235	7 - 9	Lrg Pudu Lane	20	-	Shophouse
236	39	Lebuh Pudu	20	-	Shophouse
237	37	Lebuh Pudu	20	-	Shophouse
238	23	Lebuh Pudu	20	-	Shophouse
239	71	Tun H. S. Lee	12	-	Shophouse
240	69	Tun H. S. Lee	12	-	Shophouse
241	65	Tun H. S. Lee	12	-	Shophouse
242	61	Tun H. S. Lee	12	-	Shophouse
243	41	Lebuh Ampang	12	-	Shophouse
244	57	Tun H. S. Lee	12	-	Shophouse
245	55	Tun H. S. Lee	12	-	Shophouse
246	53	Tun H. S. Lee	12	-	Shophouse
247	52	Tun H. S. Lee	13	-	Shophouse
248	54	Tun H. S. Lee	13	-	Shophouse
249	56 - 58	Tun H. S. Lee	13	-	Shophouse
250	60	Tun H. S. Lee	13	-	Shophouse
251	62 - 64	Tun H. S. Lee	13	-	Shophouse
252	39	Lebuh Ampang	12	-	Shophouse
253	66	Tun H. S. Lee	13	-	Shophouse
254	70	Tun H. S. Lee	13	-	Shophouse
255	72	Tun H. S. Lee	13	-	Shophouse
256	76	Tun H. S. Lee	13	-	Shophouse
257	78	Tun H. S. Lee	13	-	Shophouse
258	80	Tun H. S. Lee	13	-	Shophouse
259	37	Lebuh Ampang	12	-	Shophouse
260	90	Tun Tan Siew Sin	16	-	Shophouse
261	2	Hang Lekiu	13	-	Shophouse
262	10 - 18	Hang Lekiu	13	-	Shophouse
263	20 - 26	Hang Lekiu	13	-	Shophouse
264	24	Silang	50	-	Shophouse
265	26	Silang	50	-	Shophouse
266	40	Silang	50	-	Shophouse

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
267	13	Silang	18	-	Shophouse
268	94	Petaling	18	-	Shophouse
269	96	Petaling	18	-	Shophouse
270	98	Petaling	18	-	Shophouse
271	100	Petaling	18	-	Shophouse
272	27	Lebuh Ampang	12	-	Shophouse
273	122	Petaling	18	-	Shophouse
274	124	Petaling	18	-	Shophouse
275	8 - 14	Lebuh Pudu	18	-	Shophouse
276	22	Lebuh Pudu	18	-	Shophouse
277	-	Petaling	20	WISMA FUI CHUI	Shophouse
278	2	Panggong	25	-	Shophouse
279	4	Panggong	25	-	Shophouse
280	12	Panggong	25	-	Shophouse
281	14	Panggong	25	-	Shophouse
282	16	Panggong	25	-	Shophouse
283	18	Panggong	25	-	Shophouse
284	-	Panggong	25	-	Shophouse
285	-	Panggong	25	-	Shophouse
286	-	Panggong	25	-	Shophouse
287	-	Panggong	25	-	Shophouse
288	-	Panggong	25	-	Shophouse
289	-	Panggong	25	-	Shophouse
290	-	Petaling	25	-	Shophouse
291	-	Petaling	25	-	Shophouse
292	-	Petaling	25	-	Shophouse
293	-	Petaling	25	-	Shophouse
294	-	Petaling	25	-	Shophouse
295	-	Petaling	25	-	Shophouse
296	-	Petaling	25	-	Shophouse
297	-	Petaling	25	-	Shophouse
298	-	Petaling	25	-	Shophouse
299	-	Petaling	25	-	Shophouse
300	-	Petaling	25	-	Shophouse
301	-	Petaling	25	-	Shophouse
302	-	Petaling	25	-	Shophouse
303	-	Petaling	25	-	Shophouse
304	-	Petaling	25	-	Shophouse

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
305	160-162	Petaling	66	-	Shophouse
306	154	Petaling	66	-	Shophouse
307	152	Petaling	66	-	Shophouse
308	150	Petaling	66	-	Shophouse
309	142	Petaling	66	-	Shophouse
310	29	Sultan	66	-	Shophouse
311	27	Sultan	66	-	Shophouse
312	25	Sultan	66	-	Shophouse
313	23	Sultan	66	-	Shophouse
314	21	Sultan	66	-	Shophouse
315	-	Tun H. S. Lee	28	PLAZA WARISAN	Shophouse
316	104	Petaling	66	-	Shophouse
317	106	Petaling	66	-	Shophouse
318	108	Petaling	66	-	Shophouse
319	110	Petaling	66	-	Shophouse
320	112	Petaling	66	-	Shophouse
321	114	Petaling	66	-	Shophouse
322	122	Petaling	66	-	Shophouse
323	124	Petaling	66	-	Shophouse
324	-	Tun H. S. Lee	24	UO SUPERSTORE	Shophouse
325	126	Petaling	66	-	Shophouse
326	128	Petaling	66	-	Shophouse
327	130	Petaling	66	-	Shophouse
328	132	Petaling	66	-	Shophouse
329	134	Petaling	66	-	Shophouse
330	-	Hang Kasturi	29	-	Shophouse
331	-	Hang Kasturi	29	-	Shophouse
332	-	Hang Kasturi	29	-	Shophouse
333	-	Hang Lekir	30	-	Shophouse
334	49	Petaling	22	-	Shophouse
335	51	Petaling	22	-	Shophouse
336	59	Petaling	22	-	Shophouse
337	91	Petaling	22	-	Shophouse
338	93	Petaling	22	-	Shophouse
339	95	Petaling	22	-	Shophouse
340	97	Petaling	22	-	Shophouse
341	101	Petaling	22	-	Shophouse
342	103	Petaling	22	-	Shophouse

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
343	111	Petaling	22	-	Shophouse
344	18	Sultan	22	-	Shophouse
345	-	Sultan	22	-	Shophouse
346	-	Sultan	22	-	Shophouse
347	-	Sultan	22	-	Shophouse
348	210	Tun H. S. Lee	22	-	Shophouse
349	204	Tun H. S. Lee	22	-	Shophouse
350	202	Tun H. S. Lee	22	-	Shophouse
351	200	Tun H. S. Lee	22	-	Shophouse
352	198	Tun H. S. Lee	22	-	Shophouse
353	196	Tun H. S. Lee	22	-	Shophouse
354	194	Tun H. S. Lee	22	-	Shophouse
355	192	Tun H. S. Lee	22	-	Shophouse
356	190	Tun H. S. Lee	22	-	Shophouse
357	31	Petaling	21	-	Shophouse
358	188	Tun H. S. Lee	22	-	Shophouse
359	184	Tun H. S. Lee	22	-	Shophouse
360	182	Tun H. S. Lee	22	-	Shophouse
361	180	Tun H. S. Lee	22	-	Shophouse
362	176	Tun H. S. Lee	22	-	Shophouse
363	33	Petaling	21	-	Shophouse
364	150	Tun H. S. Lee	21	-	Shophouse
365	148	Tun H. S. Lee	21	-	Shophouse
366	146	Tun H. S. Lee	21	-	Shophouse
367	144	Tun H. S. Lee	21	-	Shophouse
368	142	Tun H. S. Lee	21	-	Shophouse
369	421	Tun H. S. Lee	30	-	Shophouse
370	422	Tun H. S. Lee	30	-	Shophouse
371	-	Tun H. S. Lee	30	-	Shophouse
372	-	Tun H. S. Lee	30	-	Shophouse
373	-	Tun H. S. Lee	30	-	Shophouse
374	-	Tun H. S. Lee	29	LEE RUBBER BUILDING	Shophouse
375	39	Petaling	21	-	Shophouse
376	-	Tun H. S. Lee	29	PLAZA FIRST NATIONWIDE	Shophouse
377	-	Tun H. S. Lee	29	-	Shophouse
378	107	Tun H. S. Lee	29	-	Shophouse
379	109	Tun H. S. Lee	29	-	Shophouse
380	113	Tun H. S. Lee	29	-	Shophouse

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
381	115	Tun H. S. Lee	29	-	Shophouse
382	119	Tun H. S. Lee	29	-	Shophouse
383	121	Tun H. S. Lee	29	-	Shophouse
384	123	Tun H. S. Lee	29	-	Shophouse
385	125	Tun H. S. Lee	29	-	Shophouse
386	-	Hang Kasturi	29	-	Shophouse
387	87	Sultan	64	-	Shophouse
388	61	Sultan	56	-	Shophouse
389	57	Sultan	56	-	Shophouse
390	42	Petaling	23	-	Shophouse
391	55	Sultan	56	-	Shophouse
392	53	Sultan	56	-	Shophouse
393	46	Petaling	23	-	Shophouse
394	48	Petaling	23	-	Shophouse
395	50	Petaling	23	-	Shophouse
396	62	Petaling	23	-	Shophouse
397	74	Petaling	23	-	Shophouse
398	80	Petaling	23	-	Shophouse
399	82 & 84	Petaling	23	-	Shophouse
400	86	Petaling	23	-	Shophouse
401	88	Petaling	23	-	Shophouse
402	18	Petaling	20	-	Shophouse
403	90	Petaling	23	-	Shophouse
404	92 - 96	Petaling	23	-	Shophouse
405	98	Petaling	23	-	Shophouse
406	100	Petaling	23	-	Shophouse
407	102	Petaling	23	-	Shophouse
408	20	Sultan	23	-	Shophouse
409	22	Sultan	23	-	Shophouse
410	24	Sultan	23	-	Shophouse
411	26	Sultan	23	-	Shophouse
412	28	Sultan	23	-	Shophouse
413	34	Sultan	23	-	Shophouse
414	36	Sultan	23	-	Shophouse
415	38	Sultan	23	-	Shophouse
416	40	Sultan	23	-	Shophouse
417	42	Sultan	23	-	Shophouse
418	44	Sultan	23	-	Shophouse
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Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
419	46	Sultan	23	-	Shophouse
420	48	Sultan	23	-	Shophouse
421	50	Sultan	23	-	Shophouse
422	22	Petaling	20	-	Shophouse
423	52	Sultan	23	-	Shophouse
424	54	Sultan	23	-	Shophouse
425	56	Sultan	23	-	Shophouse
426	58	Sultan	23	-	Shophouse
427	70	Sultan	23	-	Shophouse
428	72	Sultan	23	-	Shophouse
429	74	Sultan	23	-	Shophouse
430	76	Sultan	23	-	Shophouse
431	78	Sultan	23	-	Shophouse
432	25 - 27	Hang Lekir	23	-	Shophouse
433	23	Hang Lekir	23	-	Shophouse
434	21	Hang Lekir	23	-	Shophouse
435	19	Hang Lekir	23	-	Shophouse
436	28	Hang Lekir	20	-	Shophouse
437	32	Hang Lekir	20	-	Shophouse
438	38 - 40	Sultan	20	-	Shophouse
439	27	Cheng Lock	20	-	Shophouse
440	25	Cheng Lock	20	-	Shophouse
441	32	Petaling	20	-	Shophouse
442	23	Cheng Lock	20	-	Shophouse
443	21	Cheng Lock	20	-	Shophouse
444	17	Cheng Lock	20	-	Shophouse
445	121	Sultan	64	-	Shophouse
446	119	Sultan	64	-	Shophouse
447	34, 36 & 38	Petaling	20	-	Shophouse
448	103	Sultan	64	-	Shophouse
449	159	Ipoh	47	-	Shophouse
450	-	Sultan	56	-	Shophouse
451	-	Sultan	56	-	Shophouse
452	51	Sultan	56	-	Shophouse
453	51	Sultan	56	-	Shophouse
454	304	Pudu	53	-	Shophouse
455	312	Pudu	53	-	Shophouse
456	316	Pudu	53	-	Shophouse
	I				

Table 3 - Category 3 Conservation Buildings

457         320         Pudu         53         -         Shophouse           458         322         Pudu         53         -         Shophouse           459         324         Pudu         53         -         Shophouse           460         326         Pudu         56         -         Shophouse           461         NO.186         Pudu         56         -         Shophouse           462         NO.186         Pudu         54         -         Shophouse           463         222         Pudu         54         -         Shophouse           464         224         Pudu         54         -         Shophouse           465         226         Pudu         54         -         Shophouse           466         228         Pudu         54         -         Shophouse           467         232         Pudu         54         -         Shophouse           469         244         Pudu         54         -         Shophouse           470         246         Pudu         54         -         Shophouse           471         250         Pudu         54	No.	House No.	Street's Name	Section	Building's Name	Building Type
459         324         Pudu         53         -         Shophouse           460         326         Pudu         53         -         Shophouse           461         NO.186         Pudu         56         -         Shophouse           462         NO.186         Pudu         54         -         Shophouse           463         222         Pudu         54         -         Shophouse           464         224         Pudu         54         -         Shophouse           465         226         Pudu         54         -         Shophouse           466         228         Pudu         54         -         Shophouse           467         232         Pudu         54         -         Shophouse           469         244         Pudu         54         -         Shophouse           470         246         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           472         252         Pudu         54         -         Shophouse           473         256         Pudu         54	457	320	Pudu	53	-	Shophouse
460         326         Pudu         53         -         Shophouse           461         NO.184         Pudu         56         -         Shophouse           462         NO.186         Pudu         56         -         Shophouse           463         222         Pudu         54         -         Shophouse           464         224         Pudu         54         -         Shophouse           465         226         Pudu         54         -         Shophouse           466         228         Pudu         54         -         Shophouse           467         232         Pudu         54         -         Shophouse           468         240         Pudu         54         -         Shophouse           469         244         Pudu         54         -         Shophouse           470         246         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           472         252         Pudu         54         -         Shophouse           473         256         Pudu         54	458	322	Pudu	53	-	Shophouse
461         NO.184         Pudu         56         -         Shophouse           462         NO.186         Pudu         56         -         Shophouse           463         222         Pudu         54         -         Shophouse           464         224         Pudu         54         -         Shophouse           465         226         Pudu         54         -         Shophouse           466         228         Pudu         54         -         Shophouse           467         232         Pudu         54         -         Shophouse           468         240         Pudu         54         -         Shophouse           469         244         Pudu         54         -         Shophouse           470         246         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           472         252         Pudu         54         -         Shophouse           473         256         Pudu         54         -         Shophouse           475         270         Pudu         54	459	324	Pudu	53	-	Shophouse
462         NO.186         Pudu         56         -         Shophouse           463         222         Pudu         54         -         Shophouse           464         224         Pudu         54         -         Shophouse           465         226         Pudu         54         -         Shophouse           466         228         Pudu         54         -         Shophouse           467         232         Pudu         54         -         Shophouse           468         240         Pudu         54         -         Shophouse           469         244         Pudu         54         -         Shophouse           470         246         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           472         252         Pudu         54         -         Shophouse           473         256         Pudu         54         -         Shophouse           475         270         Pudu         54         -         Shophouse           475         270         Pudu         53 <td< td=""><td>460</td><td>326</td><td>Pudu</td><td>53</td><td>-</td><td>Shophouse</td></td<>	460	326	Pudu	53	-	Shophouse
463         222         Pudu         54         -         Shophouse           464         224         Pudu         54         -         Shophouse           465         226         Pudu         54         -         Shophouse           466         228         Pudu         54         -         Shophouse           467         232         Pudu         54         -         Shophouse           468         240         Pudu         54         -         Shophouse           469         244         Pudu         54         -         Shophouse           470         246         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           472         252         Pudu         54         -         Shophouse           473         256         Pudu         54         -         Shophouse           475         270         Pudu         54         -         Shophouse           477         294         Pudu         53         -<	461	NO.184	Pudu	56	-	Shophouse
464         224         Pudu         54         -         Shophouse           465         226         Pudu         54         -         Shophouse           466         228         Pudu         54         -         Shophouse           467         232         Pudu         54         -         Shophouse           468         240         Pudu         54         -         Shophouse           470         246         Pudu         54         -         Shophouse           470         246         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           472         252         Pudu         54         -         Shophouse           473         256         Pudu         54         -         Shophouse           474         258         Pudu         54         -         Shophouse           475         270         Pudu         53         -         Shophouse           477         294         Pudu         53         -<	462	NO.186	Pudu	56	-	Shophouse
465         226         Pudu         54         -         Shophouse           466         228         Pudu         54         -         Shophouse           467         232         Pudu         54         -         Shophouse           468         240         Pudu         54         -         Shophouse           469         244         Pudu         54         -         Shophouse           470         246         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           472         252         Pudu         54         -         Shophouse           473         256         Pudu         54         -         Shophouse           474         258         Pudu         54         -         Shophouse           475         270         Pudu         54         -         Shophouse           477         294         Pudu         53         -         Shophouse           478         296         Pudu         53         -<	463	222	Pudu	54	-	Shophouse
466         228         Pudu         54         -         Shophouse           467         232         Pudu         54         -         Shophouse           468         240         Pudu         54         -         Shophouse           469         244         Pudu         54         -         Shophouse           470         246         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           472         252         Pudu         54         -         Shophouse           473         256         Pudu         54         -         Shophouse           474         258         Pudu         54         -         Shophouse           475         270         Pudu         54         -         Shophouse           476         292         Pudu         53         -         Shophouse           477         294         Pudu         53         -         Shophouse           479         256         Ckt Thambi Dollah         52	464	224	Pudu	54	-	Shophouse
467         232         Pudu         54         -         Shophouse           468         240         Pudu         54         -         Shophouse           469         244         Pudu         54         -         Shophouse           470         246         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           472         252         Pudu         54         -         Shophouse           473         256         Pudu         54         -         Shophouse           474         258         Pudu         54         -         Shophouse           475         270         Pudu         54         -         Shophouse           476         292         Pudu         53         -         Shophouse           477         294         Pudu         53         -         Shophouse           478         296         Pudu         53         -         Shophouse           480         254         Ckt Thambi Dollah         52         -         Shophouse           481         296         Pudu         54	465	226	Pudu	54	-	Shophouse
468         240         Pudu         54         -         Shophouse           469         244         Pudu         54         -         Shophouse           470         246         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           472         252         Pudu         54         -         Shophouse           473         256         Pudu         54         -         Shophouse           474         258         Pudu         54         -         Shophouse           475         270         Pudu         54         -         Shophouse           476         292         Pudu         53         -         Shophouse           477         294         Pudu         53         -         Shophouse           479         256         Ckt Thambi Dollah         52         -         Shophouse           481         296         Pudu         54         -         Shophouse           481         296         Pudu         54         -         Shophouse           482         294         Pudu         54	466	228	Pudu	54	-	Shophouse
469         244         Pudu         54         -         Shophouse           470         246         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           472         252         Pudu         54         -         Shophouse           473         256         Pudu         54         -         Shophouse           474         258         Pudu         54         -         Shophouse           475         270         Pudu         54         -         Shophouse           476         292         Pudu         53         -         Shophouse           477         294         Pudu         53         -         Shophouse           479         256         Ckt Thambi Dollah         52         -         Shophouse           480         254         Ckt Thambi Dollah         52         -         Shophouse           481         296         Pudu         54         -         Shophouse           482         294         Pudu         54         -         Shophouse           483         11         Brunei Barat	467	232	Pudu	54	-	Shophouse
470         246         Pudu         54         -         Shophouse           471         250         Pudu         54         -         Shophouse           472         252         Pudu         54         -         Shophouse           473         256         Pudu         54         -         Shophouse           474         258         Pudu         54         -         Shophouse           475         270         Pudu         54         -         Shophouse           476         292         Pudu         53         -         Shophouse           477         294         Pudu         53         -         Shophouse           478         296         Pudu         53         -         Shophouse           479         256         Ckt Thambi Dollah         52         -         Shophouse           480         254         Ckt Thambi Dollah         52         -         Shophouse           481         296         Pudu         54         -         Shophouse           482         294         Pudu         54         -         Shophouse           483         11         Brunei Barat	468	240	Pudu	54	-	Shophouse
471         250         Pudu         54         -         Shophouse           472         252         Pudu         54         -         Shophouse           473         256         Pudu         54         -         Shophouse           474         258         Pudu         54         -         Shophouse           475         270         Pudu         53         -         Shophouse           476         292         Pudu         53         -         Shophouse           477         294         Pudu         53         -         Shophouse           478         296         Pudu         53         -         Shophouse           479         256         Ckt Thambi Dollah         52         -         Shophouse           480         254         Ckt Thambi Dollah         52         -         Shophouse           481         296         Pudu         54         -         Shophouse           482         294         Pudu         54         -         Shophouse           483         11         Brunei Barat         62         -         Shophouse           485         7         Brunei Barat<	469	244	Pudu	54	-	Shophouse
472         252         Pudu         54         -         Shophouse           473         256         Pudu         54         -         Shophouse           474         258         Pudu         54         -         Shophouse           475         270         Pudu         54         -         Shophouse           476         292         Pudu         53         -         Shophouse           477         294         Pudu         53         -         Shophouse           478         296         Pudu         53         -         Shophouse           479         256         Ckt Thambi Dollah         52         -         Shophouse           480         254         Ckt Thambi Dollah         52         -         Shophouse           481         296         Pudu         54         -         Shophouse           482         294         Pudu         54         -         Shophouse           483         11         Brunei Barat         62         -         Shophouse           484         9         Brunei Barat         62         -         Shophouse           485         7         Brunei	470	246	Pudu	54	-	Shophouse
473         256         Pudu         54         -         Shophouse           474         258         Pudu         54         -         Shophouse           475         270         Pudu         54         -         Shophouse           476         292         Pudu         53         -         Shophouse           477         294         Pudu         53         -         Shophouse           478         296         Pudu         53         -         Shophouse           479         256         Ckt Thambi Dollah         52         -         Shophouse           480         254         Ckt Thambi Dollah         52         -         Shophouse           481         296         Pudu         54         -         Shophouse           482         294         Pudu         54         -         Shophouse           483         11         Brunei Barat         62         -         Shophouse           484         9         Brunei Barat         62         -         Shophouse           485         7         Brunei Barat         62         -         Shophouse           487         3         B	471	250	Pudu	54	-	Shophouse
474         258         Pudu         54         -         Shophouse           475         270         Pudu         54         -         Shophouse           476         292         Pudu         53         -         Shophouse           477         294         Pudu         53         -         Shophouse           478         296         Pudu         53         -         Shophouse           479         256         Ckt Thambi Dollah         52         -         Shophouse           480         254         Ckt Thambi Dollah         52         -         Shophouse           481         296         Pudu         54         -         Shophouse           481         296         Pudu         54         -         Shophouse           482         294         Pudu         54         -         Shophouse           483         11         Brunei Barat         62         -         Shophouse           484         9         Brunei Barat         62         -         Shophouse           485         7         Brunei Barat         62         -         Shophouse           487         3         B	472	252	Pudu	54	-	Shophouse
475         270         Pudu         54         -         Shophouse           476         292         Pudu         53         -         Shophouse           477         294         Pudu         53         -         Shophouse           478         296         Pudu         53         -         Shophouse           479         256         Ckt Thambi Dollah         52         -         Shophouse           480         254         Ckt Thambi Dollah         52         -         Shophouse           481         296         Pudu         54         -         Shophouse           482         294         Pudu         54         -         Shophouse           483         11         Brunei Barat         62         -         Shophouse           484         9         Brunei Barat         62         -         Shophouse           485         7         Brunei Barat         62         -         Shophouse           486         5         Brunei Barat         62         -         Shophouse           488         1         Brunei Barat         62         -         Shophouse           489         2	473	256	Pudu	54	-	Shophouse
476         292         Pudu         53         -         Shophouse           477         294         Pudu         53         -         Shophouse           478         296         Pudu         53         -         Shophouse           479         256         Ckt Thambi Dollah         52         -         Shophouse           480         254         Ckt Thambi Dollah         52         -         Shophouse           481         296         Pudu         54         -         Shophouse           482         294         Pudu         54         -         Shophouse           483         11         Brunei Barat         62         -         Shophouse           484         9         Brunei Barat         62         -         Shophouse           485         7         Brunei Barat         62         -         Shophouse           486         5         Brunei Barat         62         -         Shophouse           487         3         Brunei Barat         62         -         Shophouse           489         2         Brunei Barat         62         -         Shophouse           490         4<	474	258	Pudu	54	-	Shophouse
477         294         Pudu         53         -         Shophouse           478         296         Pudu         53         -         Shophouse           479         256         Ckt Thambi Dollah         52         -         Shophouse           480         254         Ckt Thambi Dollah         52         -         Shophouse           481         296         Pudu         54         -         Shophouse           482         294         Pudu         54         -         Shophouse           483         11         Brunei Barat         62         -         Shophouse           484         9         Brunei Barat         62         -         Shophouse           485         7         Brunei Barat         62         -         Shophouse           486         5         Brunei Barat         62         -         Shophouse           487         3         Brunei Barat         62         -         Shophouse           488         1         Brunei Barat         62         -         Shophouse           490         4         Brunei Barat         62         -         Shophouse           491	475	270	Pudu	54	-	Shophouse
478         296         Pudu         53         -         Shophouse           479         256         Ckt Thambi Dollah         52         -         Shophouse           480         254         Ckt Thambi Dollah         52         -         Shophouse           481         296         Pudu         54         -         Shophouse           482         294         Pudu         54         -         Shophouse           483         11         Brunei Barat         62         -         Shophouse           484         9         Brunei Barat         62         -         Shophouse           485         7         Brunei Barat         62         -         Shophouse           486         5         Brunei Barat         62         -         Shophouse           487         3         Brunei Barat         62         -         Shophouse           488         1         Brunei Barat         62         -         Shophouse           489         2         Brunei Barat         62         -         Shophouse           490         4         Brunei Barat         62         -         Shophouse           491	476	292	Pudu	53	-	Shophouse
479         256         Ckt Thambi Dollah         52         -         Shophouse           480         254         Ckt Thambi Dollah         52         -         Shophouse           481         296         Pudu         54         -         Shophouse           482         294         Pudu         54         -         Shophouse           483         11         Brunei Barat         62         -         Shophouse           484         9         Brunei Barat         62         -         Shophouse           485         7         Brunei Barat         62         -         Shophouse           486         5         Brunei Barat         62         -         Shophouse           487         3         Brunei Barat         62         -         Shophouse           488         1         Brunei Barat         62         -         Shophouse           489         2         Brunei Barat         62         -         Shophouse           490         4         Brunei Barat         62         -         Shophouse           491         6         Brunei Barat         62         -         Shophouse           492 </td <td>477</td> <td>294</td> <td>Pudu</td> <td>53</td> <td>-</td> <td>Shophouse</td>	477	294	Pudu	53	-	Shophouse
480         254         Ckt Thambi Dollah         52         -         Shophouse           481         296         Pudu         54         -         Shophouse           482         294         Pudu         54         -         Shophouse           483         11         Brunei Barat         62         -         Shophouse           484         9         Brunei Barat         62         -         Shophouse           485         7         Brunei Barat         62         -         Shophouse           486         5         Brunei Barat         62         -         Shophouse           487         3         Brunei Barat         62         -         Shophouse           488         1         Brunei Barat         62         -         Shophouse           489         2         Brunei Barat         62         -         Shophouse           490         4         Brunei Barat         62         -         Shophouse           491         6         Brunei Barat         62         -         Shophouse           492         8         Brunei Barat         62         -         Shophouse           493	478	296	Pudu	53	-	Shophouse
481         296         Pudu         54         -         Shophouse           482         294         Pudu         54         -         Shophouse           483         11         Brunei Barat         62         -         Shophouse           484         9         Brunei Barat         62         -         Shophouse           485         7         Brunei Barat         62         -         Shophouse           486         5         Brunei Barat         62         -         Shophouse           487         3         Brunei Barat         62         -         Shophouse           488         1         Brunei Barat         62         -         Shophouse           489         2         Brunei Barat         62         -         Shophouse           490         4         Brunei Barat         62         -         Shophouse           491         6         Brunei Barat         62         -         Shophouse           492         8         Brunei Barat         62         -         Shophouse           493         10         Brunei Barat         62         -         Shophouse	479	256	Ckt Thambi Dollah	52	-	Shophouse
482       294       Pudu       54       -       Shophouse         483       11       Brunei Barat       62       -       Shophouse         484       9       Brunei Barat       62       -       Shophouse         485       7       Brunei Barat       62       -       Shophouse         486       5       Brunei Barat       62       -       Shophouse         487       3       Brunei Barat       62       -       Shophouse         488       1       Brunei Barat       62       -       Shophouse         489       2       Brunei Barat       62       -       Shophouse         490       4       Brunei Barat       62       -       Shophouse         491       6       Brunei Barat       62       -       Shophouse         492       8       Brunei Barat       62       -       Shophouse         493       10       Brunei Barat       62       -       Shophouse	480	254	Ckt Thambi Dollah	52	-	Shophouse
483       11       Brunei Barat       62       -       Shophouse         484       9       Brunei Barat       62       -       Shophouse         485       7       Brunei Barat       62       -       Shophouse         486       5       Brunei Barat       62       -       Shophouse         487       3       Brunei Barat       62       -       Shophouse         488       1       Brunei Barat       62       -       Shophouse         489       2       Brunei Barat       62       -       Shophouse         490       4       Brunei Barat       62       -       Shophouse         491       6       Brunei Barat       62       -       Shophouse         492       8       Brunei Barat       62       -       Shophouse         493       10       Brunei Barat       62       -       Shophouse	481	296	Pudu	54	-	Shophouse
484       9       Brunei Barat       62       -       Shophouse         485       7       Brunei Barat       62       -       Shophouse         486       5       Brunei Barat       62       -       Shophouse         487       3       Brunei Barat       62       -       Shophouse         488       1       Brunei Barat       62       -       Shophouse         489       2       Brunei Barat       62       -       Shophouse         490       4       Brunei Barat       62       -       Shophouse         491       6       Brunei Barat       62       -       Shophouse         492       8       Brunei Barat       62       -       Shophouse         493       10       Brunei Barat       62       -       Shophouse	482	294	Pudu	54	-	Shophouse
485       7       Brunei Barat       62       -       Shophouse         486       5       Brunei Barat       62       -       Shophouse         487       3       Brunei Barat       62       -       Shophouse         488       1       Brunei Barat       62       -       Shophouse         489       2       Brunei Barat       62       -       Shophouse         490       4       Brunei Barat       62       -       Shophouse         491       6       Brunei Barat       62       -       Shophouse         492       8       Brunei Barat       62       -       Shophouse         493       10       Brunei Barat       62       -       Shophouse	483	11	Brunei Barat	62	-	Shophouse
486       5       Brunei Barat       62       -       Shophouse         487       3       Brunei Barat       62       -       Shophouse         488       1       Brunei Barat       62       -       Shophouse         489       2       Brunei Barat       62       -       Shophouse         490       4       Brunei Barat       62       -       Shophouse         491       6       Brunei Barat       62       -       Shophouse         492       8       Brunei Barat       62       -       Shophouse         493       10       Brunei Barat       62       -       Shophouse	484	9	Brunei Barat	62	-	Shophouse
487       3       Brunei Barat       62       -       Shophouse         488       1       Brunei Barat       62       -       Shophouse         489       2       Brunei Barat       62       -       Shophouse         490       4       Brunei Barat       62       -       Shophouse         491       6       Brunei Barat       62       -       Shophouse         492       8       Brunei Barat       62       -       Shophouse         493       10       Brunei Barat       62       -       Shophouse	485	7	Brunei Barat	62	-	Shophouse
488       1       Brunei Barat       62       -       Shophouse         489       2       Brunei Barat       62       -       Shophouse         490       4       Brunei Barat       62       -       Shophouse         491       6       Brunei Barat       62       -       Shophouse         492       8       Brunei Barat       62       -       Shophouse         493       10       Brunei Barat       62       -       Shophouse	486	5	Brunei Barat	62	-	Shophouse
489       2       Brunei Barat       62       -       Shophouse         490       4       Brunei Barat       62       -       Shophouse         491       6       Brunei Barat       62       -       Shophouse         492       8       Brunei Barat       62       -       Shophouse         493       10       Brunei Barat       62       -       Shophouse	487	3	Brunei Barat	62	-	Shophouse
490       4       Brunei Barat       62       -       Shophouse         491       6       Brunei Barat       62       -       Shophouse         492       8       Brunei Barat       62       -       Shophouse         493       10       Brunei Barat       62       -       Shophouse	488	1	Brunei Barat	62	-	Shophouse
491       6       Brunei Barat       62       -       Shophouse         492       8       Brunei Barat       62       -       Shophouse         493       10       Brunei Barat       62       -       Shophouse	489	2	Brunei Barat	62	-	Shophouse
492         8         Brunei Barat         62         -         Shophouse           493         10         Brunei Barat         62         -         Shophouse	490	4	Brunei Barat	62	-	Shophouse
493 10 Brunei Barat 62 - Shophouse	491	6	Brunei Barat	62	-	Shophouse
	492	8	Brunei Barat	62	-	Shophouse
494 12 Brunei Barat 62 - Shophouse	493	10	Brunei Barat	62	-	Shophouse
	494	12	Brunei Barat	62	-	Shophouse

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
495	1	Brunei	62	-	Shophouse
496	1	Brunei Selatan	62	-	Shophouse
497	3	Brunei Selatan	62	-	Shophouse
498	5	Brunei Selatan	62	-	Shophouse
499	7	Brunei Selatan	62	-	Shophouse
500	-	Ckt Thambi Dollah	62	-	Shophouse
501	440	Pudu	54	-	Shophouse
502	450	Pudu	54	-	Shophouse
503	454	Pudu	54	-	Shophouse
504	4	Pasar	62	-	Shophouse
505	4	Pasar	62	-	Shophouse
506	412	Pudu	54	-	Shophouse
507	-	Pasar	65	-	Shophouse
508	486	Pudu	62	-	Shophouse
509	488	Pudu	62	-	Shophouse
510	490	Pudu	62	-	Shophouse
511	492	Pudu	62	-	Shophouse
512	494	Pudu	62	-	Shophouse
513	496	Pudu	62	-	Shophouse
514	498	Pudu	62	-	Shophouse
515	506	Pudu	62	-	Shophouse
516	516	Pudu	62	-	Shophouse
517	112-1	Ipoh	46	-	Shophouse
518	41	Chow Kit	46	-	Residential
519	29	Chow Kit	46	-	Shophouse
520	27	Chow Kit	46	-	Shophouse
521	110-1	Ipoh	46	-	Shophouse
522	25	Chow Kit	46	-	Shophouse
523	23	Chow Kit	46	-	Shophouse
524	21	Chow Kit	46	-	Shophouse
525	19	Chow Kit	46	-	Shophouse
526	484	Chow Kit	46	-	Shophouse
527	484	Chow Kit	46	-	Shophouse
528	484	Chow Kit	46	-	Shophouse
529	484	Chow Kit	46	-	Shophouse
530	484	Chow Kit	46	-	Shophouse
531	484	Chow Kit	46	-	Shophouse

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
532	108-1	lpoh	46	-	Shophouse
533	520	Tar	46	-	Shophouse
534	522	Tar	46	-	Shophouse
535	524	Tar	46	-	Shophouse
536	54	lpoh	46	-	Shophouse
537	56	lpoh	46	-	Shophouse
538	58	lpoh	46	-	Shophouse
539	60	lpoh	46	-	Shophouse
540	72	lpoh	46	-	Shophouse
541	124-1	lpoh	46	-	Shophouse
542	120-1	lpoh	46	-	Shophouse
543	40	lpoh	46	-	Shophouse
544	36	lpoh	46	-	Shophouse
545	34	lpoh	46	-	Shophouse
546	118-1	lpoh	46	-	Shophouse
547	16	lpoh	46	-	Shophouse
548	116-1	lpoh	46	-	Shophouse
549	8	lpoh	46	-	Shophouse
550	6	lpoh	46	-	Shophouse
551	72-C	Lkk Raja Laut	46	-	Shophouse
552	72-B	Lkk Raja Laut	46	-	Shophouse
553	72-A	Lkk Raja Laut	46	-	Shophouse
554	72	Lkk Raja Laut	46	-	Shophouse
555	7	Lkk Raja Laut	46	-	Shophouse
556	47	Chow Kit	46	-	Shophouse
557	45	Chow Kit	46	-	Shophouse
558	-	TAR	46	-	Shophouse
559	-	TAR	46	-	Shophouse
560	-	TAR	46	-	Shophouse
561	-	TAR	46	-	Shophouse
562	-	TAR	46	-	Shophouse
563	366, 368, 370	TAR	46	-	Shophouse
564	372	TAR	46	-	Shophouse
565	384	TAR	46	-	Shophouse
566	412	TAR	46	-	Shophouse
567	414	TAR	46	-	Shophouse
568	416	TAR	46	-	Shophouse
569	418	TAR	46	-	Shophouse

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
570	420	TAR	46	-	Shophouse
571	422	TAR	46	-	Shophouse
572	358	TAR	46	-	Shophouse
573	323	TAR	46	HOTEL NANKIN	Shophouse
574	324	TAR	46	-	Shophouse
575	338	TAR	46	BUMIPUTRA COMMERCE	Shophouse
576	342	TAR	46	-	Shophouse
577	343	TAR	46	-	Shophouse
578	346	TAR	46	-	Shophouse
579	38	TAR	34	-	Shophouse
580	40	TAR	34	-	Shophouse
581	42	TAR	34	-	Shophouse
582	44	TAR	34	-	Shophouse
583	2	TAR	05	-	Shophouse
584	-	TAR	35	-	Shophouse
585	99 & 100	TAR	35	-	Shophouse
586	102	TAR	35	-	Shophouse
587	4	TAR	05	-	Shophouse
588	202	TAR	38	-	Shophouse
589	132	TAR	38	-	Shophouse
590	134	TAR	38	-	Shophouse
591	138	TAR	38	-	Shophouse
592	140	TAR	38	-	Shophouse
593	156	TAR	38	-	Shophouse
594	158	TAR	38	-	Shophouse
595	160	TAR	38	-	Shophouse
596	162 & 164	TAR	38	-	Shophouse
597	166	TAR	38	-	Shophouse
598	30	TAR	34	-	Shophouse
599	32	TAR	34	-	Shophouse
600	4	Tkt Thong Shin	19	-	Shophouse
601	8	Tkt Thong Shin	19	-	Shophouse
602	12	Tkt Thong Shin	19	-	Shophouse
603	16	Tkt Thong Shin	19	-	Shophouse
604	20	Tkt Thong Shin	19	-	Residential
605	2	Tkt Thong Shin	19	-	Shophouse
606	6	Tkt Thong Shin	19	-	Shophouse
607	10	Tkt Thong Shin	19	-	Shophouse

Table 3 - Category 3 Conservation Buildings

ame	Section	Building's Name	Building Type
Shin	19	-	Residential
Shin	19	-	Shophouse
Shin	19	-	Shophouse
Shin	19	-	Shophouse
Shin	19	-	Residential
Shin	19	-	Shophouse
n	19	-	Shophouse
	67	-	Shophouse
ang	67	-	Shophouse
ang	67	-	Shophouse
ang	67	-	Shophouse
ang	67	-	Shophouse
	-	-	Residential
hulan 1	-	-	Residential
ın	-	-	Residential
ın	-	-	Residential
ın	-	-	Residential
n	-	-	Residential
ın	-	-	Residential
n	-	-	Residential
n	-	-	Residential
n	-	-	Residential
	-	-	Shophouse
la la	lan lan lan lan	lan - lan -	lan

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
646	2-10	Lrg Hick	-	-	Shophouse
647	41 - 51	Maharajalela	69	BGN CHONG WING CHAN	Shophouse
648	55	Maharajalela	69	MANDRIN COURT	Shophouse
649	5	Talalla	69	-	Residential
650	6	Talalla	69	-	Residential
651	7	Talalla	69	-	Residential
652	8	Talalla	69	-	Residential
653	75 &75A	Choo Cheng Khey	69	-	Residential
654	73&73A	Choo Cheng Khey	69	-	Residential
655	71&71A	Choo Cheng Khey	69	-	Residential
656	69&69A	Choo Cheng Khey	69	-	Residential
657	67&67A	Choo Cheng Khey	69	-	Residential
658	65&65A	Choo Cheng Khey	69	-	Residential
659	63&63A	Choo Cheng Khey	69	-	Residential
660	61&61A	Choo Cheng Khey	69	-	Residential
661	59&59A	Choo Cheng Khey	69	-	Residential
662	-	Tun Sambathan	-	-	Shophouse
663	-	Tun Sambathan	-	-	Shophouse
664	-	Tun Sambathan	-	-	Shophouse
665	-	Tun Sambathan	-	-	Shophouse
666	-	Tun Sambathan	-	HOTEL GRAND PARADISE	Shophouse
667	10	Scott	-	-	Shophouse
668	11	Scott	-	-	Shophouse
669	15	Scott	-	-	Shophouse
670	16	Scott	-	-	Shophouse
671	17	Scott	-	-	Shophouse
672	21	Scott	-	-	Shophouse
673	24	Scott	-	-	Shophouse
674	25	Scott	-	-	Shophouse
675	26	Scott	-	-	Shophouse
676	27	Scott	-	-	Shophouse
677	8	Scott	-	-	Shophouse
678	9	Scott	-	-	Residential
679	A-1-19	Rozario	-	-	Residential
680	A1-21-39	Rozario	-	-	Residential
681	A2-41-59	Rozario	-	-	Residential
682	A3-61-79	Rozario	-	-	Residential
683	B-2-20	Chan Ah Tong	-	-	Residential

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
684	B1-22-40	Chan Ah Tong	-	-	Residential
685	B2-42-60	Chan Ah Tong	-	-	Residential
686	B3-62-80	Chan Ah Tong	-	-	Residential
687	C-41-59	Chan Ah Tong	-	-	Residential
688	C1-61-79	Chan Ah Tong	-	-	Residential
689	D-16-22	Lrg Chan Ah Tong	-	-	Residential
690	D1-24-30	Lrg Chan Ah Tong	-	-	Residential
691	152	Tun Sambathan	-	-	Shophouse
692	154	Tun Sambathan	-	-	Shophouse
693	156	Tun Sambathan	-	-	Shophouse
694	158	Tun Sambathan	-	-	Shophouse
695	160	Tun Sambathan	-	-	Shophouse
696	162	Tun Sambathan	-	-	Shophouse
697	164	Tun Sambathan	-	-	Shophouse
698	166	Tun Sambathan	-	-	Shophouse
699	176	Tun Sambathan	-	-	Shophouse
700	178	Tun Sambathan	-	-	Shophouse
701	180	Tun Sambathan	-	-	Shophouse
702	182	Tun Sambathan	-	-	Shophouse
703	184	Tun Sambathan	-	-	Shophouse
704	186	Tun Sambathan	-	-	Shophouse
705	98	Tun Sambathan	-	-	Shophouse
706	100	Tun Sambathan	-	-	Shophouse
707	102	Tun Sambathan	-	-	Shophouse
708	104	Tun Sambathan	-	-	Shophouse
709	87	Tun Sambathan 4	-	-	Residential
710	-	Imbi	67	-	Shophouse
711	11	Barat	67	-	Shophouse
712	13	Barat	67	-	Shophouse
713	17	Barat	67	-	Shophouse
714	19	Barat	67	-	Shophouse
715	21	Barat	67	-	Shophouse
716	74	Sg Besi	53	-	Shophouse
717	76	Sg Besi	53	-	Shophouse
718	80	Sg Besi	53	-	Shophouse
719	32-32A	Sg Besi	53	-	Shophouse
720	22	Sg Besi	53	-	Shophouse
721	20	Sg Besi	53	-	Shophouse

Table 3 - Category 3 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
722	-	Tangsi	59	-	Shophouse
723	-	Cenderasari	59	KEMENTERIAN KESIHATAN	Institution
724	-	Cenderasari	59	S MEN METHODIST (P)	School
725	-	Tangsi	59	WISMA IKRAN	Office
726	-	Tangsi	59	GALERI TANGSI	Gallery
727	-	Tangsi	59	PAM	Institution
728	-	Tangsi	59	-	Shophouse
729	-	Tangsi	59	-	Shophouse
730	-	Bukit Aman	59	BRITISH COUNCIL	
				(previous use)	Vacant

Table 4 - Category 4 Conservation Buildings

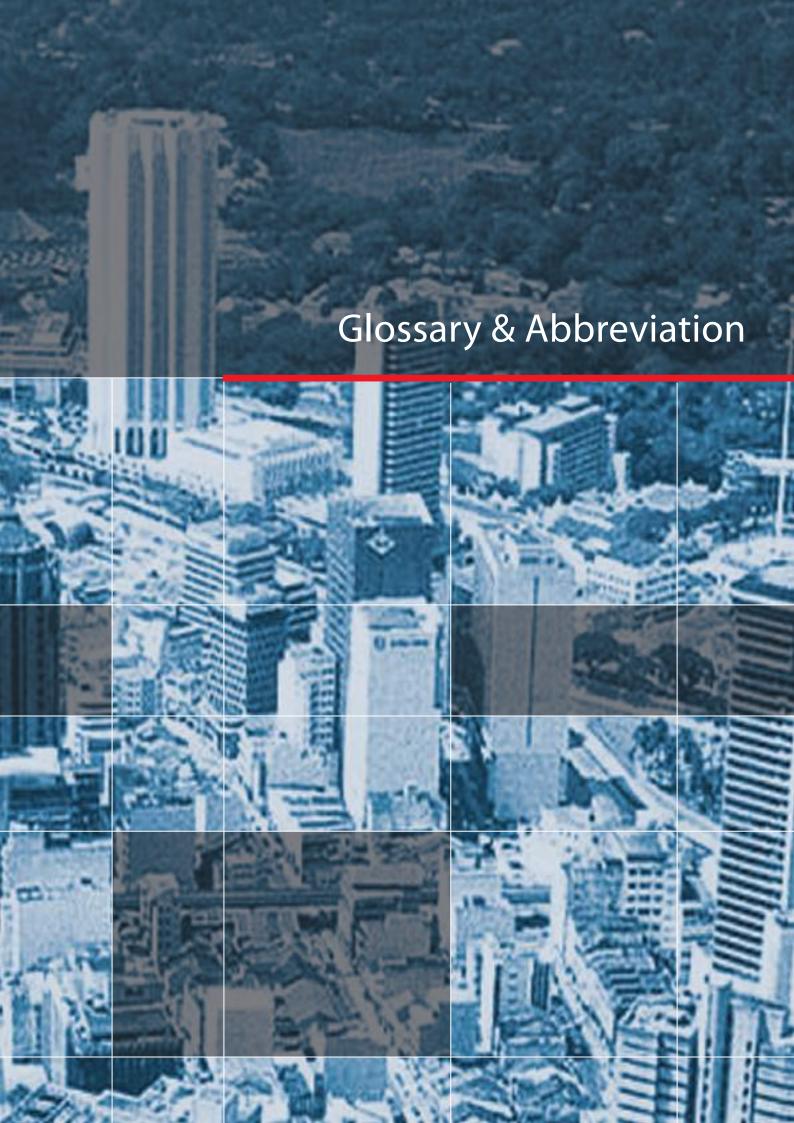
No.	House No.	Street's Name	Section	Building's Name	Building Type
1	53	Tar	33	-	Shophouse
2	-	Melayu	06	-	Shophouse
3	31	Ampang	49	-	Shophouse
4	29	Ampang	49	-	Shophouse
5	27	Ampang	49	-	Shophouse
6	38	Lebuh Ampang	08	-	Shophouse
7	40A	Lebuh Ampang	08	-	Shophouse
8	-	Pudu	19	-	Shophouse
9	-	Pudu	19	-	Shophouse
10	-	Pudu	19	-	Shophouse
11	-	Pudu	19	-	Shophouse
12	-	Pudu Lama	19	-	Residential
13	-	Pudu Lama	19	-	Residential
14	-	Pudu Lama	19	-	Residential
15	-	Pudu Lama	19	-	Residential
16	-	Petaling	21	-	Shophouse
17	37	Silang	20	-	Shophouse
18	35	Silang	20	-	Shophouse
19	35	Lebuh Ampang	12	-	Shophouse
20	33	Lebuh Ampang	12	-	Shophouse
21	29	Silang	18	-	Shophouse
22	31	Lebuh Ampang	12	-	Shophouse
23	19	Silang	18	-	Shophouse
24	116	Petaling	66	-	Shophouse
25	-	Hang Lekir	30	-	Shophouse
26	-	Hang Lekir	30	-	Shophouse
27	57	Petaling	22	-	Shophouse
28	172 & 174	Tun H. S. Lee	22	-	Shophouse
29	-	Tun H. S. Lee	30	-	Shophouse
30	107	Sultan	64	-	Shophouse
31	105	Sultan	64	-	Shophouse
32	318	Pudu	53	-	Shophouse
33	188	Pudu	56	-	Shophouse
34	192	Pudu	56	-	Shophouse
35	196	Pudu	56	-	Shophouse
36	198	Pudu	56	-	Shophouse
37	200	Pudu	56	-	Shophouse
38	202	Pudu	56	-	Shophouse

Table 4 - Category 4 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
39	204	Pudu	56	-	Shophouse
40	206	Pudu	56	-	Shophouse
41	208	Pudu	56	-	Shophouse
42	210	Pudu	56	-	Shophouse
43	260-262	Pudu	54	-	Shophouse
44	272	Pudu	54	-	Shophouse
45	280	Pudu	54	-	Shophouse
46	282	Pudu	54	-	Shophouse
47	284	Pudu	54	-	Shophouse
48	288	Pudu	54	-	Shophouse
49	270	Pudu	54	-	Shophouse
50	280	Pudu	54	-	Shophouse
51	282	Pudu	54	-	Shophouse
52	284	Pudu	54	-	Shophouse
53	286	Pudu	54	-	Shophouse
54	304	Pudu	54	-	Shophouse
55	302	Pudu	54	-	Residential
56	430	Pudu	54	-	Shophouse
57	434	Pudu	54	-	Shophouse
58	436	Pudu	54	-	Shophouse
59	438	Pudu	54	-	Shophouse
60	452	Pudu	54	-	Shophouse
61	456	Pudu	54	-	Shophouse
62	458	Pudu	54	-	Shophouse
63	460	Pudu	54	-	Shophouse
64	466	Pudu	54	-	Shophouse
65	35	Chow Kit	46	-	Shophouse
66	33	Chow Kit	46	-	Shophouse
67	31	Chow Kit	46	-	Shophouse
68	494	TAR	46	-	Shophouse
69	496	TAR	46	-	Shophouse
70	498	TAR	46	-	Shophouse
71	30	lpoh	46	-	Shophouse
72	28	lpoh	46	-	Shophouse
73	24	lpoh	46	-	Shophouse
74	14	Chow Kit	46	-	Shophouse
75	12	Chow Kit	46	-	Shophouse
76	10	Chow Kit	46	-	Shophouse

Table 4 - Category 4 Conservation Buildings

No.	House No.	Street's Name	Section	Building's Name	Building Type
77	62	TAR	34	-	Shophouse
78	64	TAR	34	-	Shophouse
79	66	TAR	34	-	Shophouse
80	68	TAR	34	-	Shophouse
81	7	Scott	-	-	Shophouse
82	55	Medan Imbi	67	-	Residential
83	53	Medan Imbi	67	-	Residential
84	51	Medan Imbi	67	-	Shophouse
85	49	Medan Imbi	67	-	Shophouse
86	47	Medan Imbi	67	-	Shophouse
87	45	Medan Imbi	67	-	Shophouse
88	43	Medan Imbi	67	-	Shophouse
89	41	Medan Imbi	67	-	Shophouse
90	35	Medan Imbi	67	-	Shophouse
91	33	Medan Imbi	67	-	Shophouse



## **GLOSSARY**

## **Adaptive Reuse**

Adaptive reuse is the change in use of a building so as to be able to give it a new lease of life. Such adaptive reuse often involves the change of institutional buildings such as old government offices or schools to cultural or commercial usage.

#### **Build To line**

Build to lines are those which the outside face of external walls of existing or new developments must be built up to without any setback subject to the application of other envelope controls and streetblock plans.

## **Building Conservation**

Building Conservation requires that some or all of the elements of the designated building are to be preserved and/or reinstated to their original condition.

## **City Centre Commercial**

Refers to commercial areas generally located within the Kuala Lumpur City Centre and some part of Damansara Penchala Strategic Zone as identified in the Zoning Map. It is the highest hierarchy of commercial zone that reflects its function as the main commercial zone within Kuala Lumpur.

### **Conservation Areas**

Area Conservation requires that certain characteristics and elements of the designated areas are to be retained and enhanced. Developments within these areas are controlled so as to be in keeping with their intended character.

## **Conservation Zone**

As an area of special architectural or historic interest and character of which it is desirable to preserve or conserve.

#### DCP 1 - Land Use Zoning Map

A Map showing the distribution of land use zoning in a spatial form for the entire Kuala Lumpur city.

# **Density**

The intensity of use of land reckoned or expressed in terms of the number of person, dwelling units, or habitable rooms, or any combination of those factors, per unit area of land.

## **District Centre Commercial**

Commercial land located within the boundary of district growth centres as identified in the Zoning Map.

## **Envelope Control**

Envelope Control is the determination of certain bounding planes within which new development or alterations and additions to existing buildings may occur. Any projection of the development outside these bounding planes is not permitted except as specified in the guidelines or streetblock plans.

### **Environmental Protection Zone (EPZ)**

An area where additional control and guide with regards to protecting the environment is imposed and specified on development and use of land.

#### **Established Housing Area (EHA)**

Areas which consist of housing development (old or new) that are in existence and well planned and is expected to remain residential within the planning period of the KL City Plan 2020.

#### **General Improvement Areas**

These are contiguous areas for which a coordinated programme of landscaping, streetscape and infrastructural improvements will be implemented by KLCH which are intended to raise the quality of the urban environment and stimulate investment and improvements by the private sector.

### **Height Control Zones**

Additional overlays of control on heights of new buildings that falls within the designated height control zones in Kuala Lumpur and are imposed on the following areas:

- (a) Areas located within Petronas Twin Tower and KL Tower view corridor.
- (b) Areas located within selected hills backdrop.
- (c) Areas surrounding the Istana Negara Damansara reserve.
- (d) Areas located within designated historical zones.
- (e) Areas surrounding Sungai Besi Airport.

#### **Heights Elevations**

Vertical datum to the top of building, inclusive of mechanical, water tank and other equipment.

#### Industrial

Area designated and zoned for manufacturing and its associated activities in the Zoning Map.

## **Infill Development**

Infill development refers to the rebuilding of a vacant lot or lots or the demolition and rebuilding of an existing building (or groups of buildings).

### **Landmark View Corridor**

View looking directly towards either one of the landmark buildings from designated locations.

## **Major Landmark Buildings**

Refers to Petronas Twin Tower and KL Tower.

## Mixed Use Commercial

Refers to land zoned for mixed-use commercial development in the Zoning Map. It is intended to promote residential component in commercial zone and at transit planning zones. The zone is predominantly commercial with minimum 30% residential.

## **Mixed Use Commercial & Industry**

Refers to land zoned for mixed-use industry and commercial development as identified in the Zoning Map. It is intended to promote clustering of similar activities incorporating compatible and non-polluting industrial activities with support services such as storage, distributive trade, and service industries. This zone is predominantly industrial with maximum 30% commercial use.

#### Mixed Use Residential

Refers to land zoned for mixed-use development within general residential area as identified in the Zoning Map. It is intended to promote commercial component in residential zone and at transit planning zone. This zone is generally located within transit planning zone to encourage street level activities. This zone is predominantly residential where the commercial activity allowed is not more than 30% of the allowable gross floor area.

## **Neighbourhood Centre Commercial**

Refers to commercial land located within residential neighbourhood and serves the neighbourhood area to provide small-scale retail and services. Intensity of development and type of activities permissible should be consistent with the character of the neighbourhood area in order to limit adverse impacts on nearby residential lots.

#### **Panoramic View Corridor**

View from an oblique angle at designated location looking into the city towards both the Petronas Twin Tower and the KL Tower.

### Plinth area

The portion of a plot of land that is covered by a building or structure having a solid roof. It includes balconies or other projections from the building but does not include eaves up to a maximum of 600mm in width. It is measured as a proportion of building area over land area.

#### **Plot Ratio**

Plot ratio means the ratio between the total floor area of a building and the area of the building plot." as defined in the Federal Territory Planning Act 1982 (Act 267) (Part I, Section 2).

## Preservation

Preservation of either the whole or a part of a building refers to the retention of the items to be retained in their original condition.

## **Private Institutional**

Institutional uses such as private art galleries, private museum, health services etc.

## **Private Open Space**

Private open space refers to parks and green areas that is owned by individuals or businesses and are used at the discretion of the owner. Private parks are privately maintained and usage to the public is at the discretion of the owners. One example of such park in Kuala Lumpur is the golf courses.

## **Public Housing**

Refers to land designated for public housing only and these includes KLCH's and government's public housing areas.

#### **Public Institutional**

Major institutional and civic uses such as art galleries, museum, government offices, palace reserve, military reserve, health & emergency such as police head quarters, hospitals, universities, and other civic use.

## **Public Open Space**

Parks and open spaces are public spaces that provides for a variety of active and passive needs of ages of the community and local residents.

## **Residential 1**

This residential zone refers to low density residential of 4 persons to 40 persons per acre.

#### **Residential 2**

This residential zone refers to medium density residential of 48 persons to 120 persons per acre.

#### **Residential 3**

This residential zone refers to high density residential of 160 persons to 400 persons per acre.

#### Restoration

Restoration of a designated conservation building or part thereof is the reinstatement of the building (or part thereof) to its original condition.

## **Special Planning Zones**

Additional development control which constitutes 4 SPZ:

- Environmental Protection Zone (SPZ 1)
- 2. Heritage Zone (SPZ 2)
- 3. Height Control Zone (SPZ 3)
- Transit Planning Zone (SPZ 4)

#### **Technology Park**

Zone designated for Technology Park Malaysia in Bukit Jalil.

#### **Transit Planning Zone (TPZ)**

Areas located within 400m of a radius of a Transit Station - a Light Rail Transit (LRT), Monorail, or Bus Rapid Transit station.

# White Zone

Refer to public facilities, infrastructure and utilities services.

## **Zoning Schedule**

Identifies specifically the ancillary, related or compatible activities within the permissible predominant use of each particular land use zone.

# **Zoning Schedule (General)**

Applies to the zone identified for each of the 20 land use zoning classification

## Zoning Schedule (Specific)

Prescribed to certain areas of the land use zones only. The situation where specific areas are prescribed or limited to specific use classification.

## **ABBREVIATION**

**AMSL** Above Mean Sea Level

DCP **Development Control Plan** 

**EPZ Environmental Protection Zone** 

FAR Floor Area Requirement

**GPT Gross Pollutant Trap** 

HCZ Height Control Zone

MSL Mean Sea Level

**KLCH** Kuala Lumpur City Hall

**KL DCP 2008** Kuala Lumpur Development Control Plan 2008

**Draft KL City Plan 2020** Draft Kuala Lumpur City Plan 2020

**PMU** Pencawang Masuk Utama

PPU Pencawang Pengagih Utama

RDF Refuse Derive Fuel plant (RDF)

R&D Research and Development

TPZ Transit Planning Zone

WTE Waste to Energy Plant

ZS Zoning Schedule



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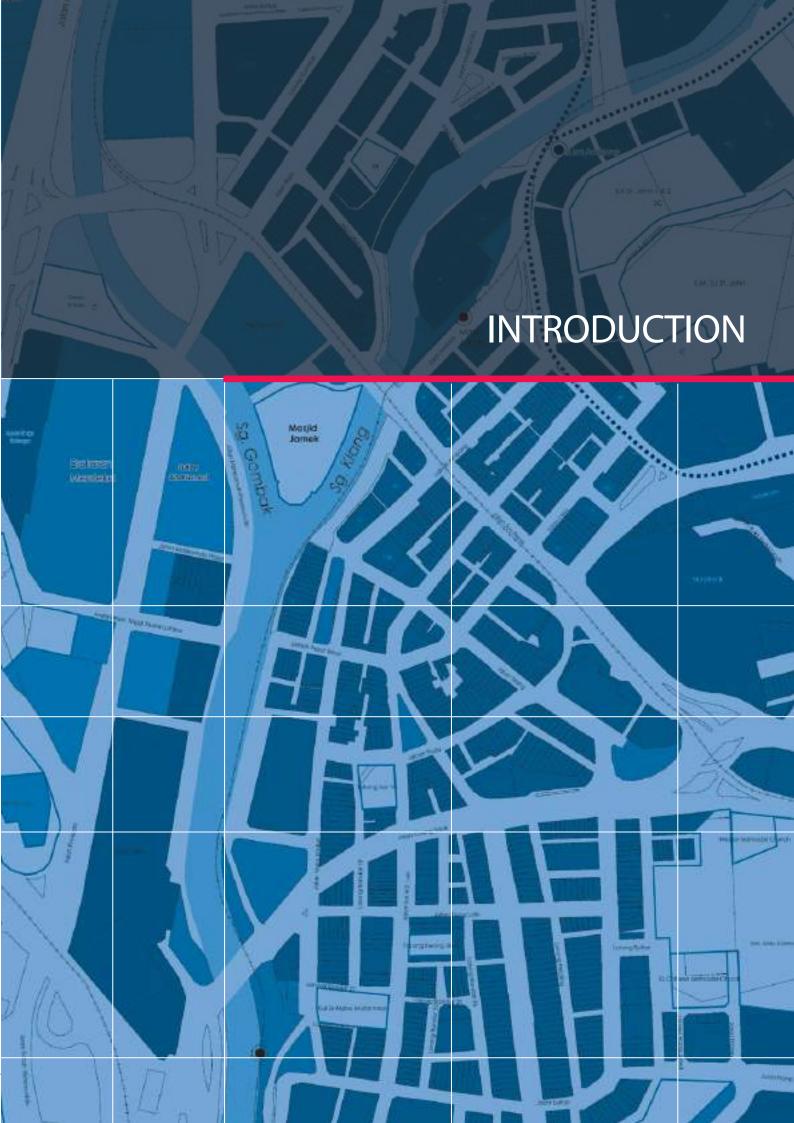
Towards a World Class City



Volume 2 - Part 2







### Introduction

The KLDCP 2008 consists of two components namely Key Planning and Development Control and the Special Planning Controls which are to be simultaneously referred by all development proposals.

Key Planning and Development Control are used in controlling use of land and intensity of developments and the Special Planning Controls are additional overlays of control imposed on the underlying Key Planning and Development Control Plans. The relevant plans are:-

### **Key Planning and Development Control**

1. DCP 1: Land Use Zoning Map

2. DCP 2: Density Map 3. DCP3: Plot Ratio Map

### **Special Planning Control**

1. SPZ 1: Environmental Protection Zone

2. SPZ 2: Heritage Zone 3. SPZ 3: Height Control Zone 4. SPZ 4: Transit Planning Zone

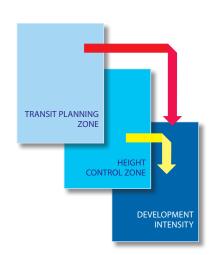
The above maps are described in Part 1 of the KLDCP 2008. This document is the second part of the KLDCP 2008 which translates all the above maps as overlays and presented on a scale of 1:5,000 through index or grid system. In which the whole Kuala Lumpur area is mapped in 320 grids. This is to facilitate easy reference by the land owners and users of the document where key information necessary for development application are put together.

# **How to Use This Document?**

Each grid consists of two pieces of maps labelled as LAND USE ZONING and DEVELOPMENT INTENSITY. The Land Use Zoning Map informs users of the designated land use for each plot of land within the grid. The Development Intensity Map informs users of the allowable density for residential zones and allowable plot ratio for commercial, mixed use and industrial zones.

On both maps, the relevant Special Planning Controls are overlaid to indicate where the Special Planning Control applies. Special Planning Controls on Environmental Protection Zone and Heritage Zone are overlaid on the Land Use Zoning Map whilst the Height Control Zone and Transit Planning Zone are overlaid on the Development Intensity Map.





### **How to Use This Document**

## STEP 1

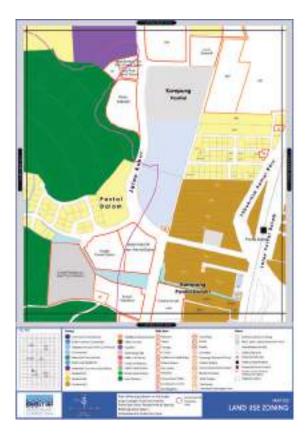
Refer to Key Index Plan to determine the location of the area and its corresponding grid number.



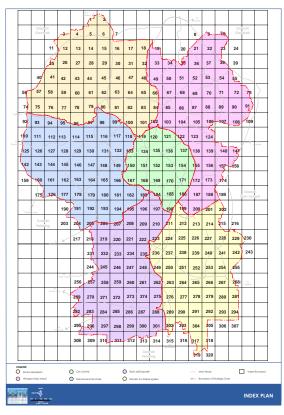
#### STEP 2

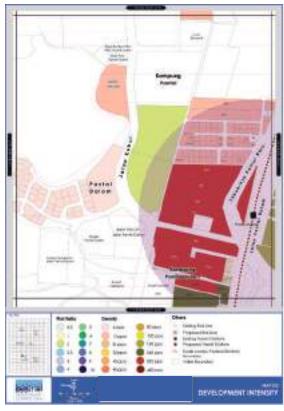
Once the Map Grid Number is identified, refer to the corresponding Land Use Zoning and **Development Intensity Maps.** 

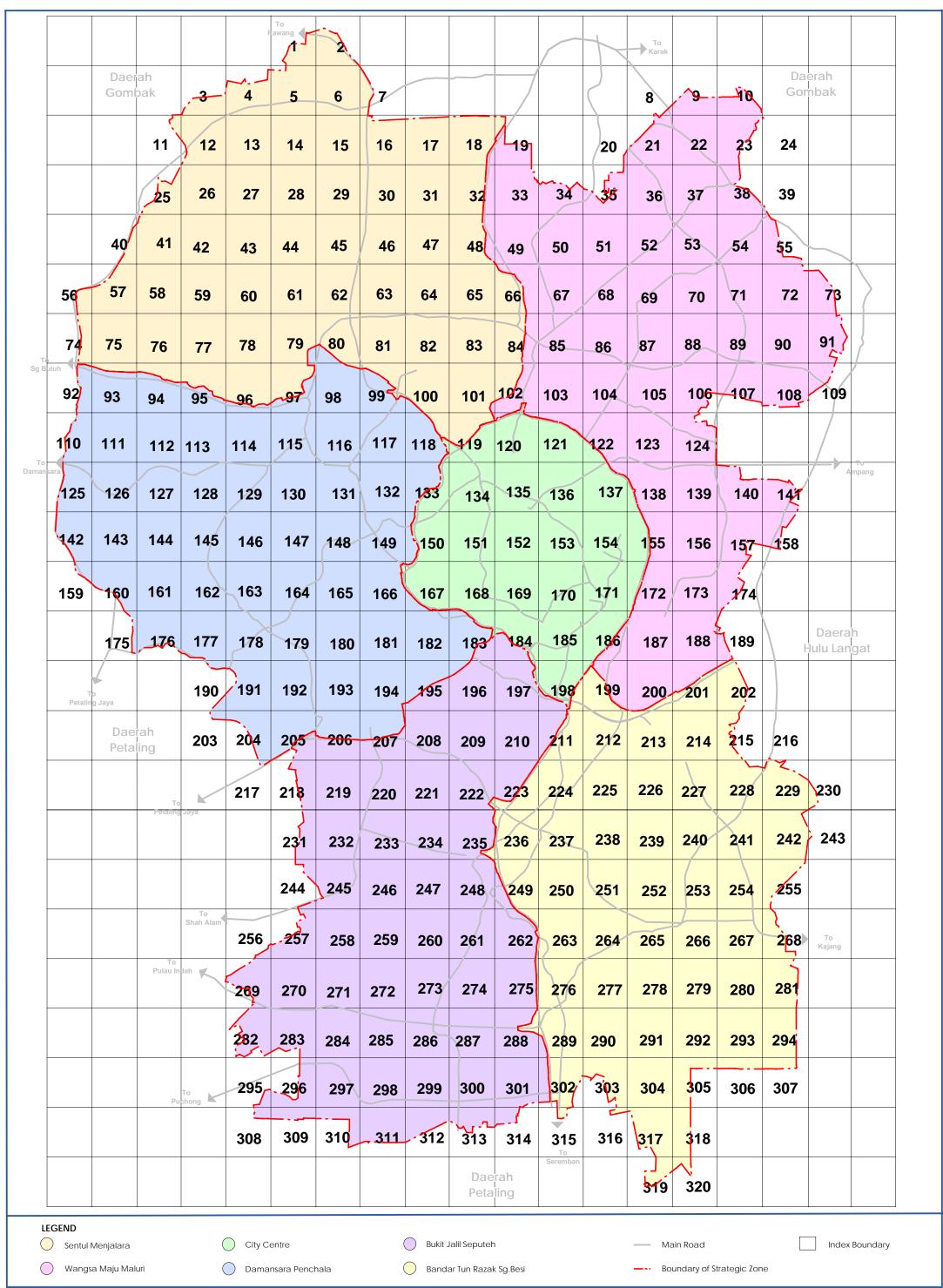




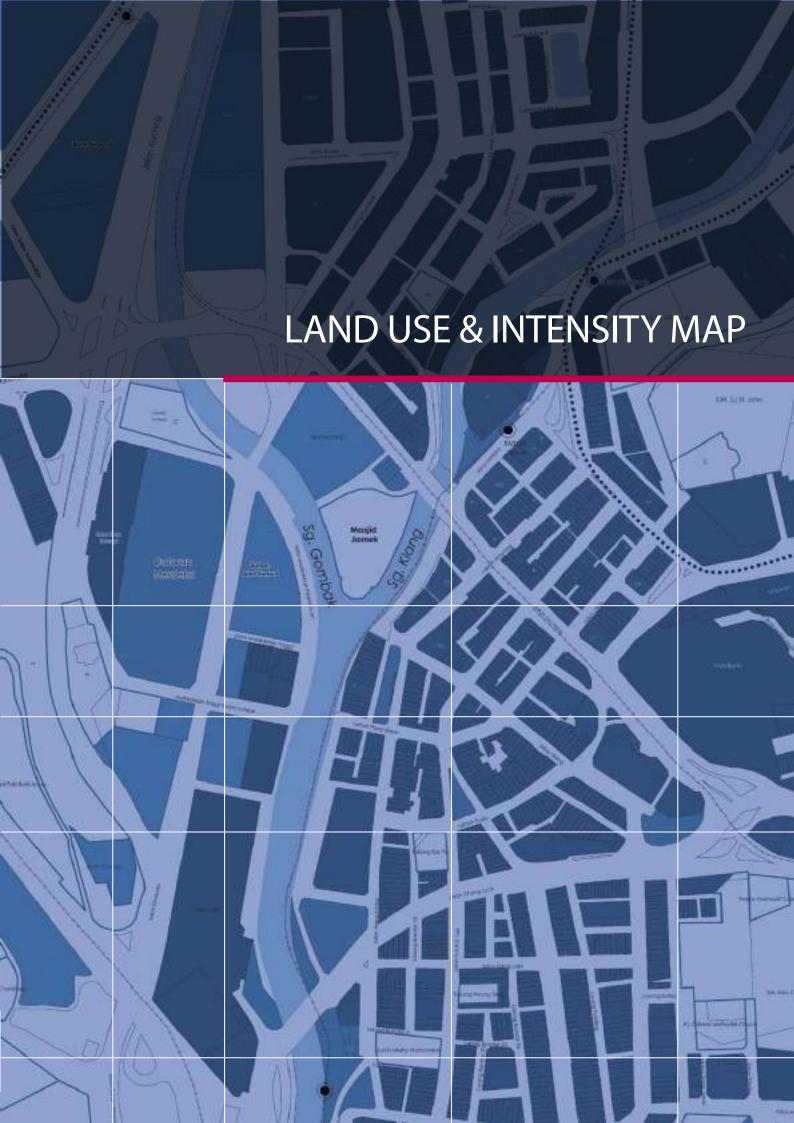
Users are to refer to key plan for the grid number and the two maps described above. The following diagram explains the steps on how to refer to both Land Use Zoning and Intensity Maps in this document.

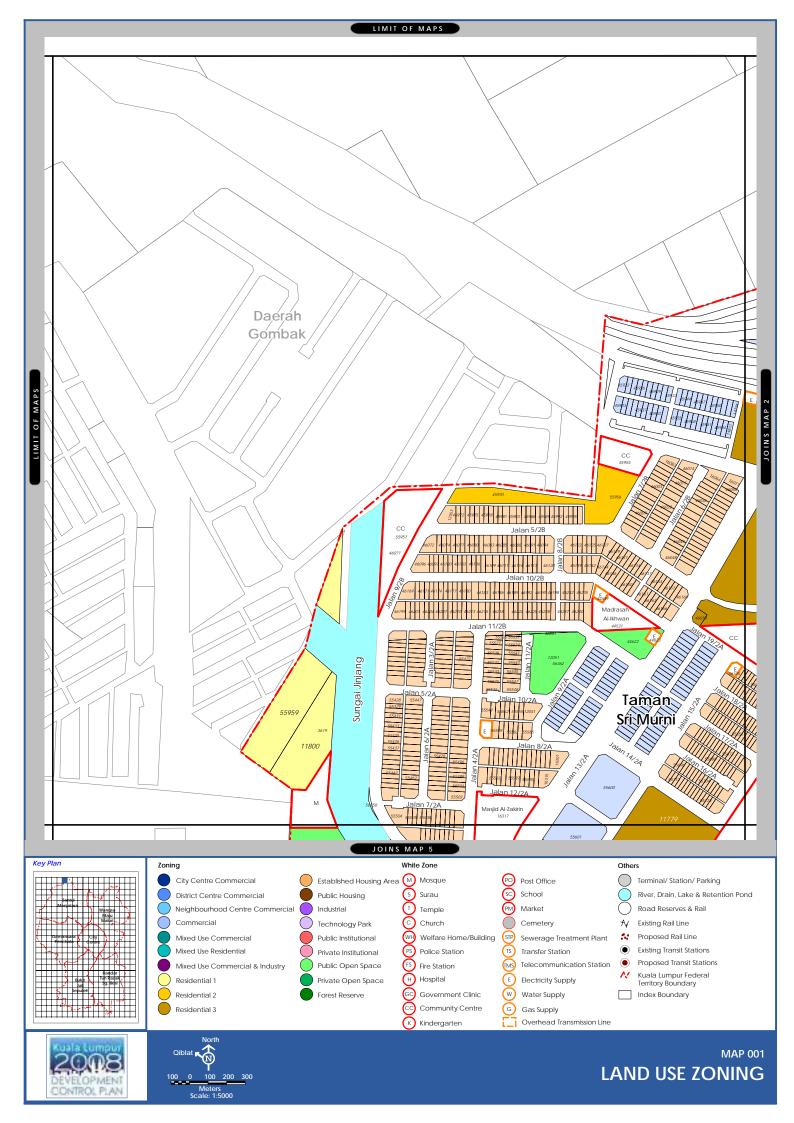


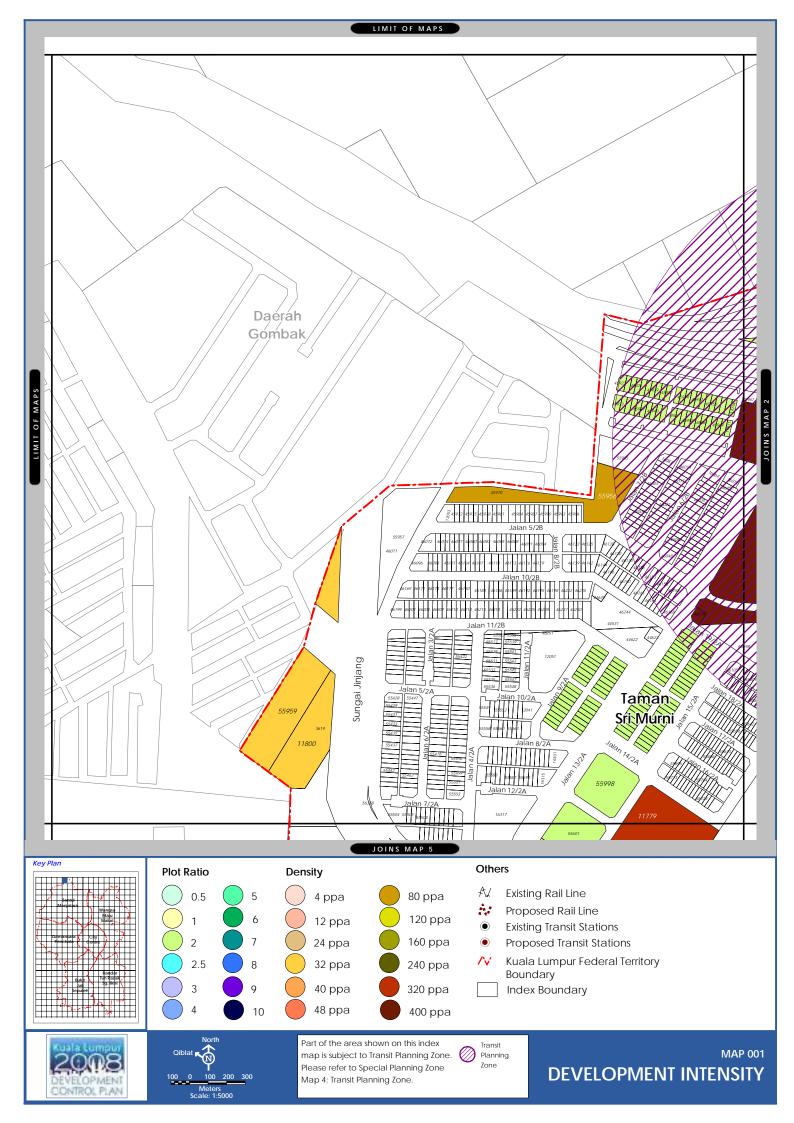


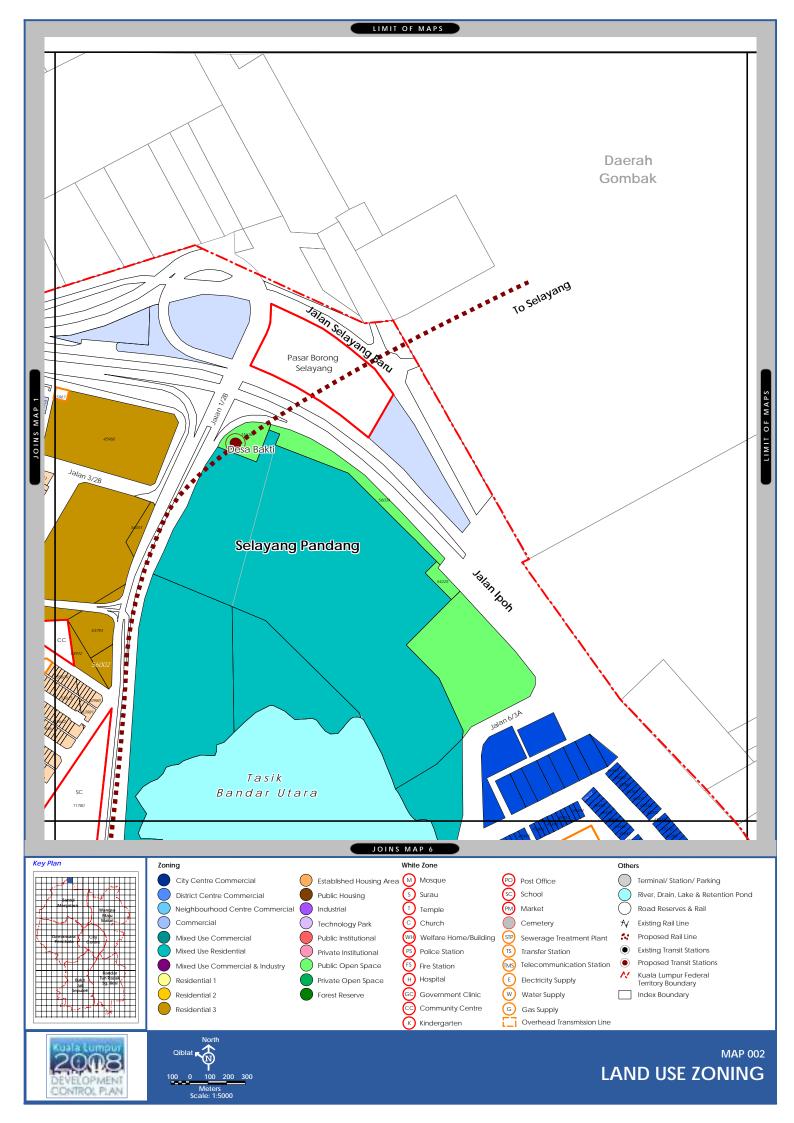


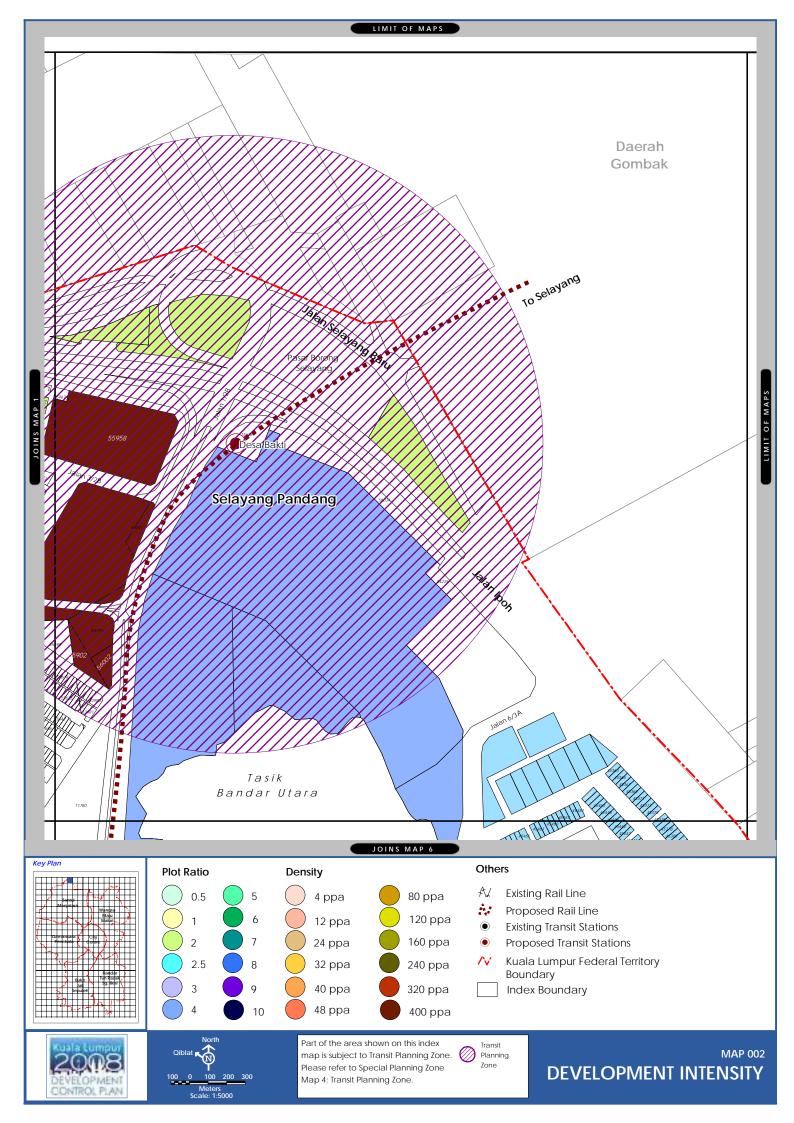


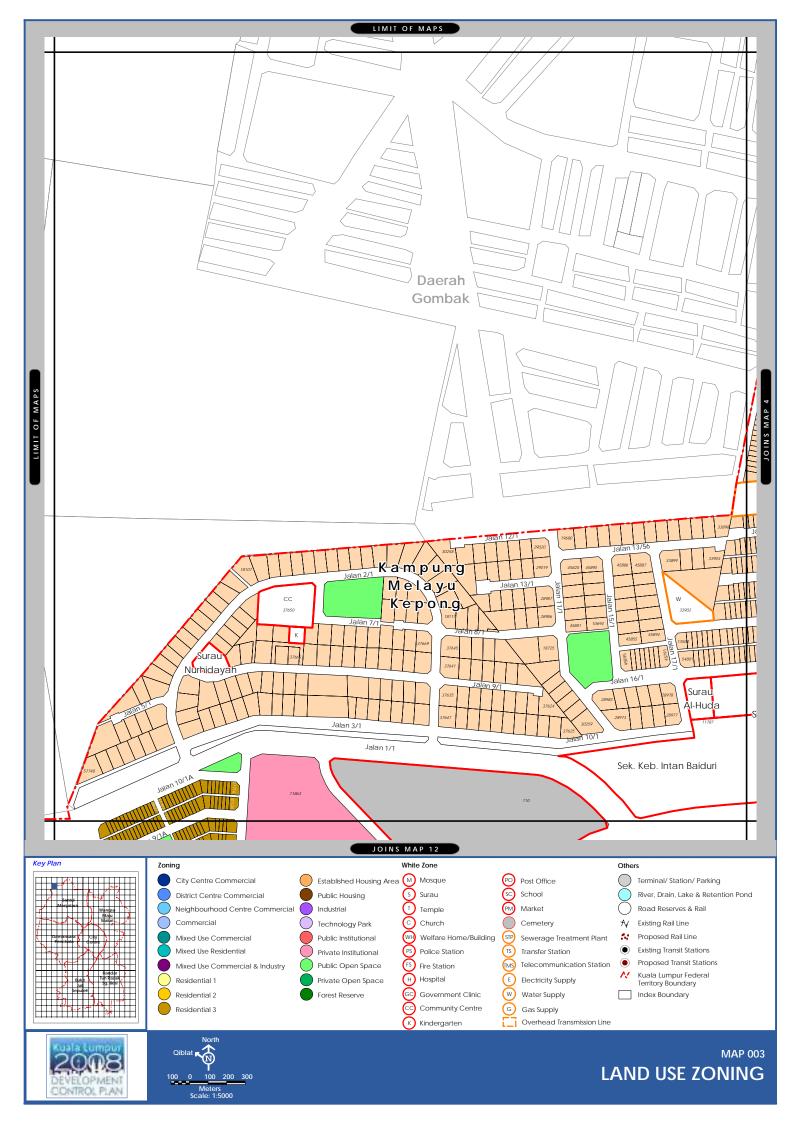


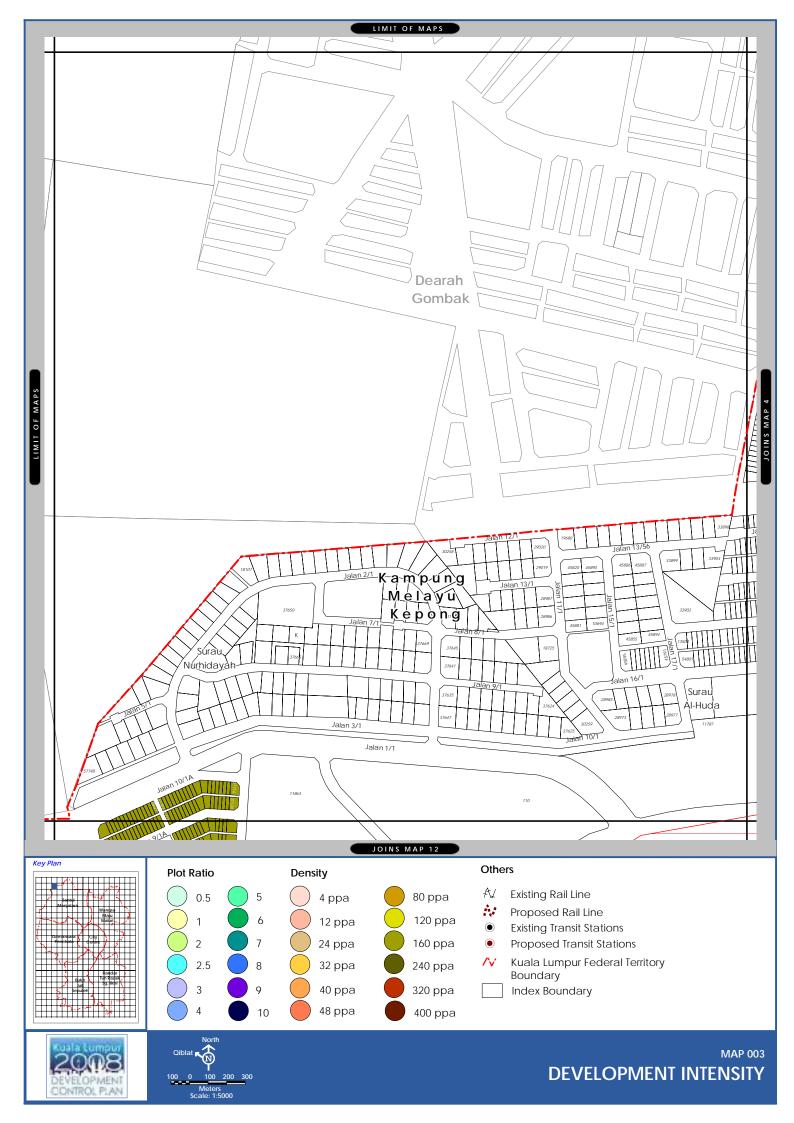


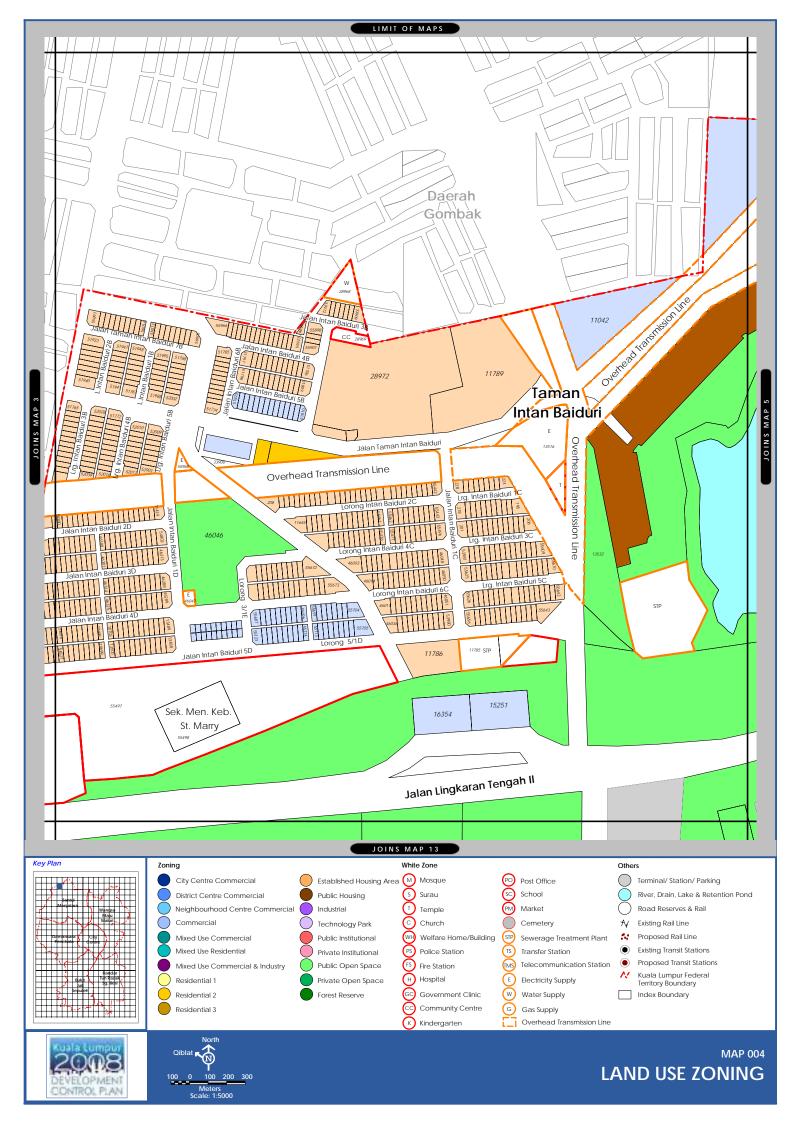


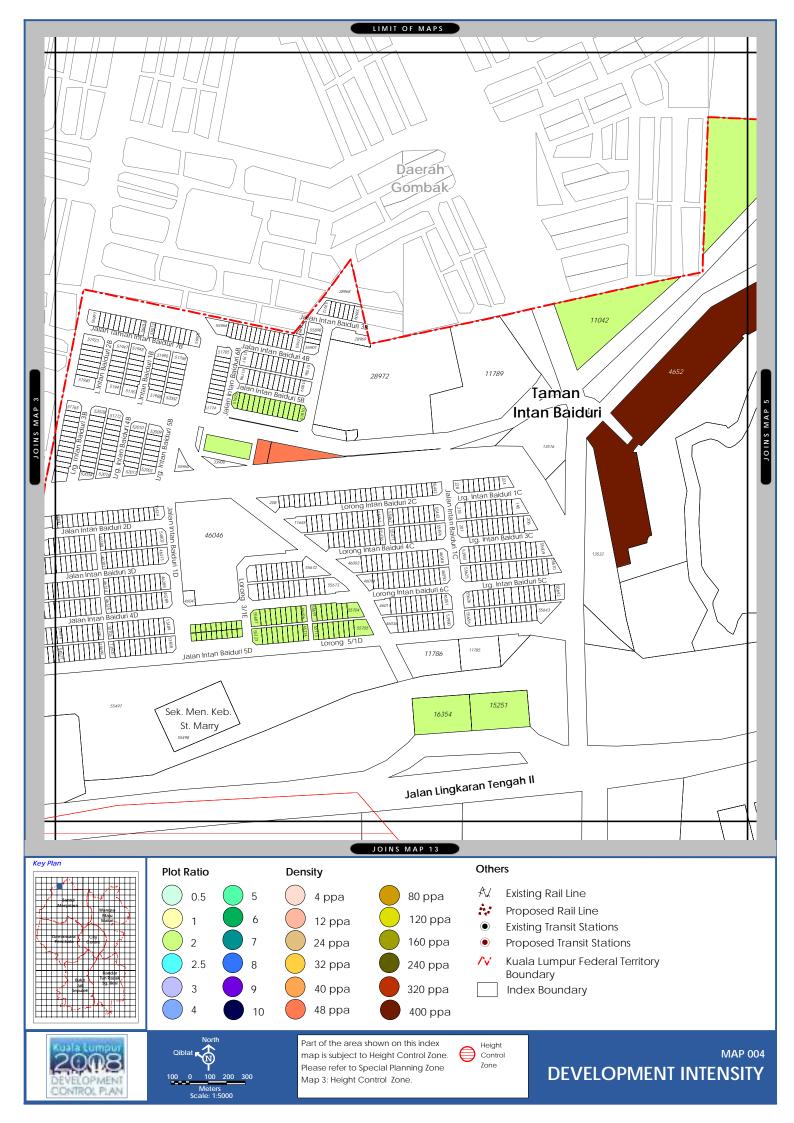


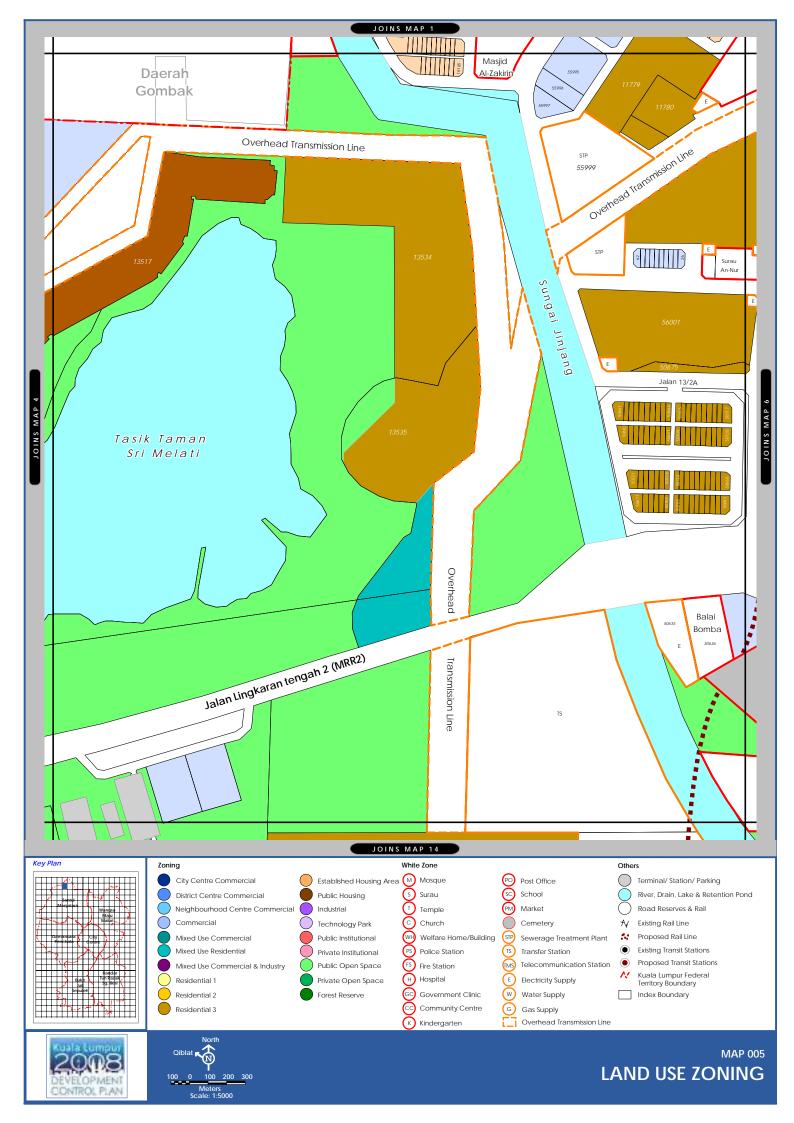


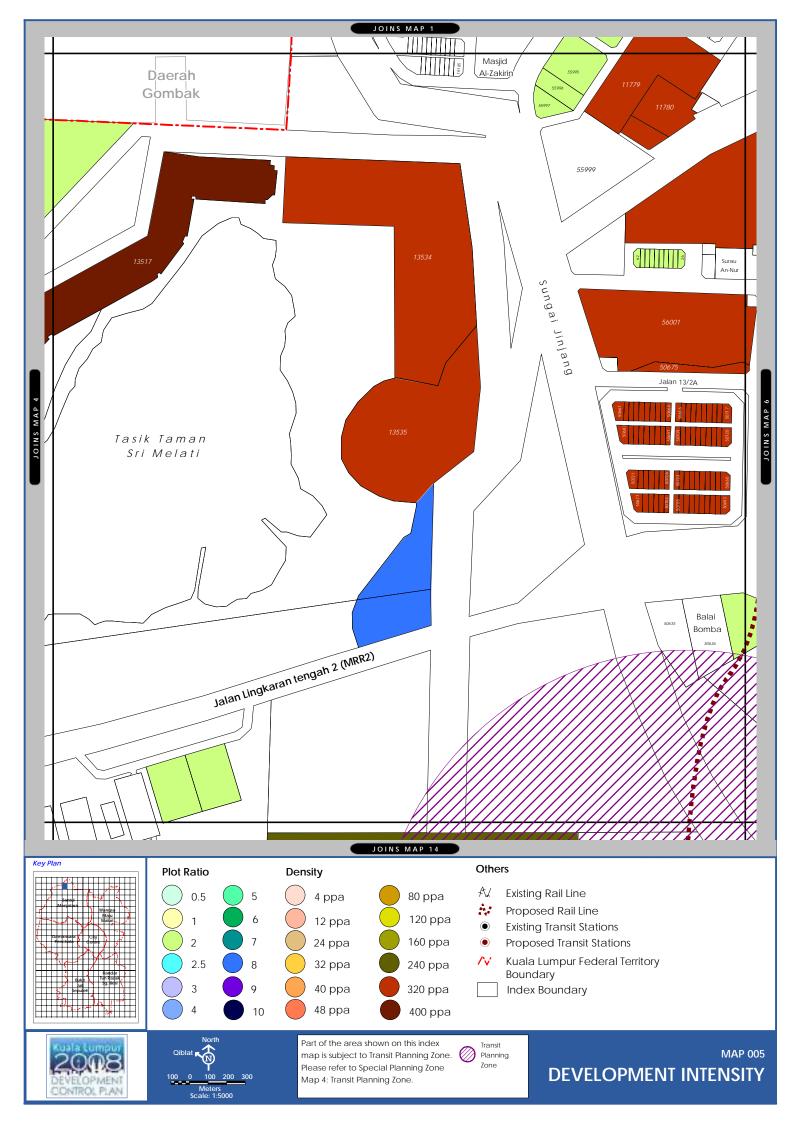


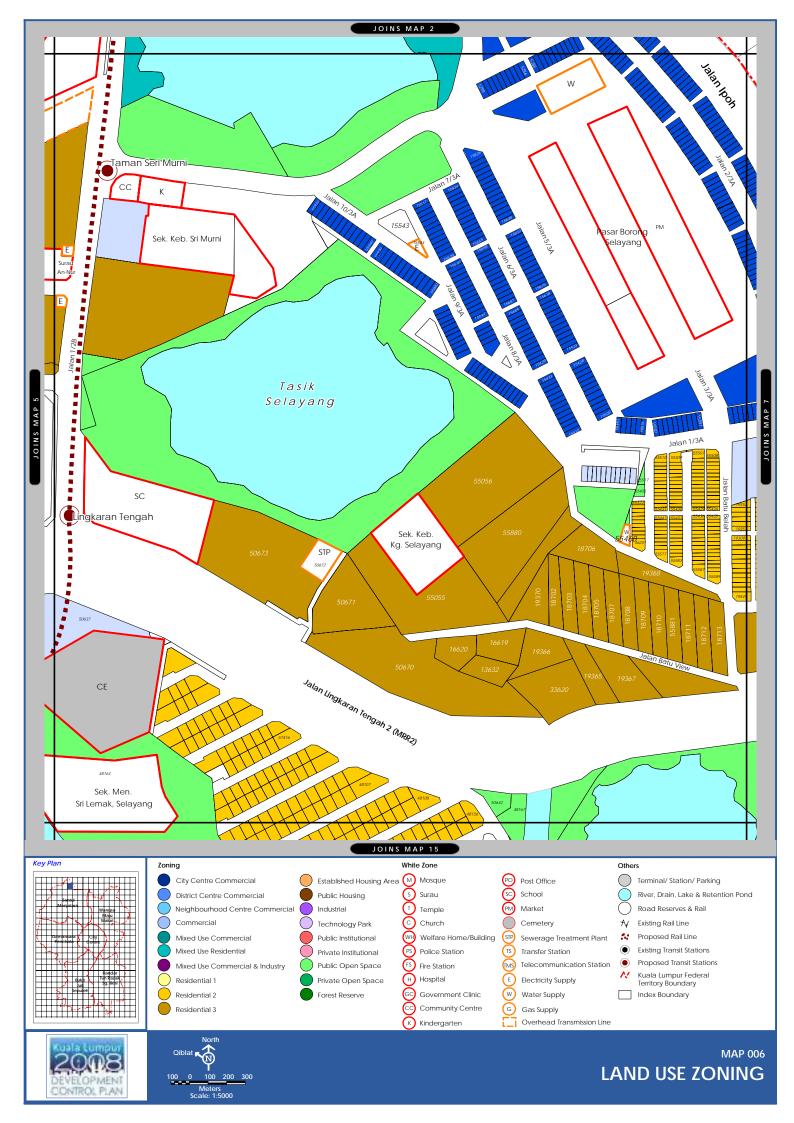


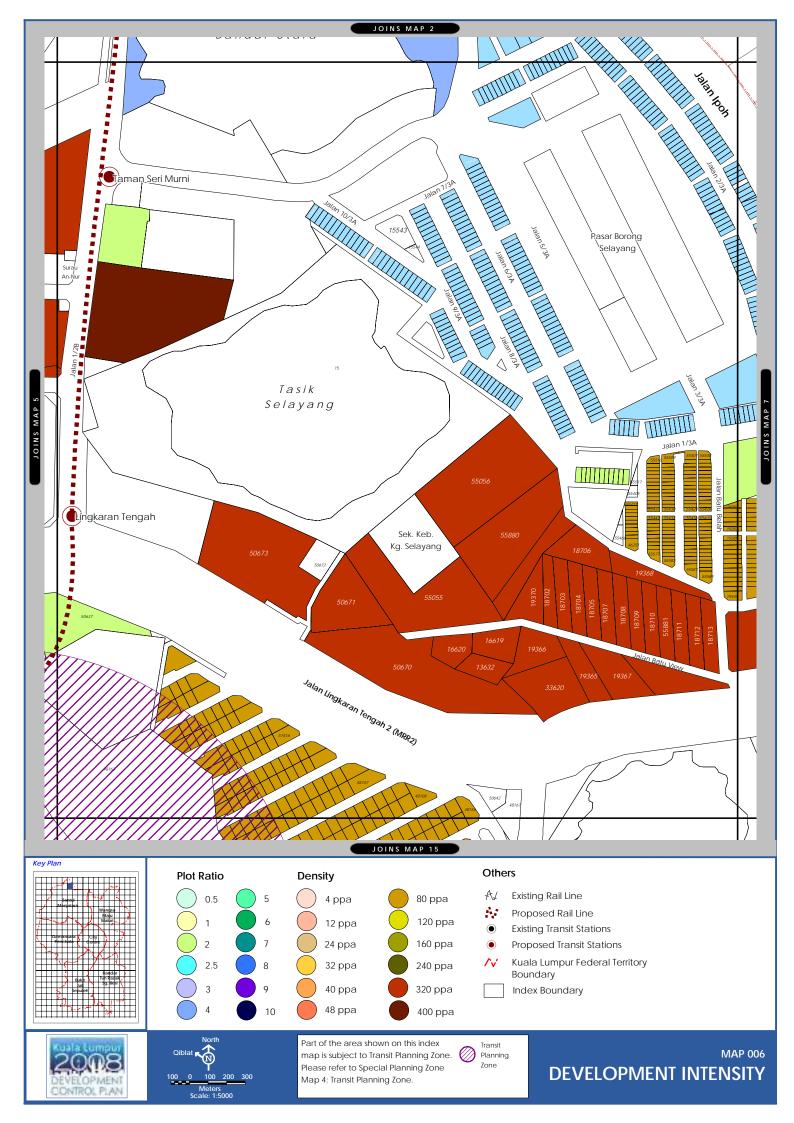


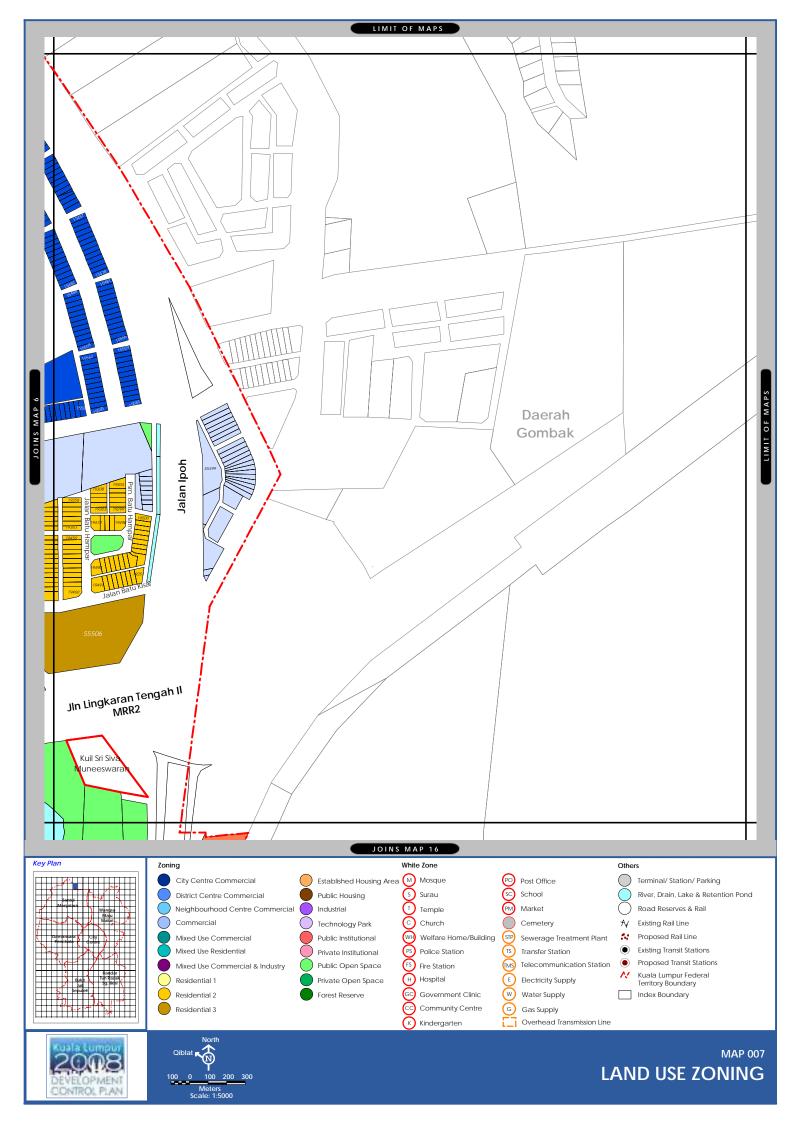


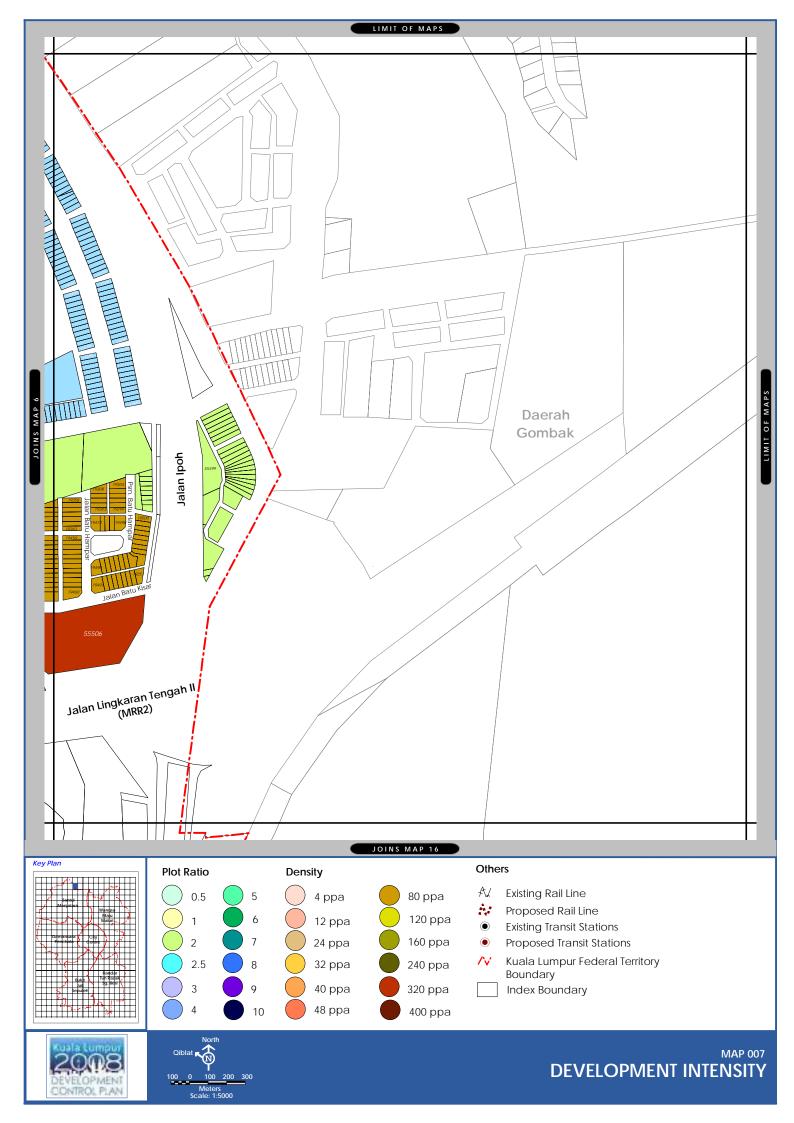


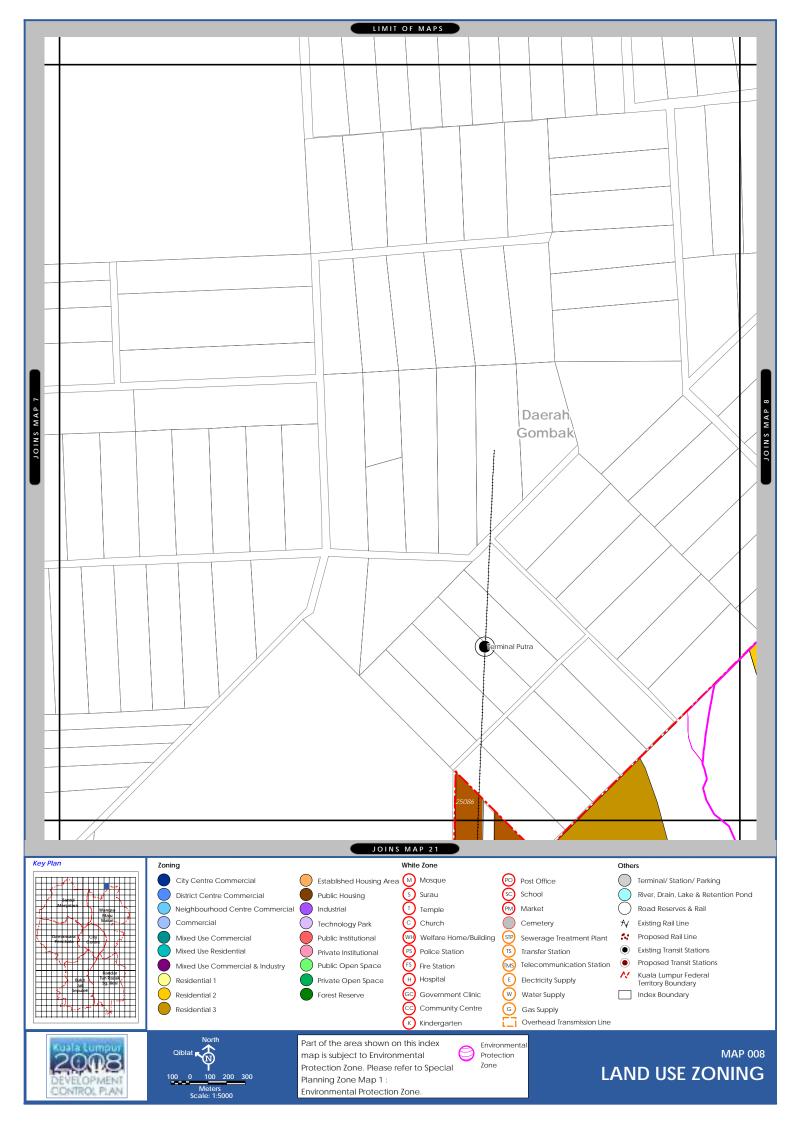




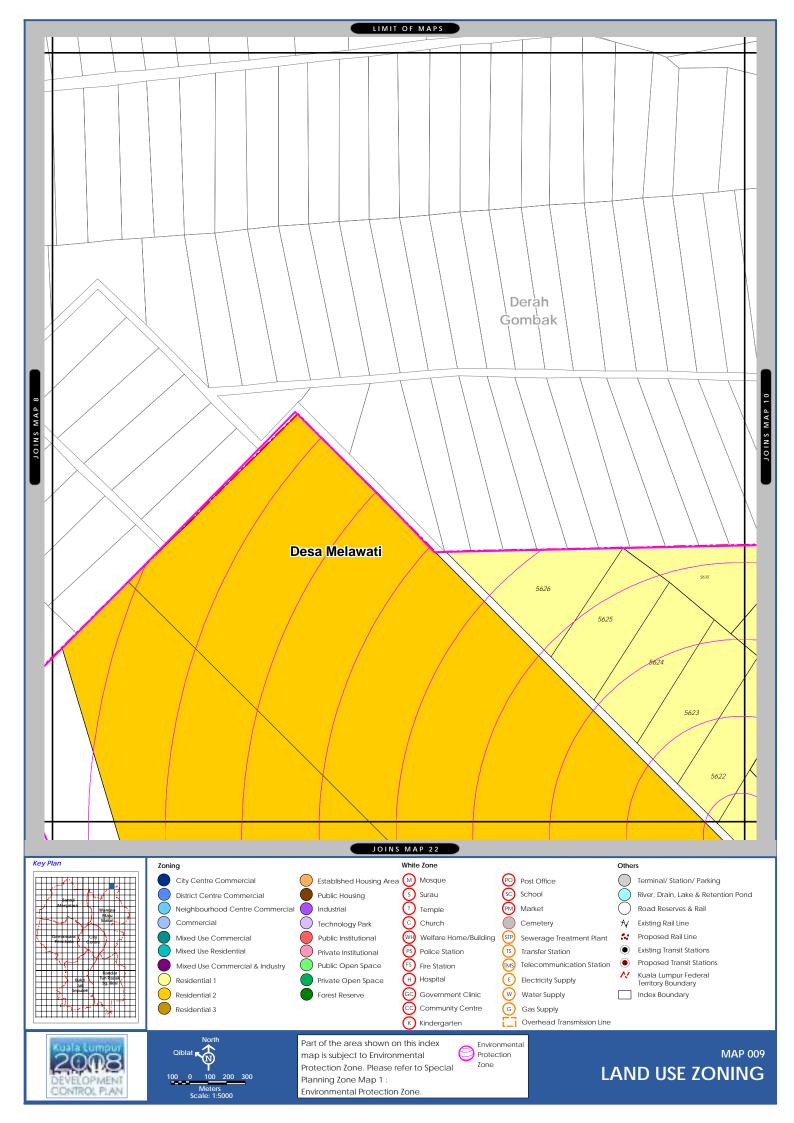


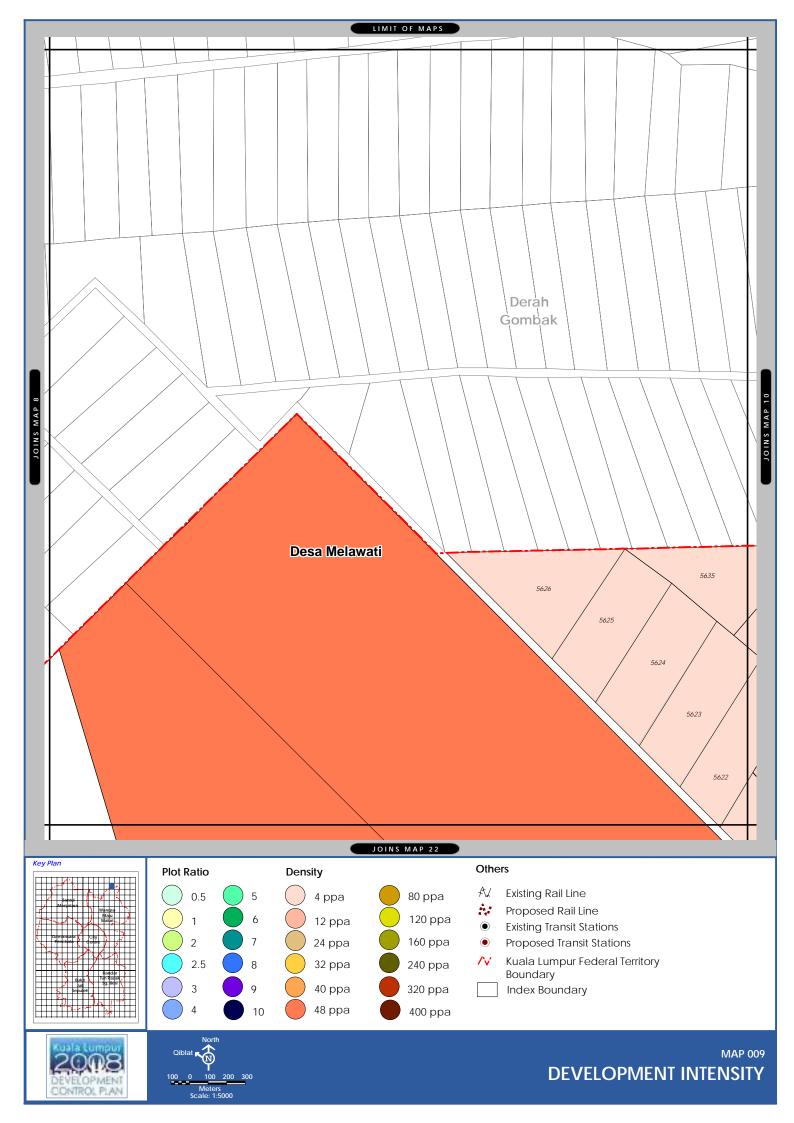


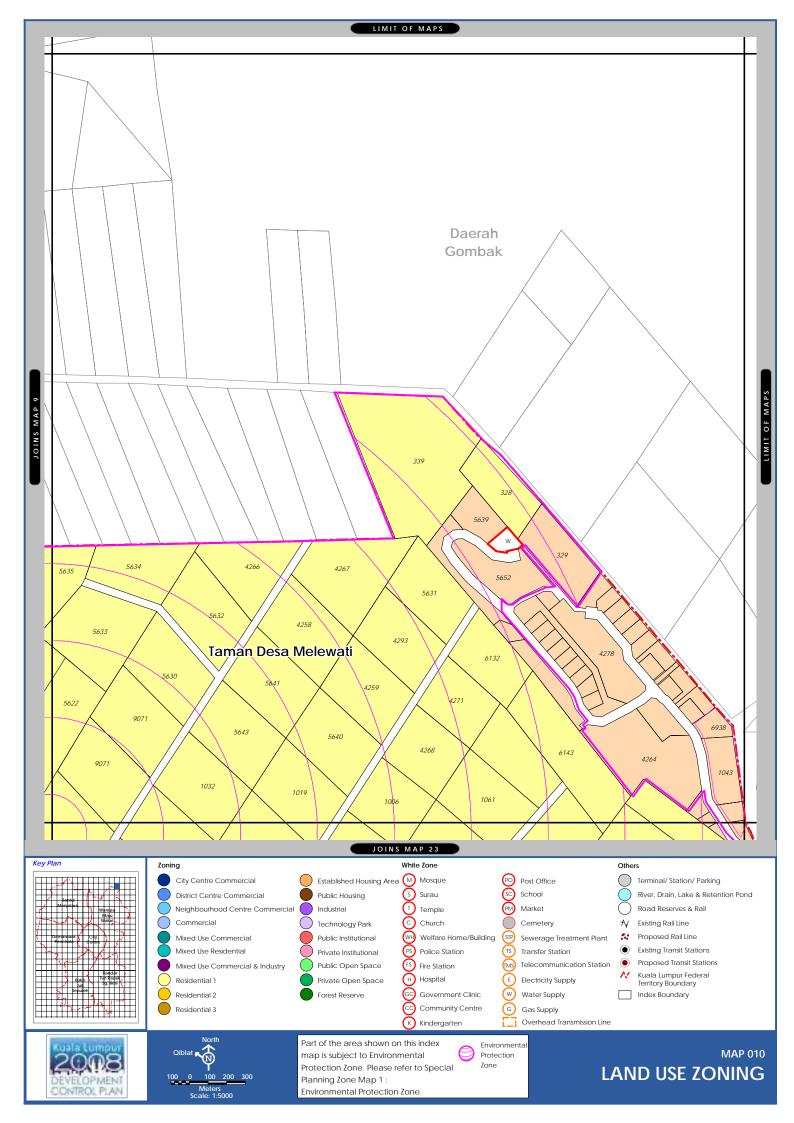


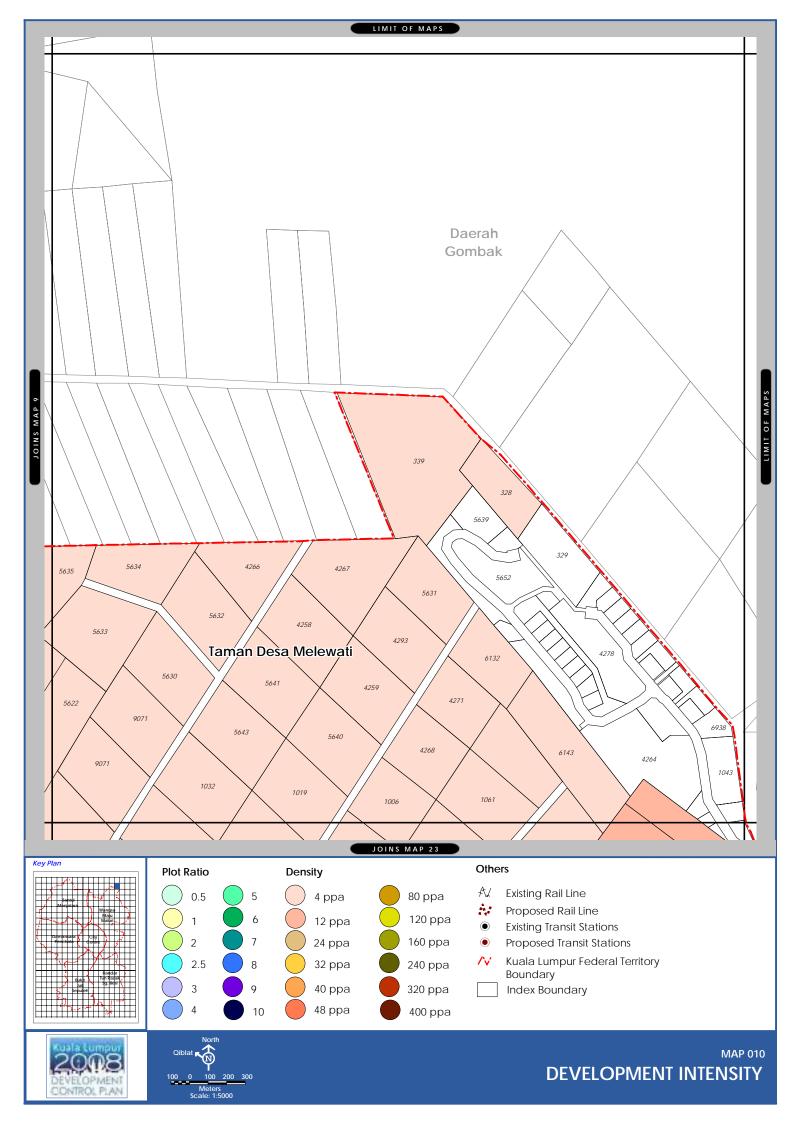


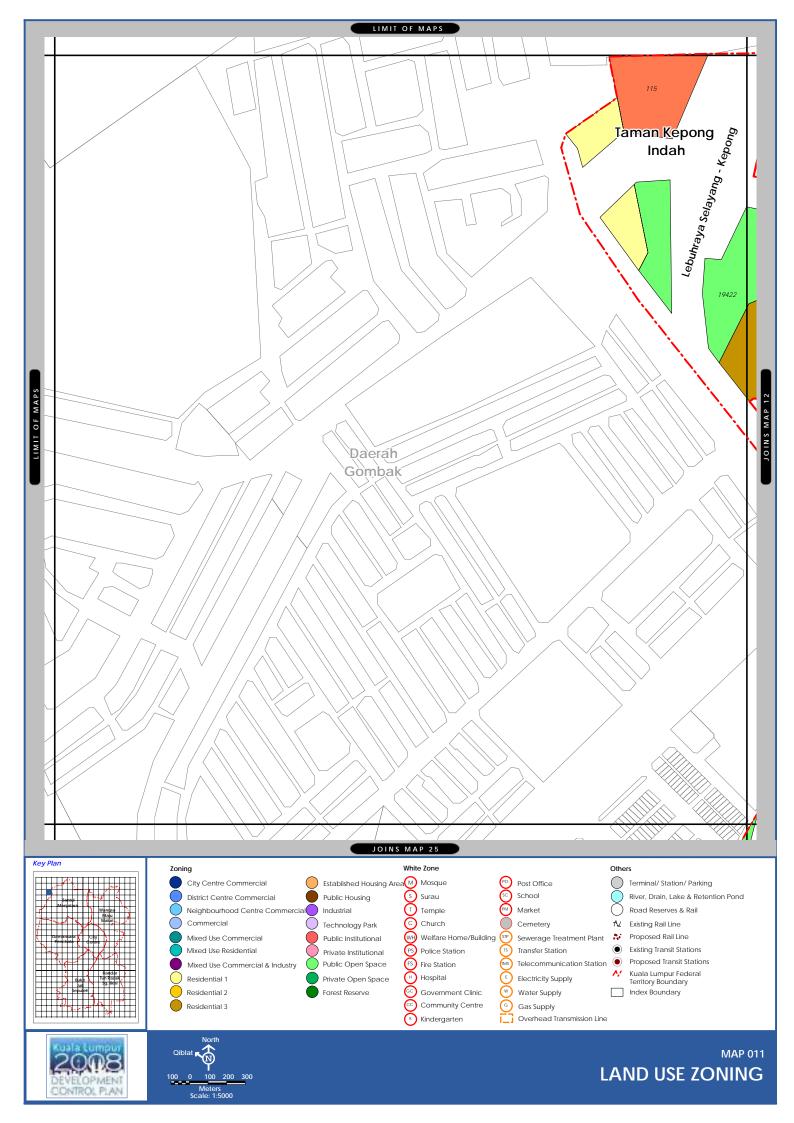


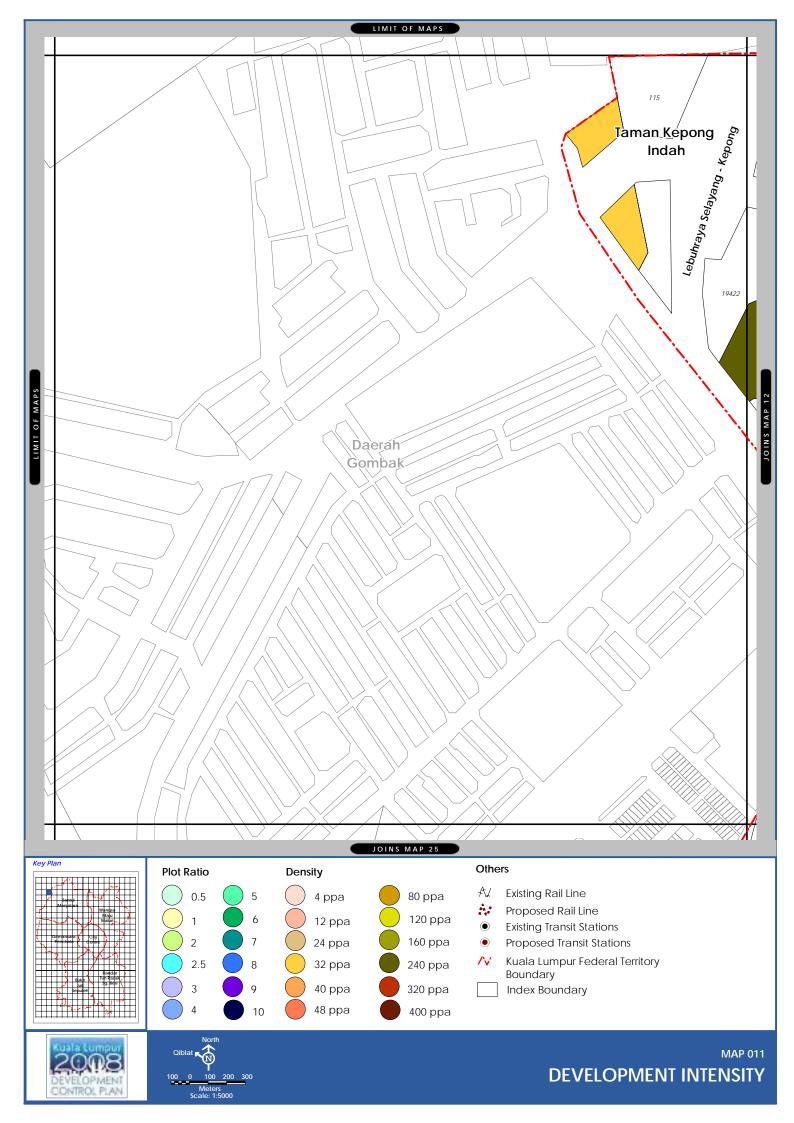


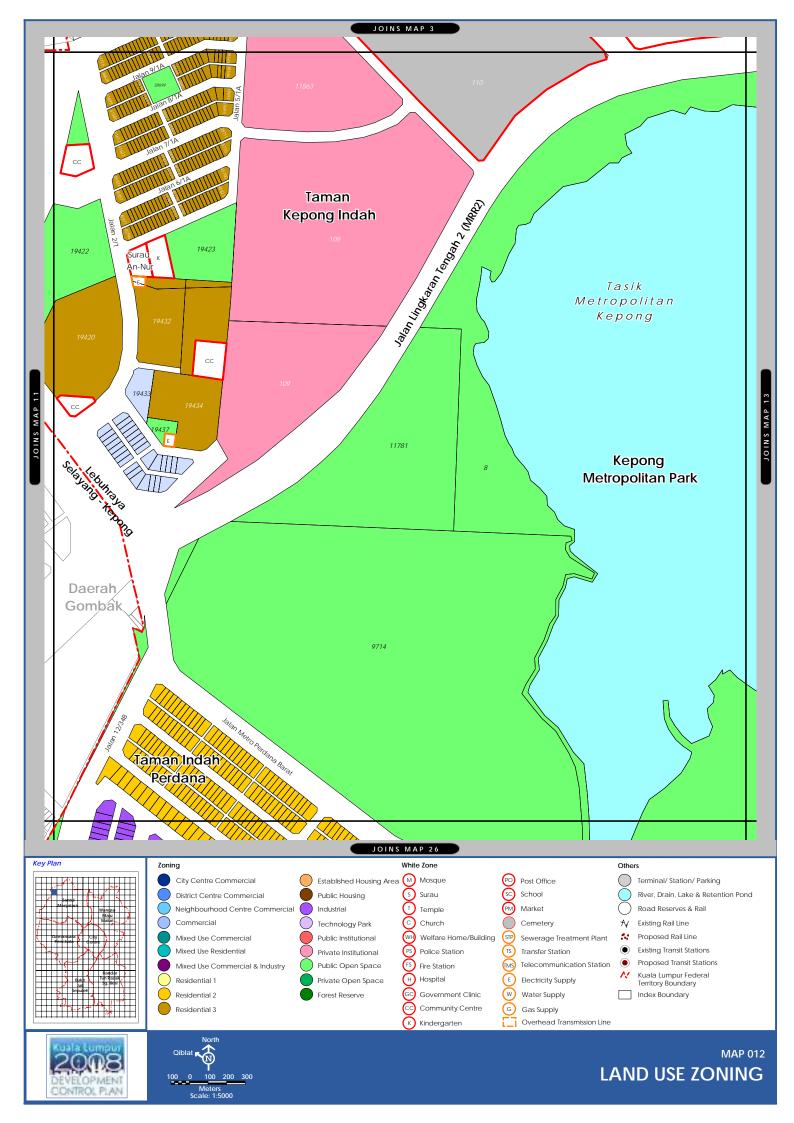


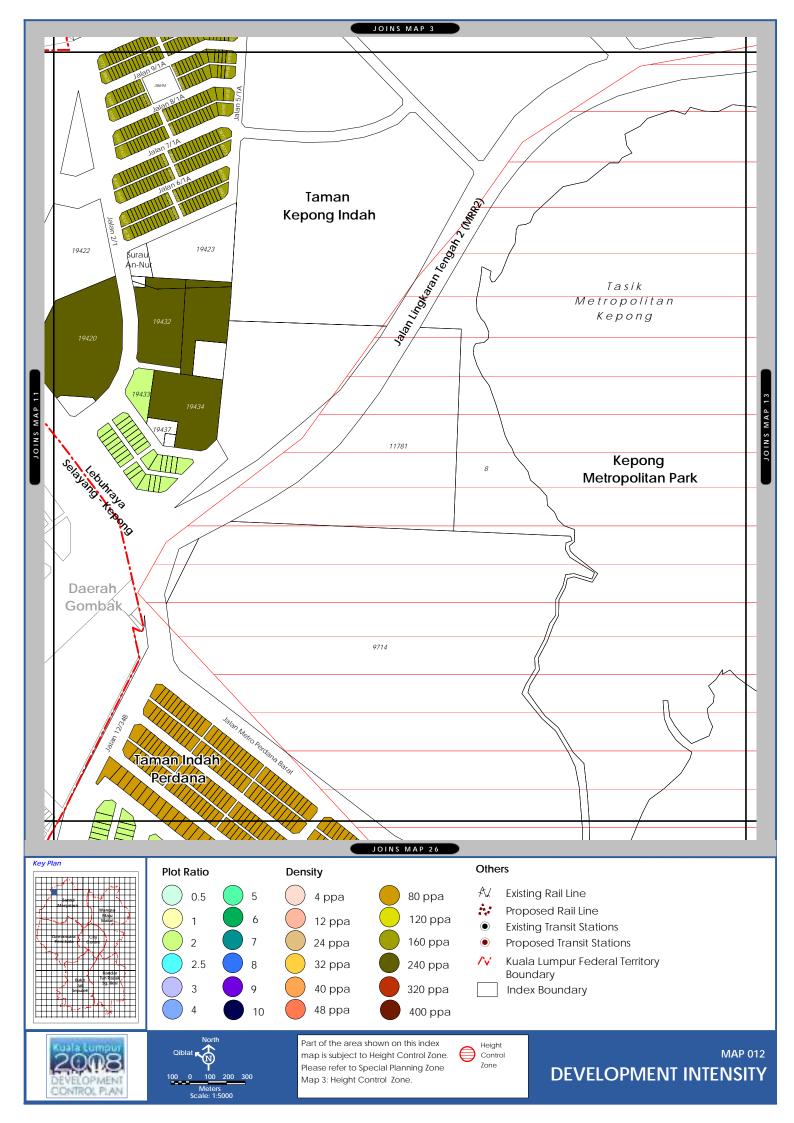


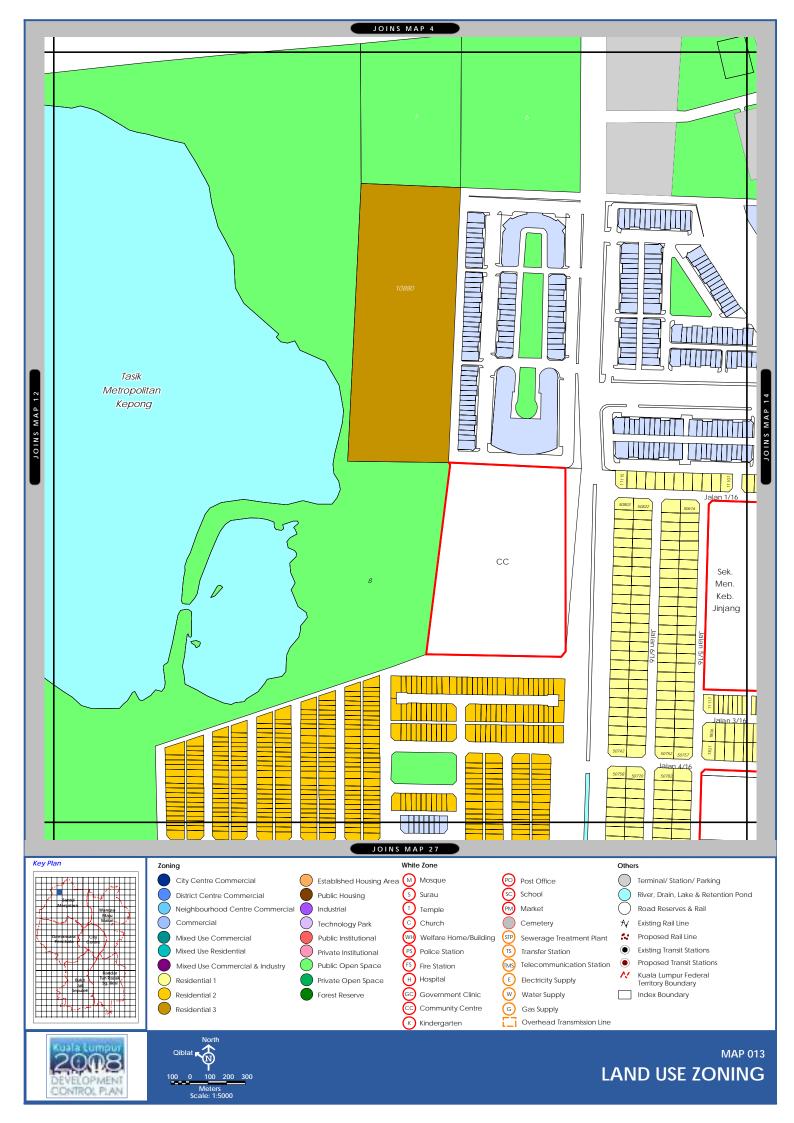


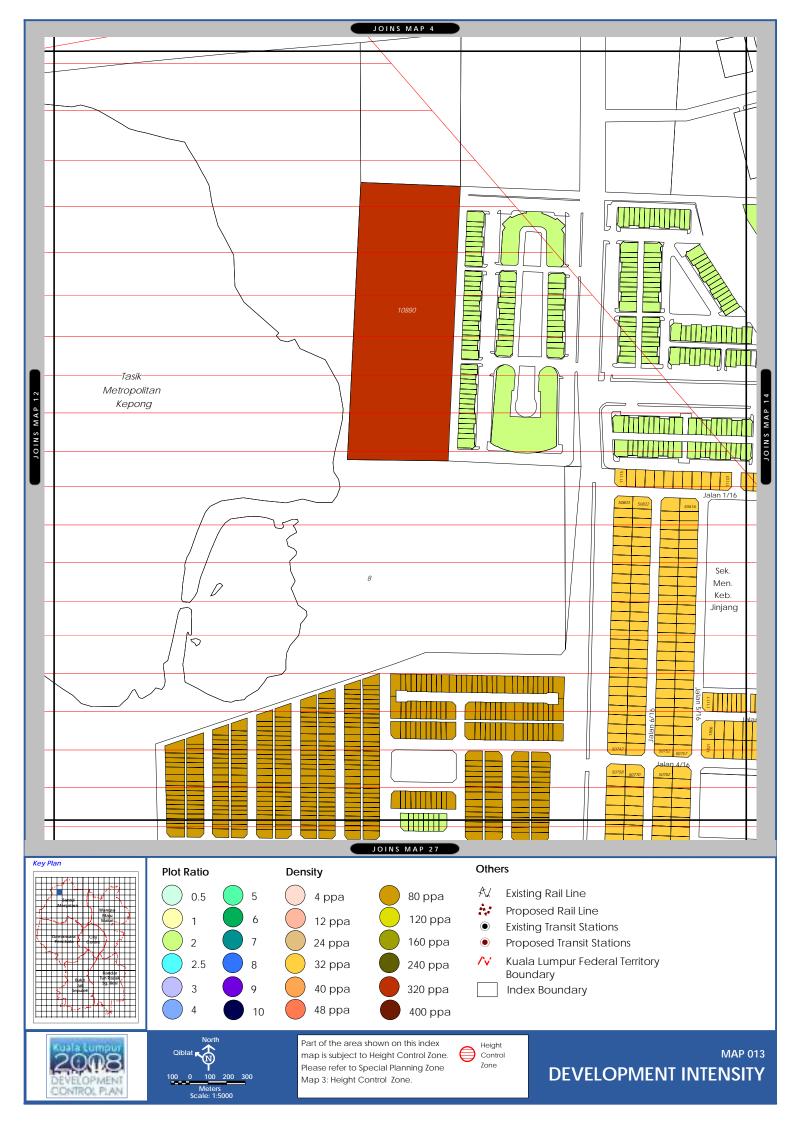


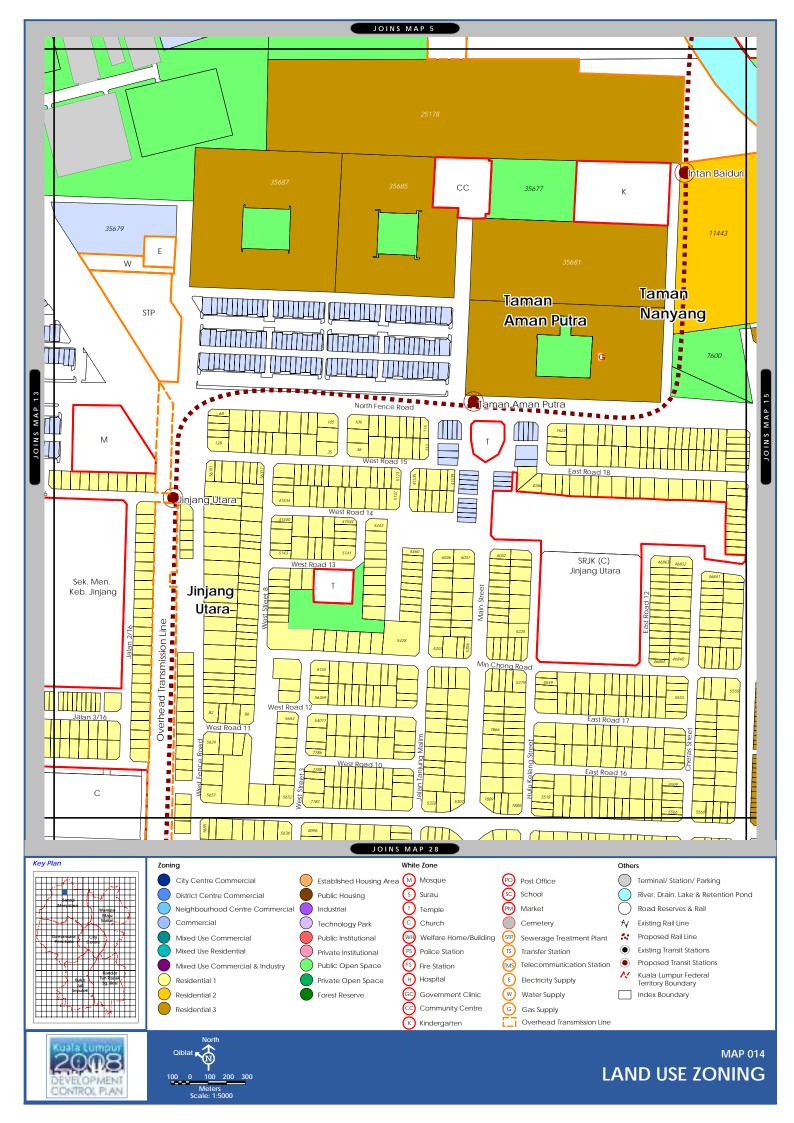


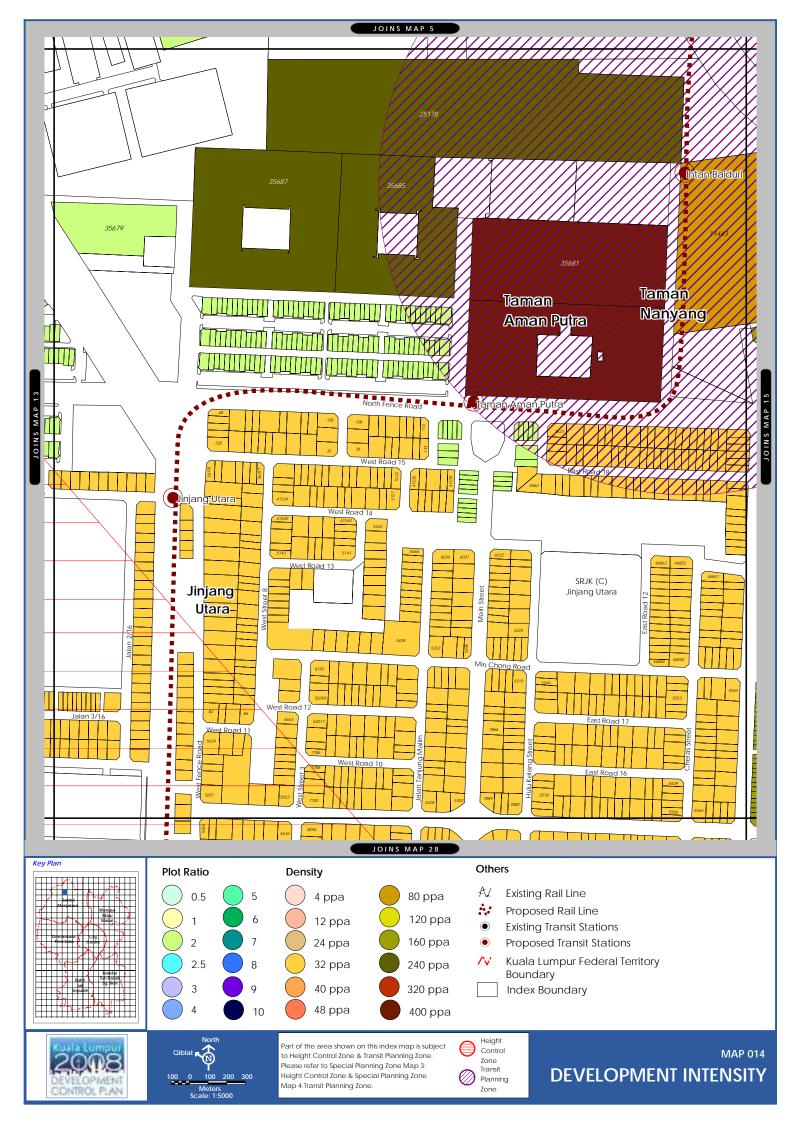


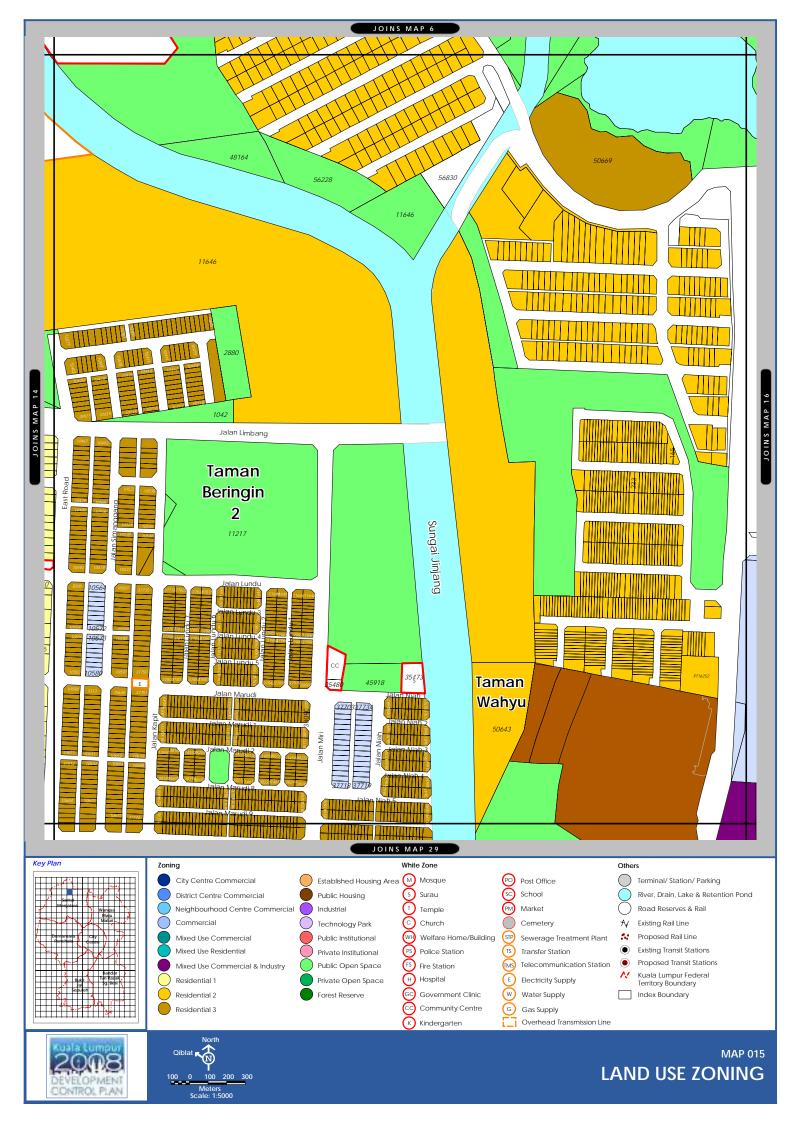


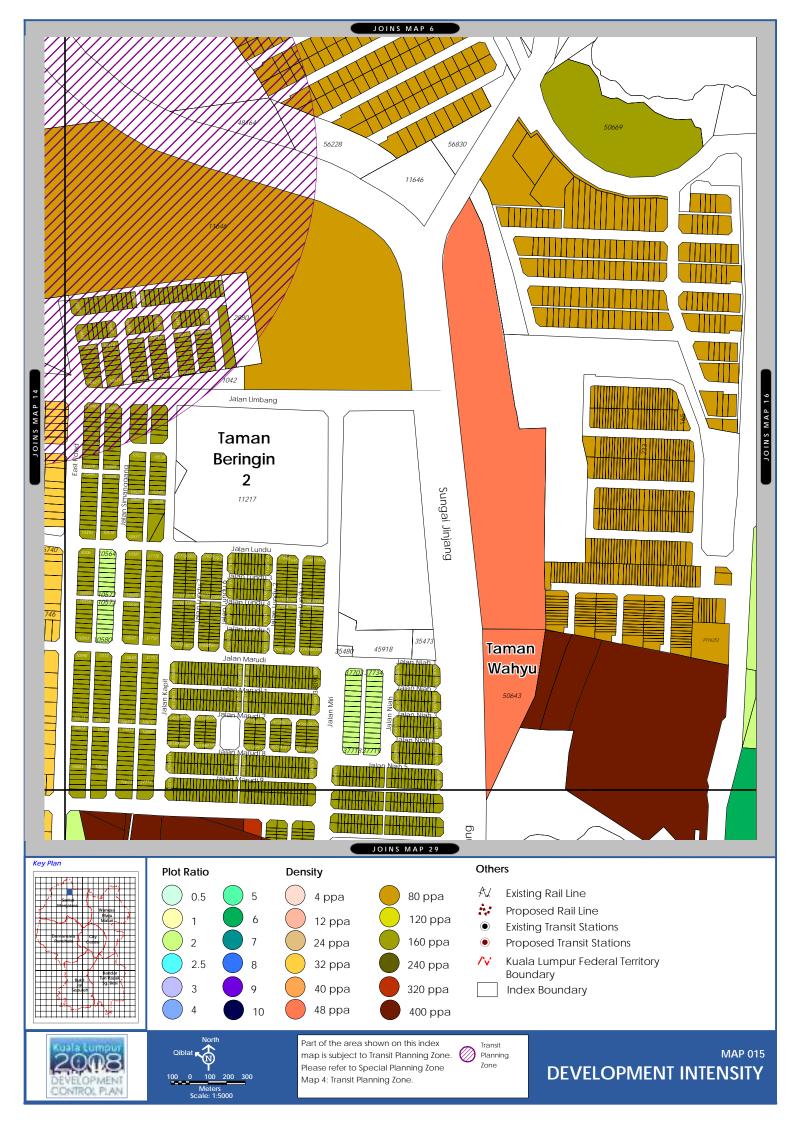


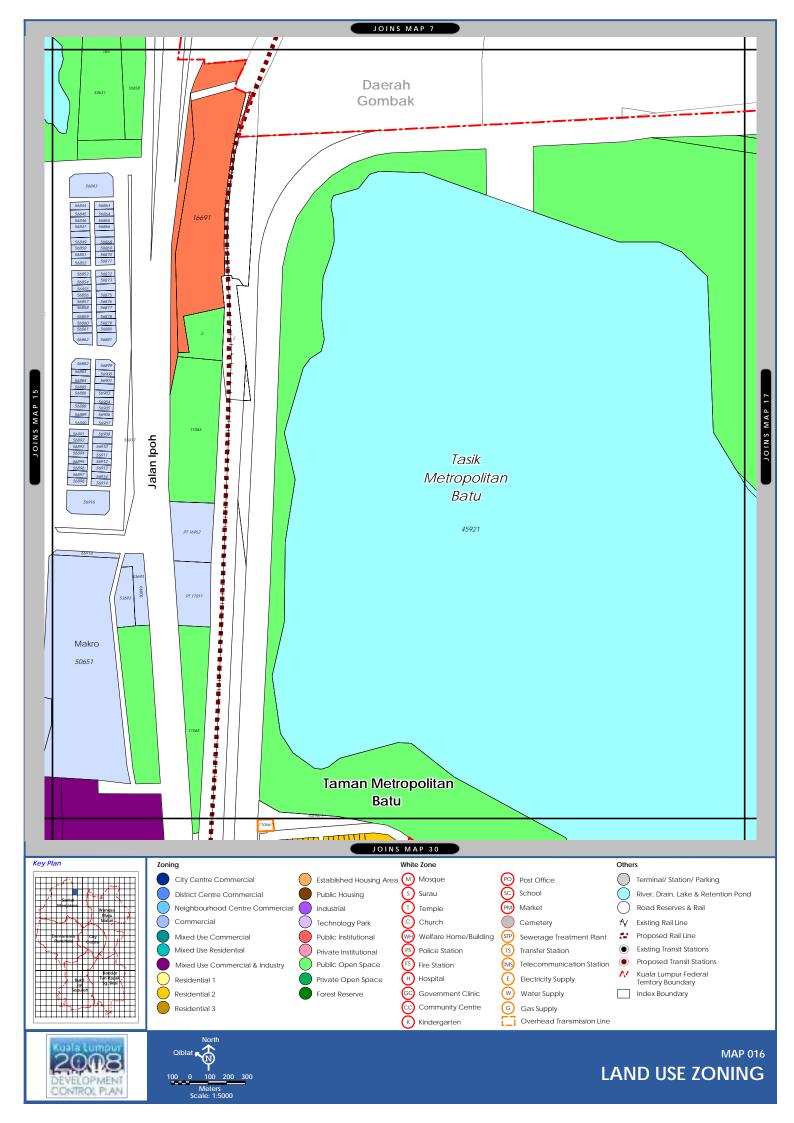


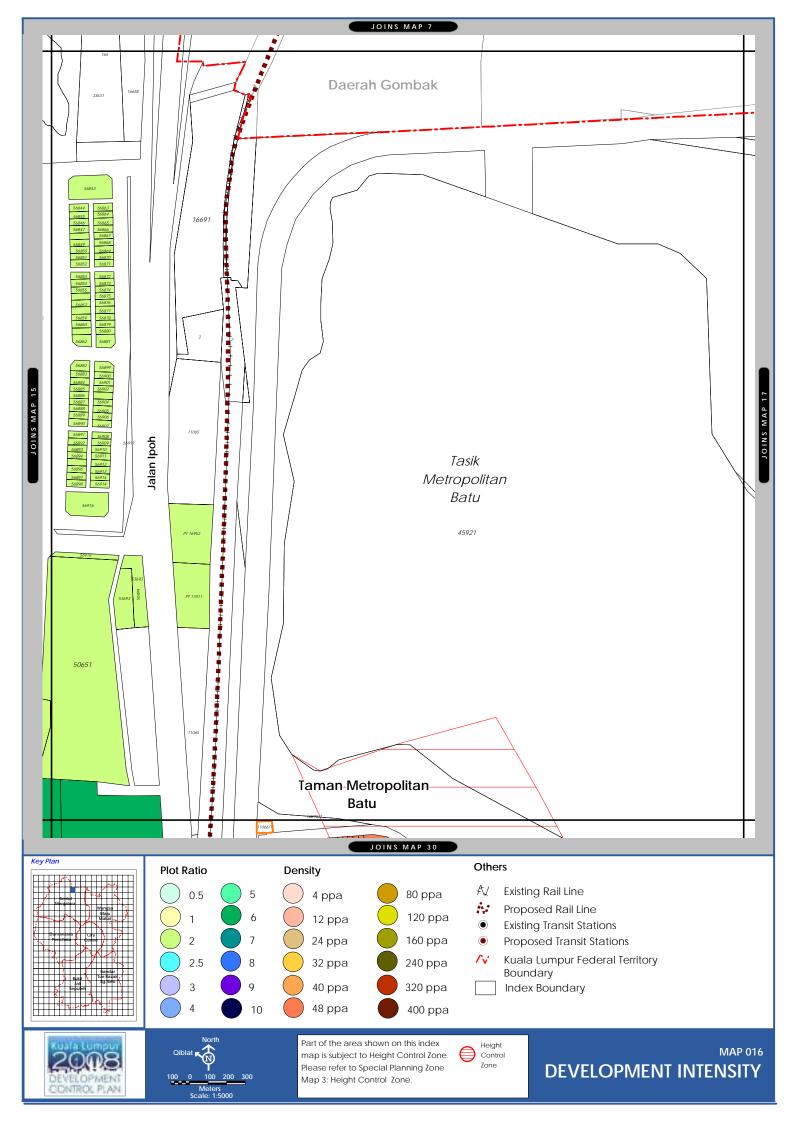


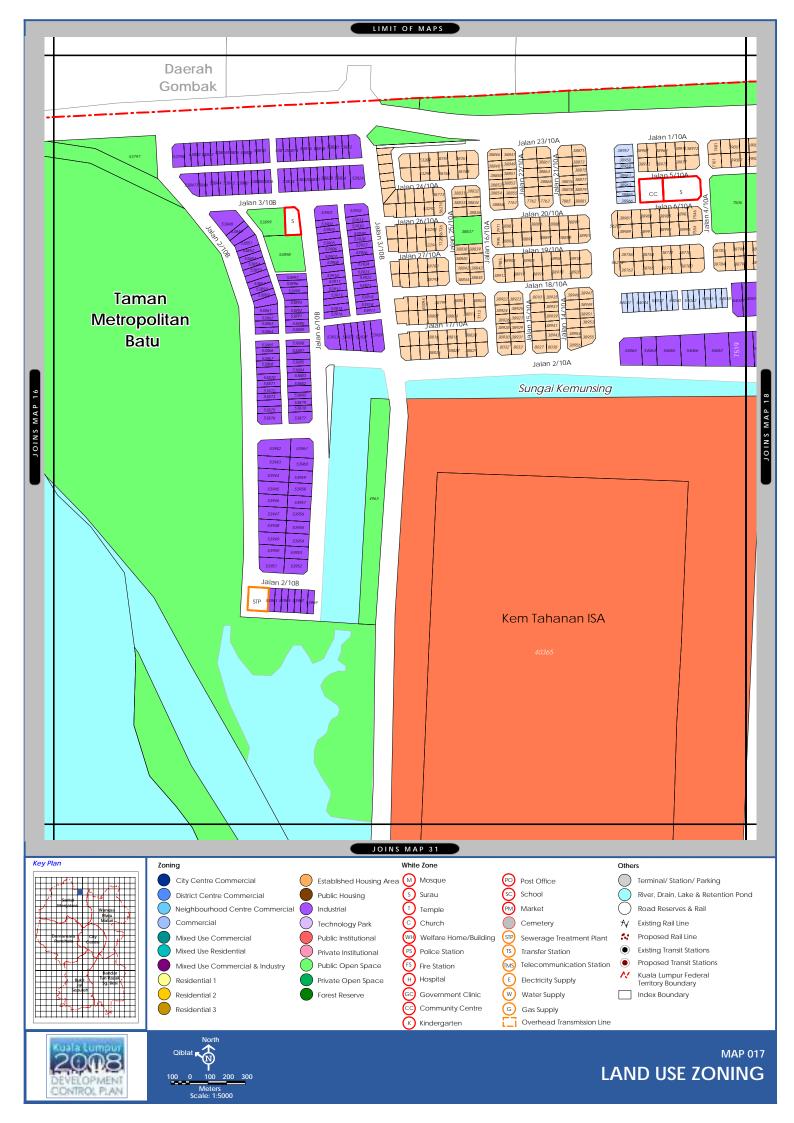


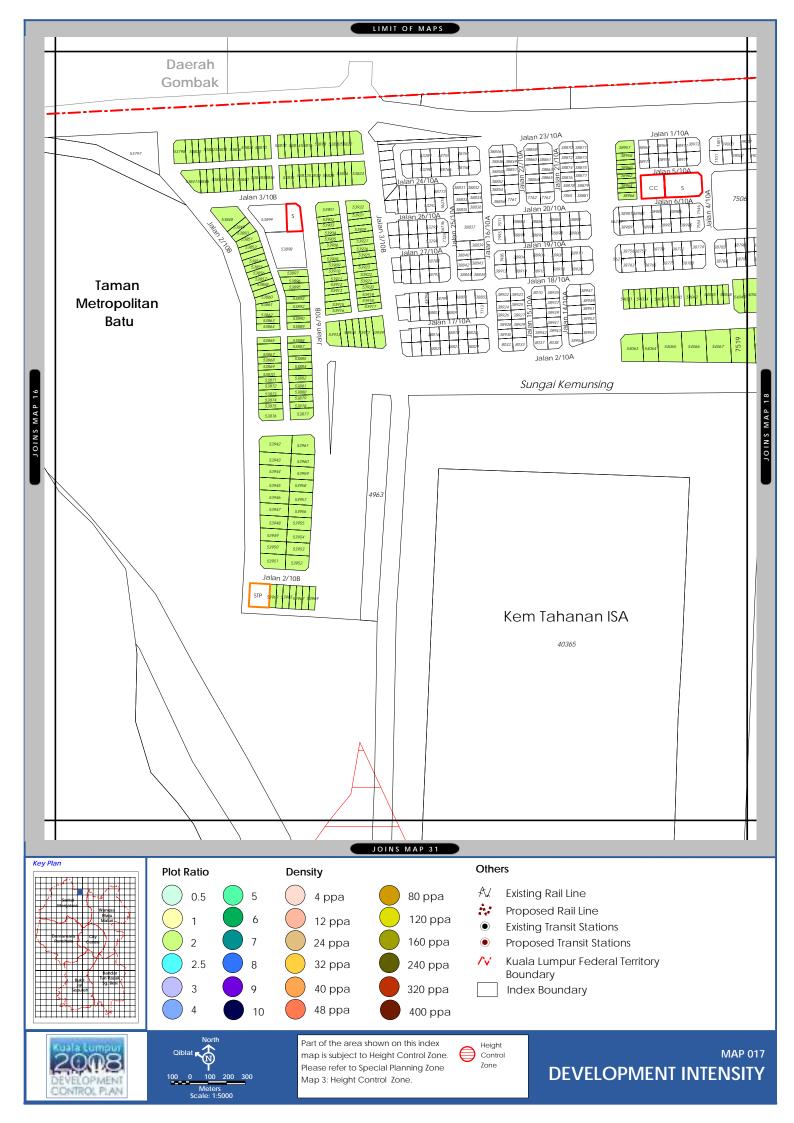


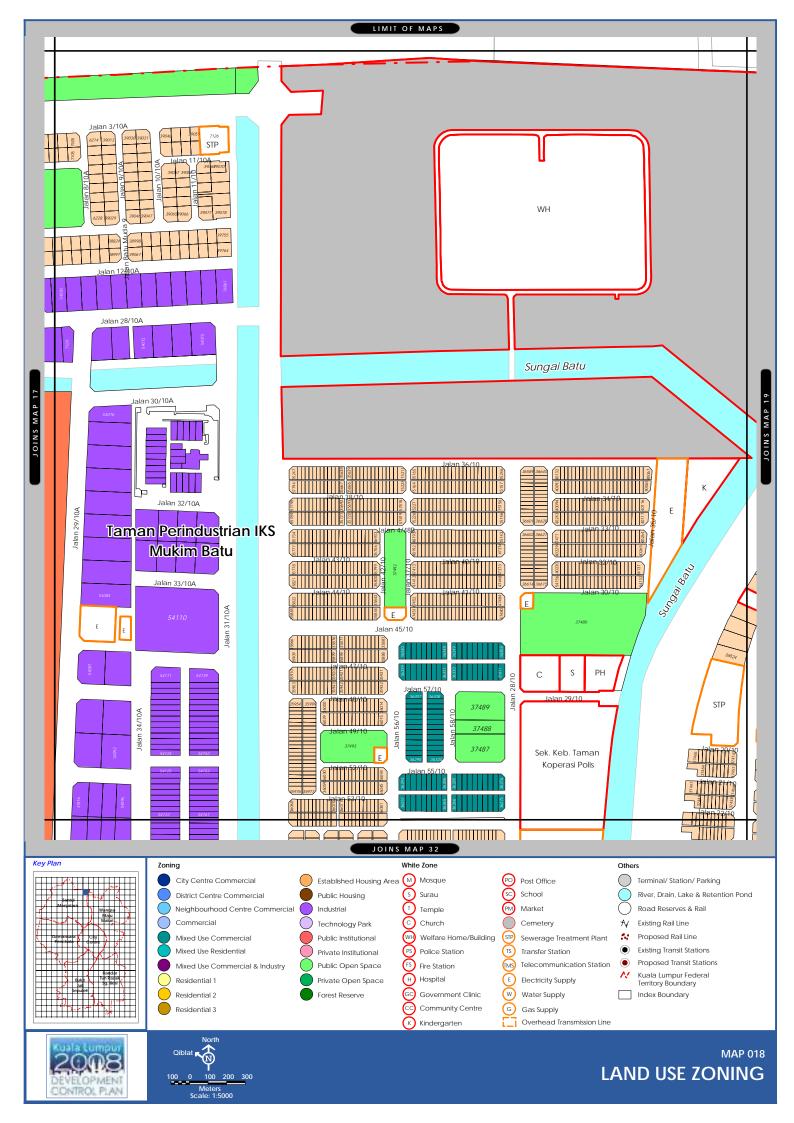


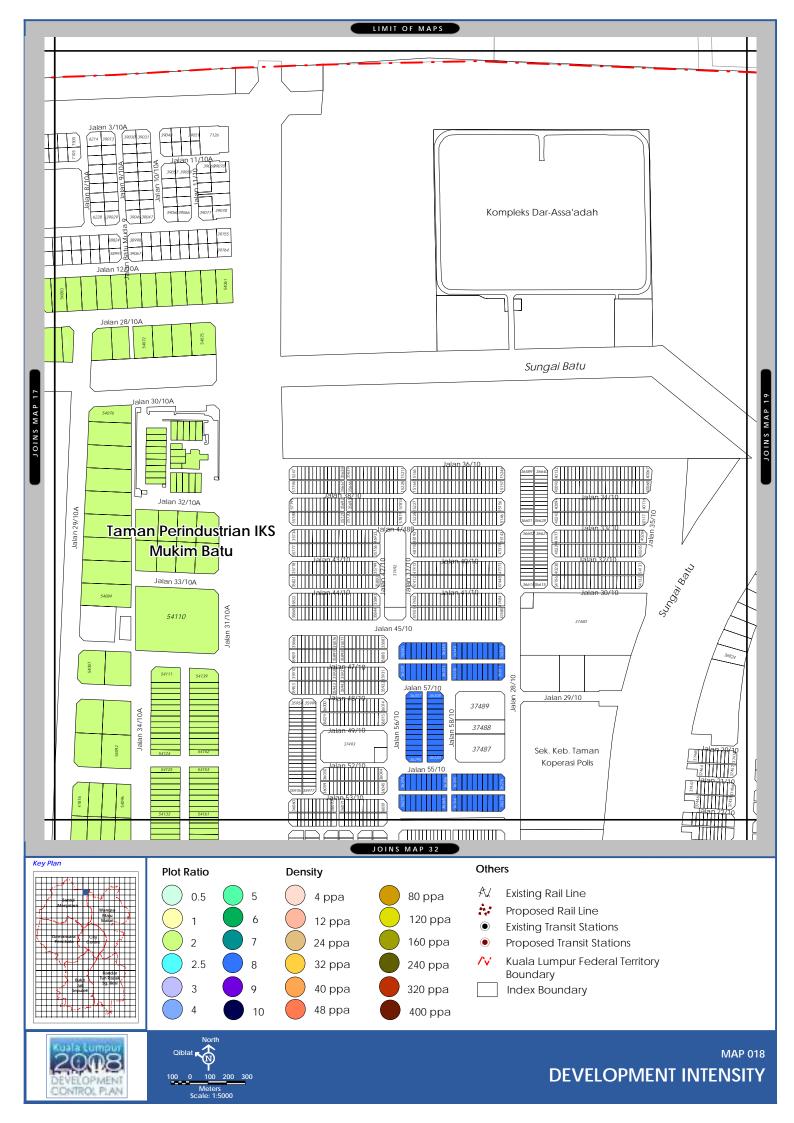


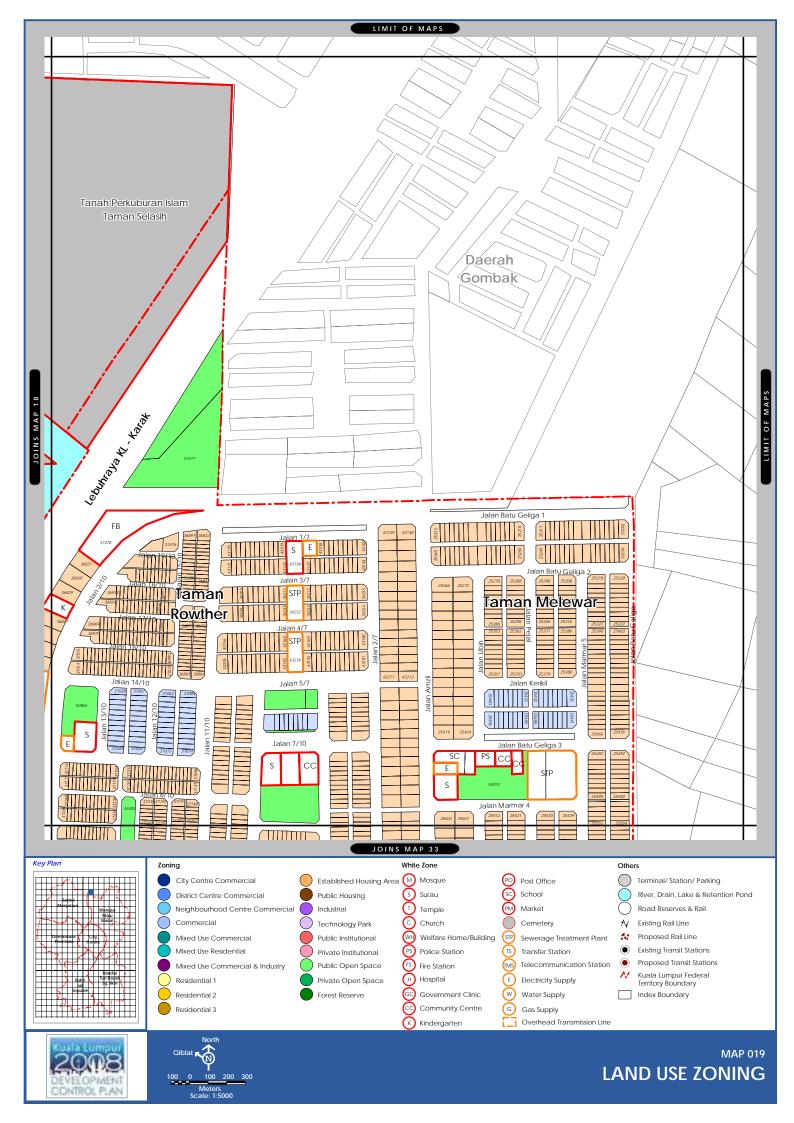


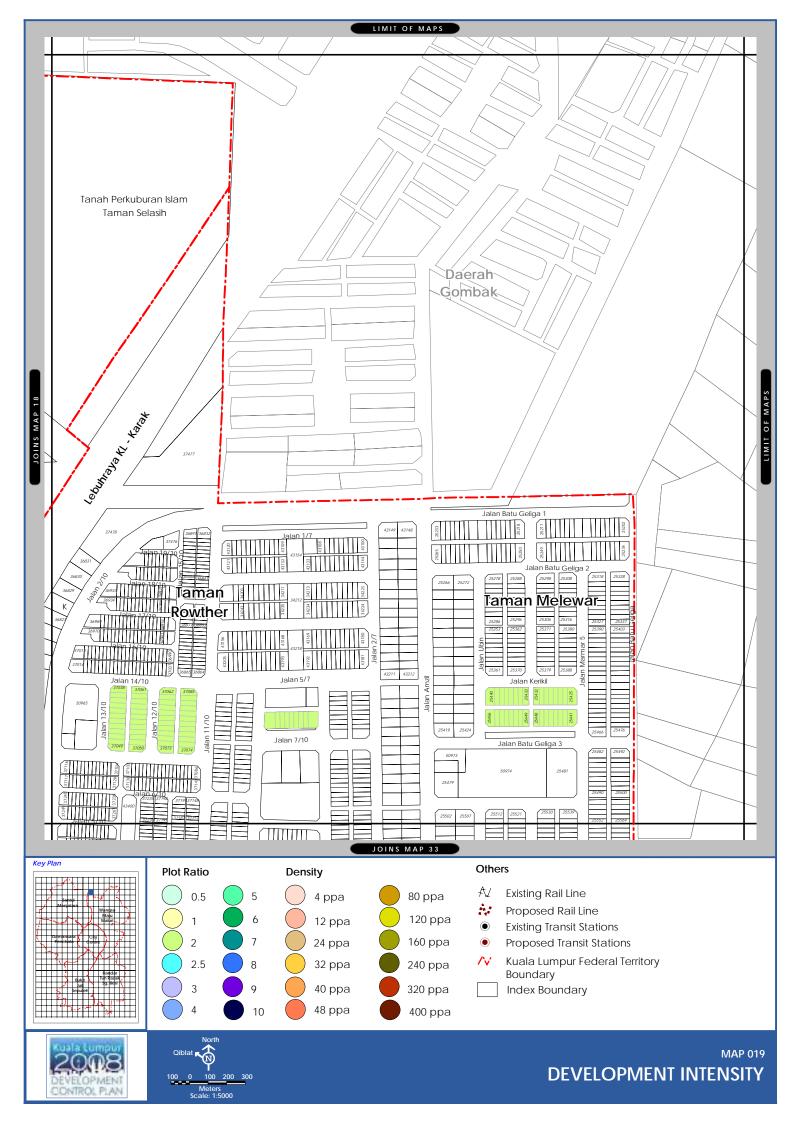


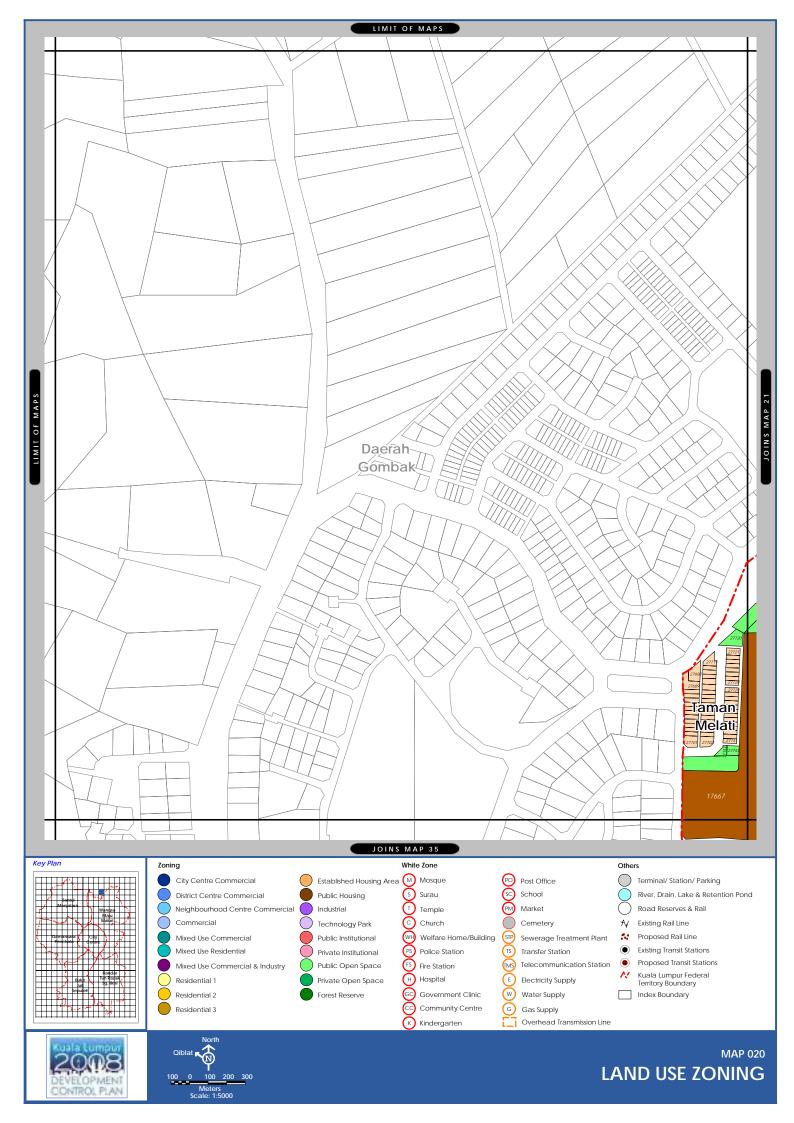


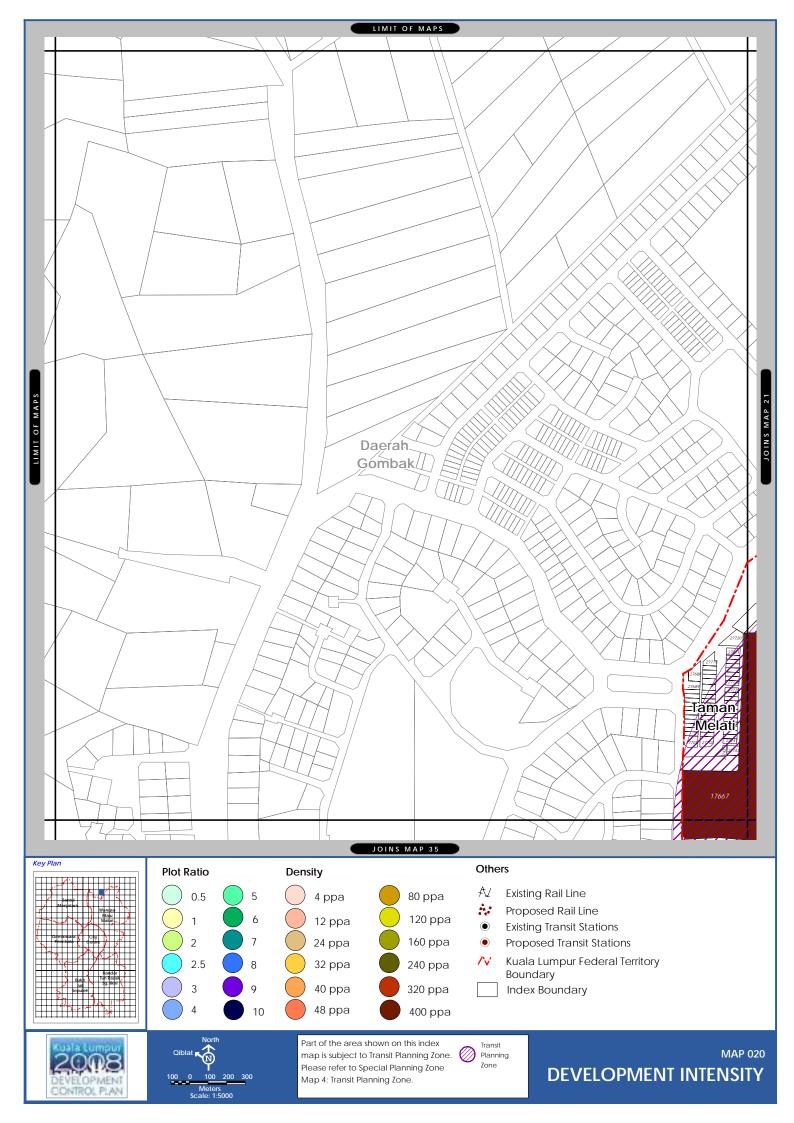


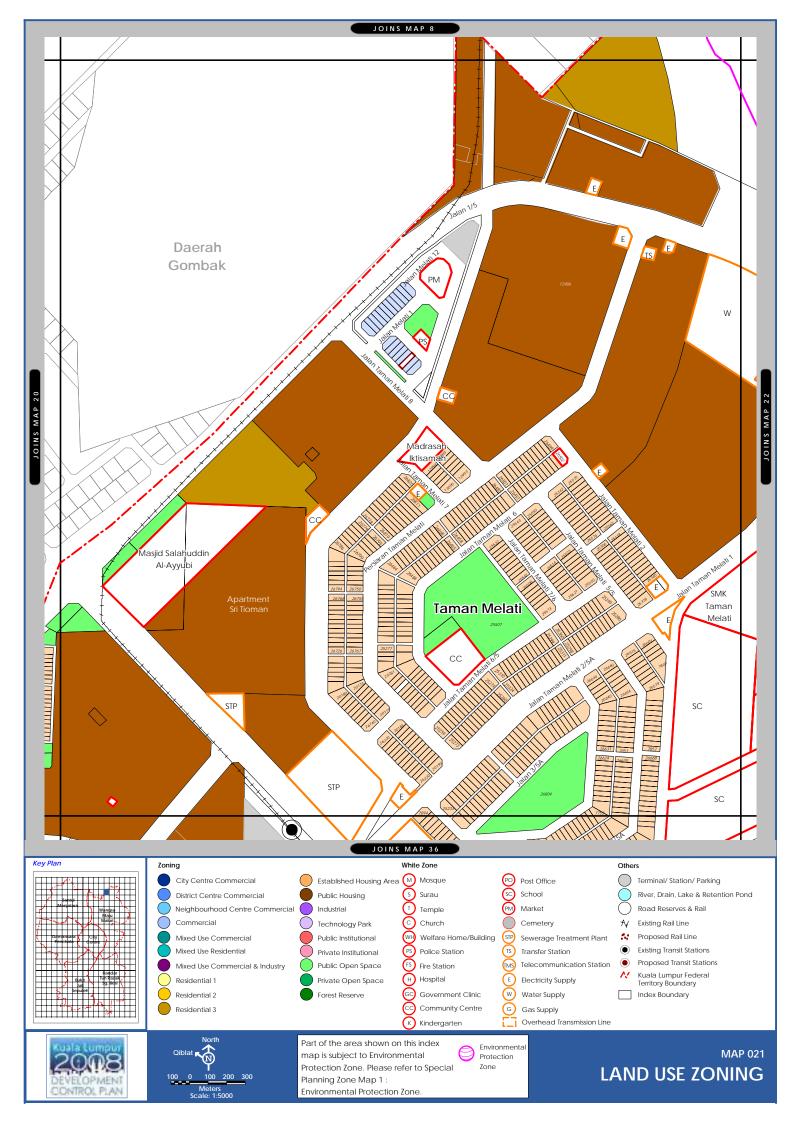


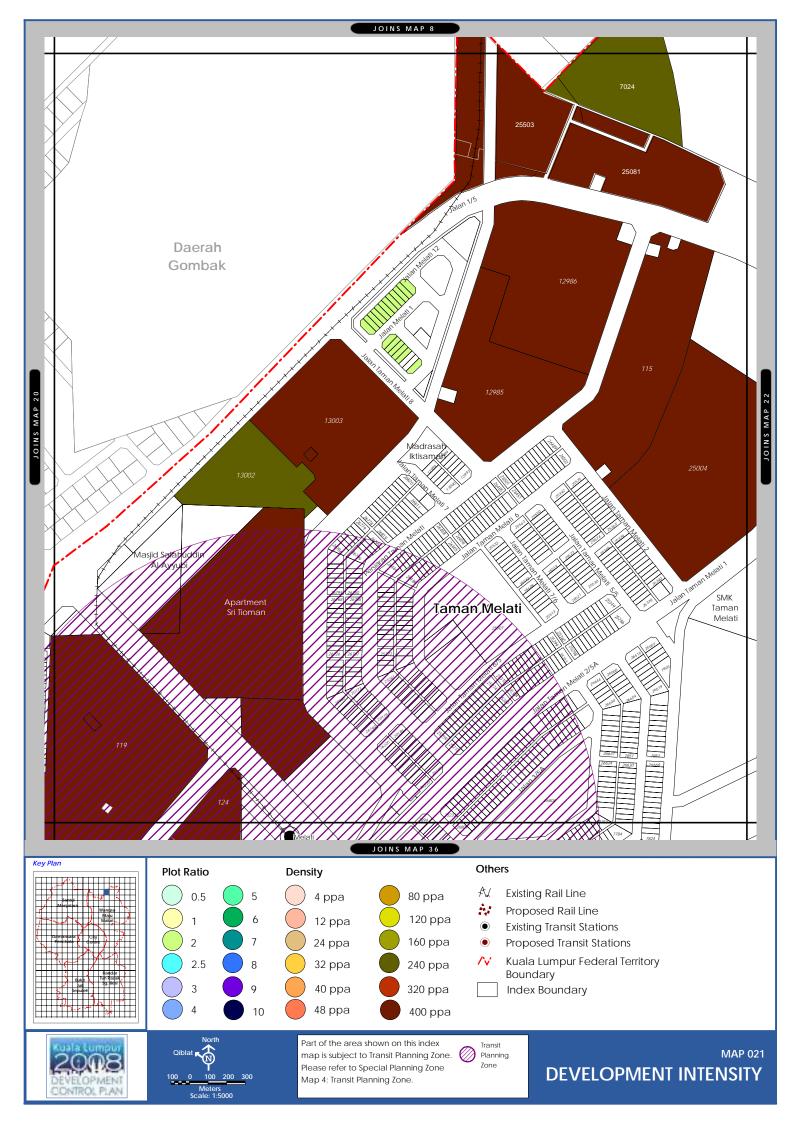


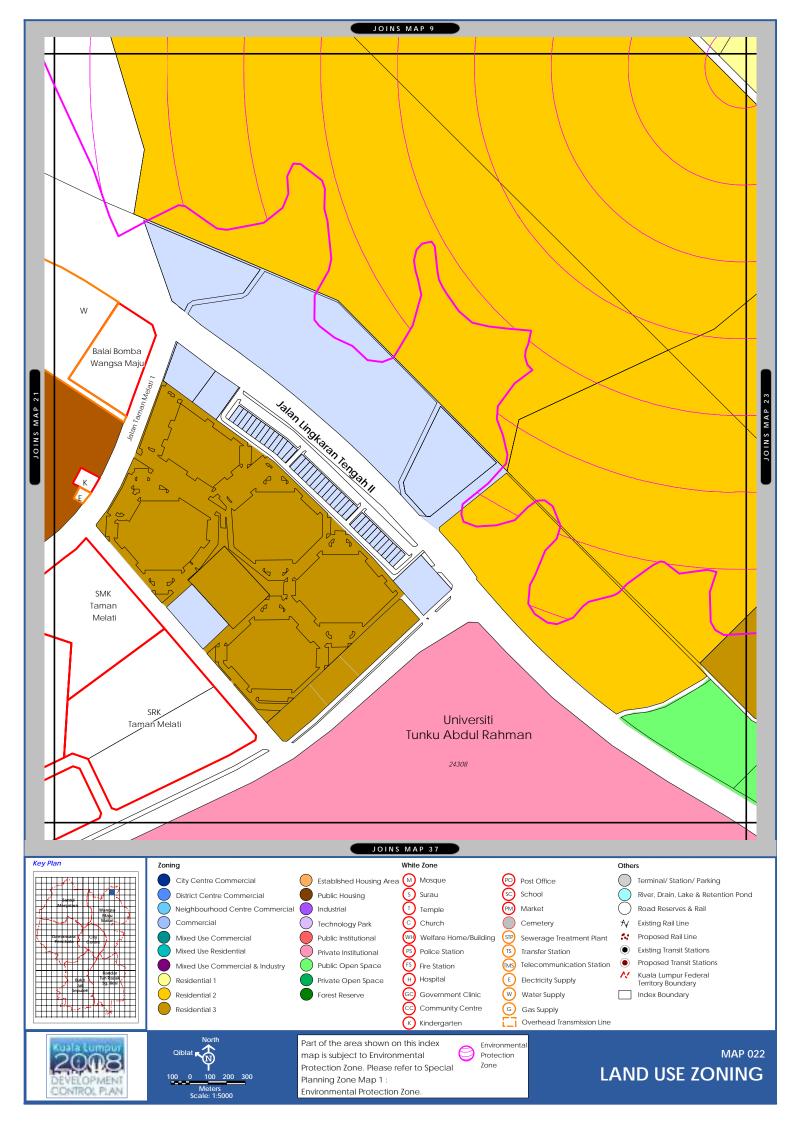


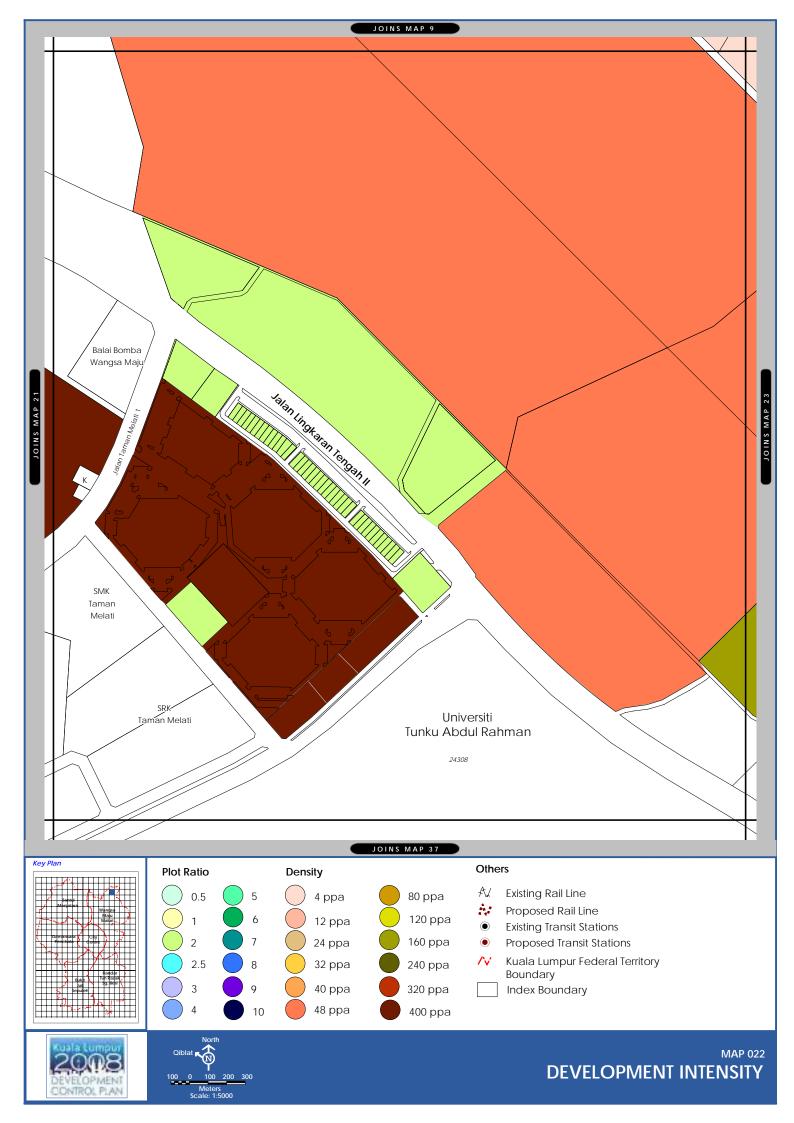


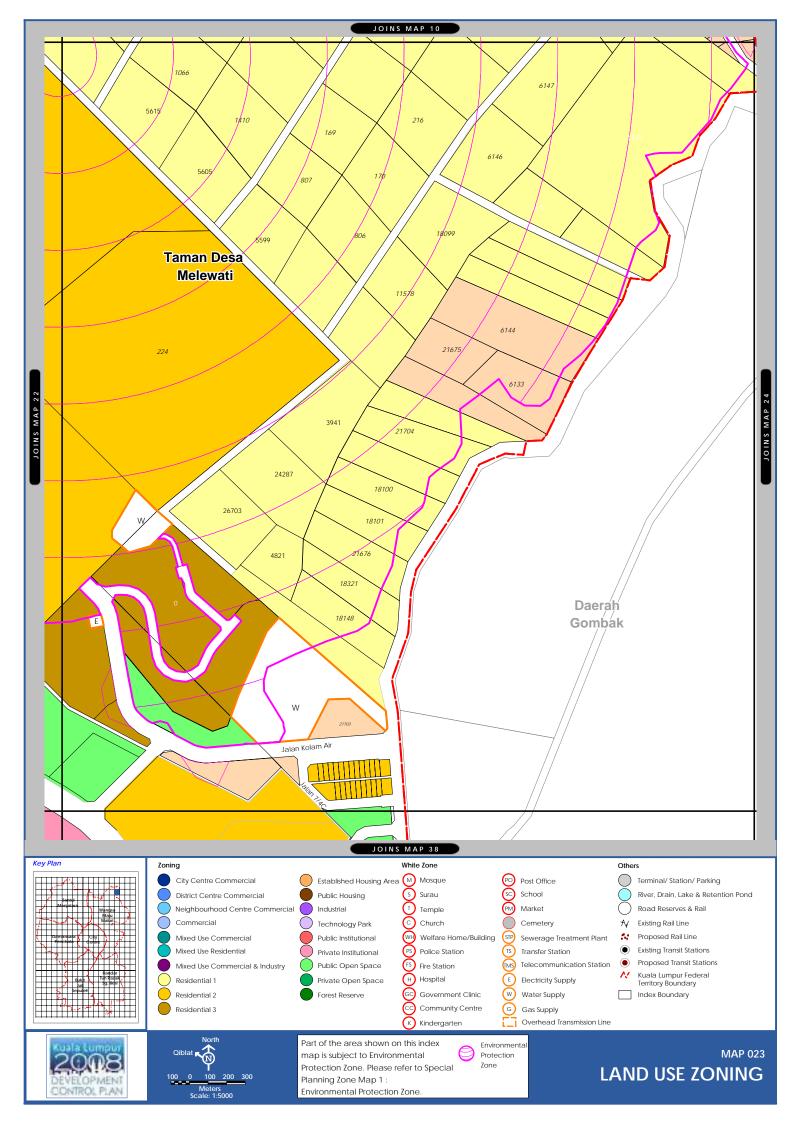


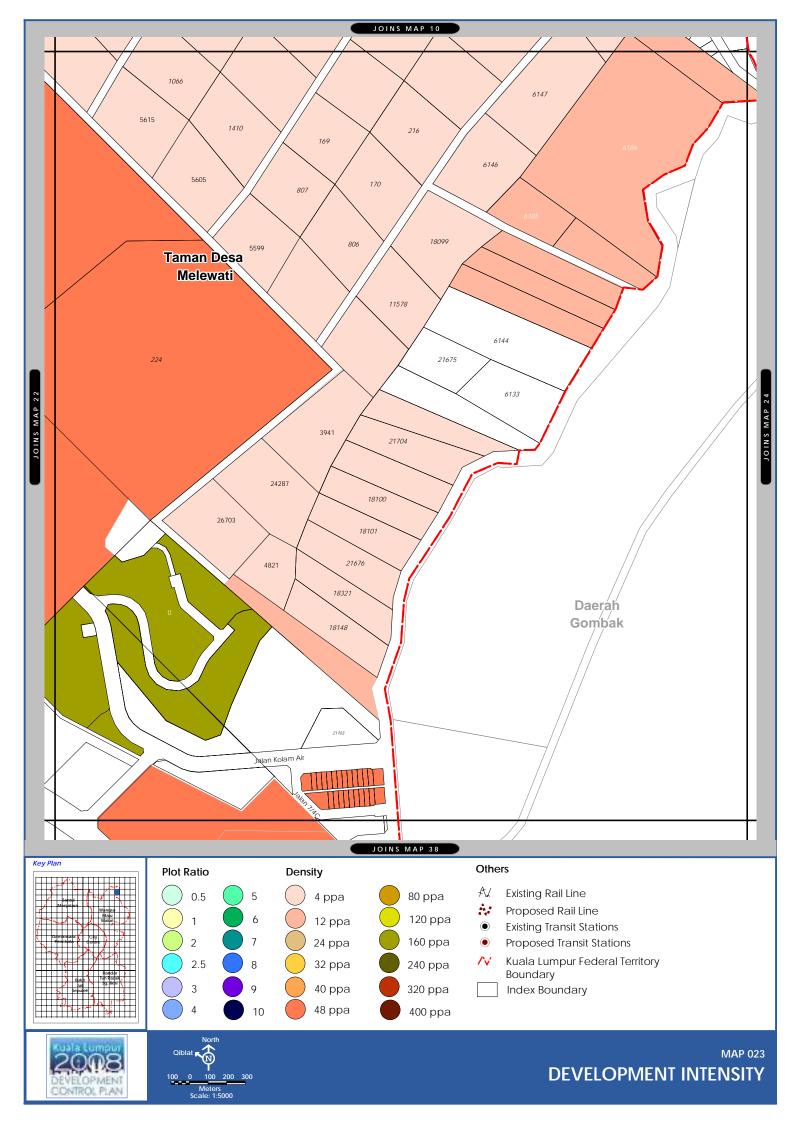


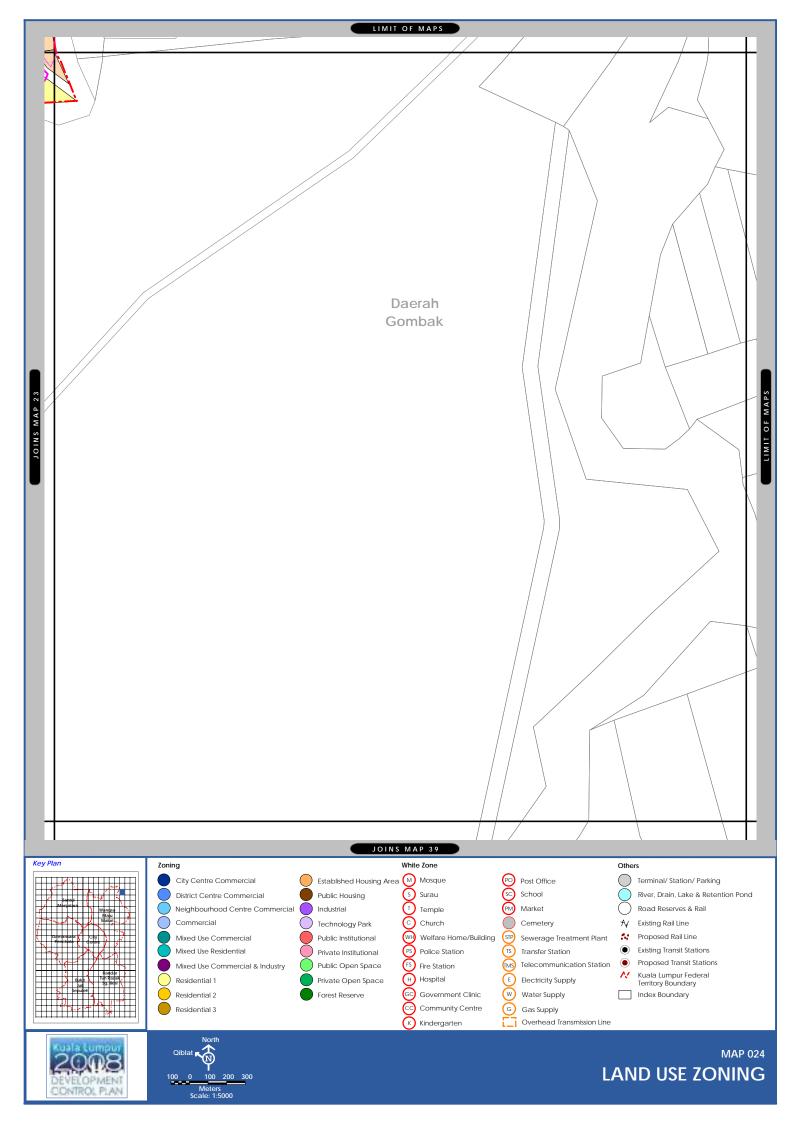


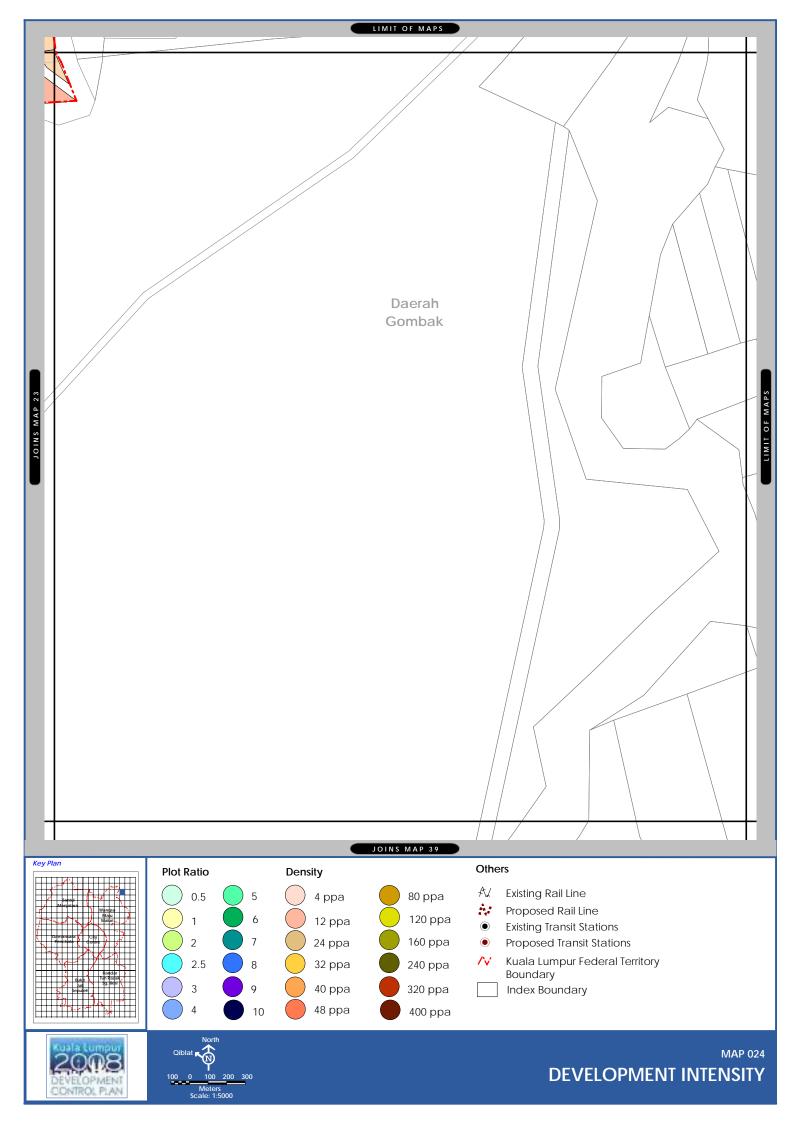


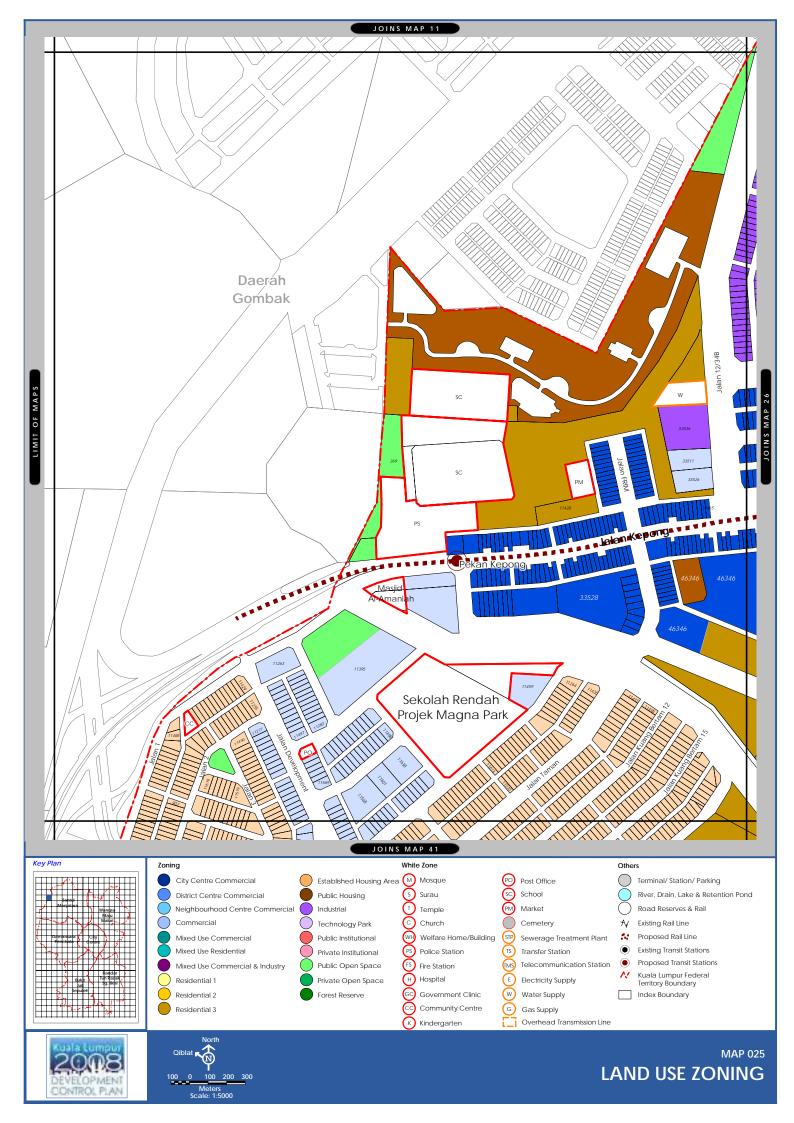


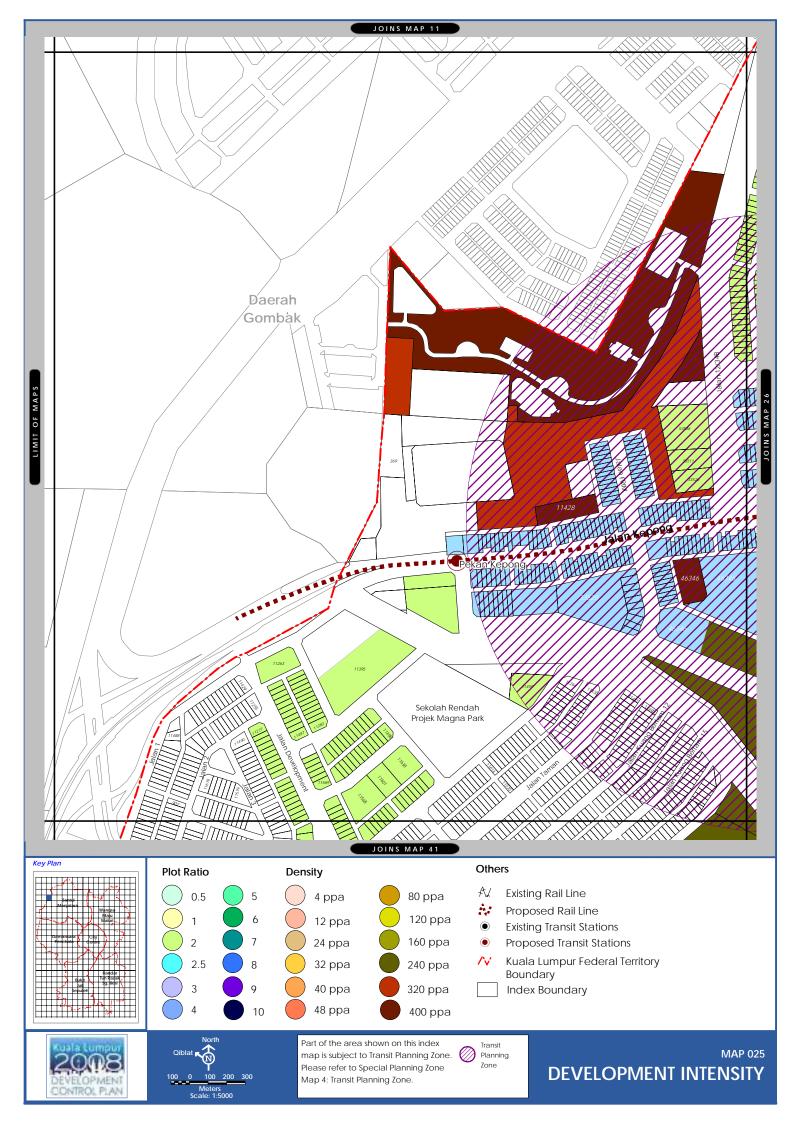


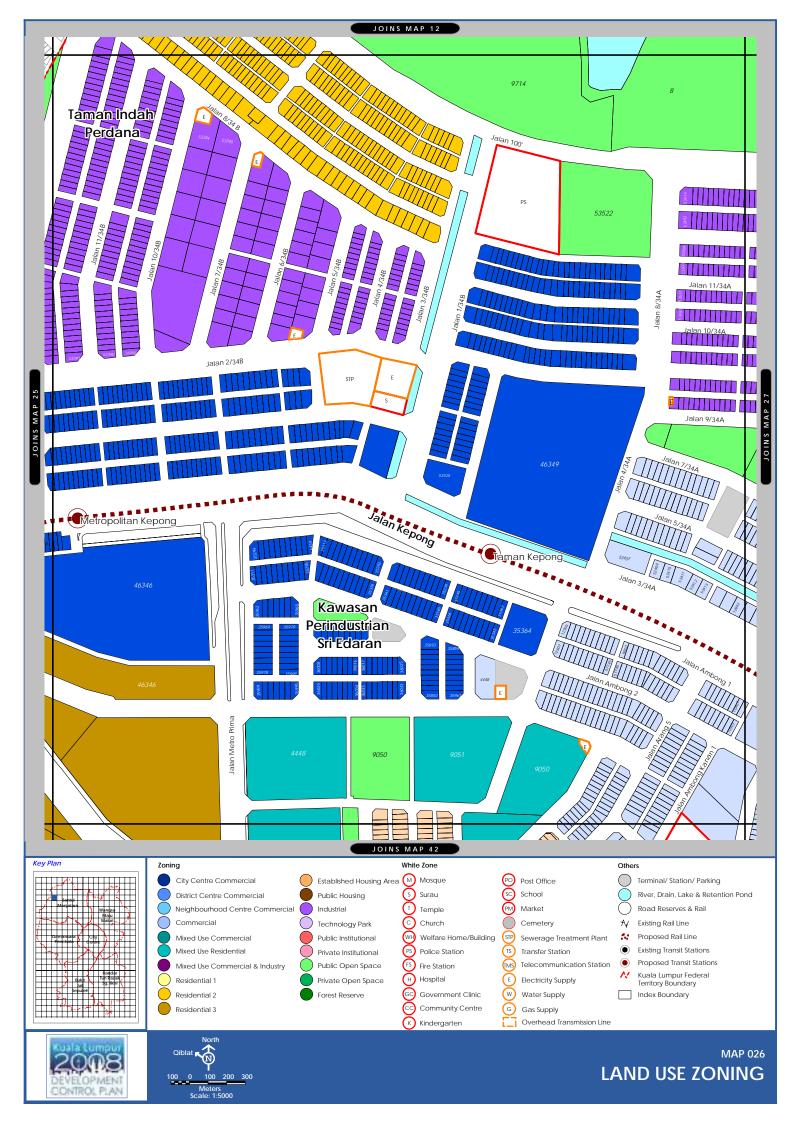


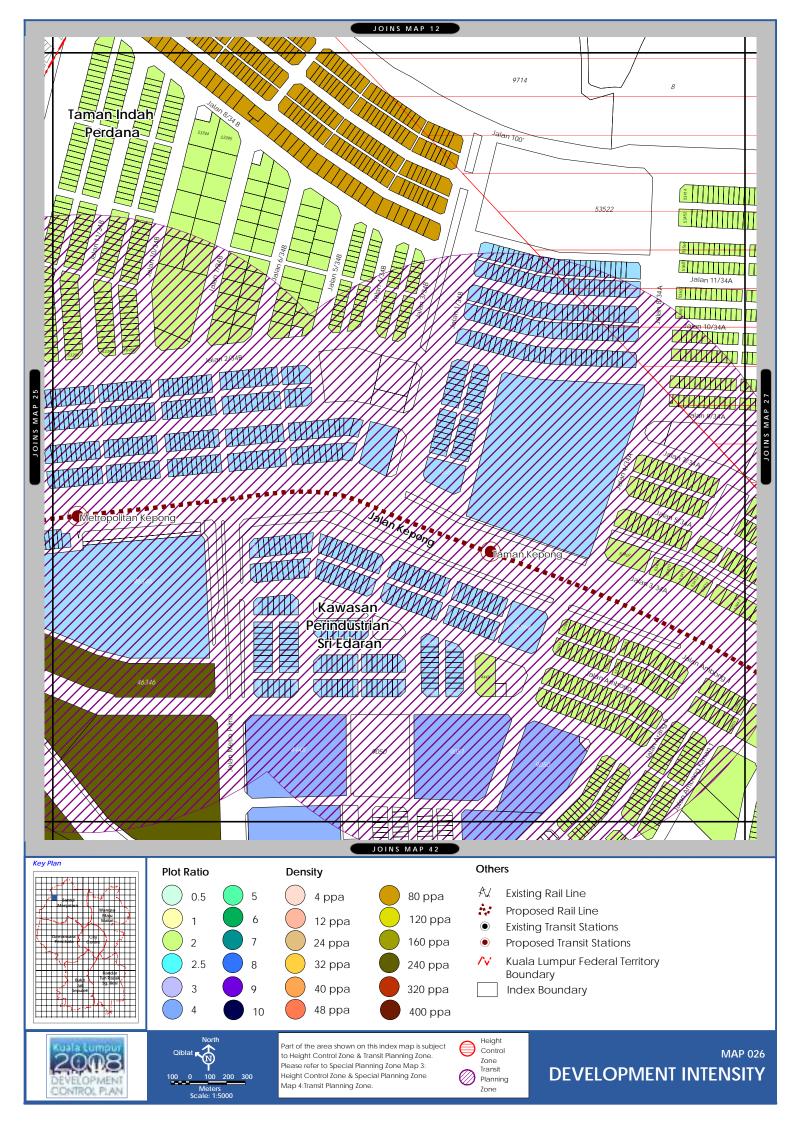


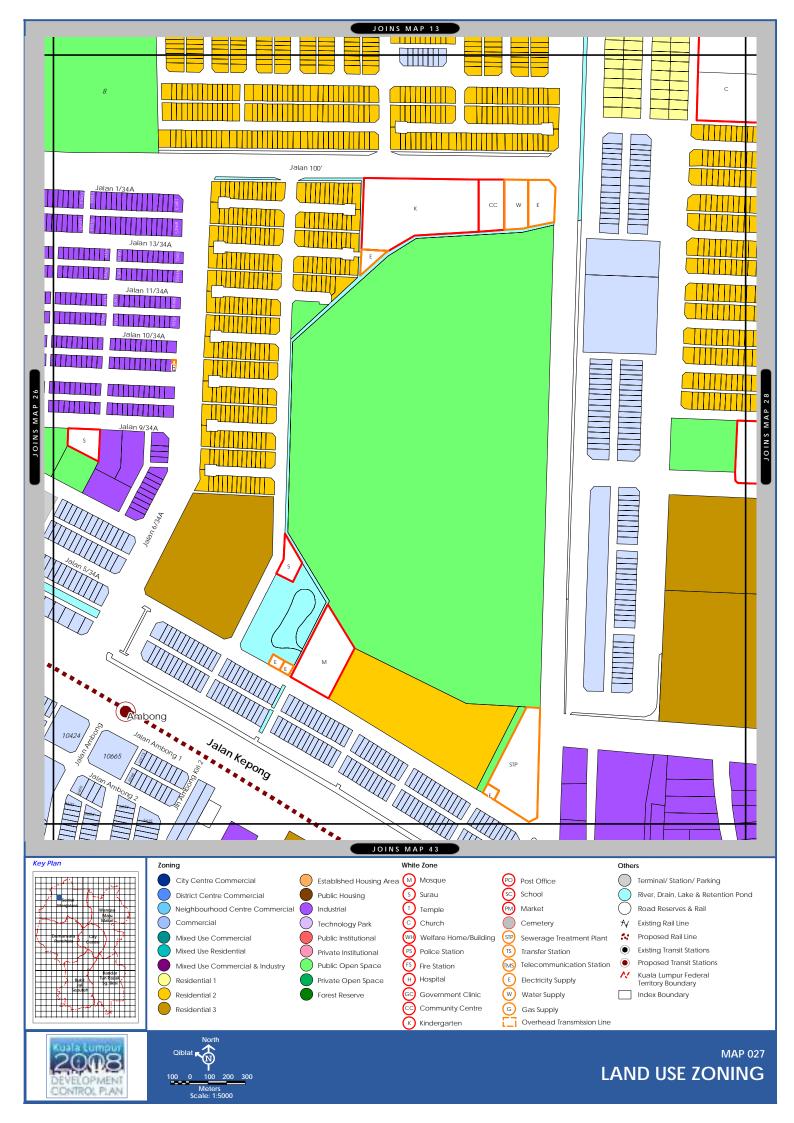


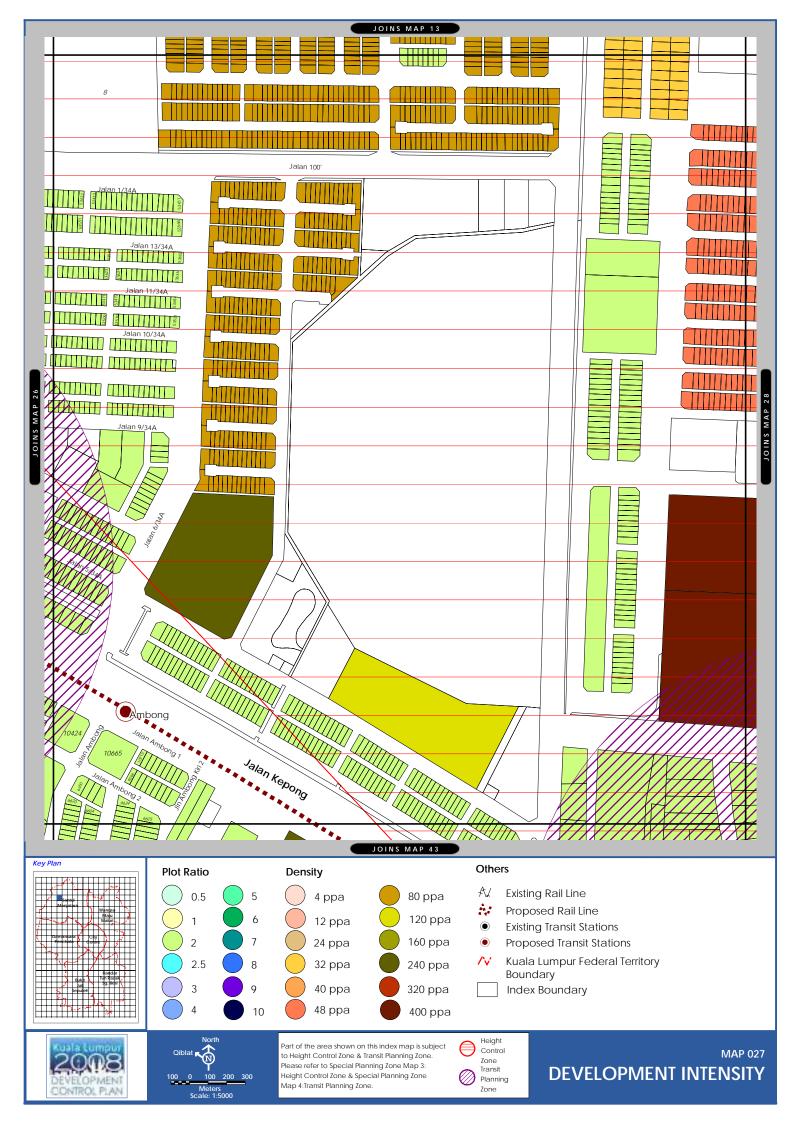


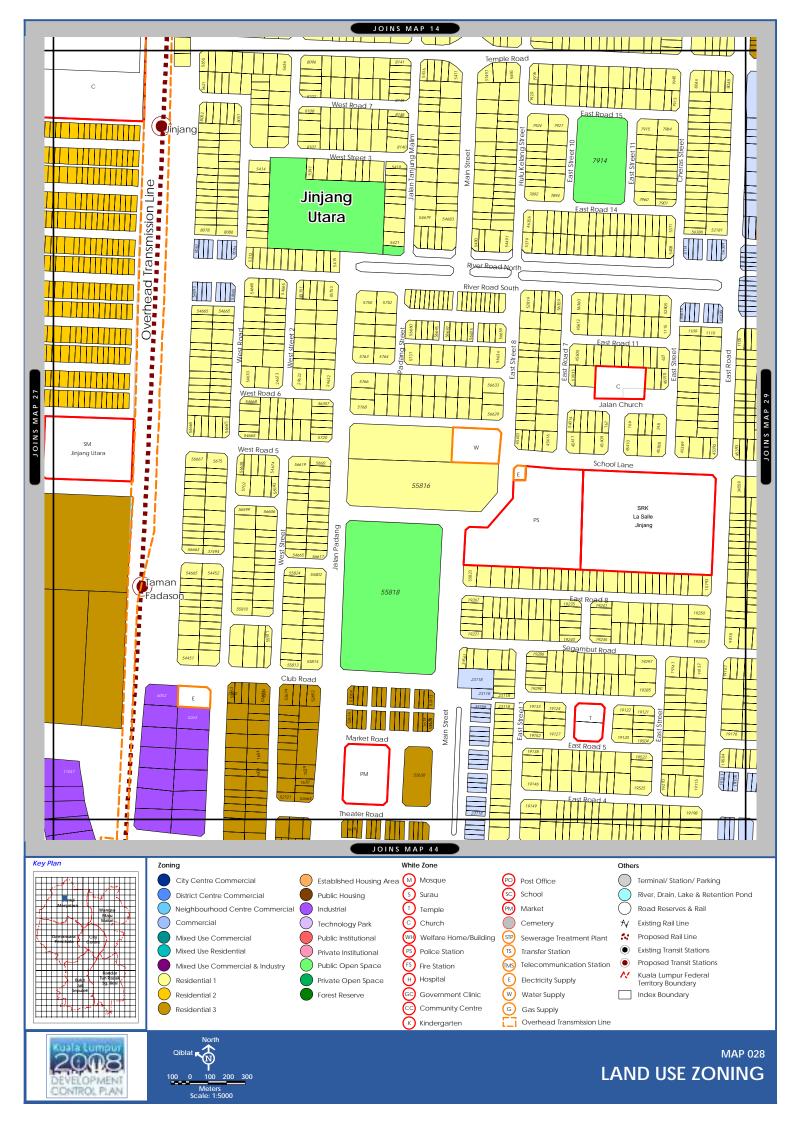


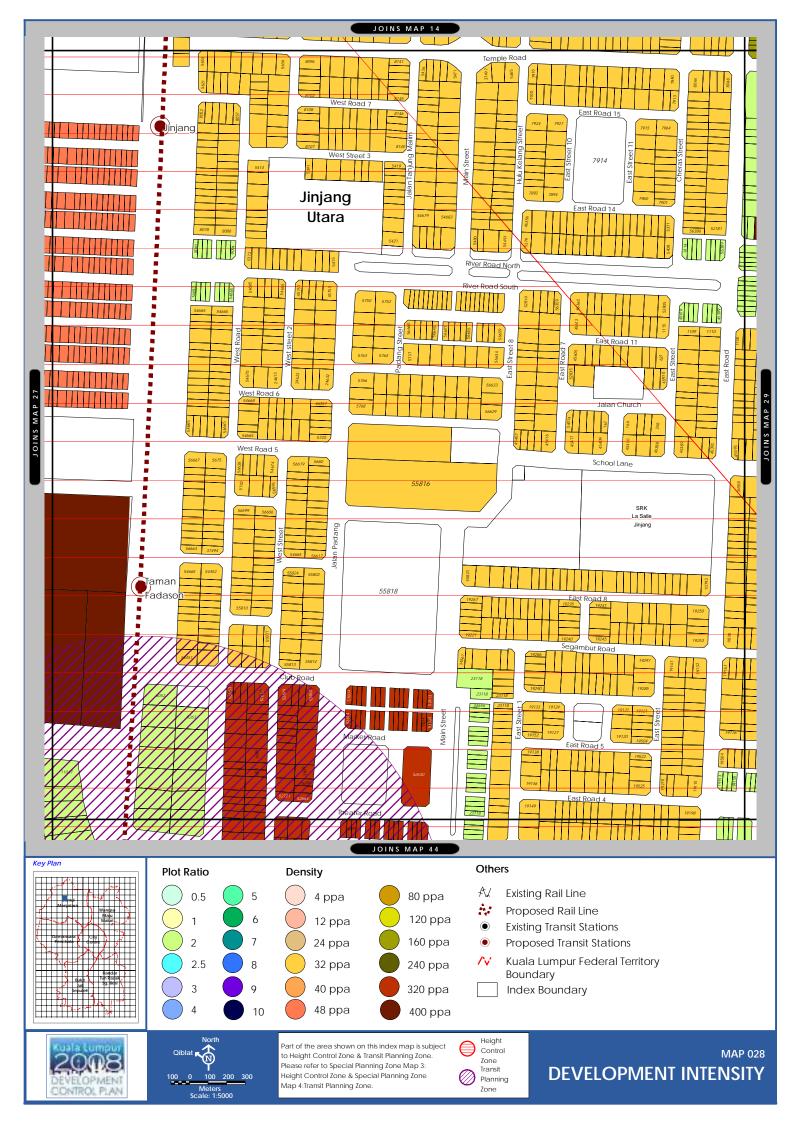


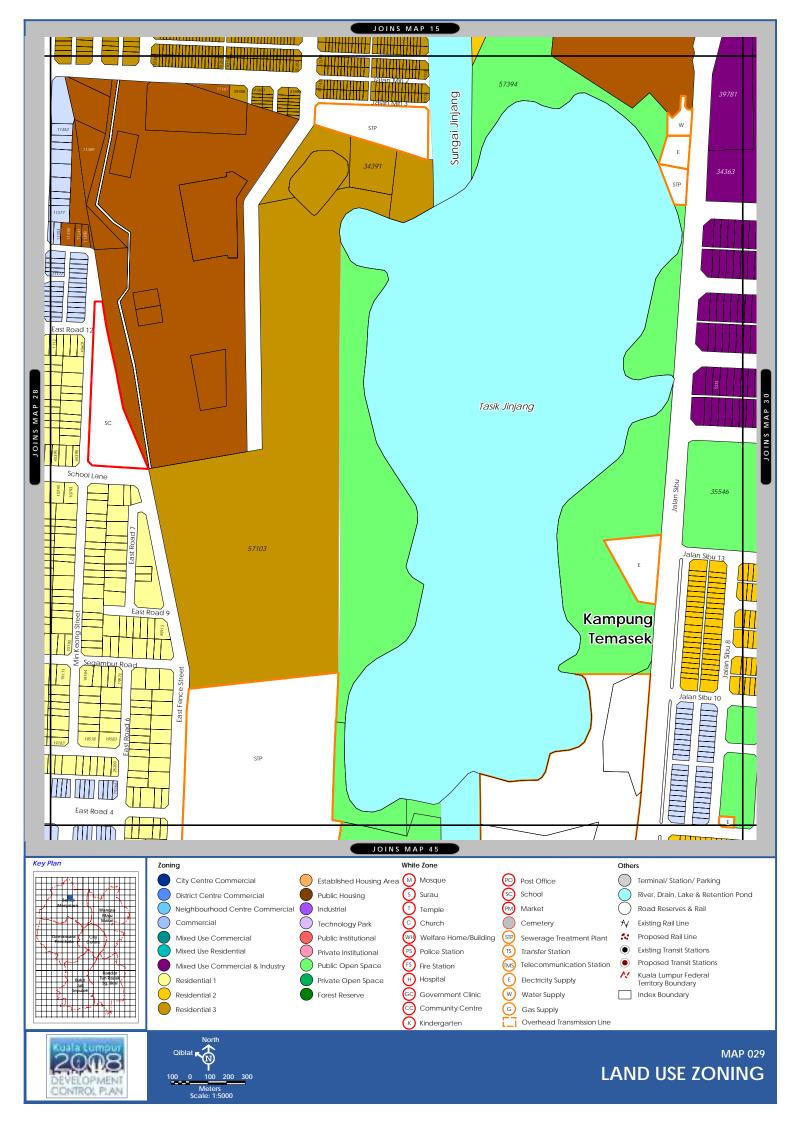


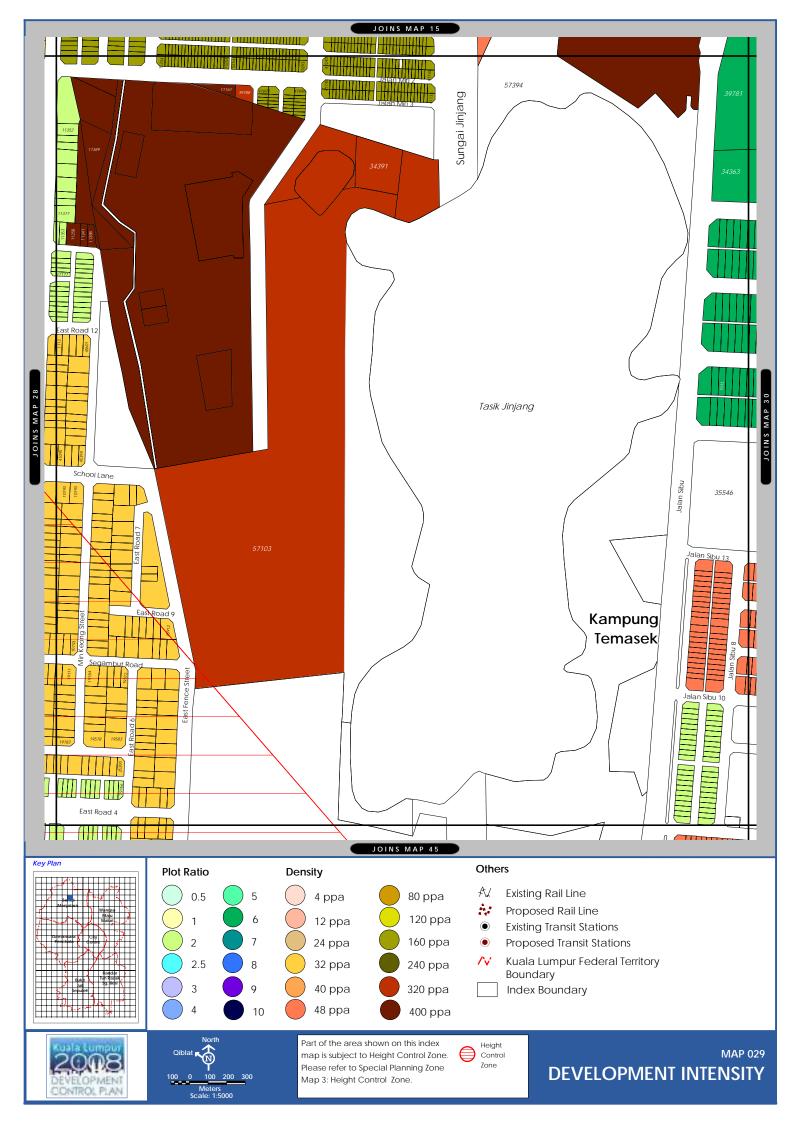




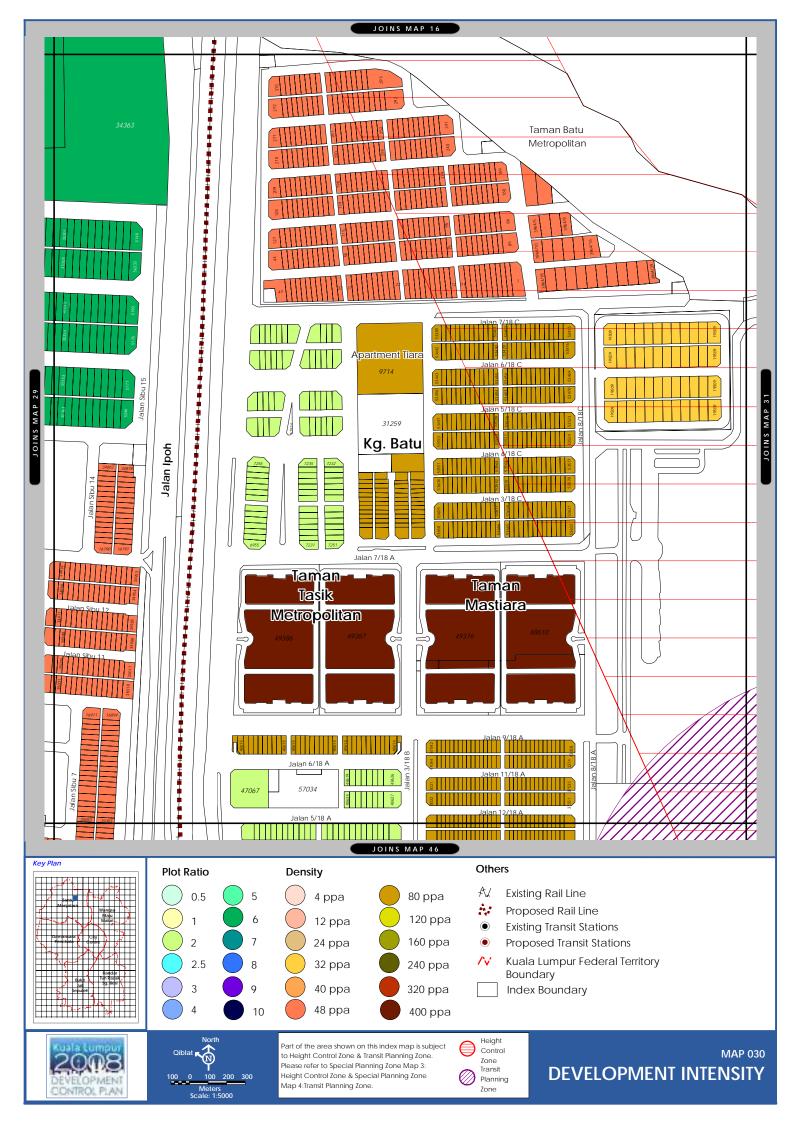


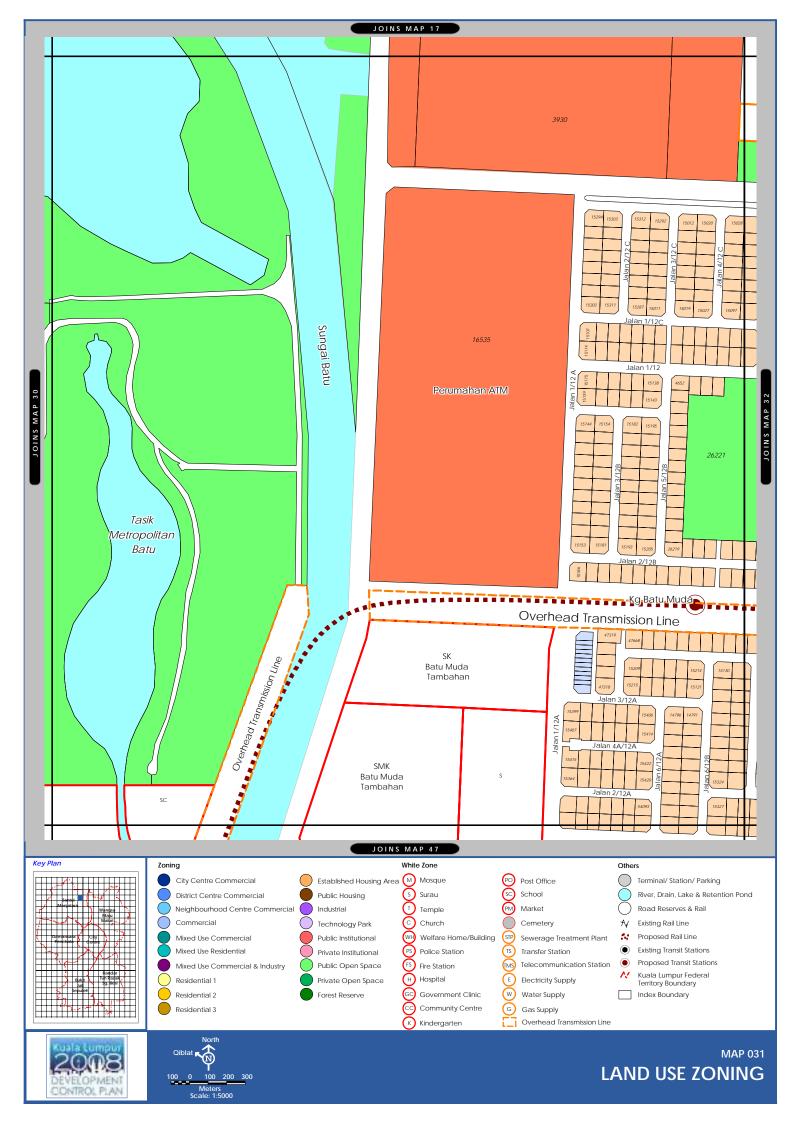




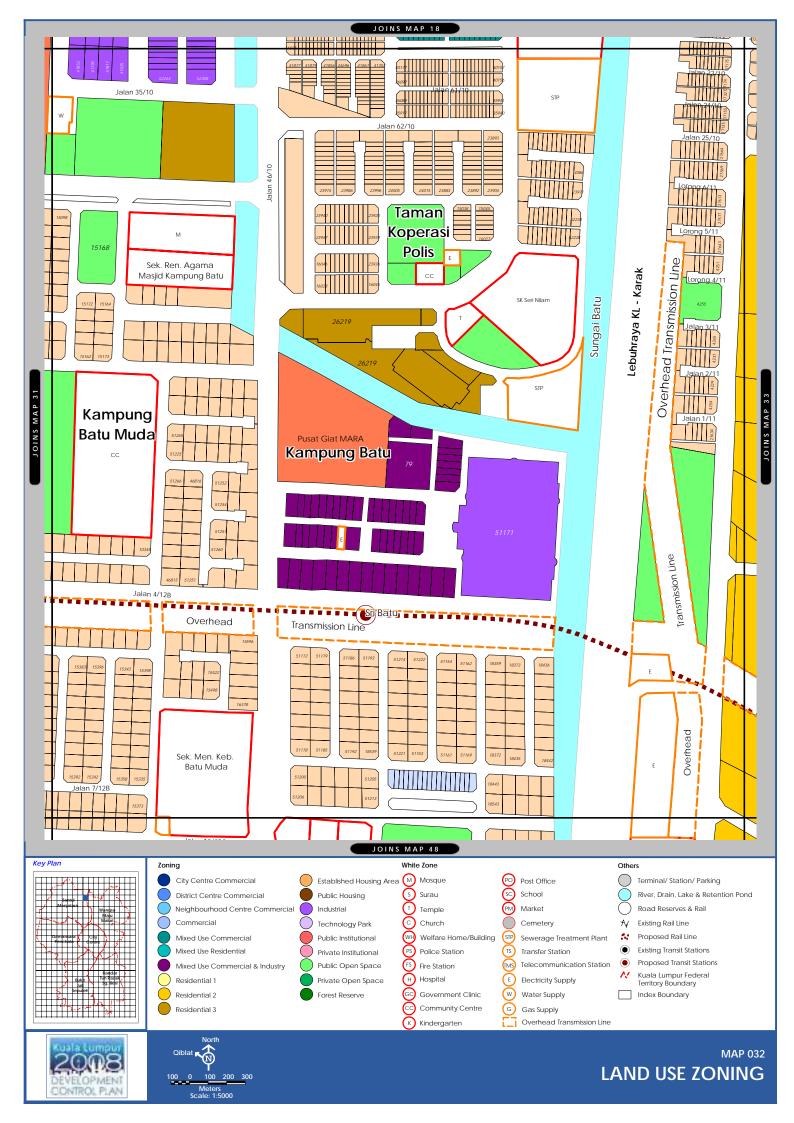




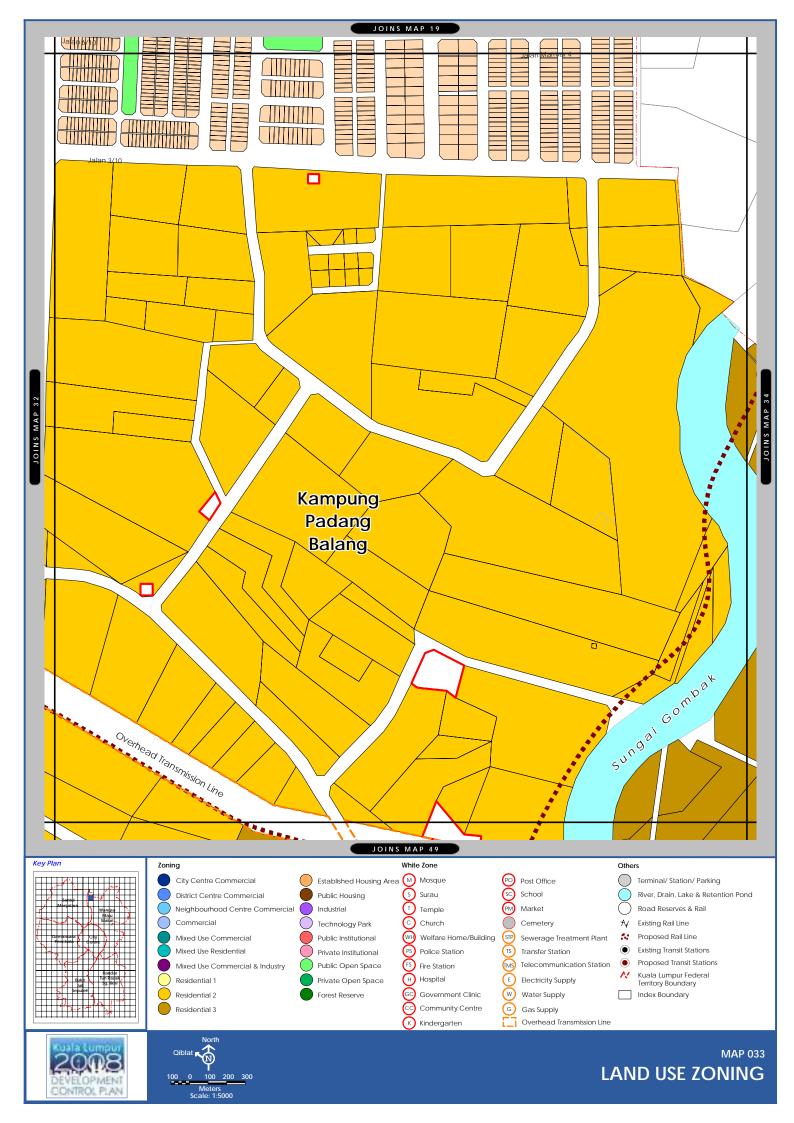


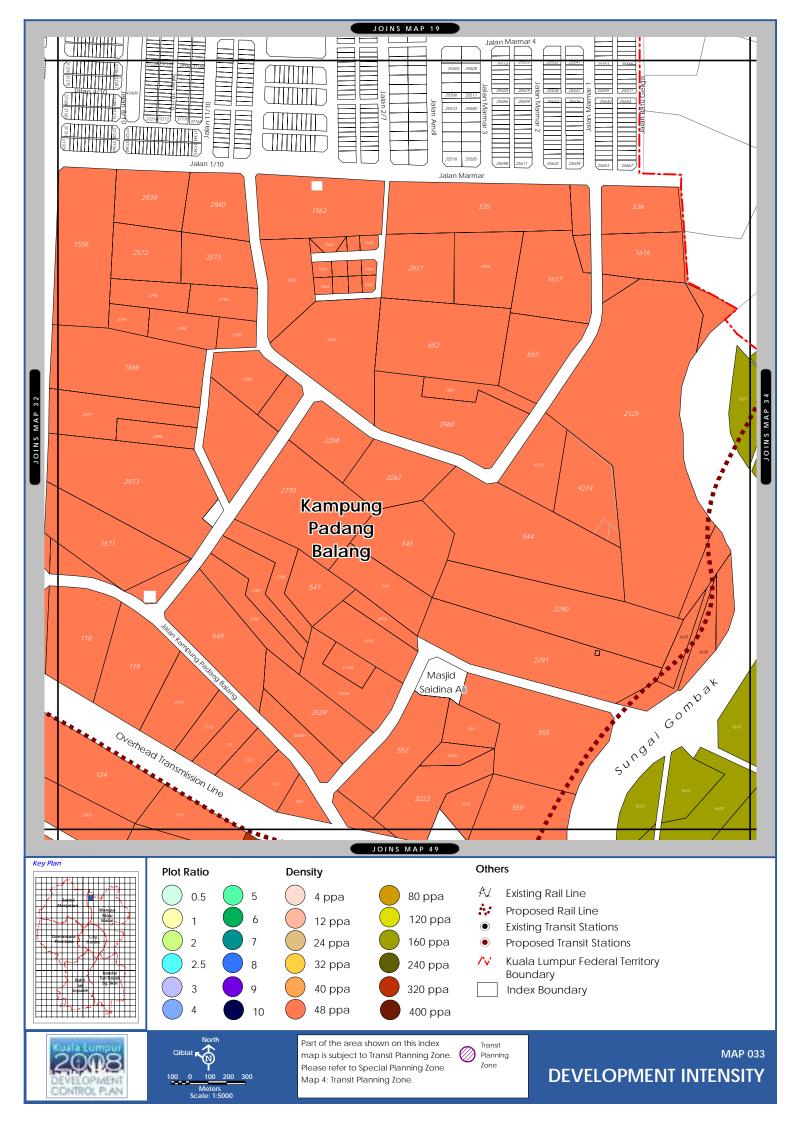


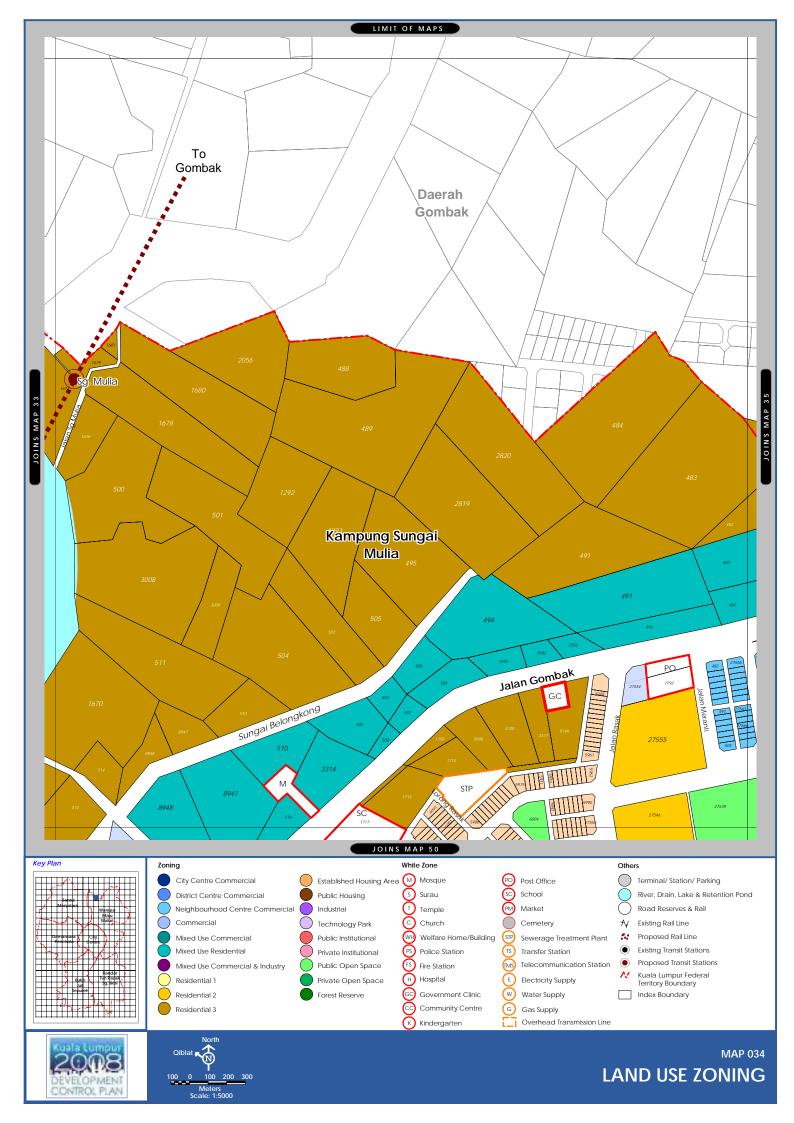


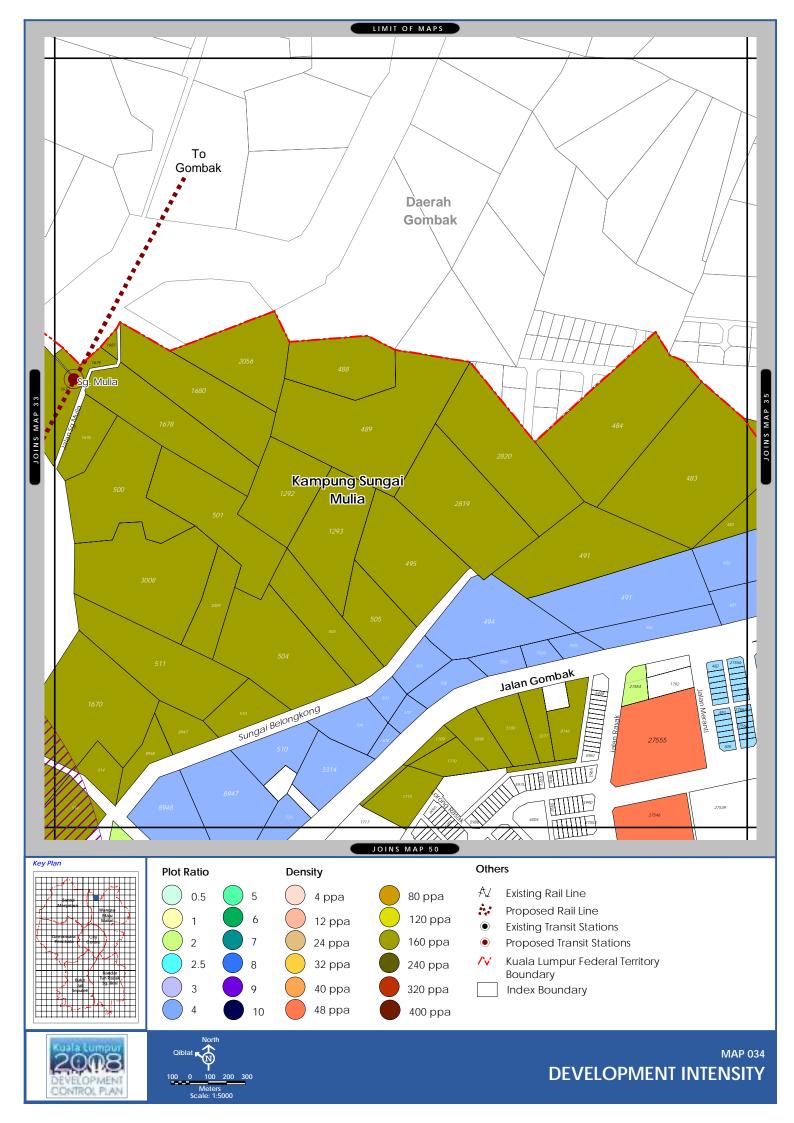


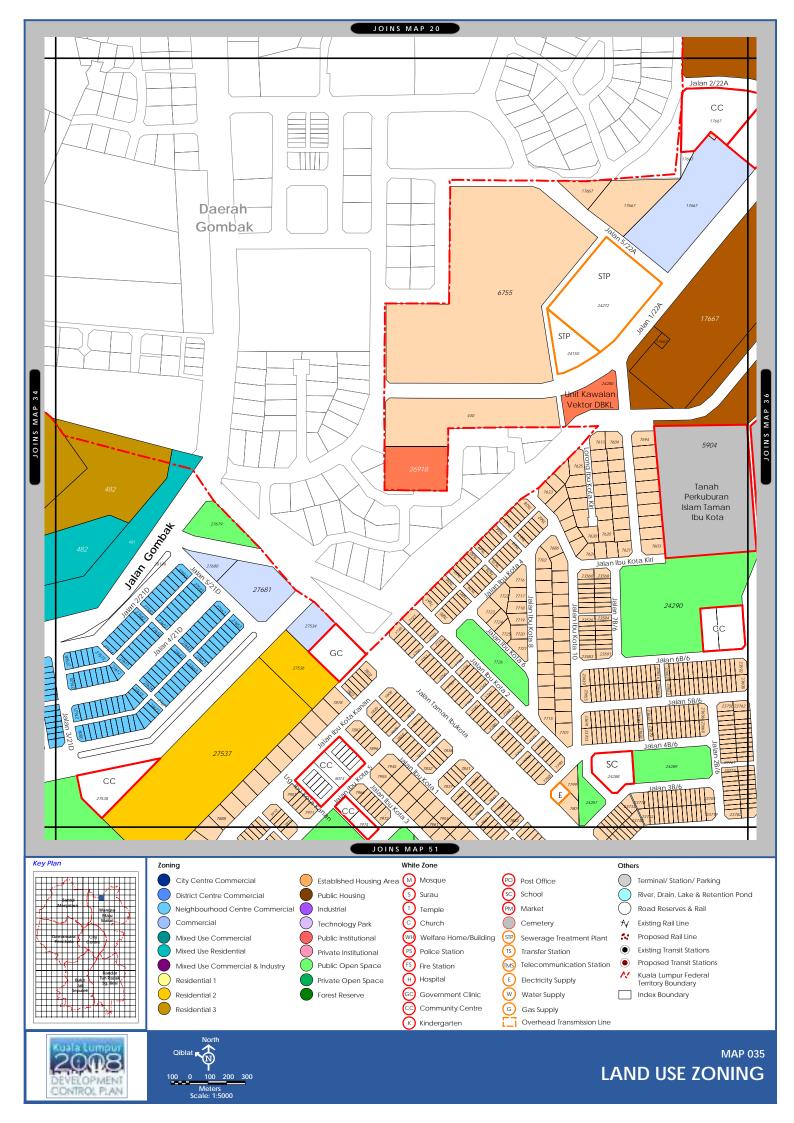


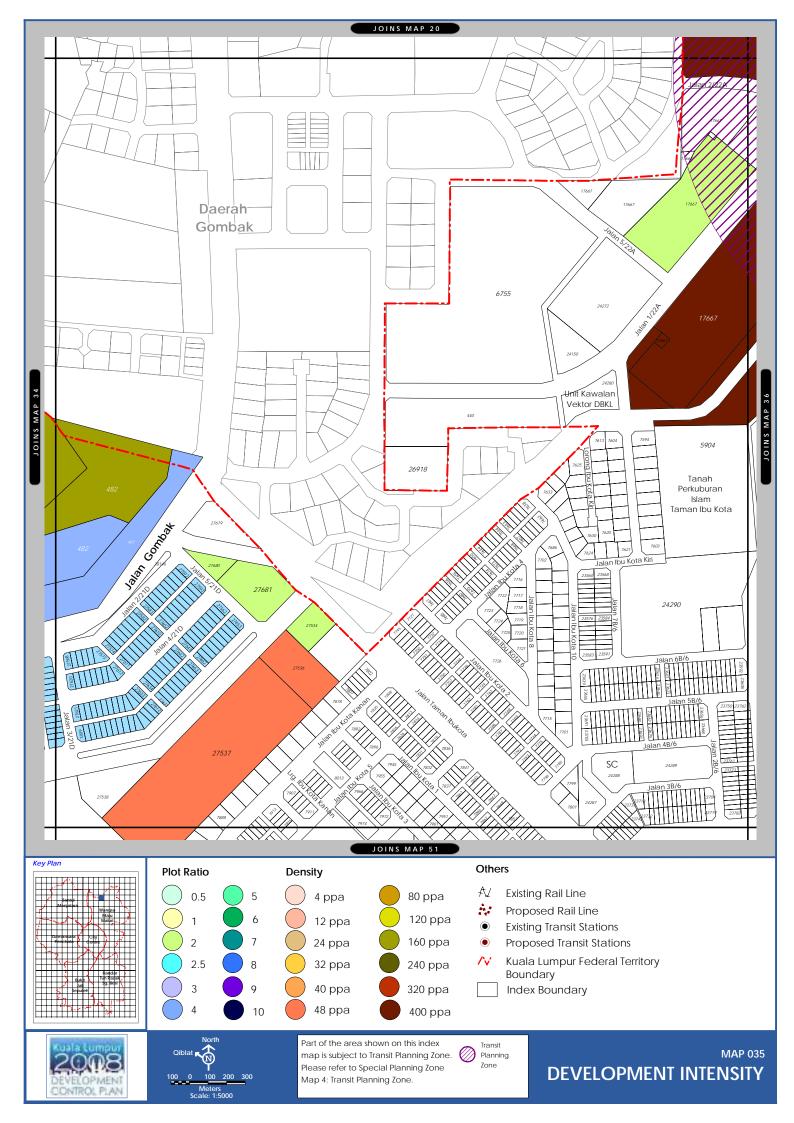


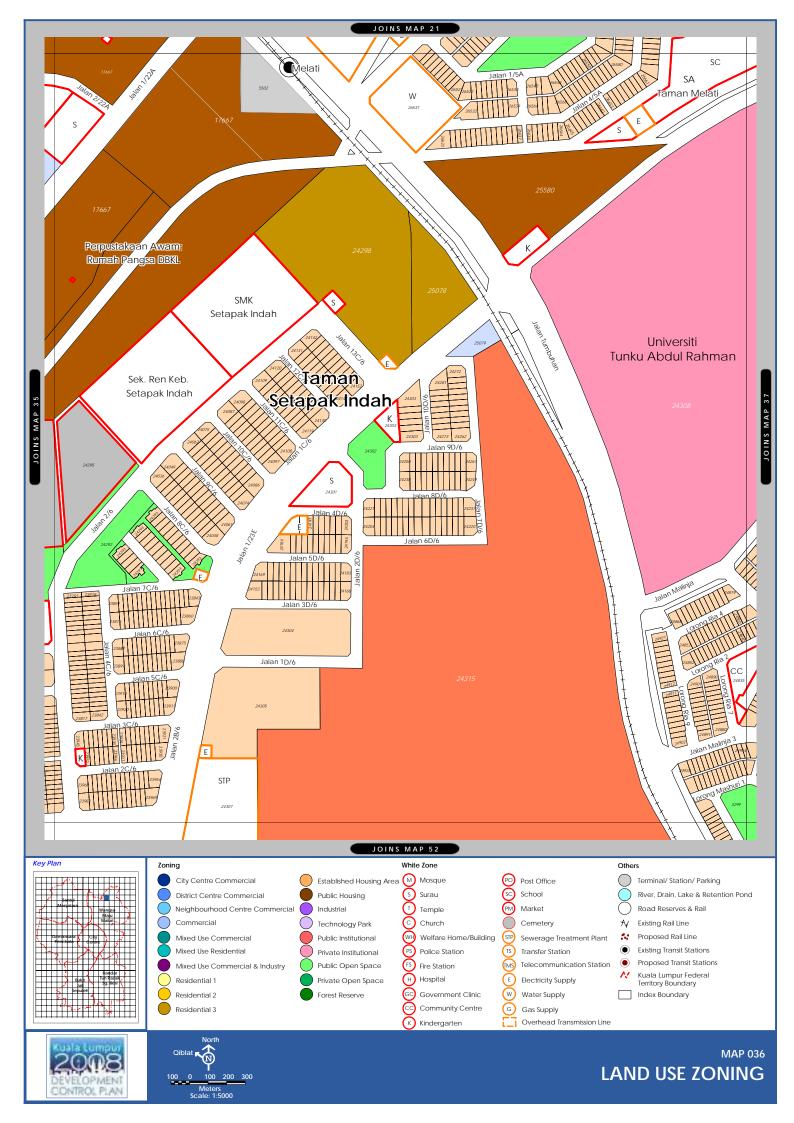


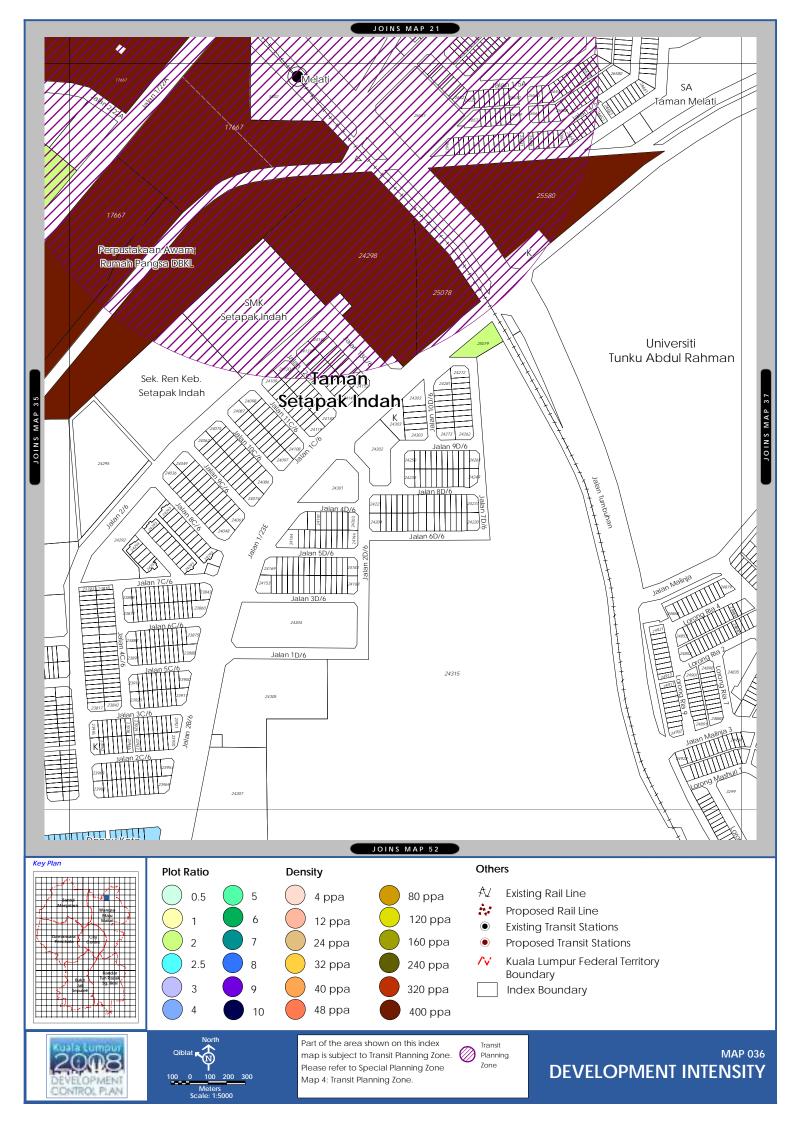




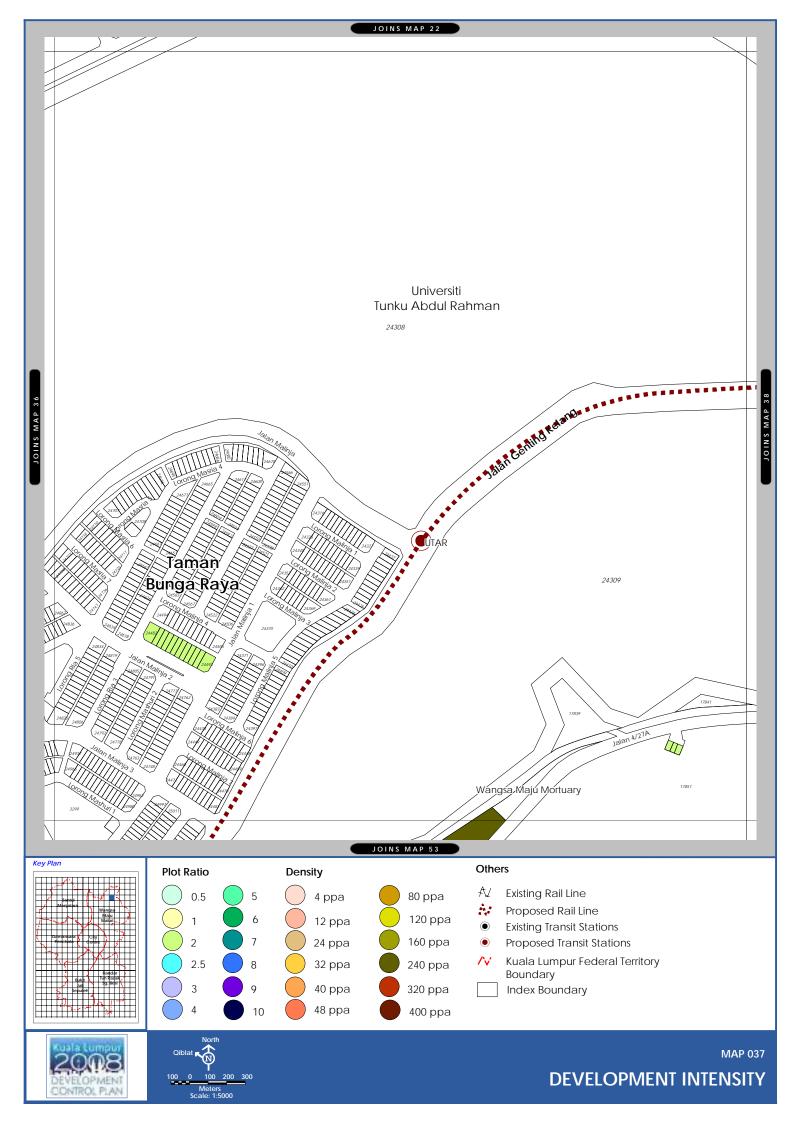


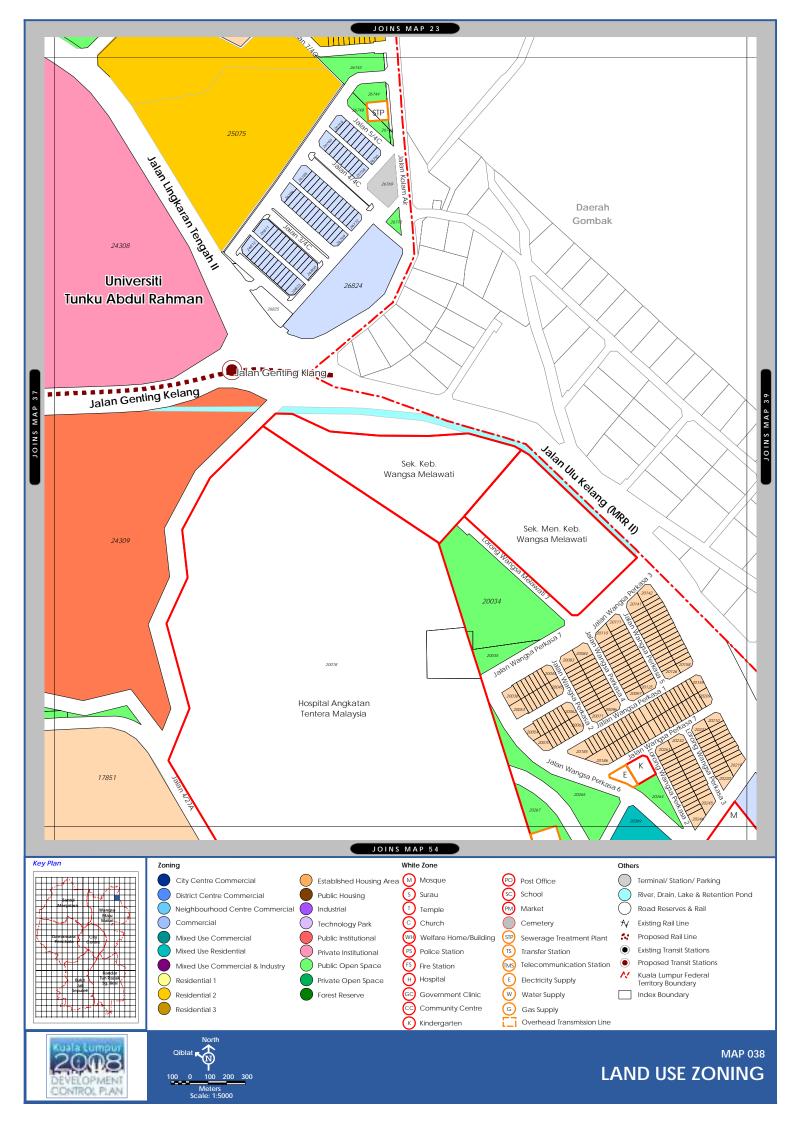


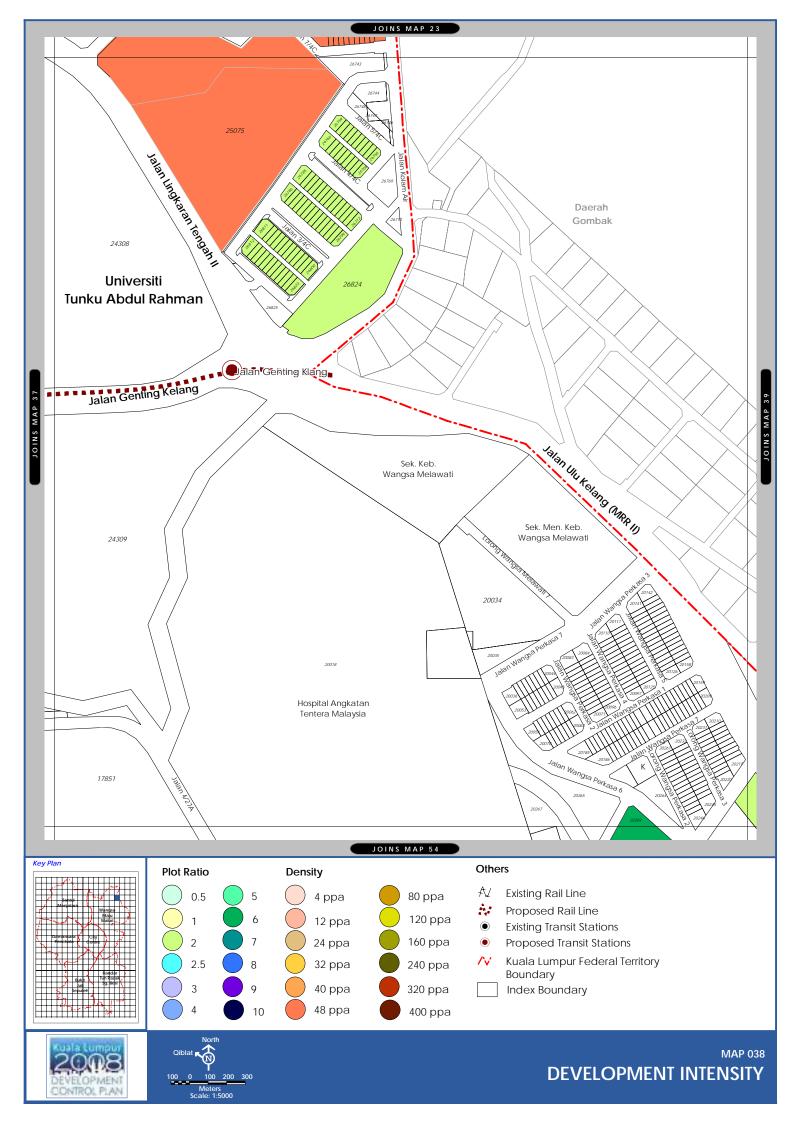


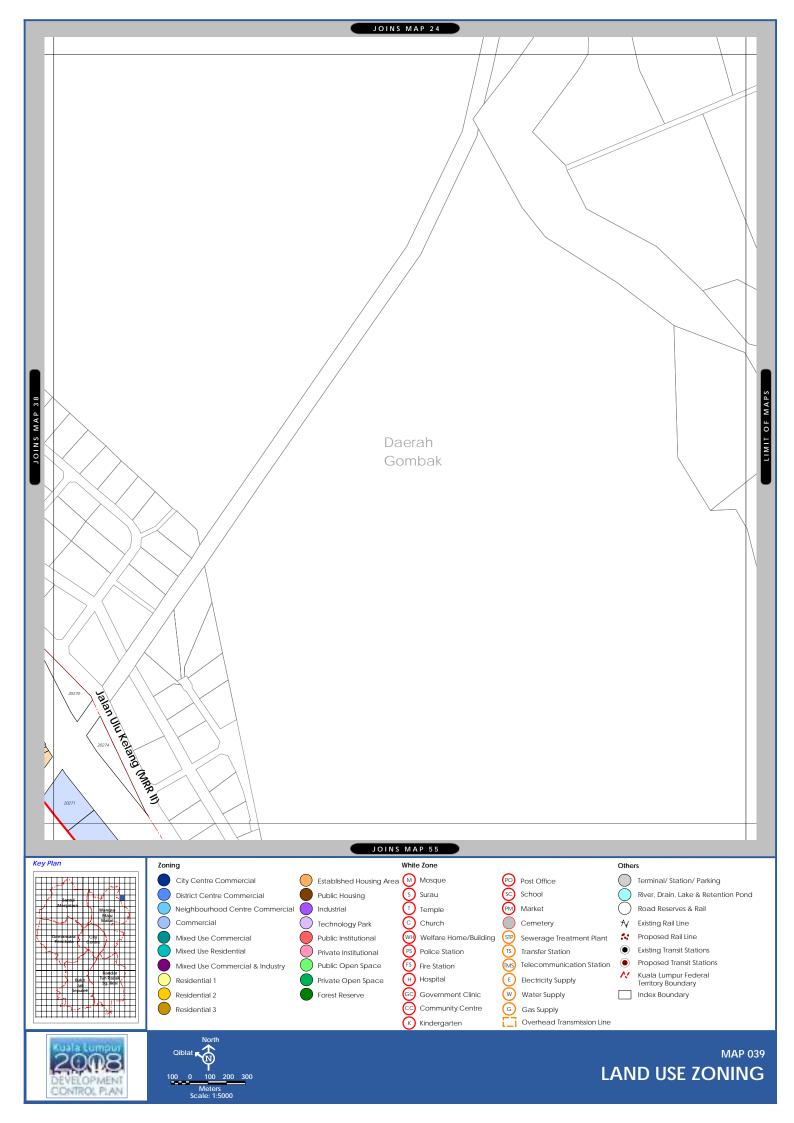


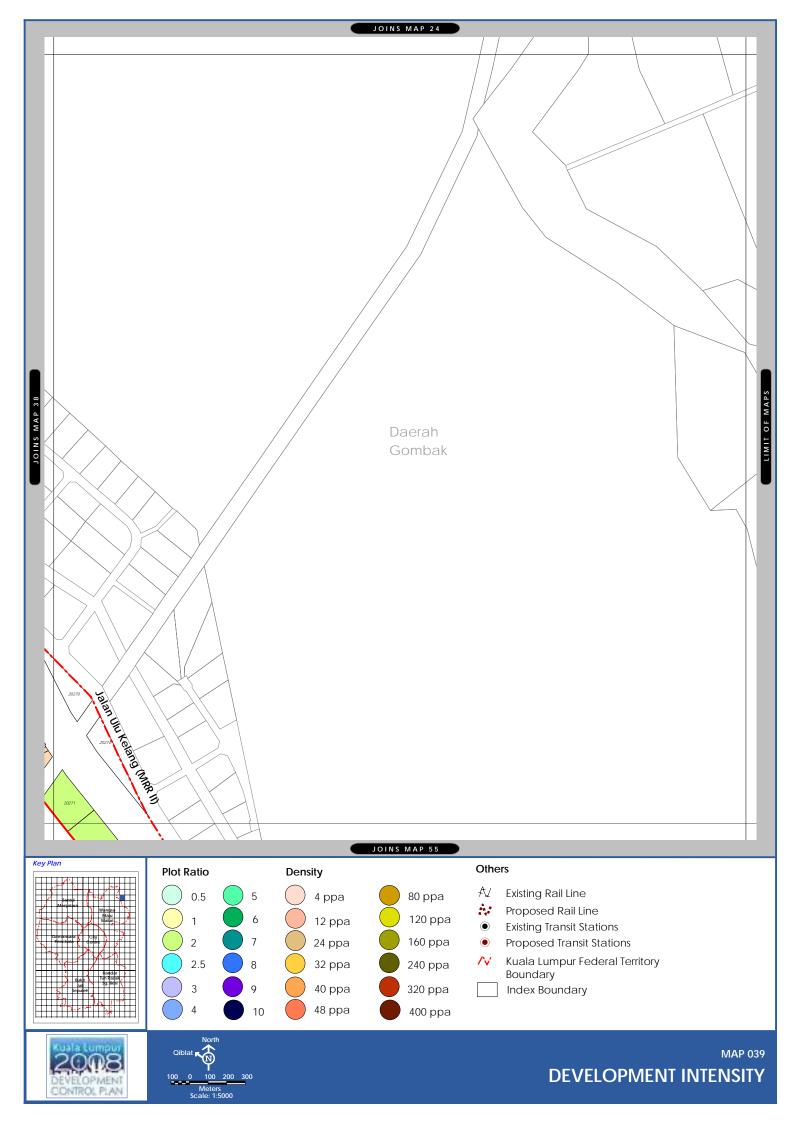


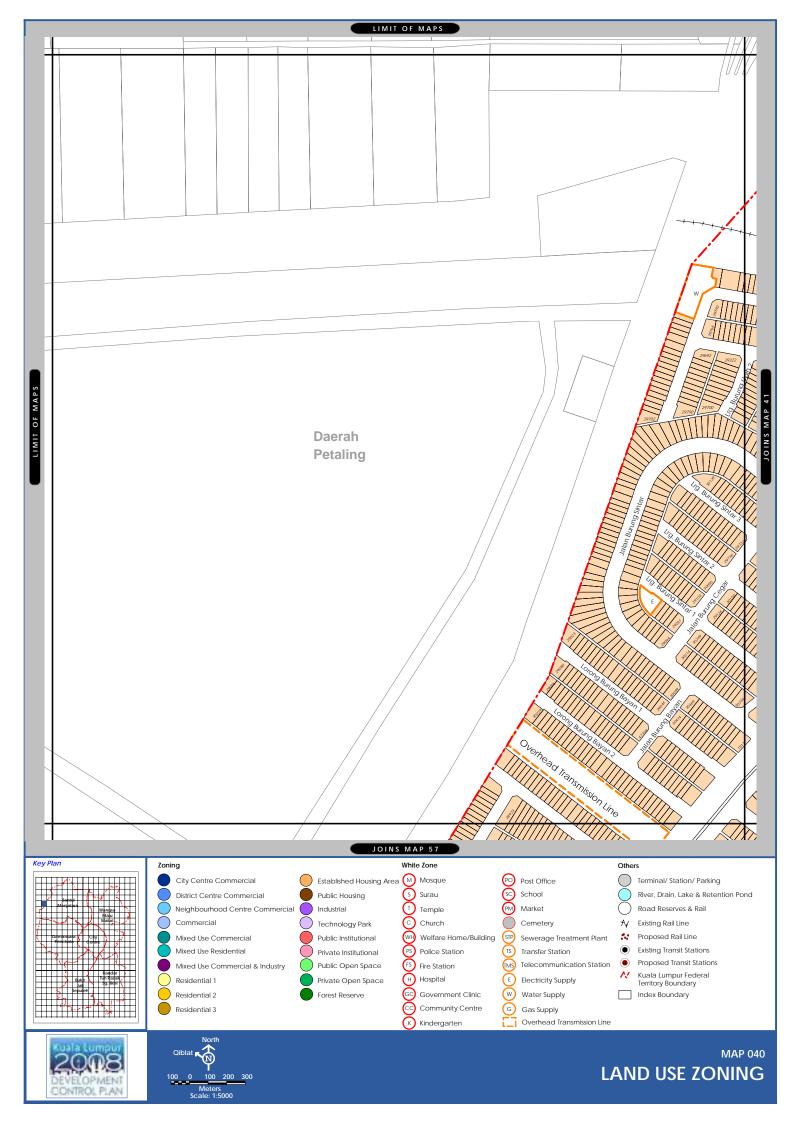


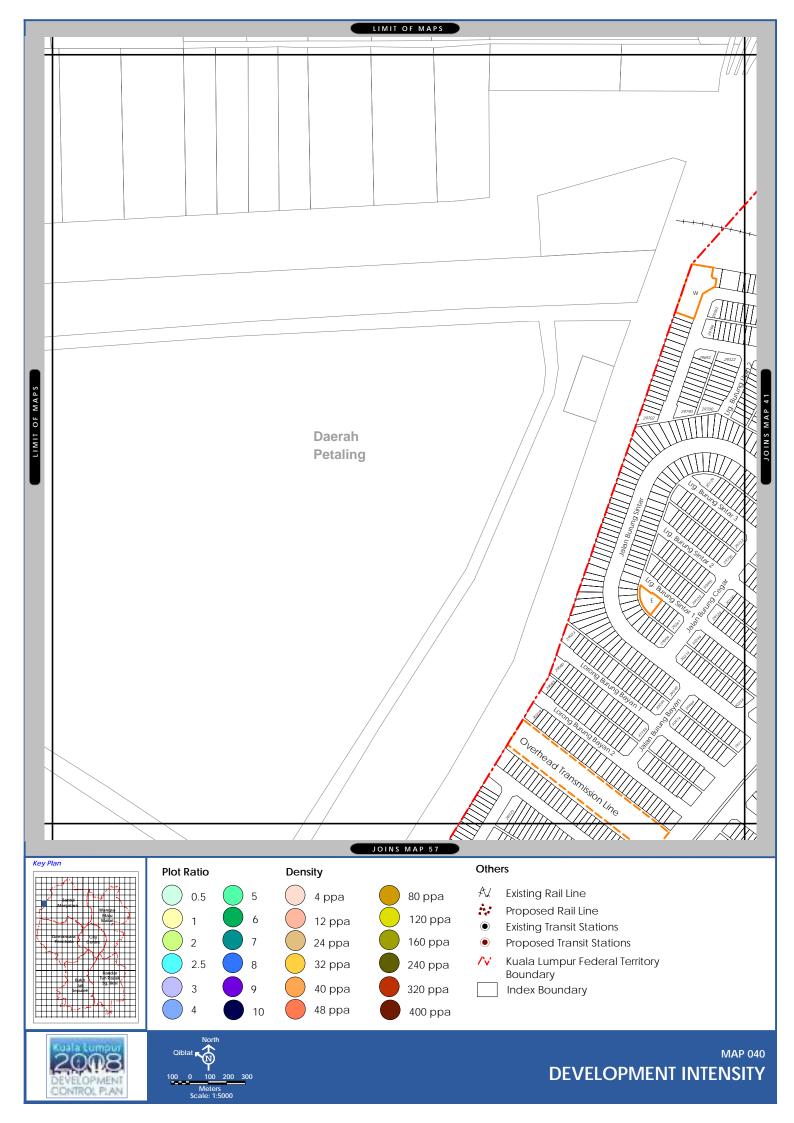


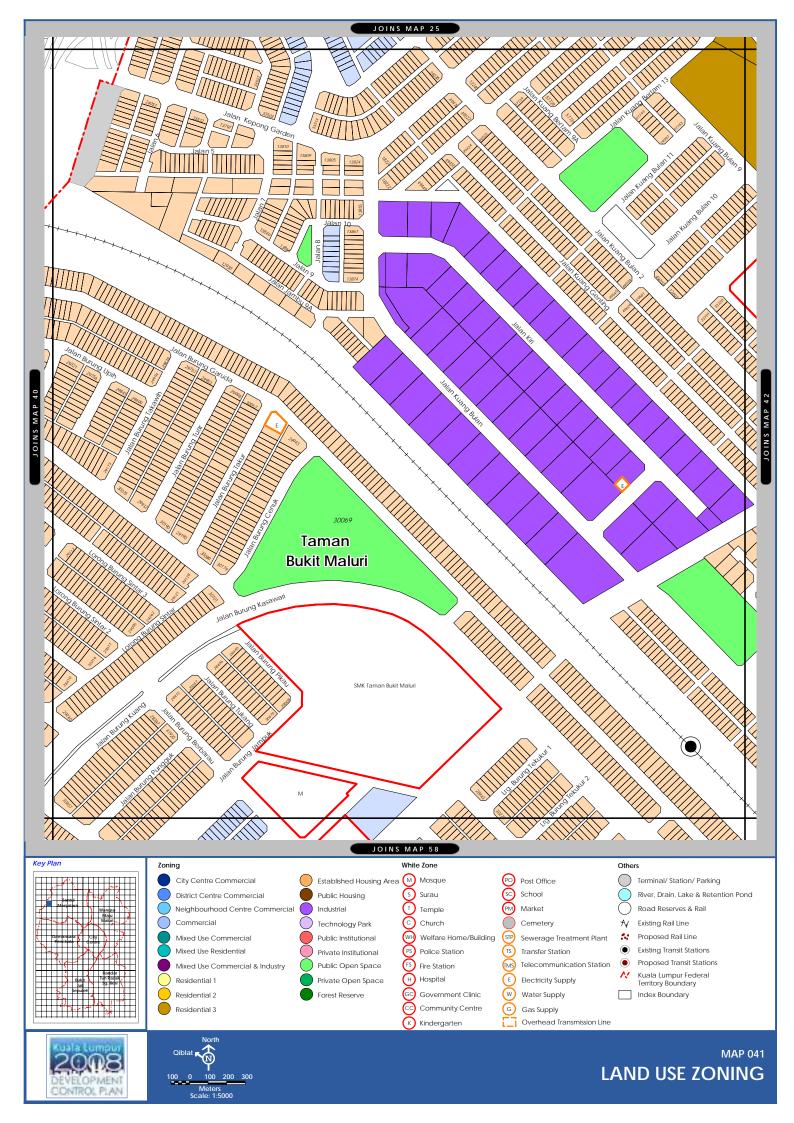


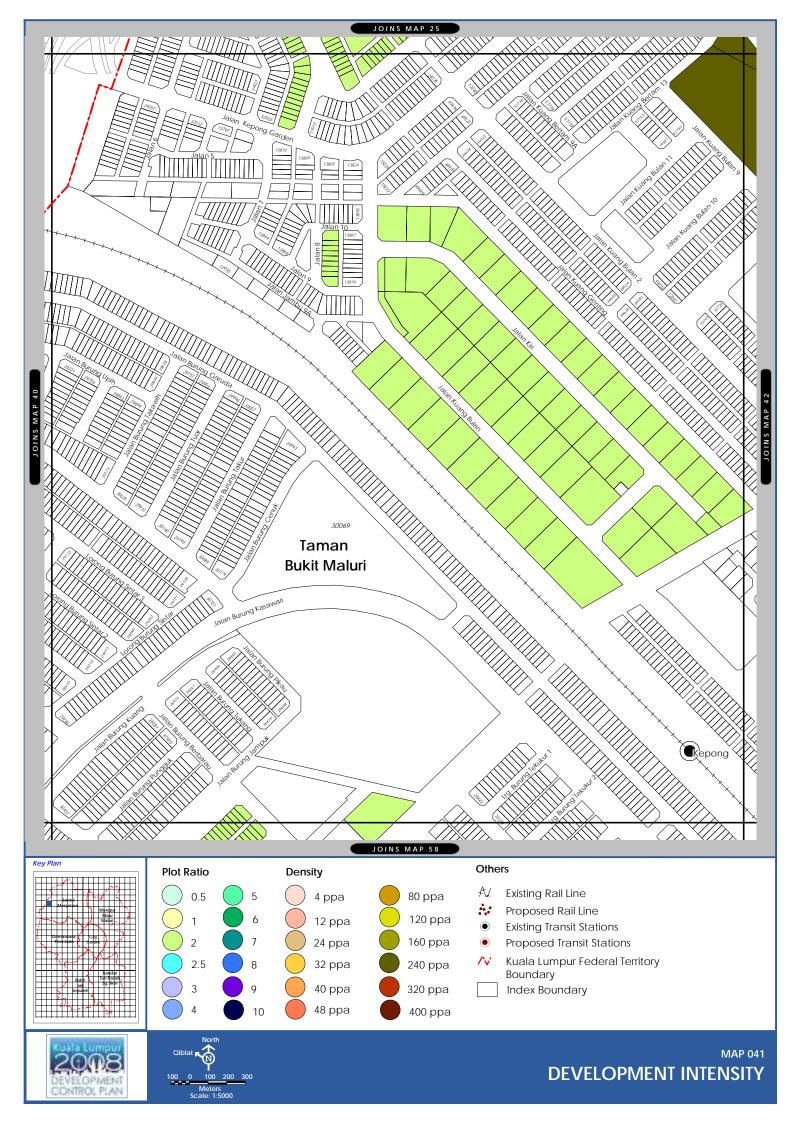


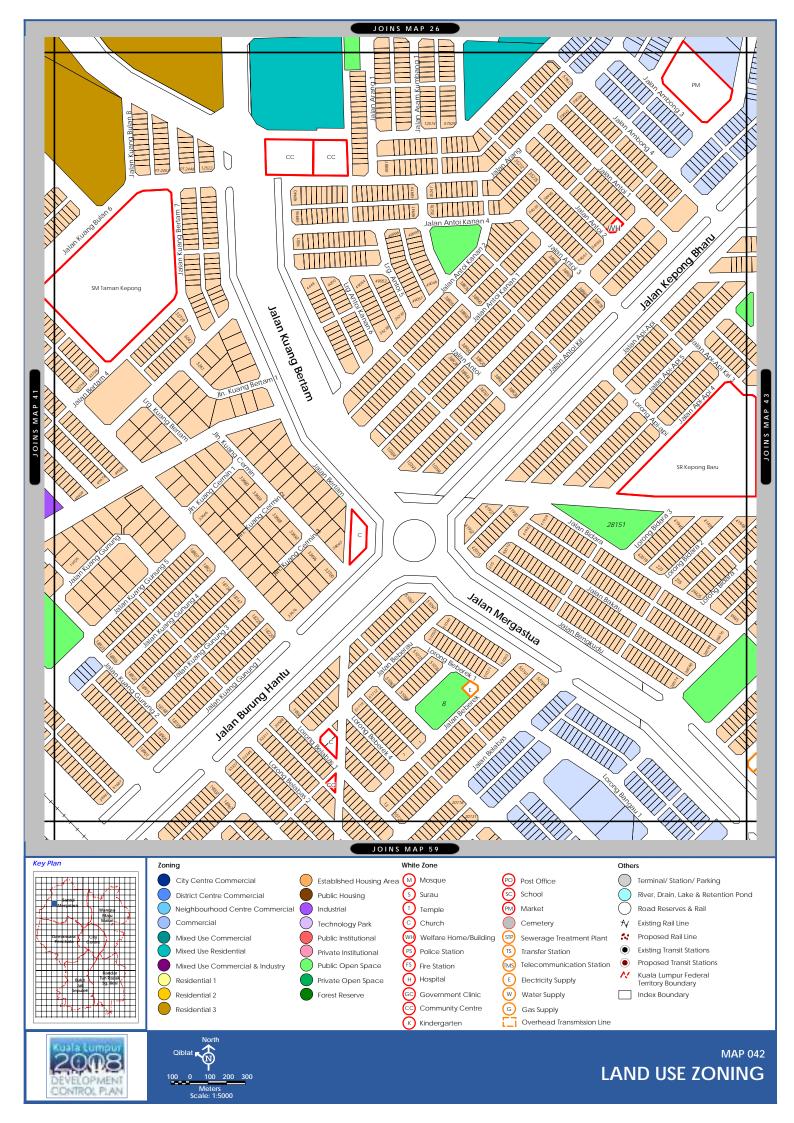




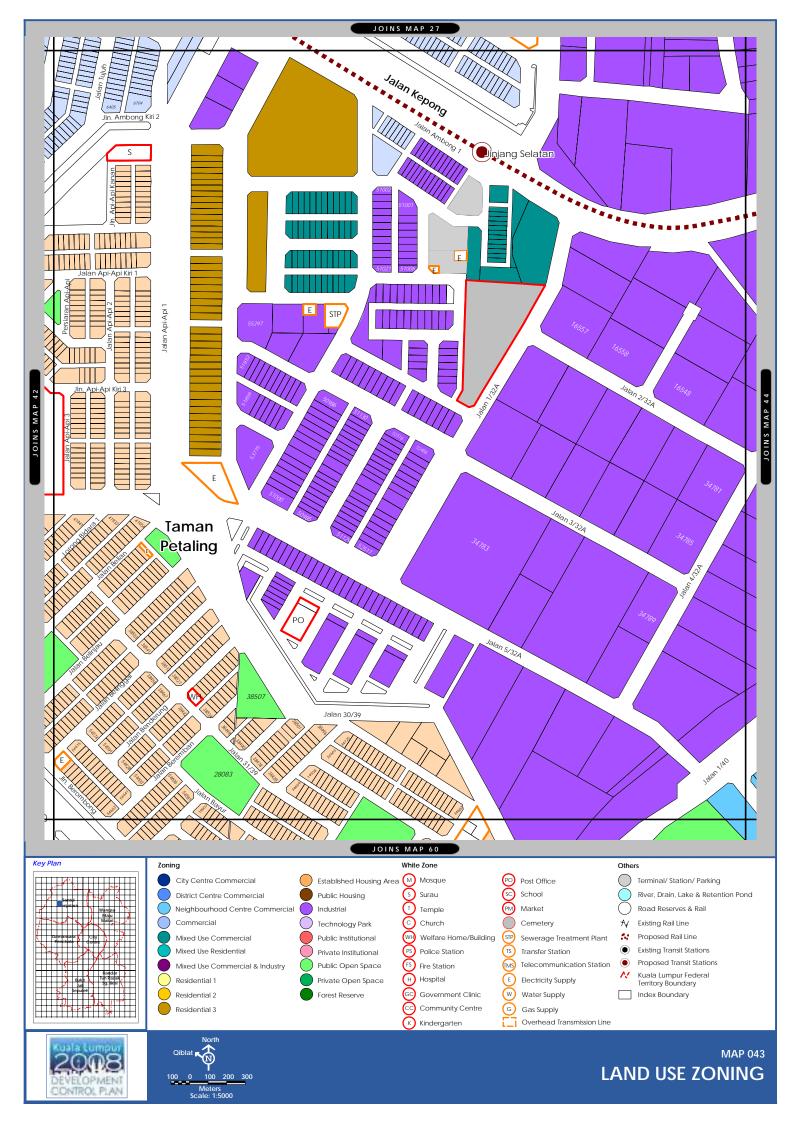


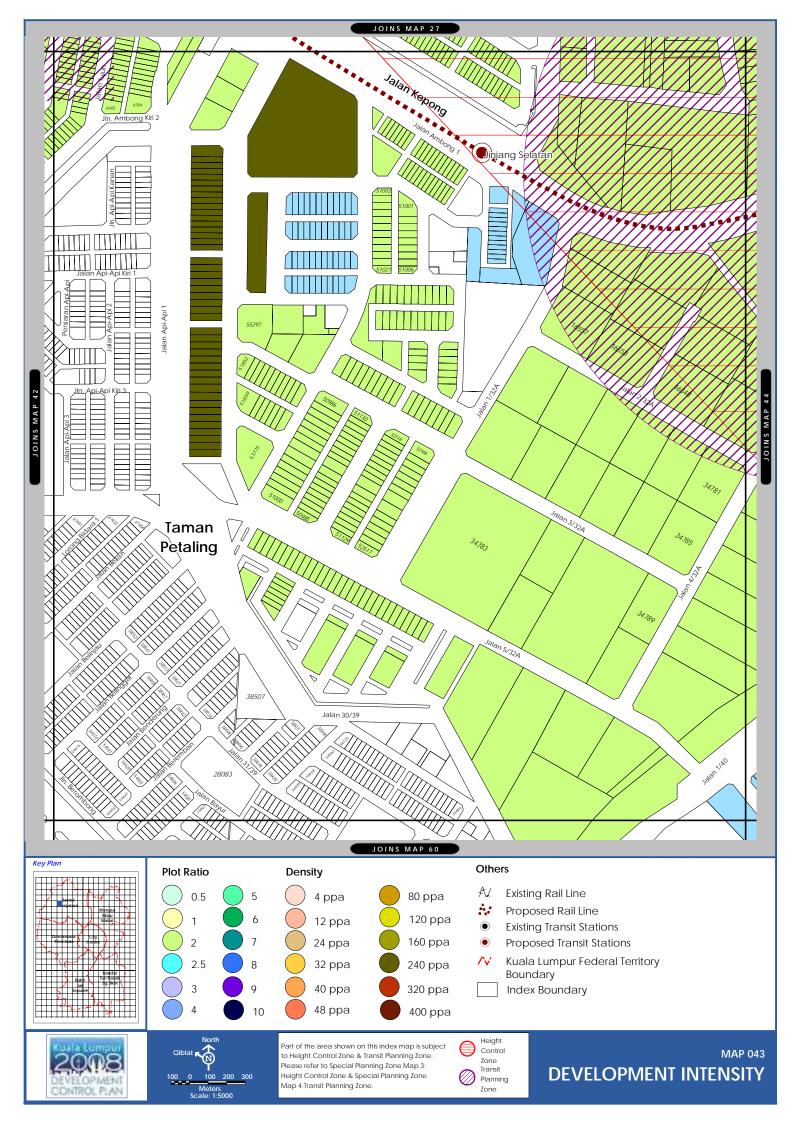


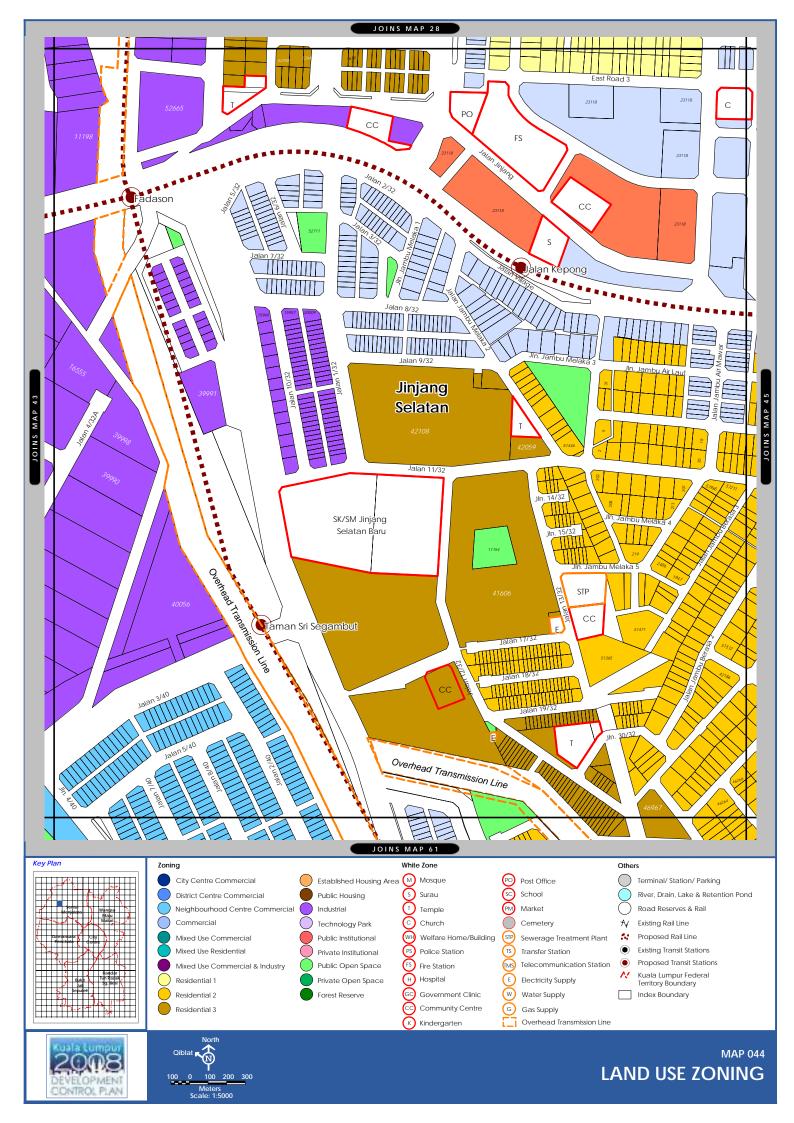


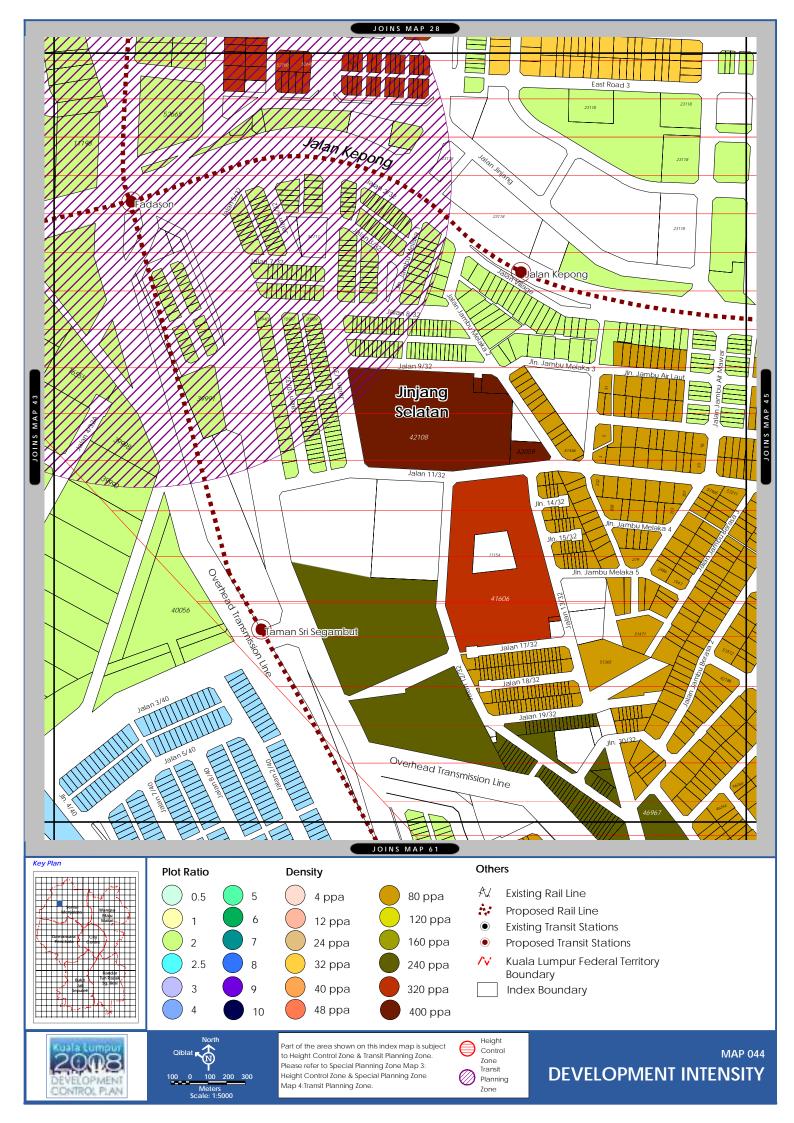


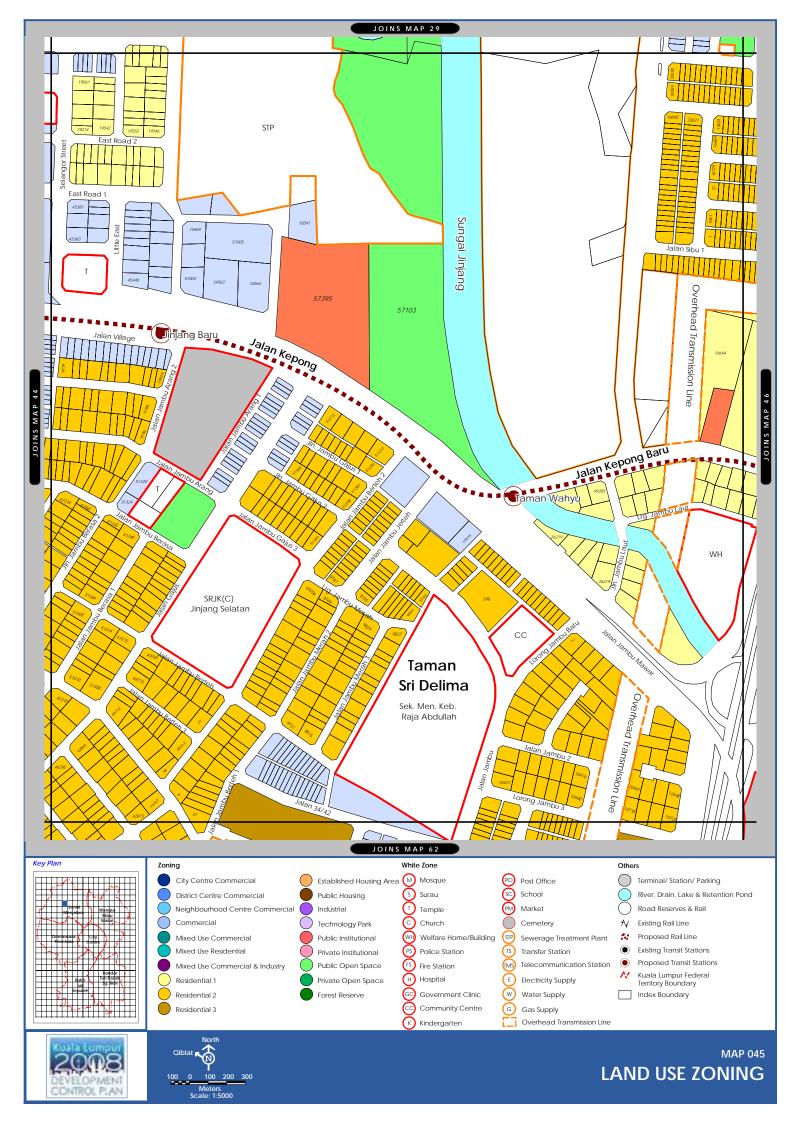


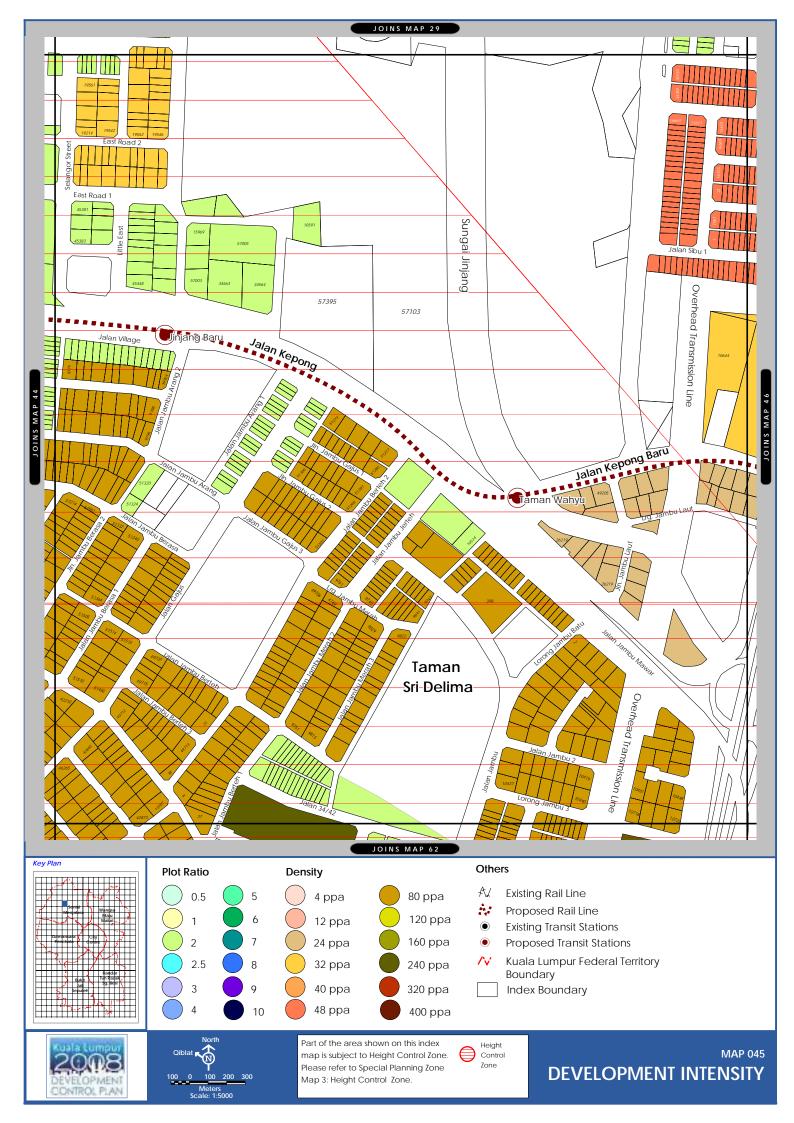


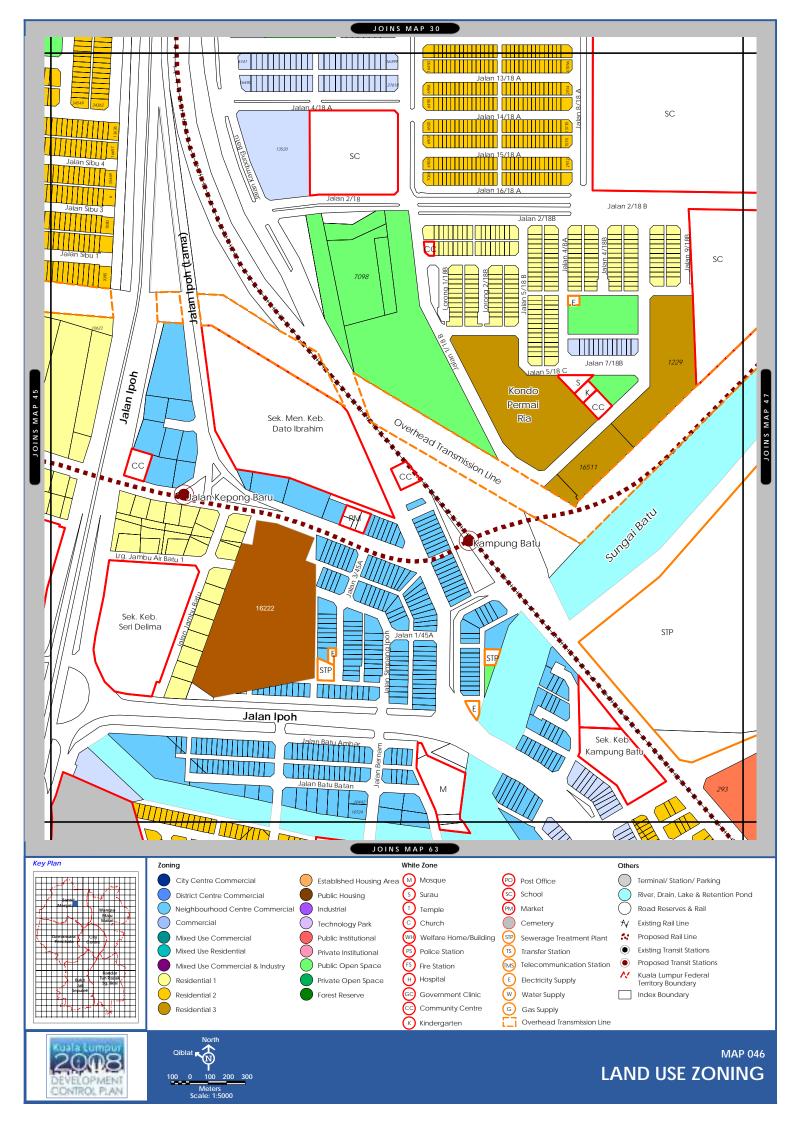


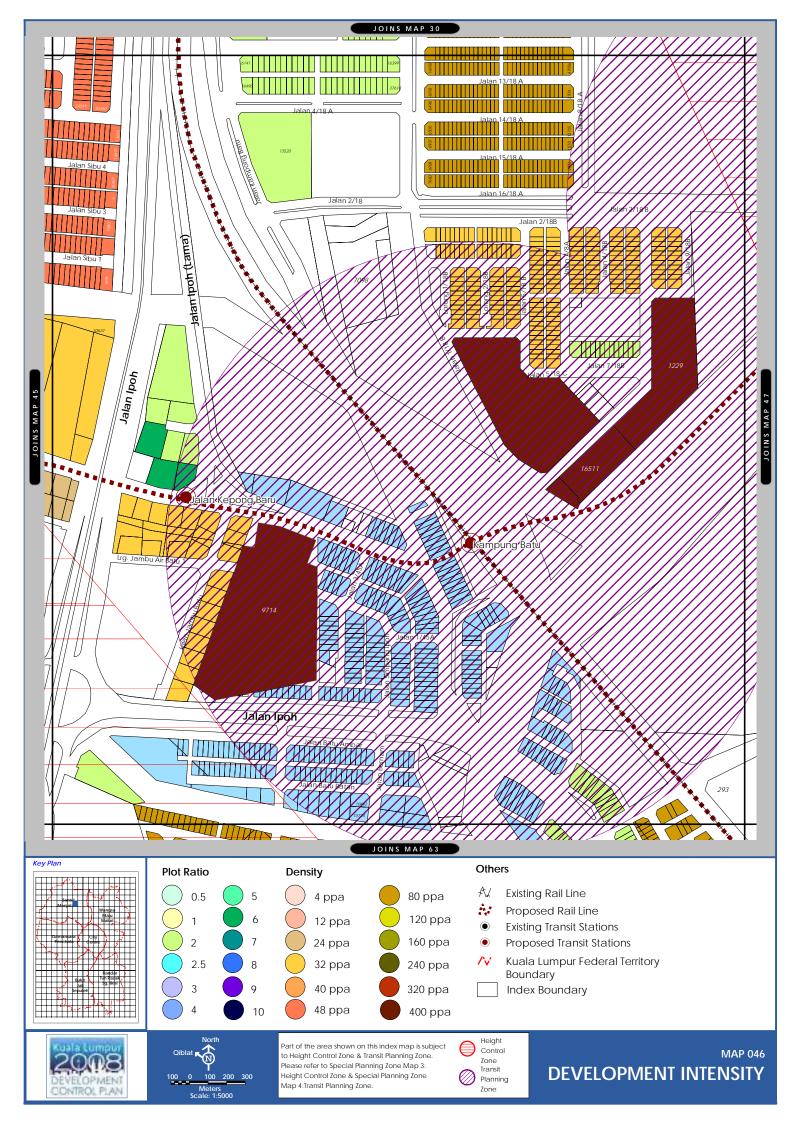


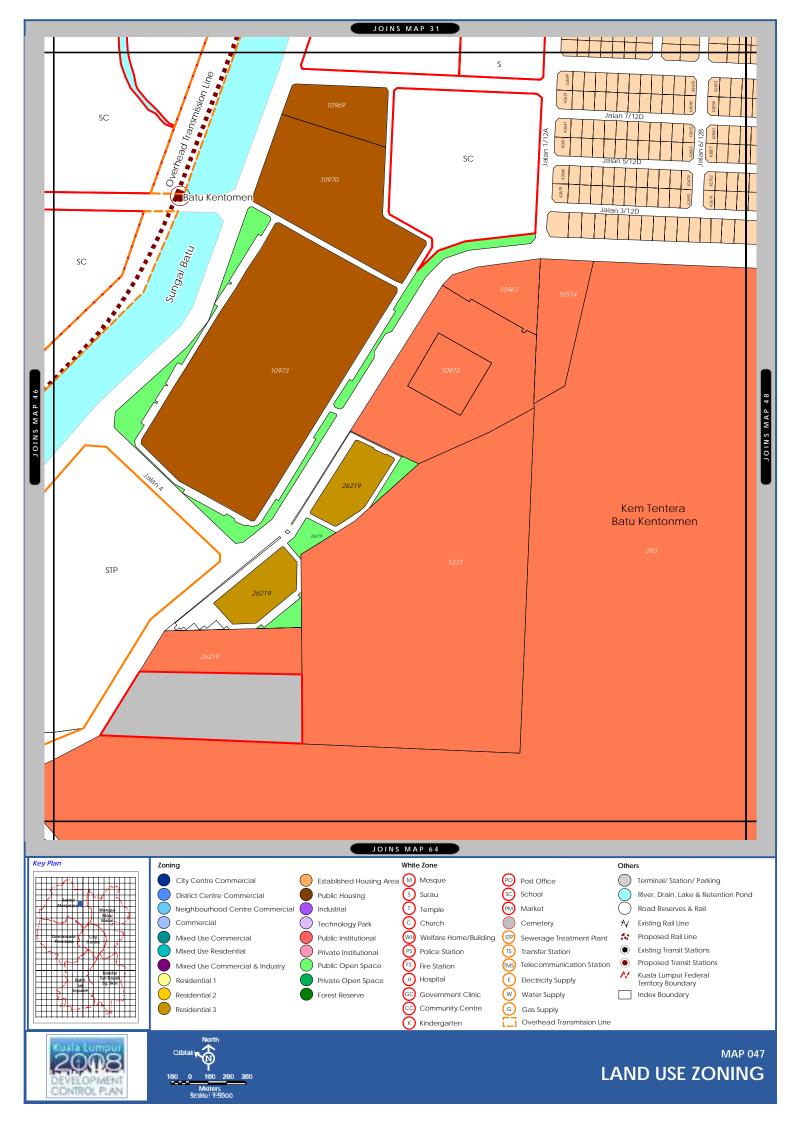


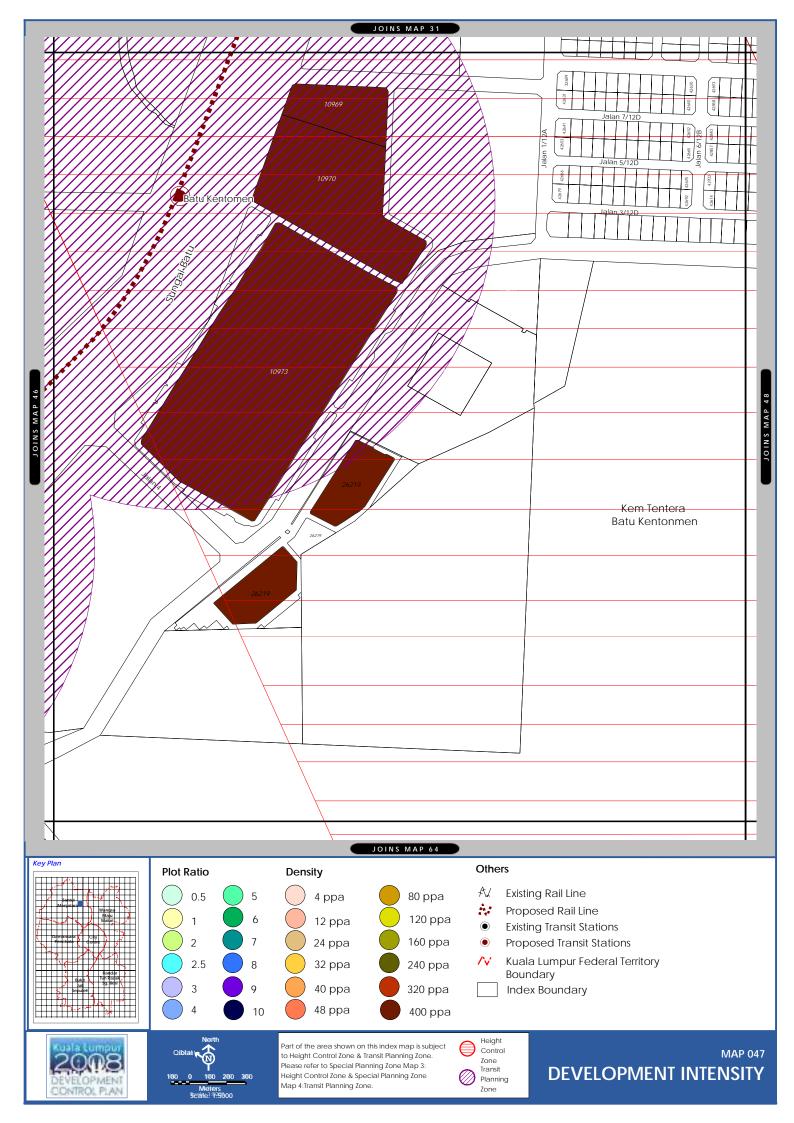


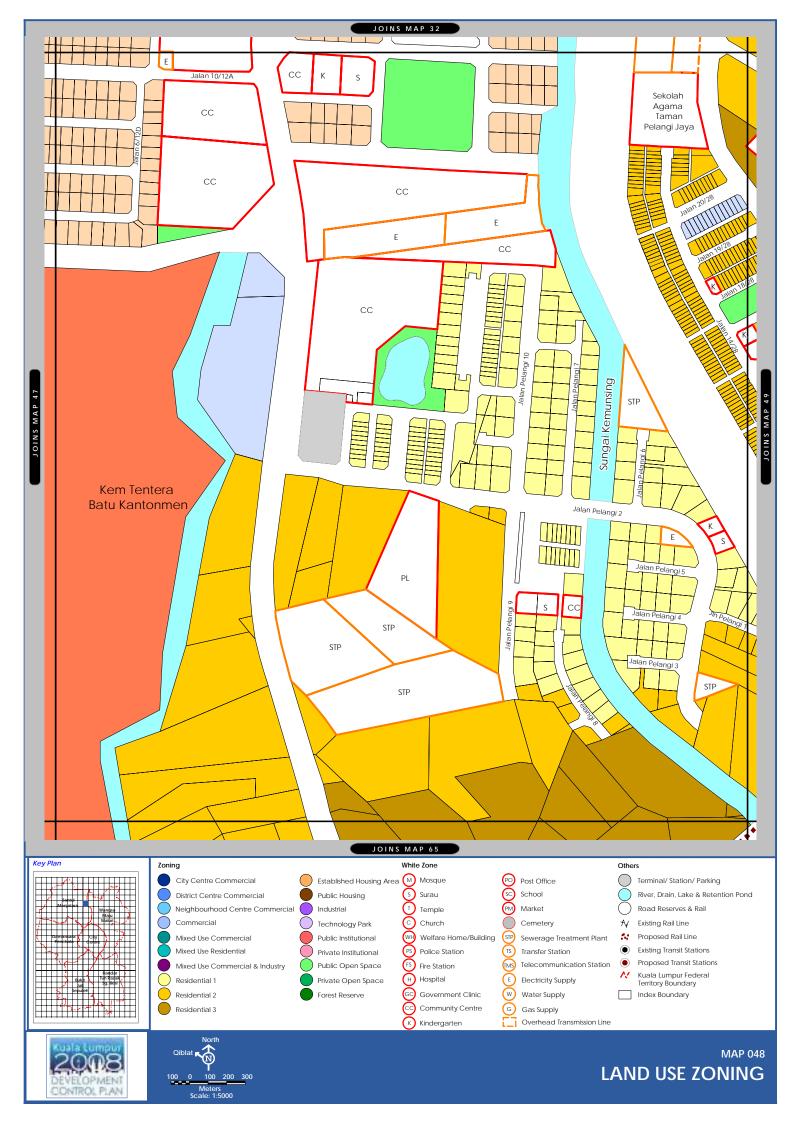


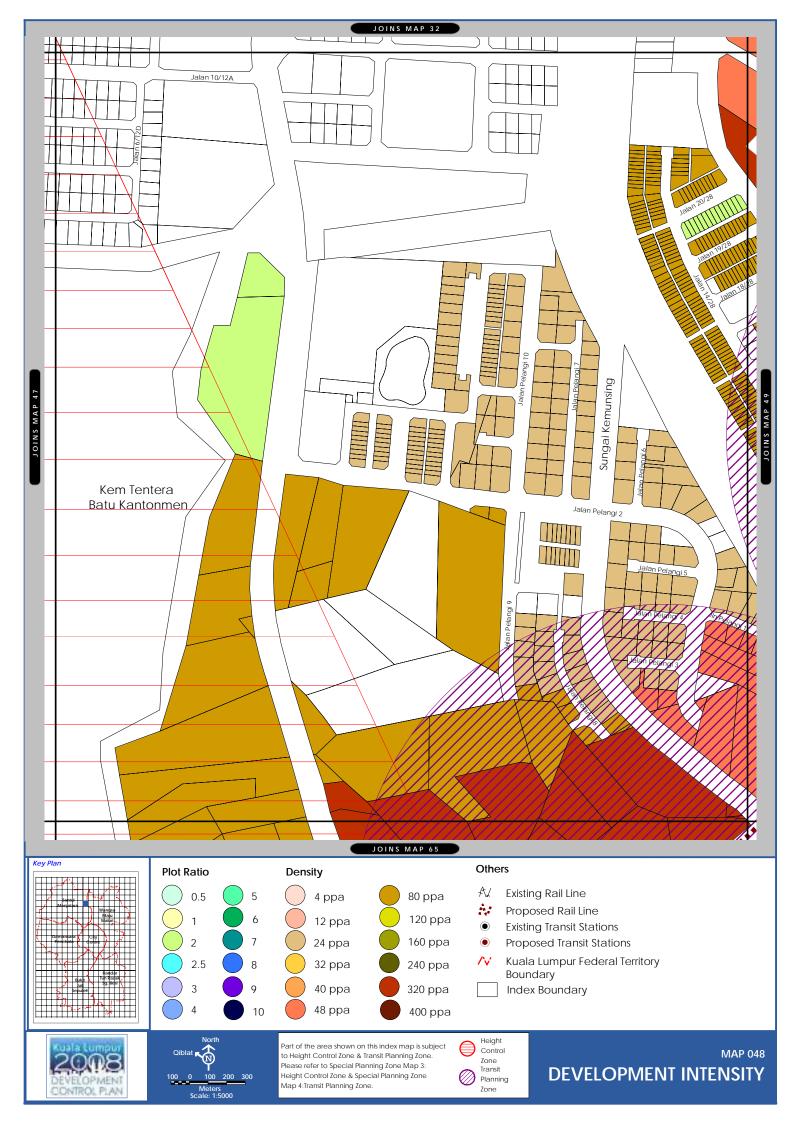


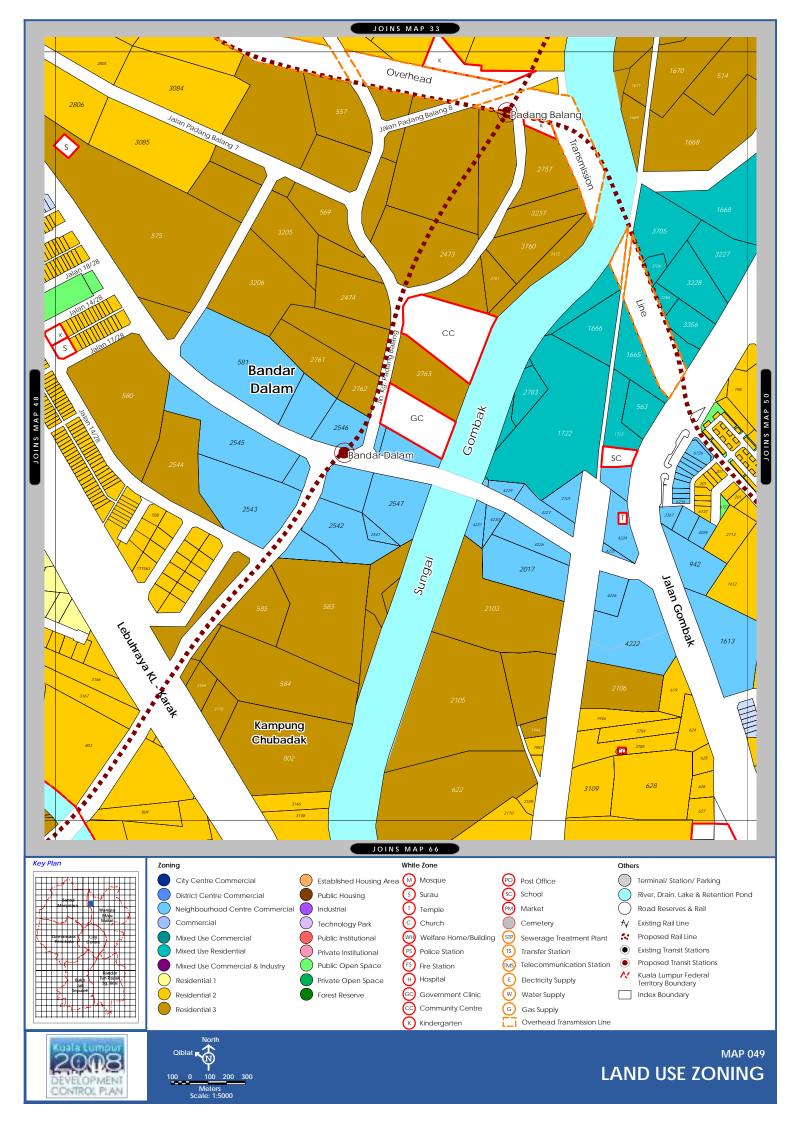


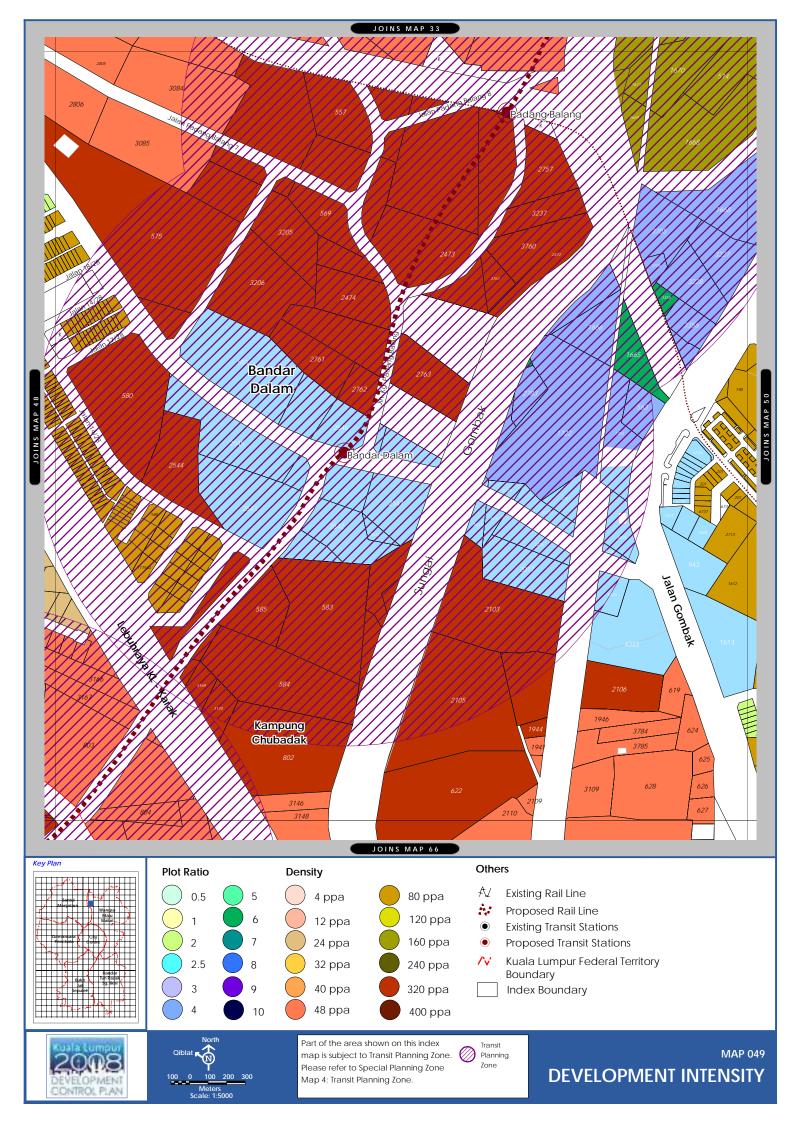


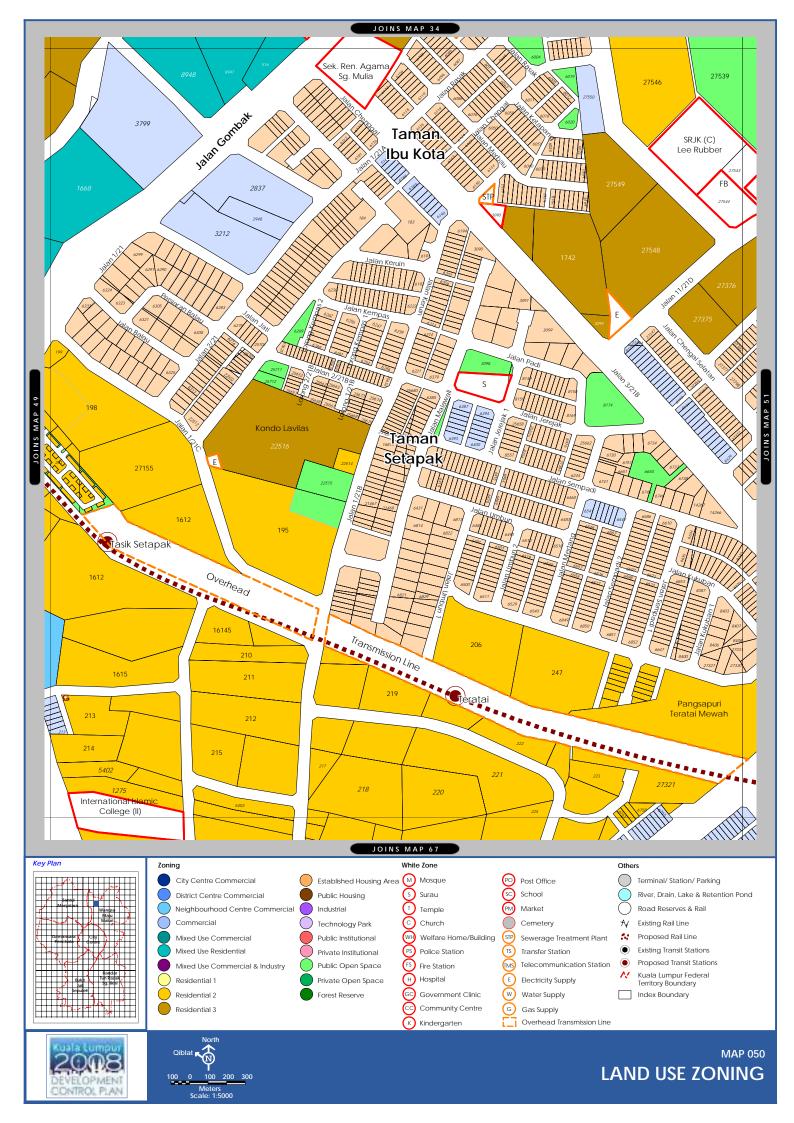


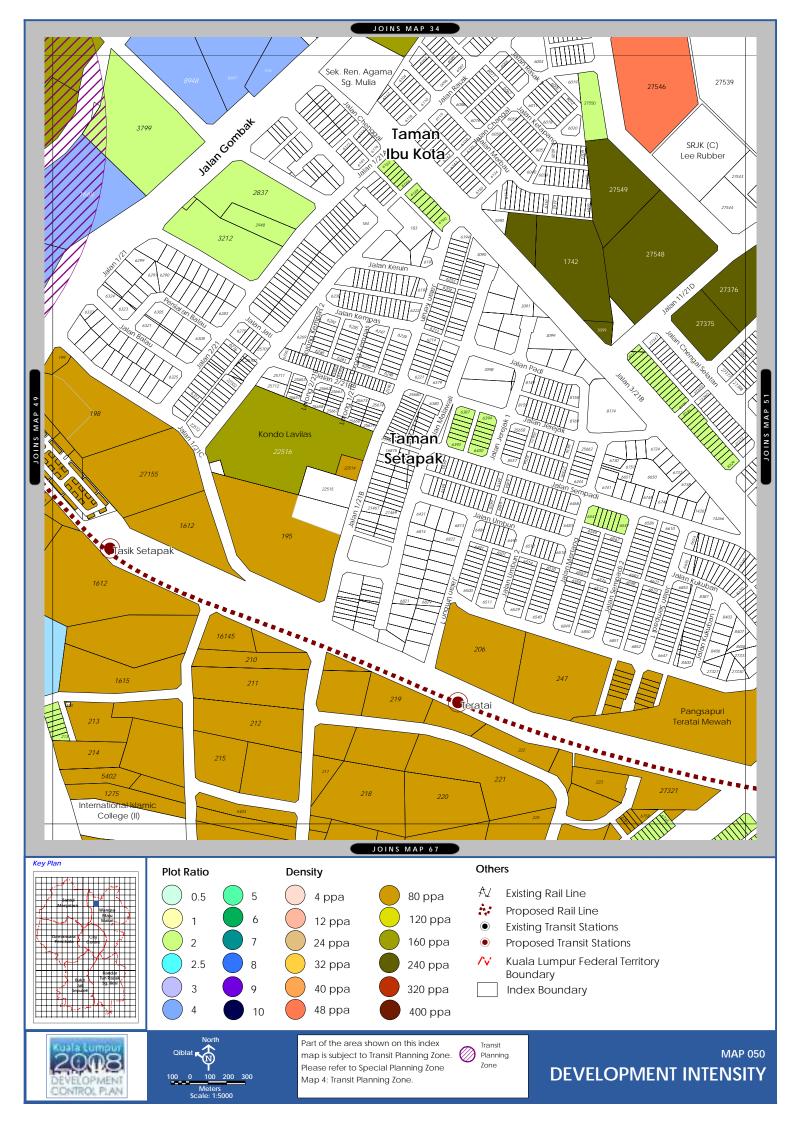


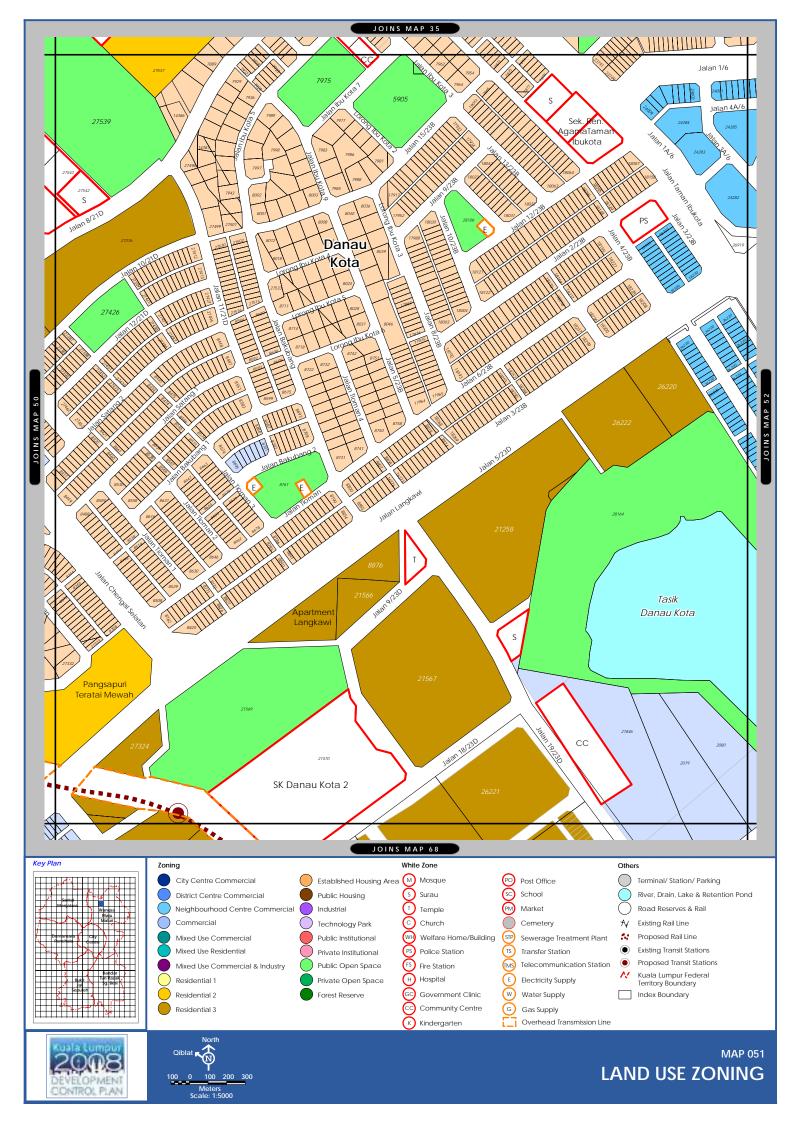


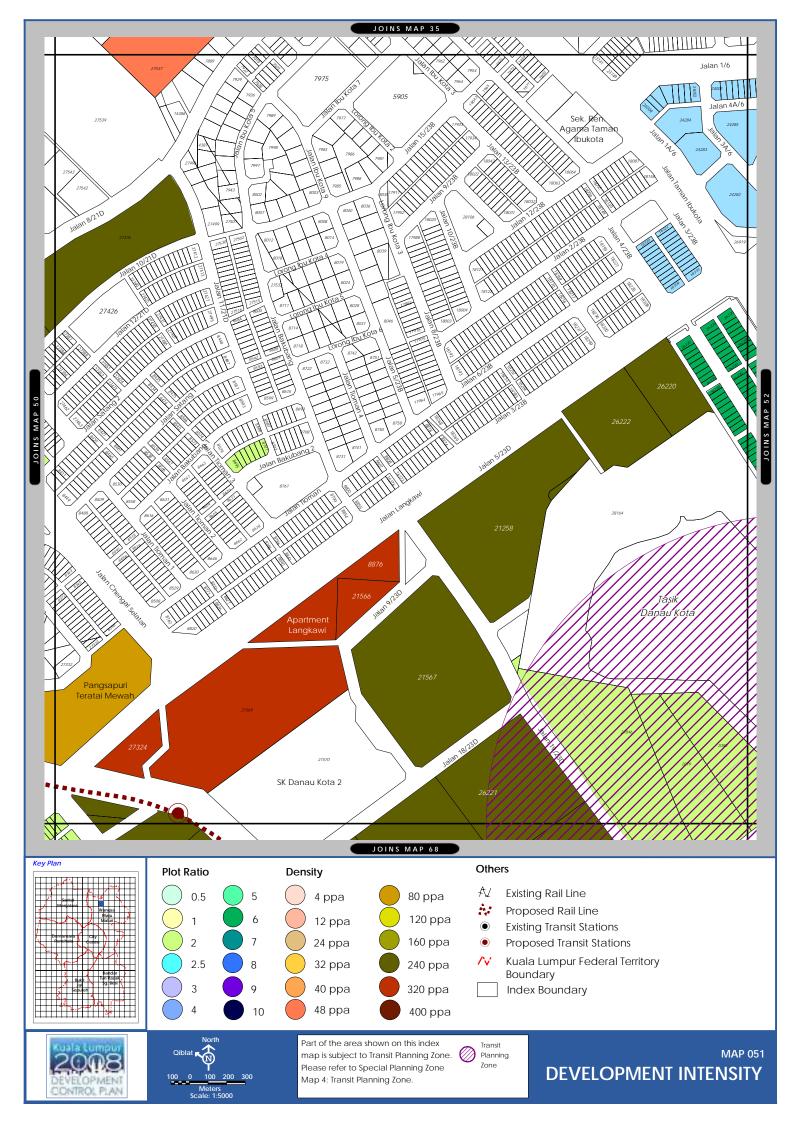


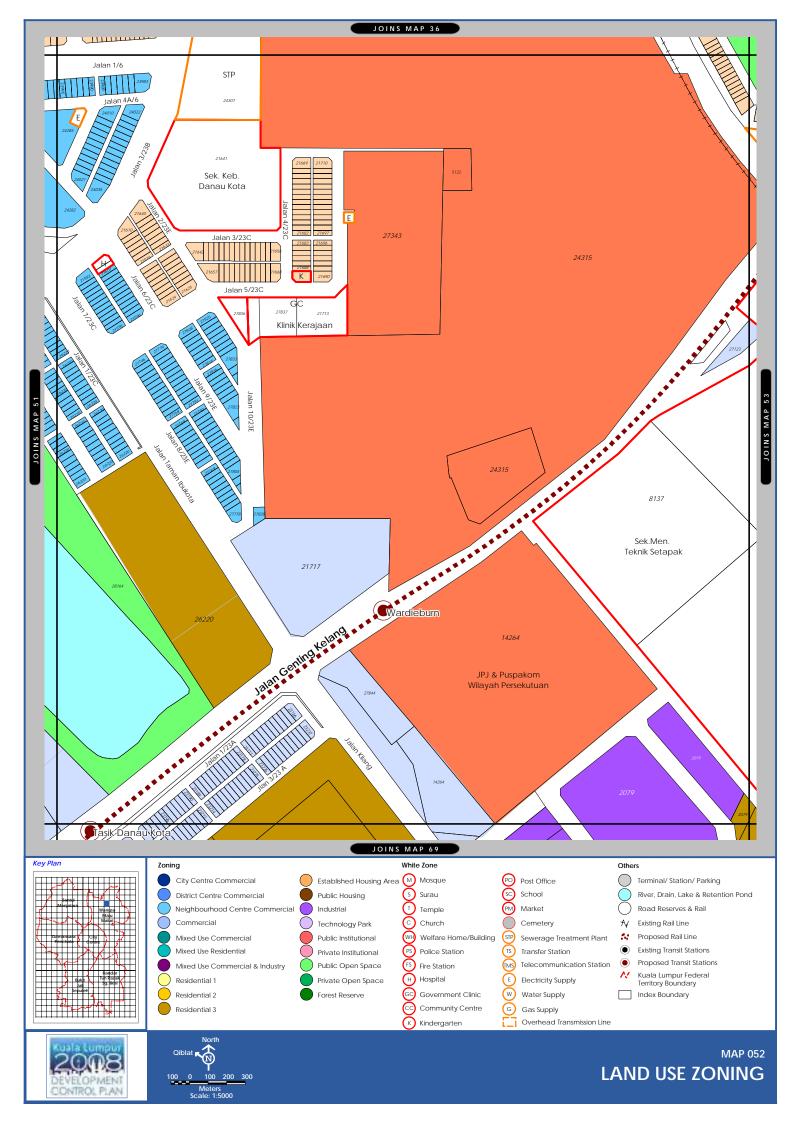


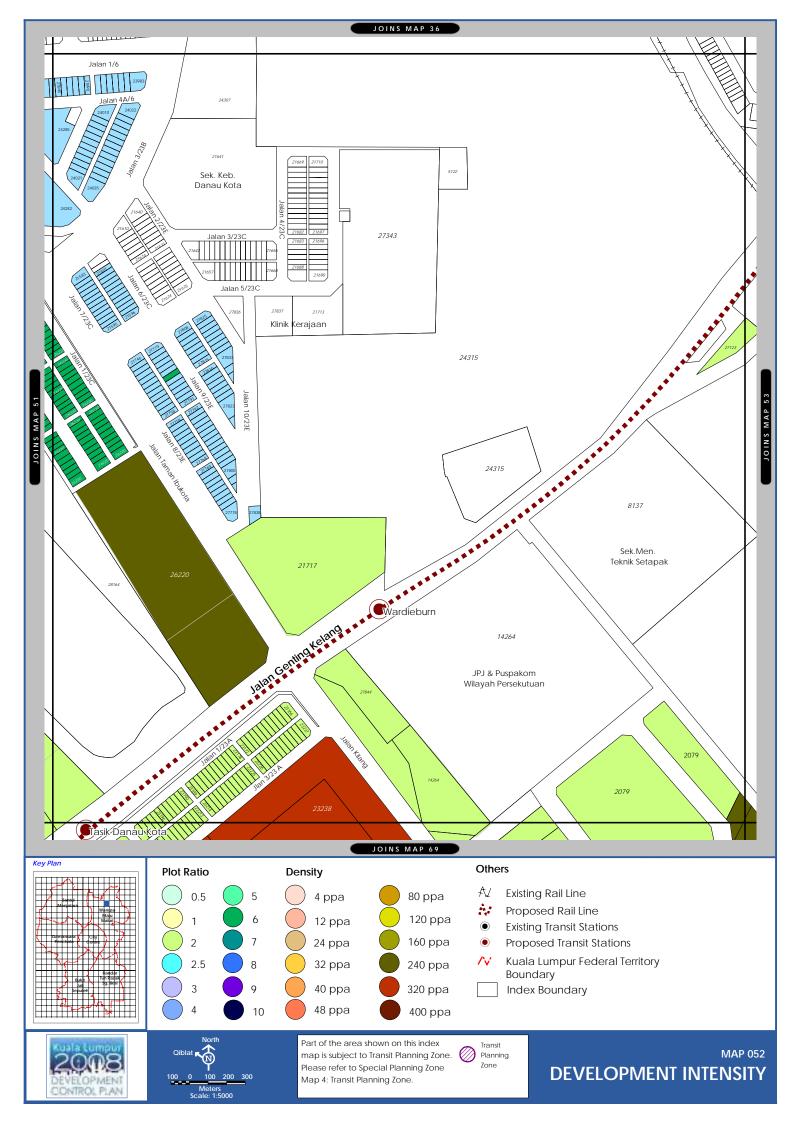


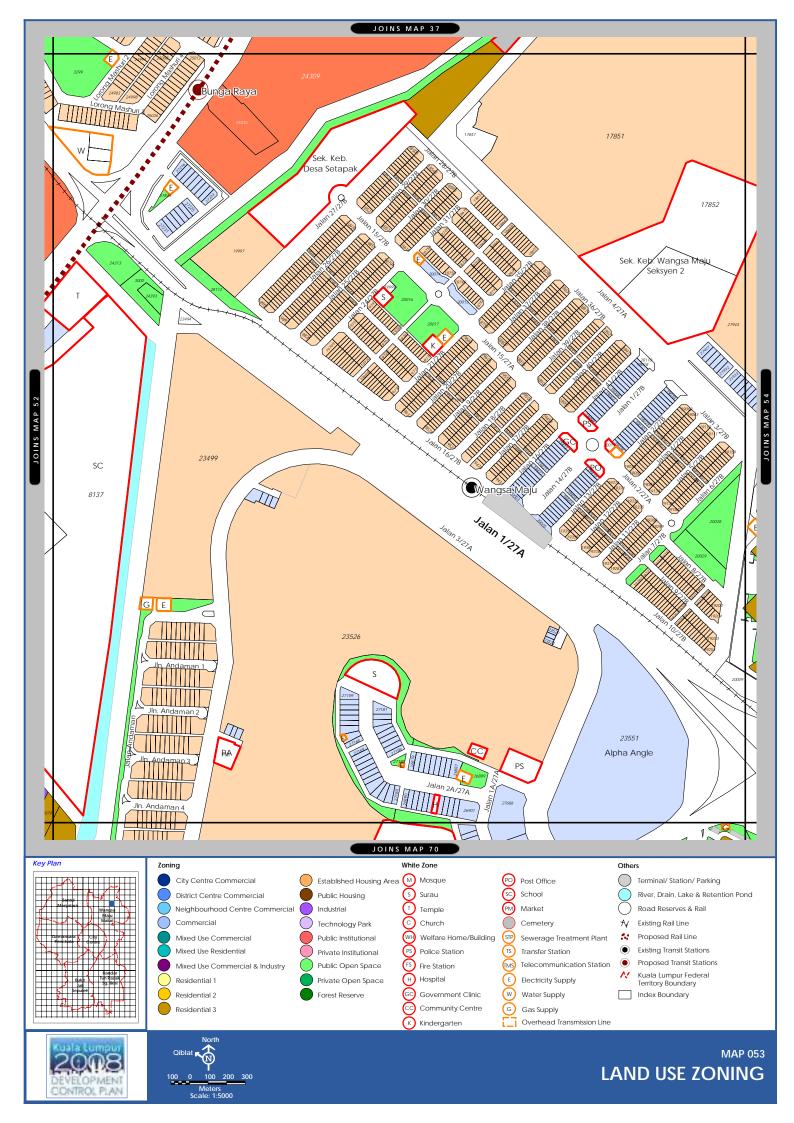


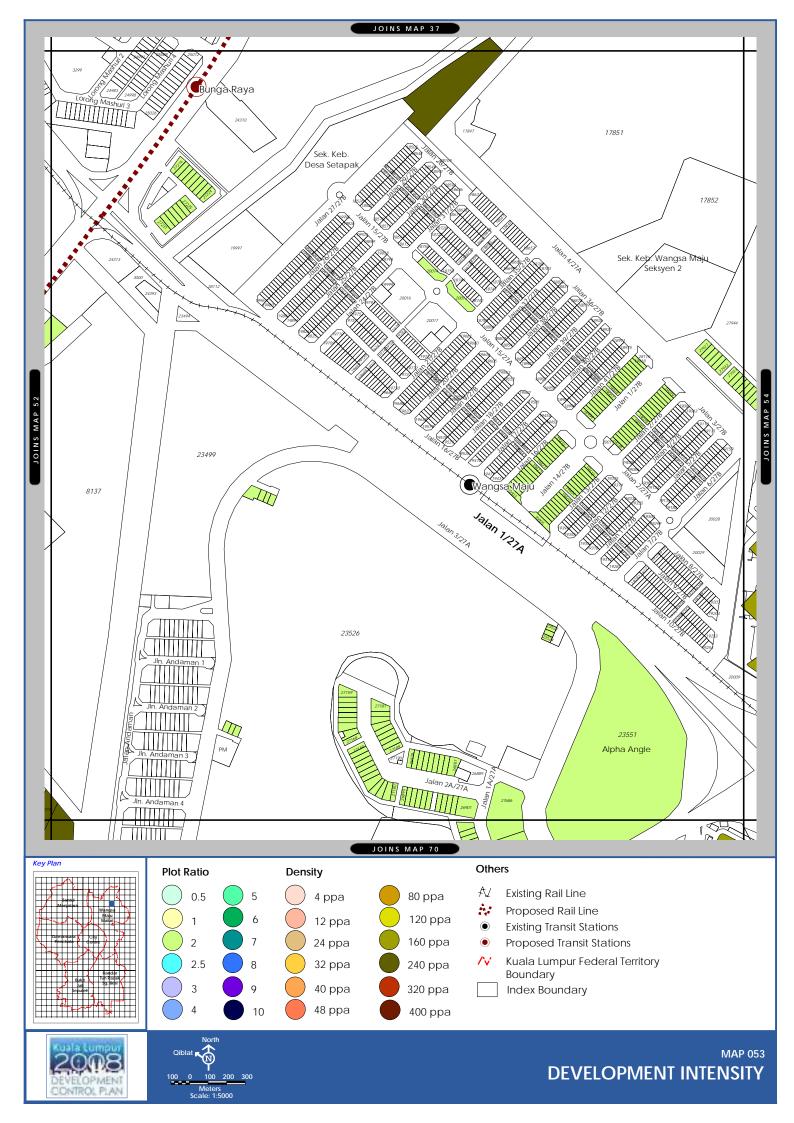


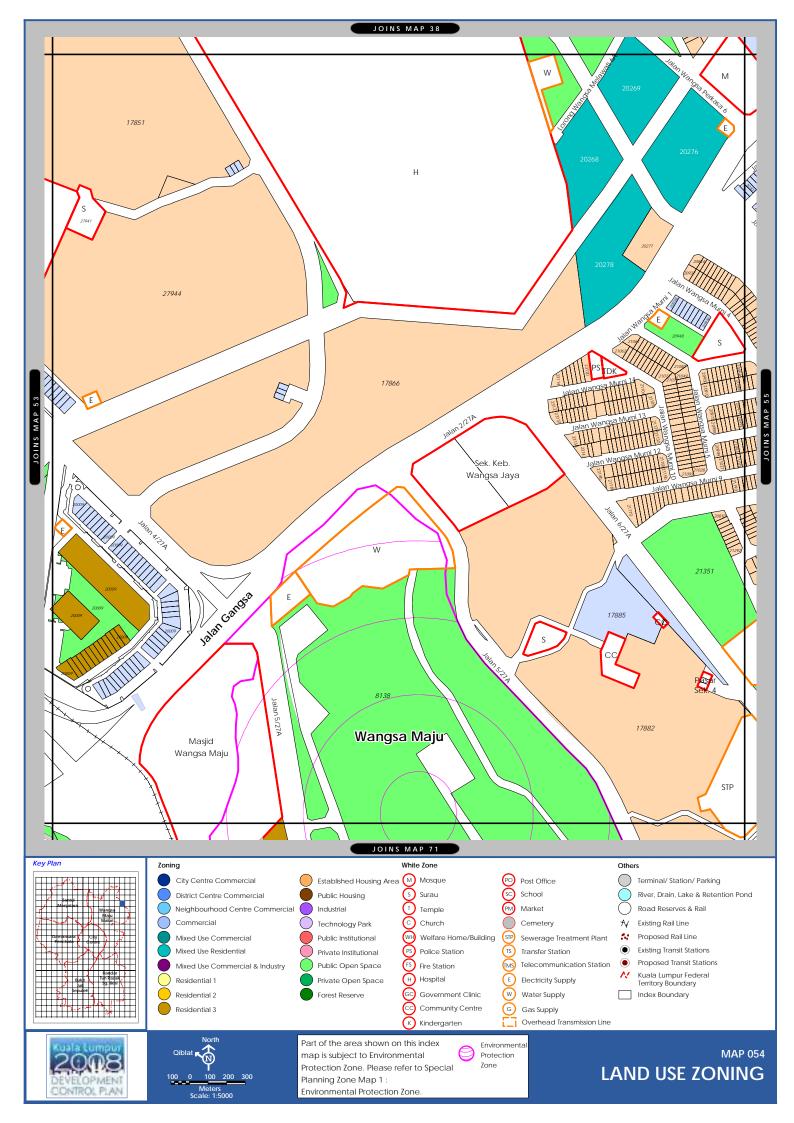


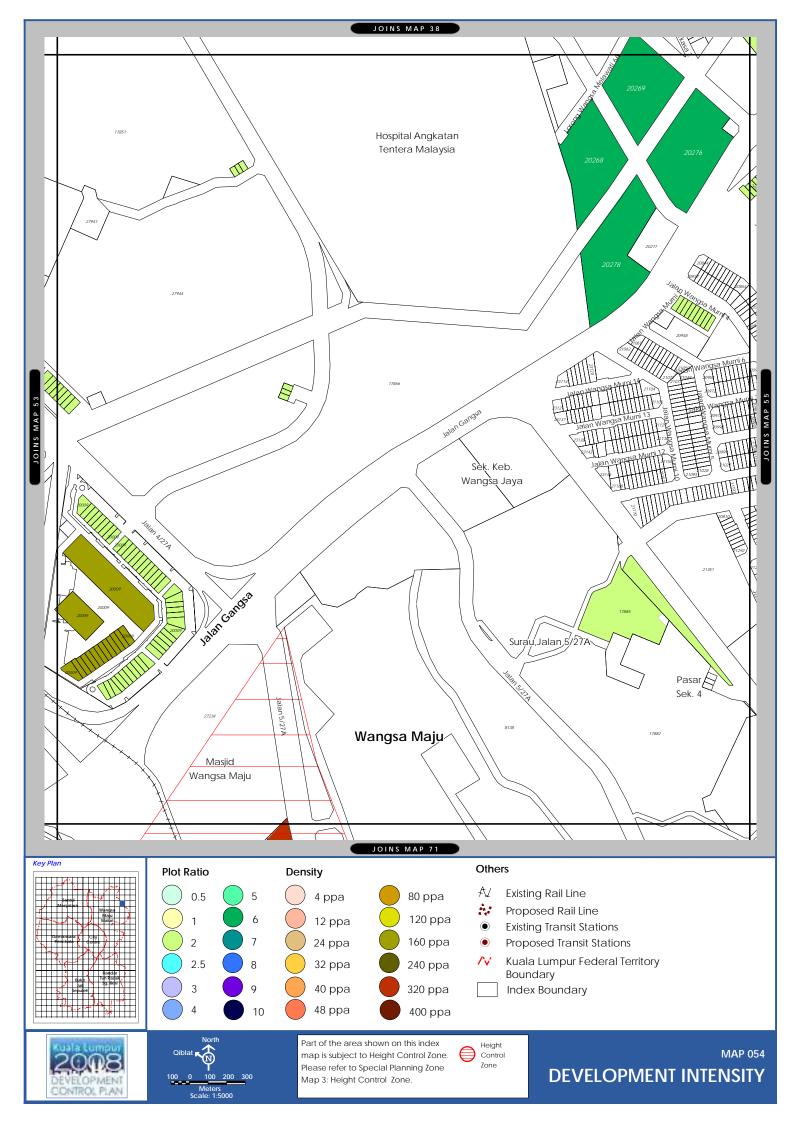


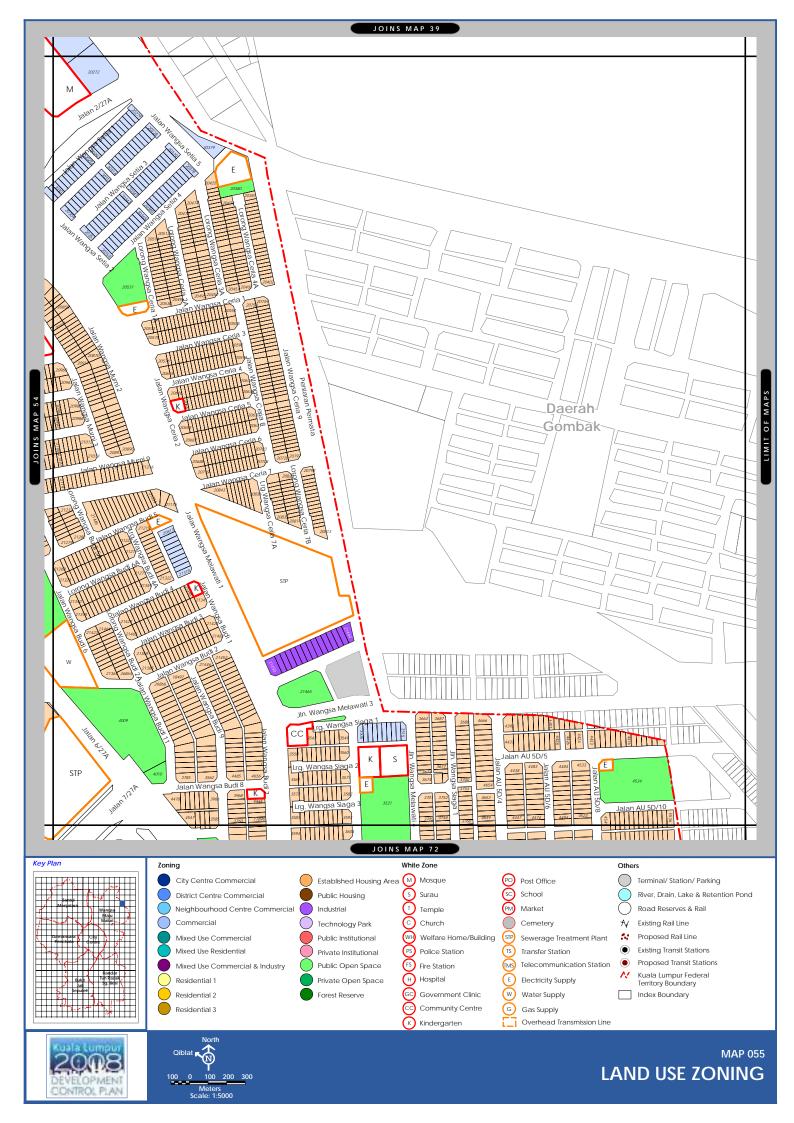


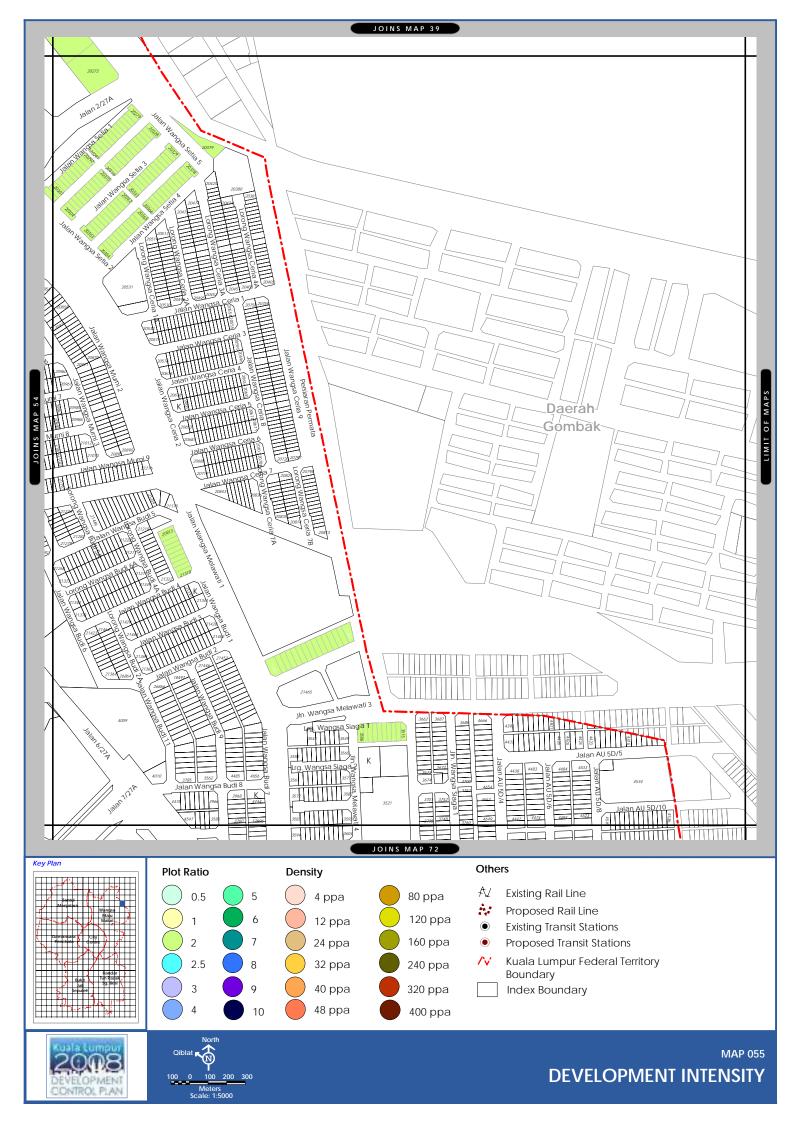


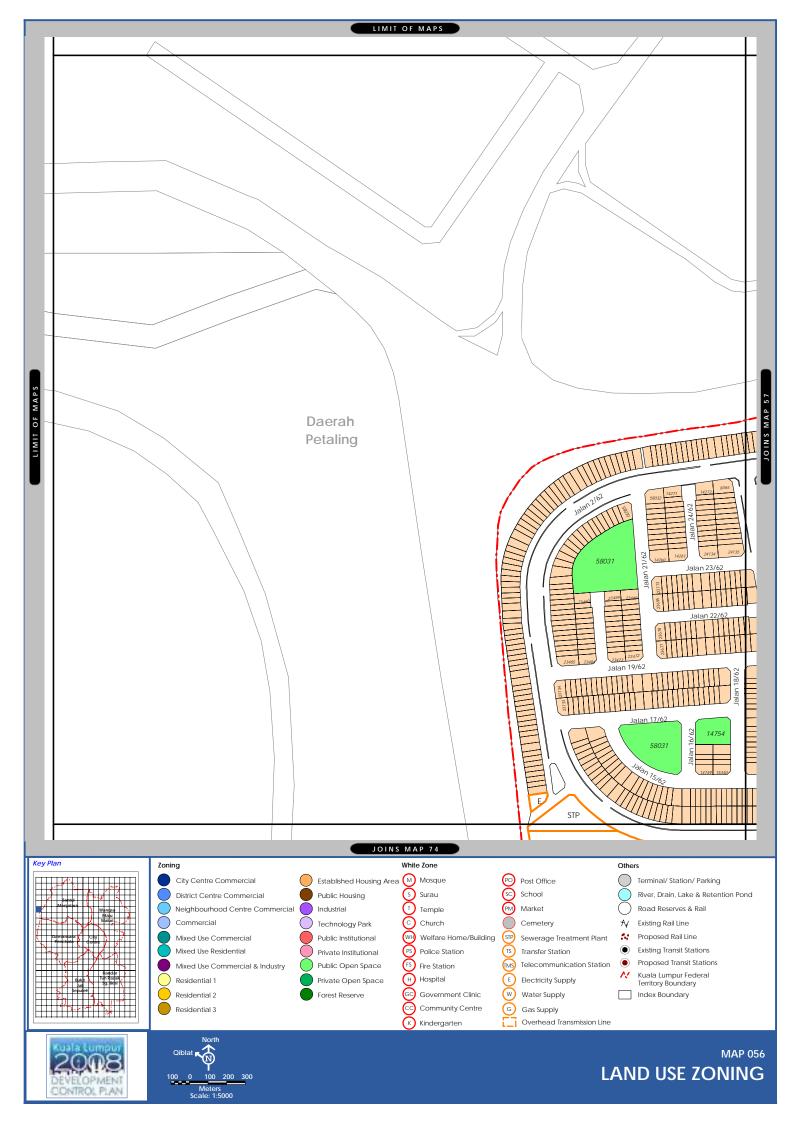


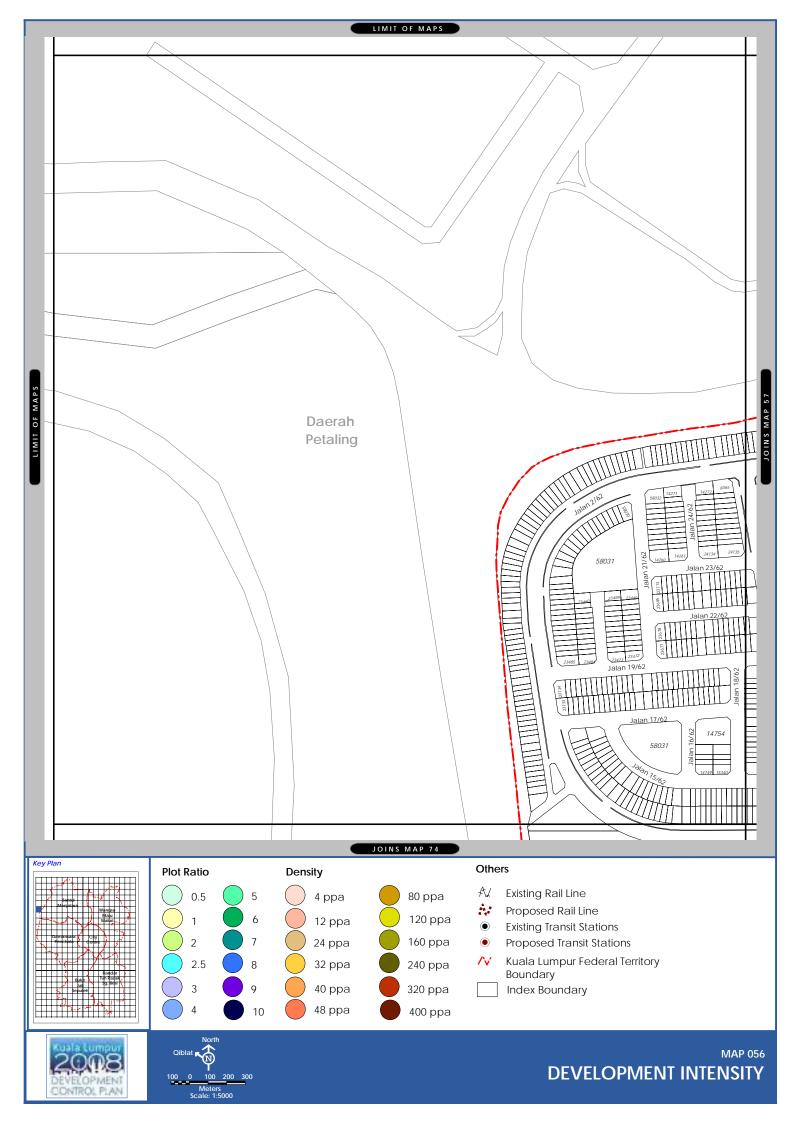


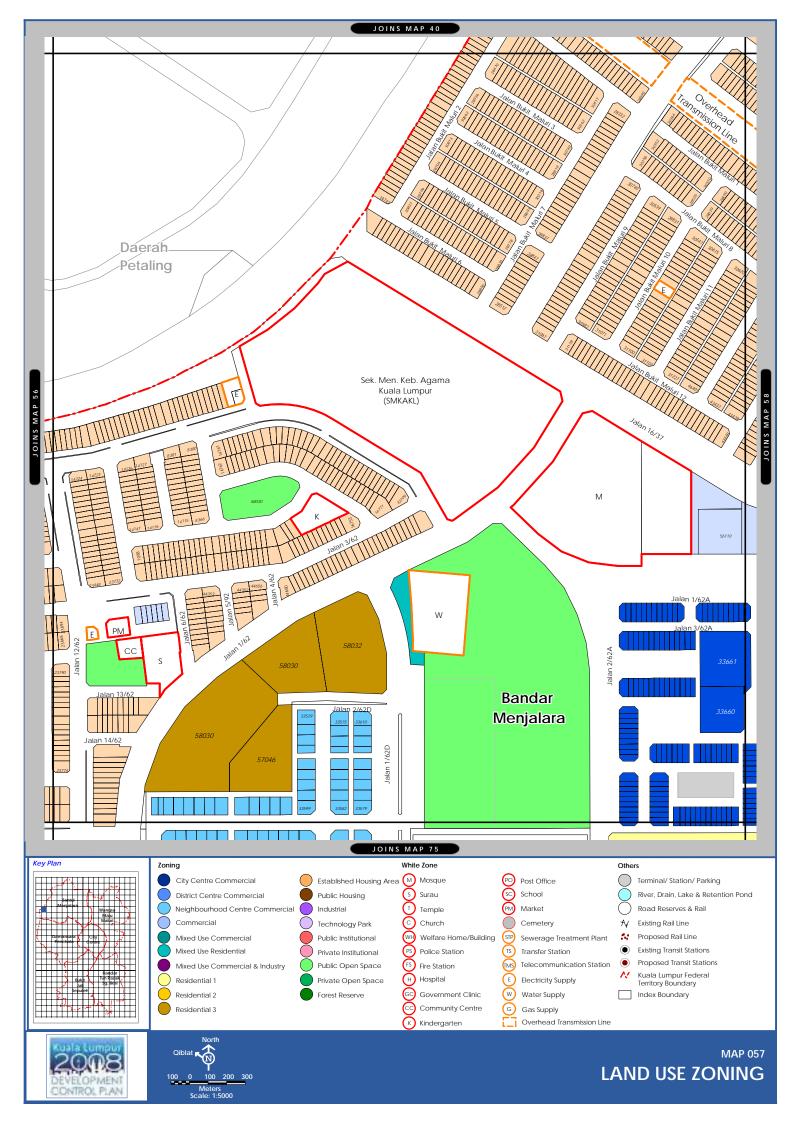


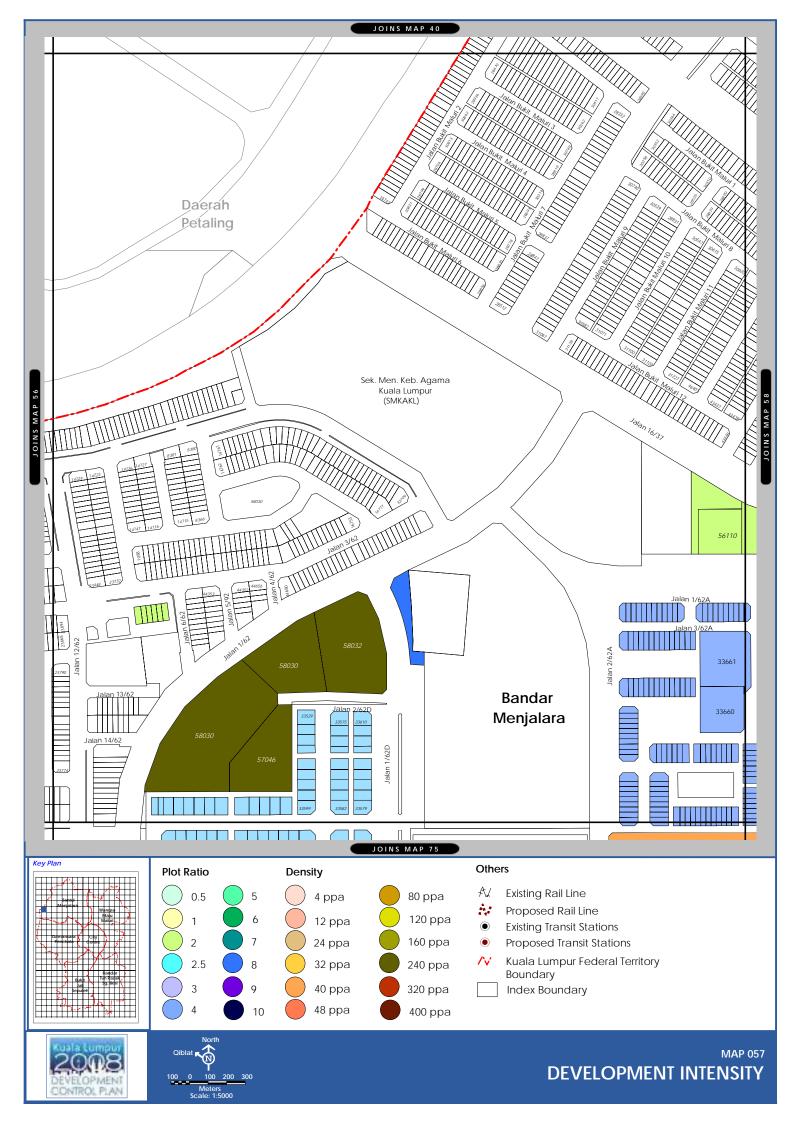


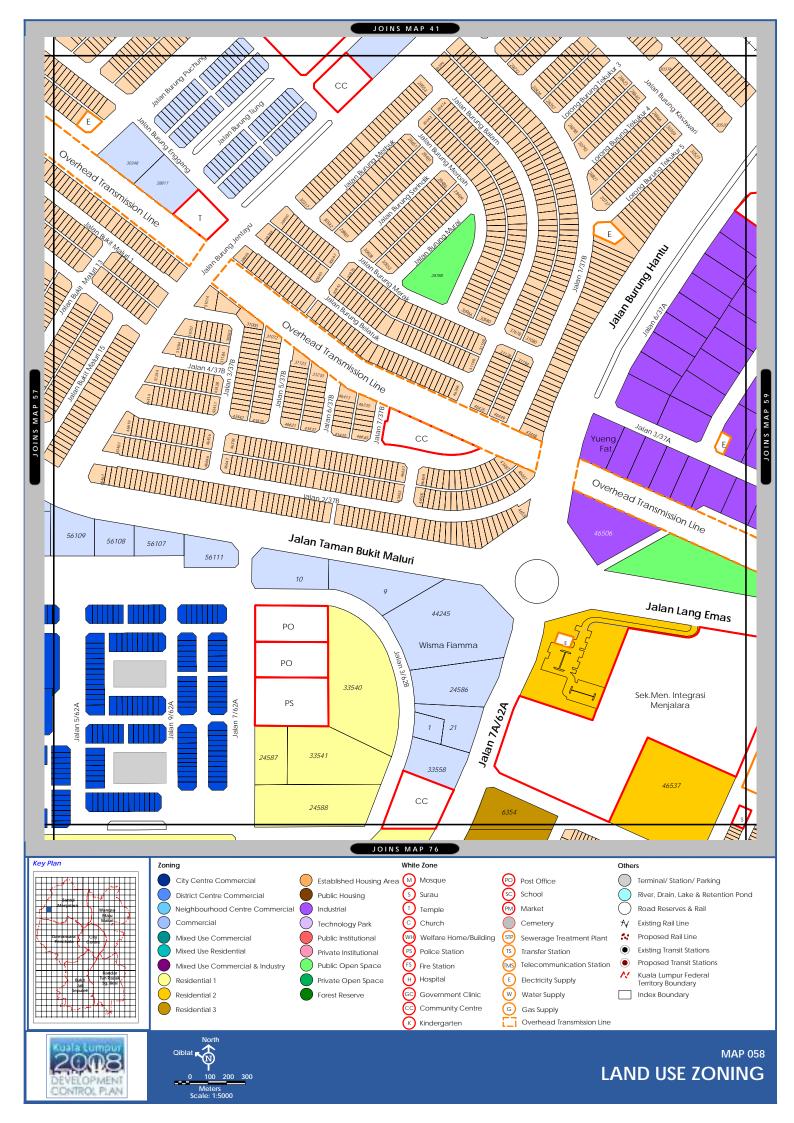


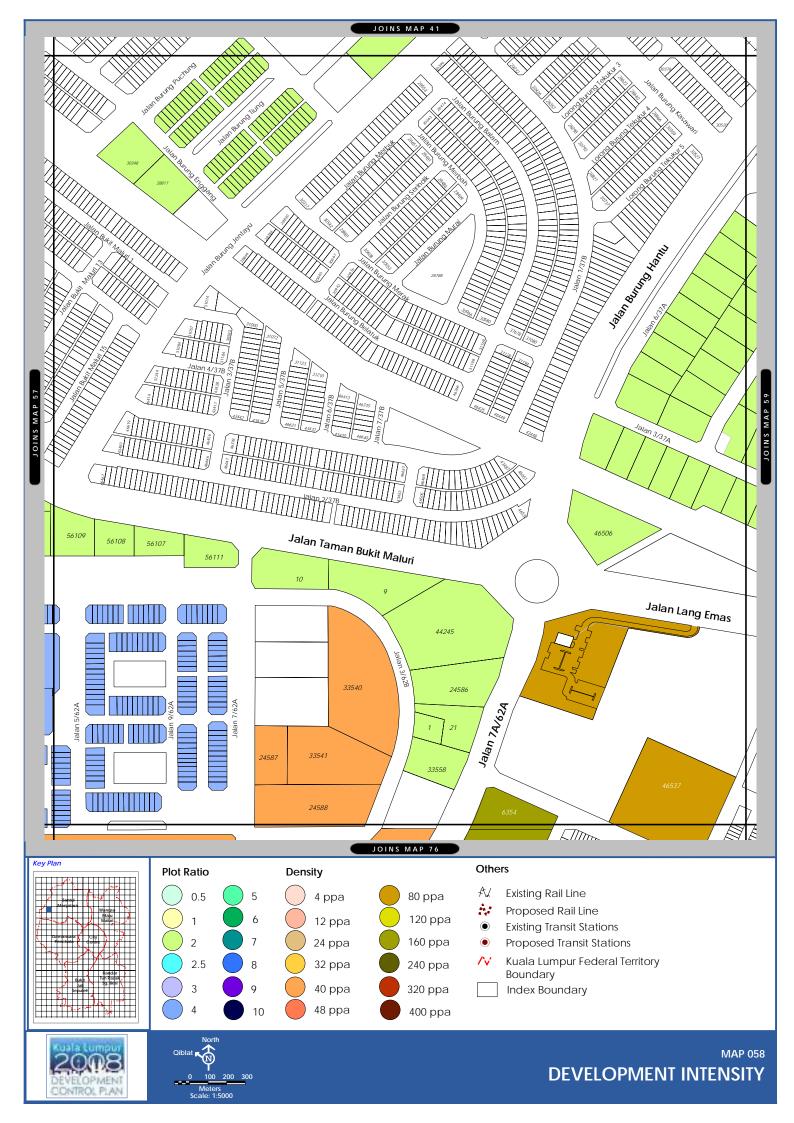


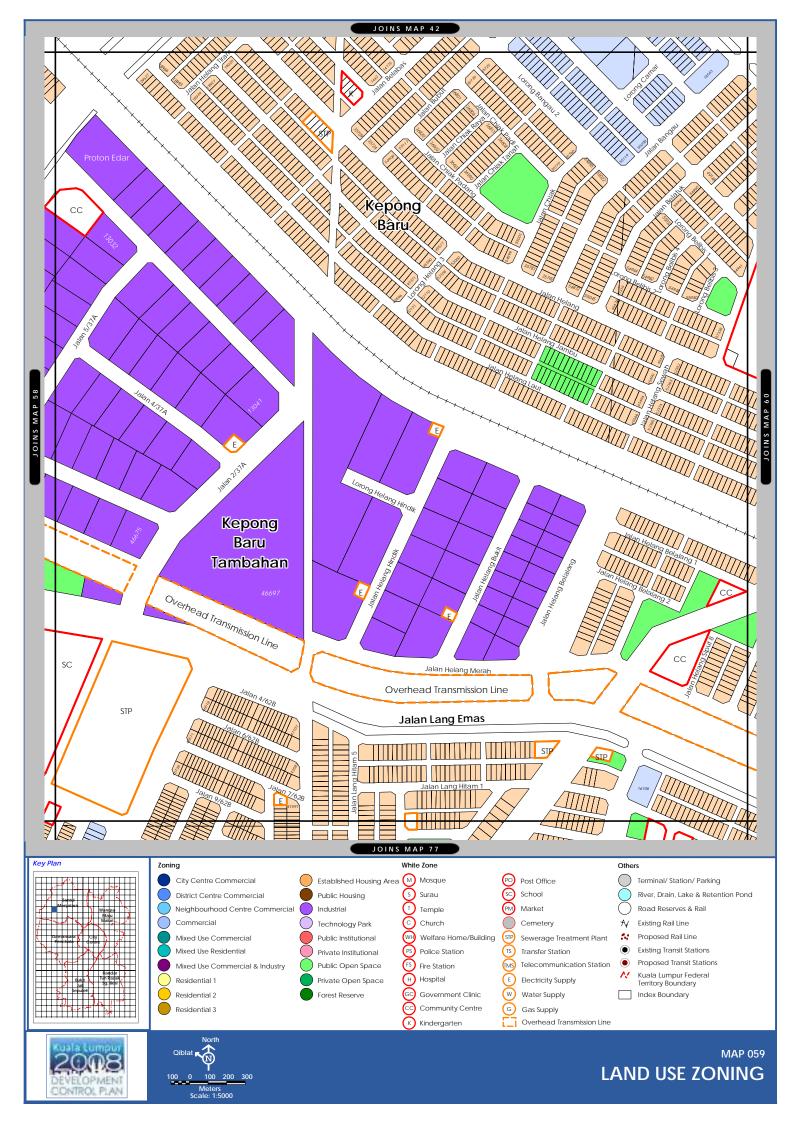


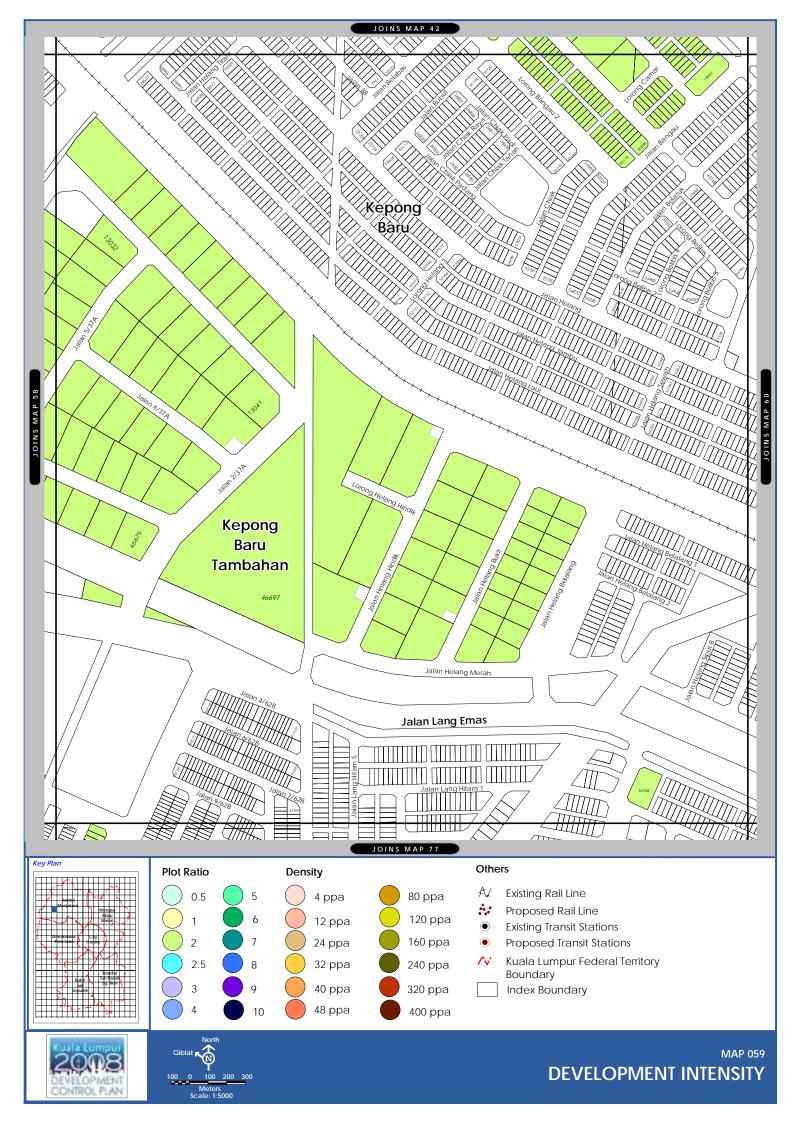


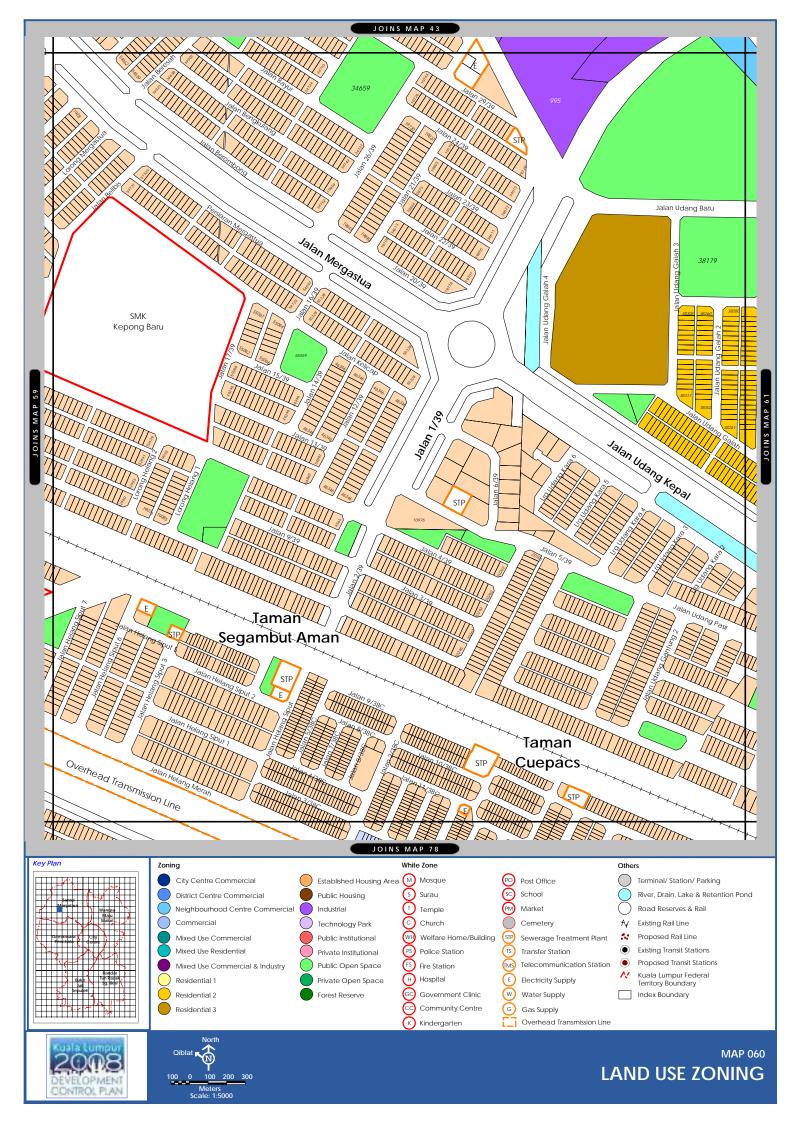


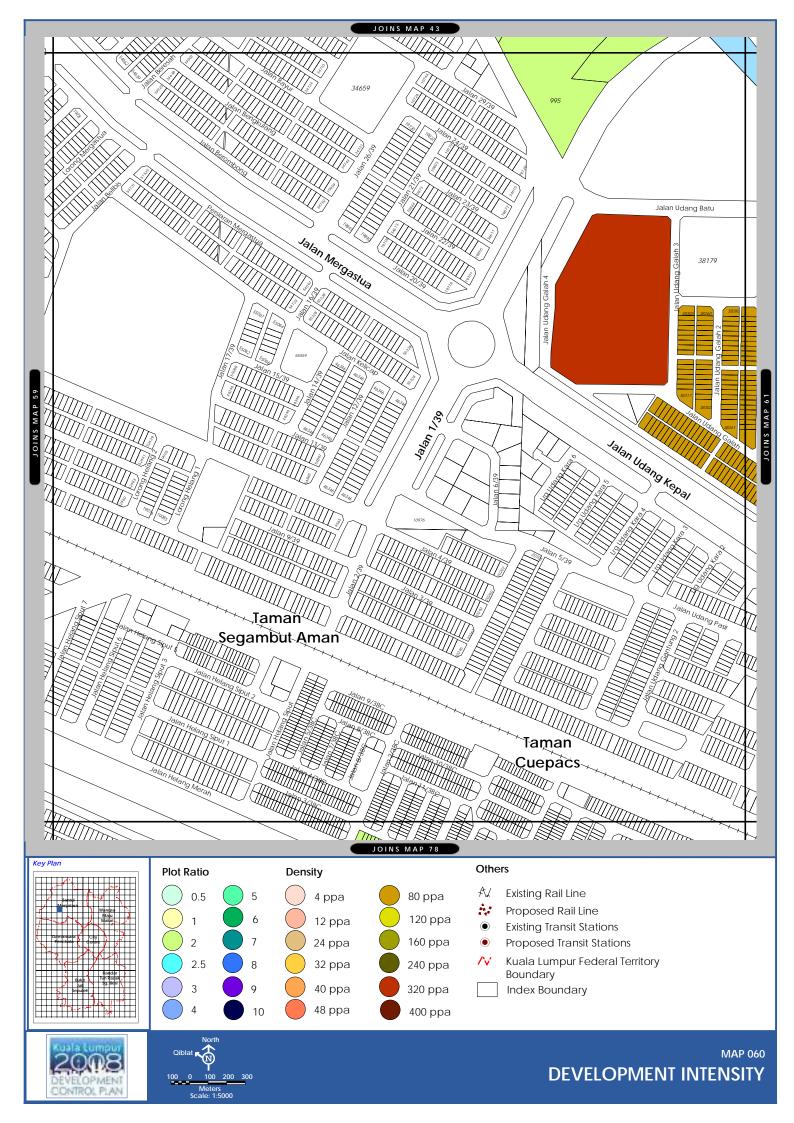


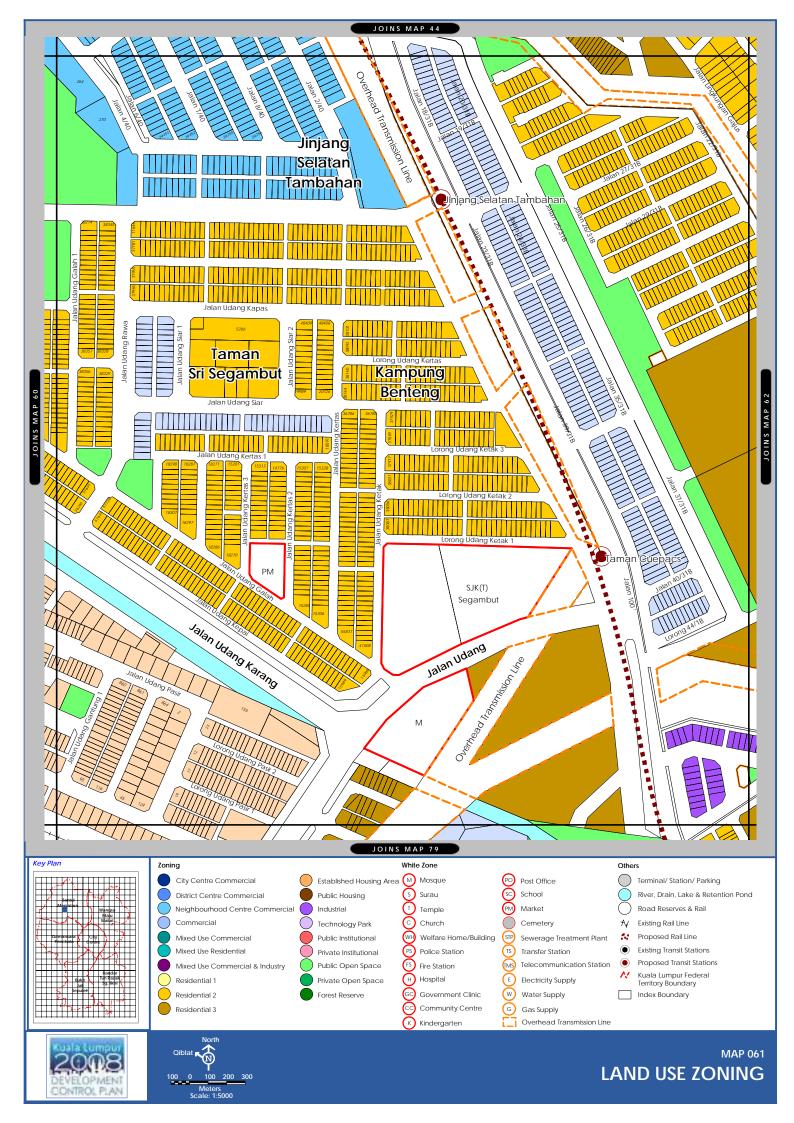


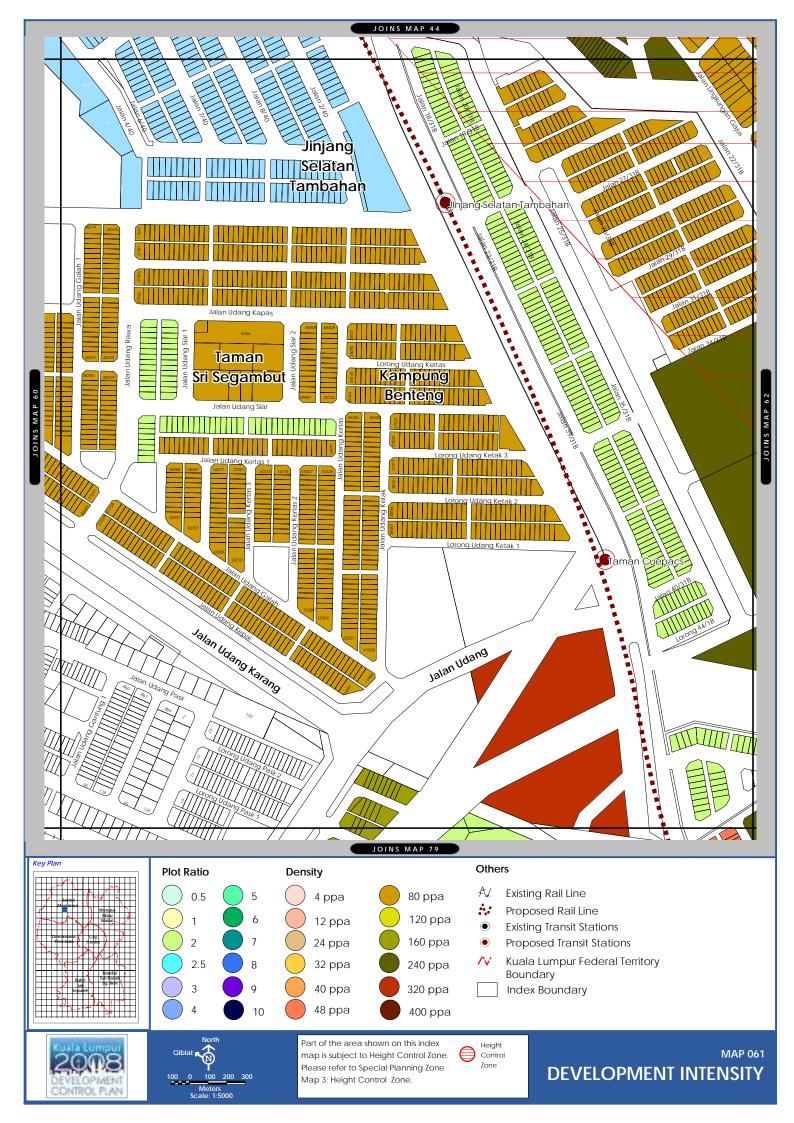


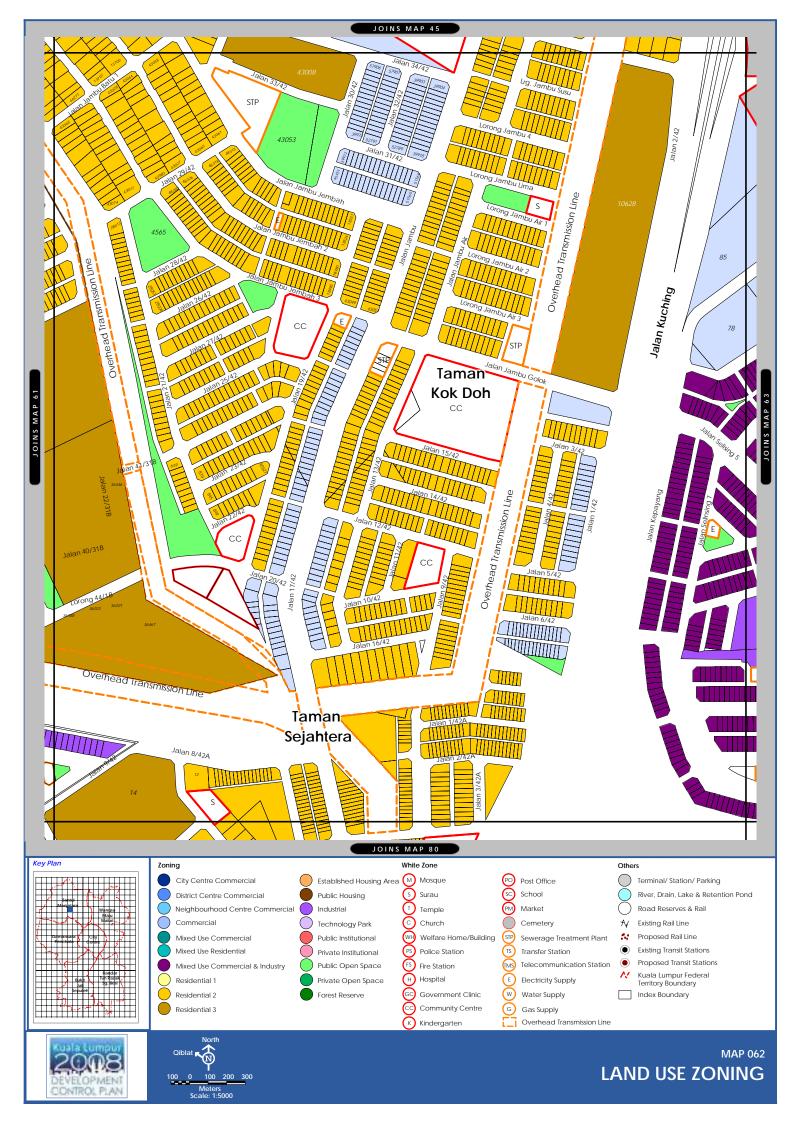


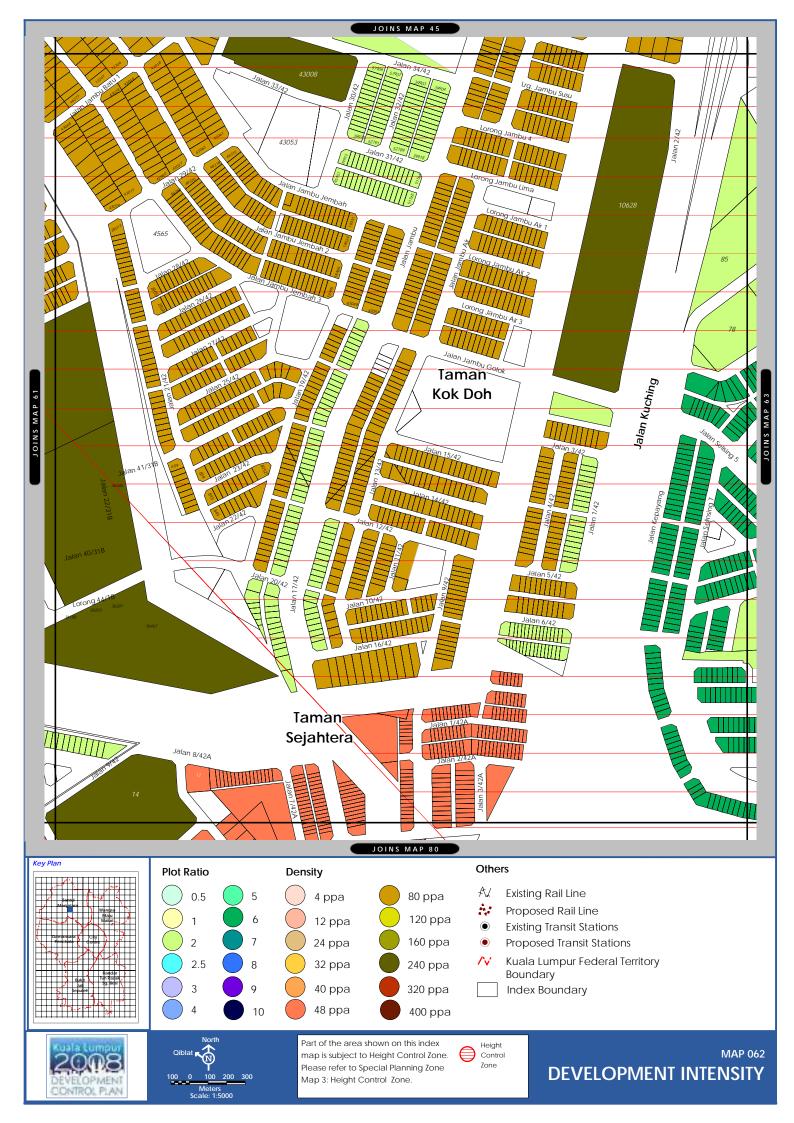


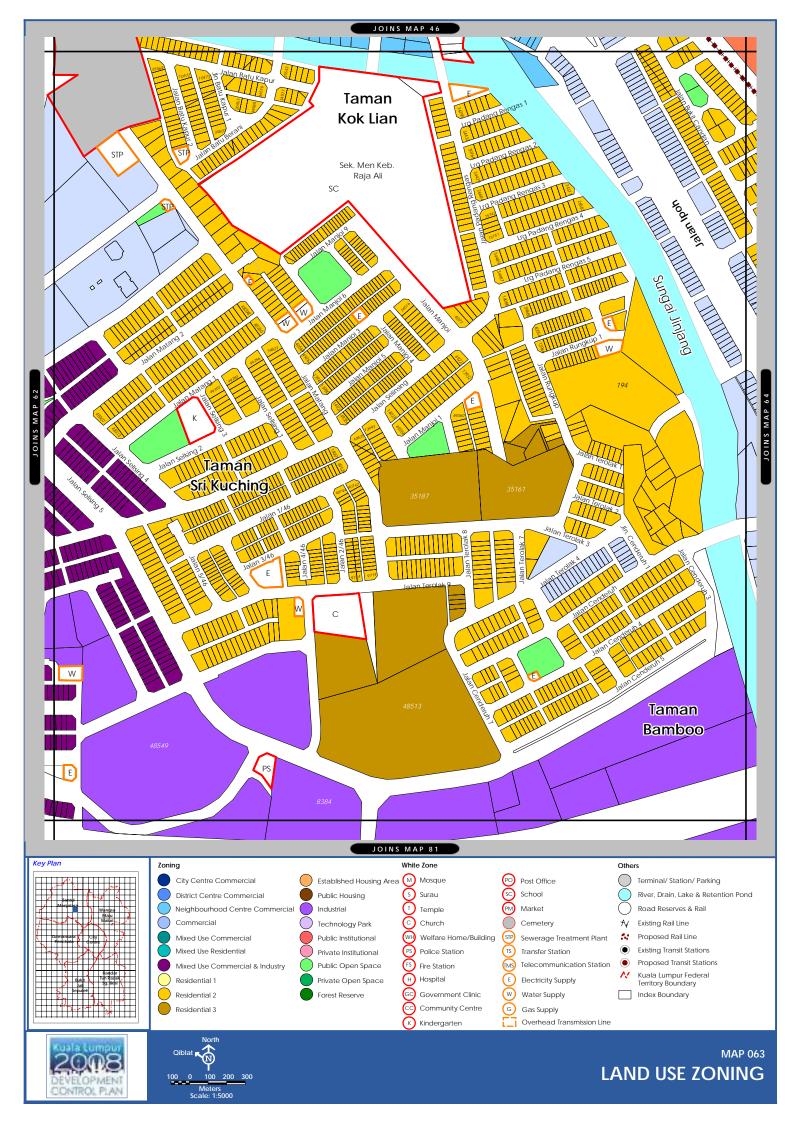


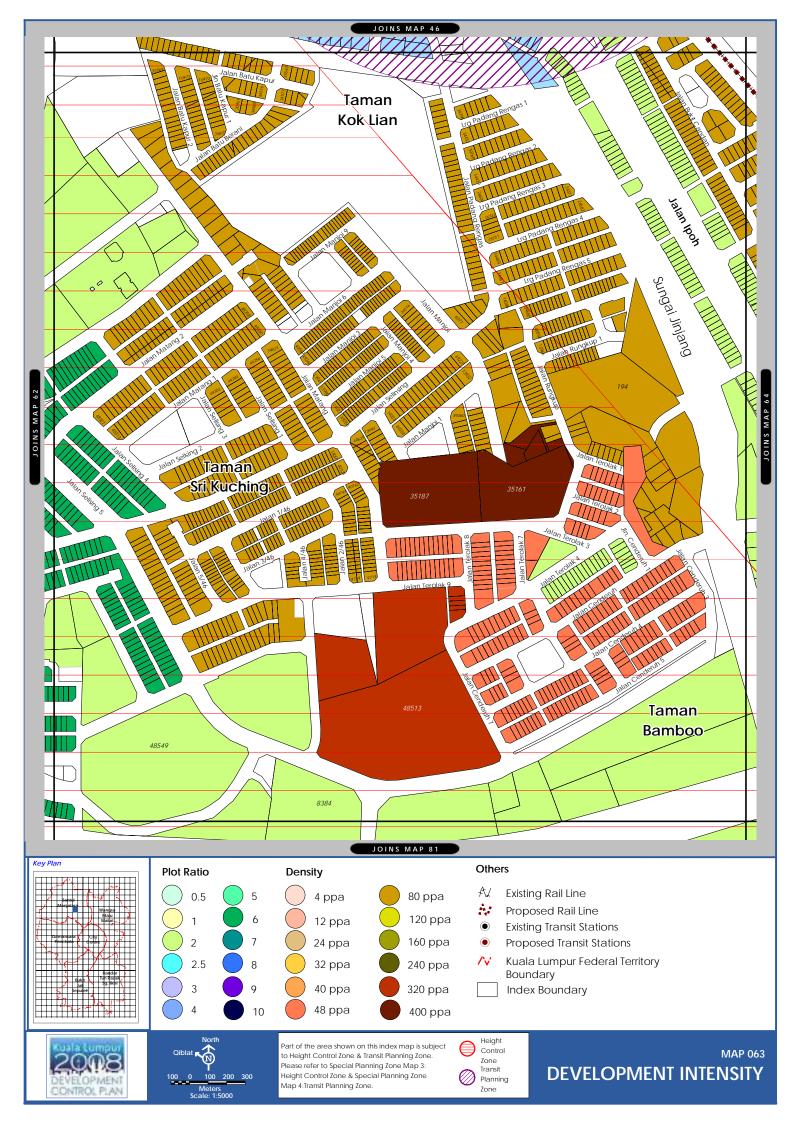


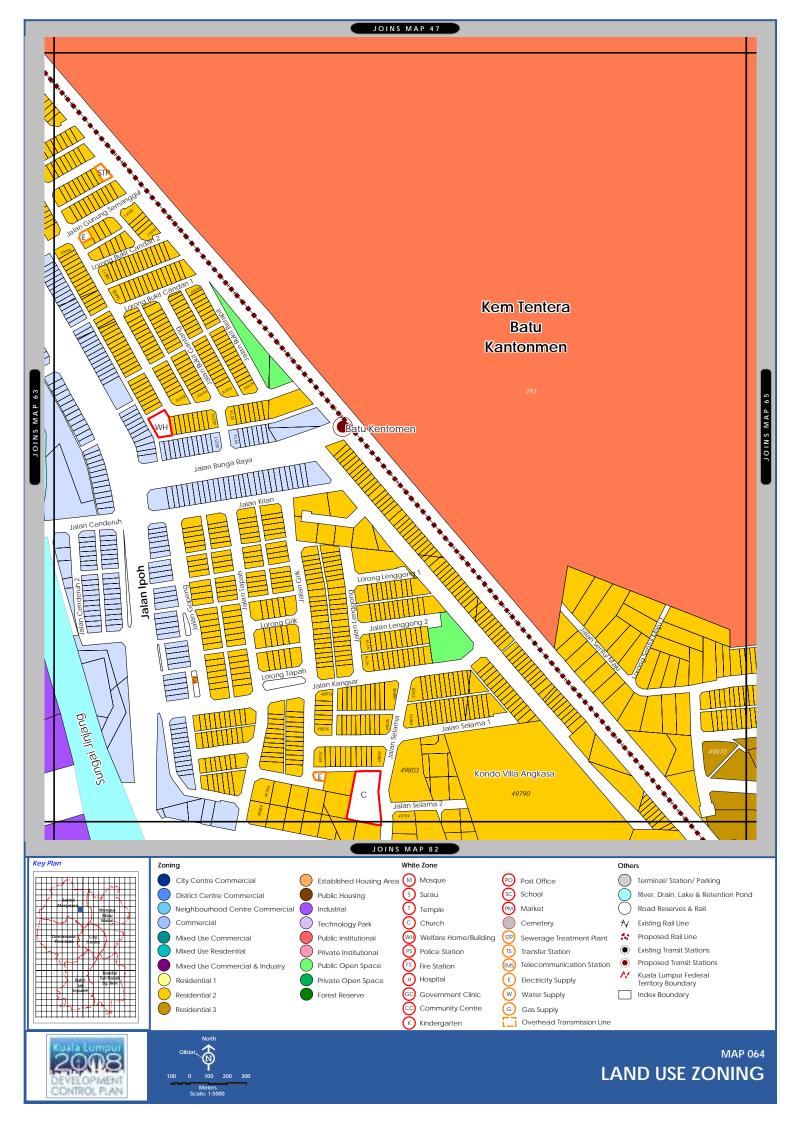


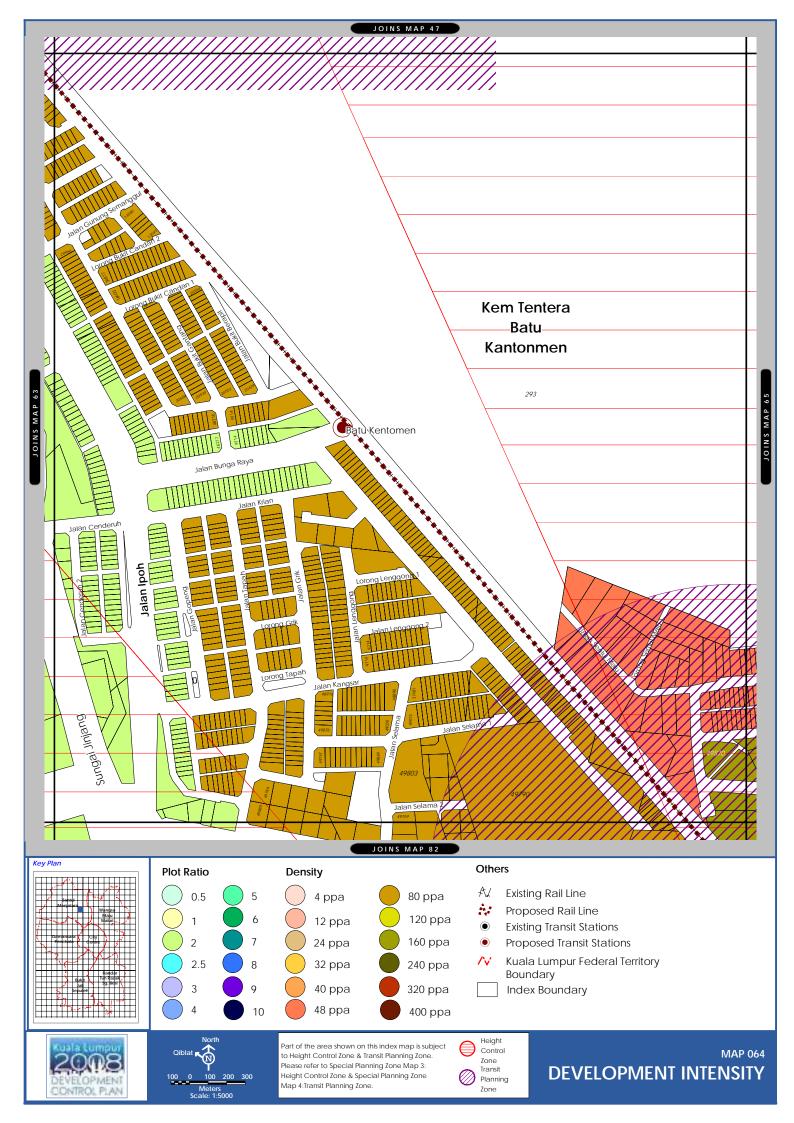


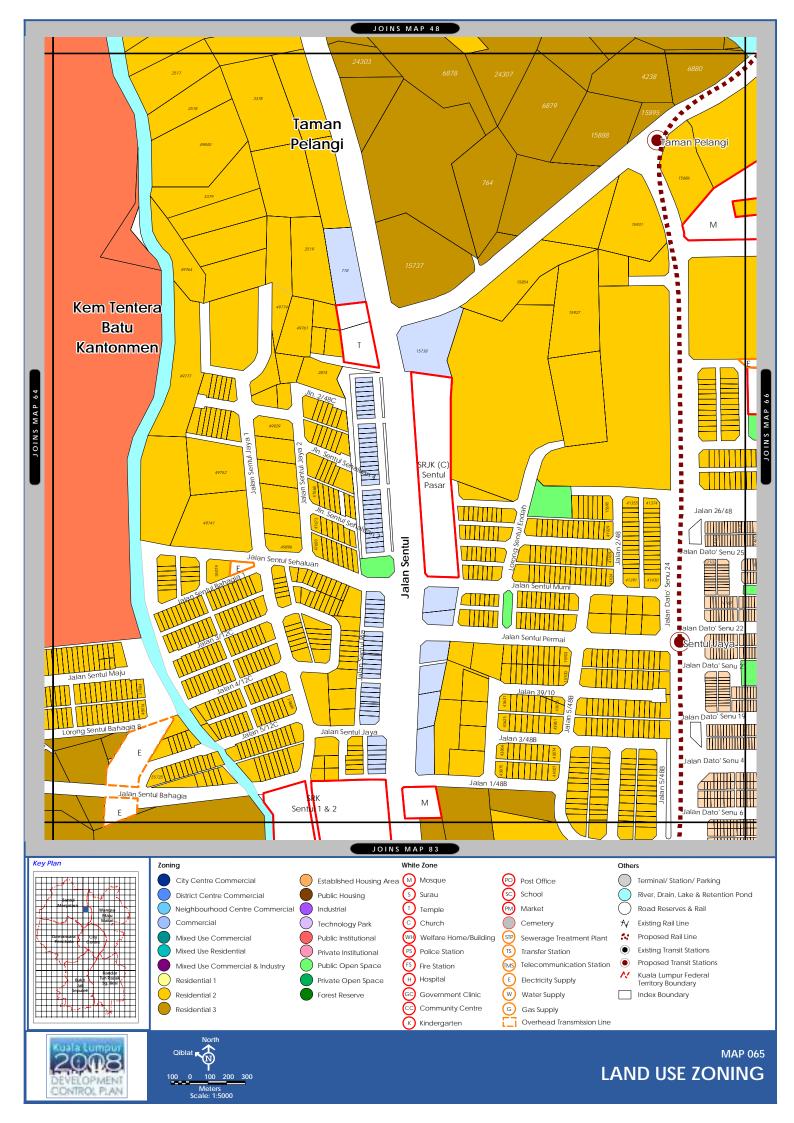


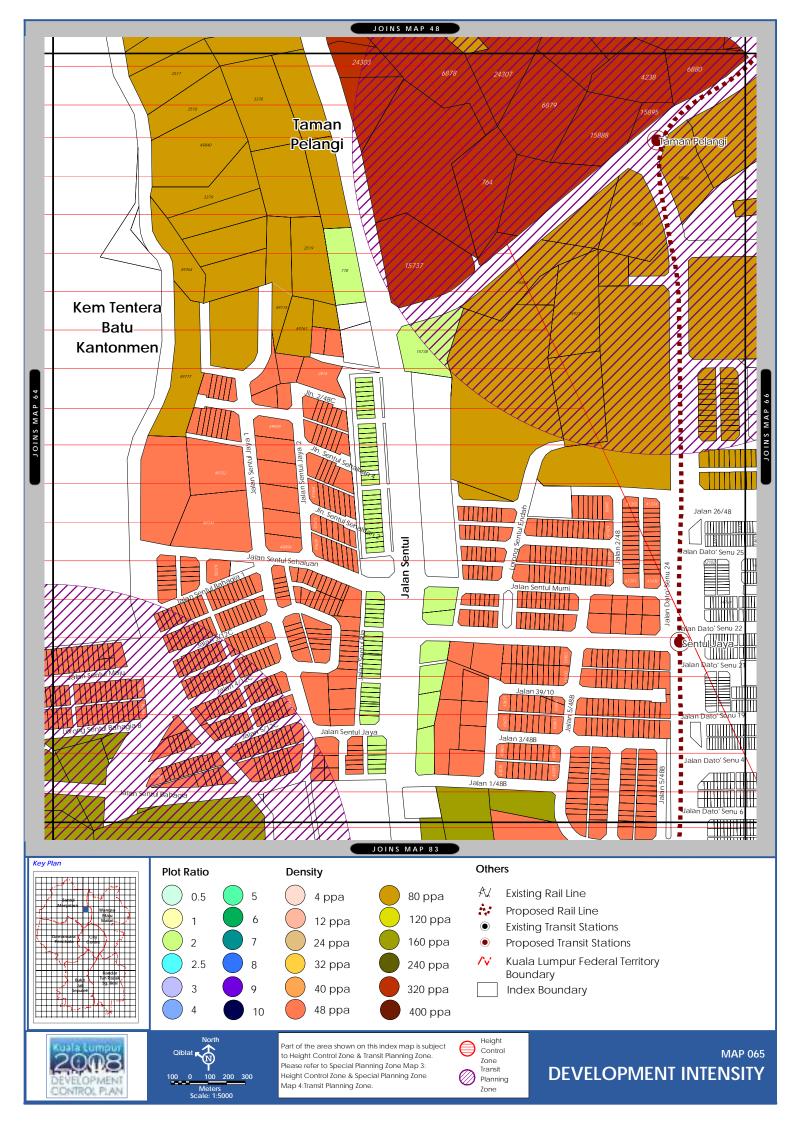


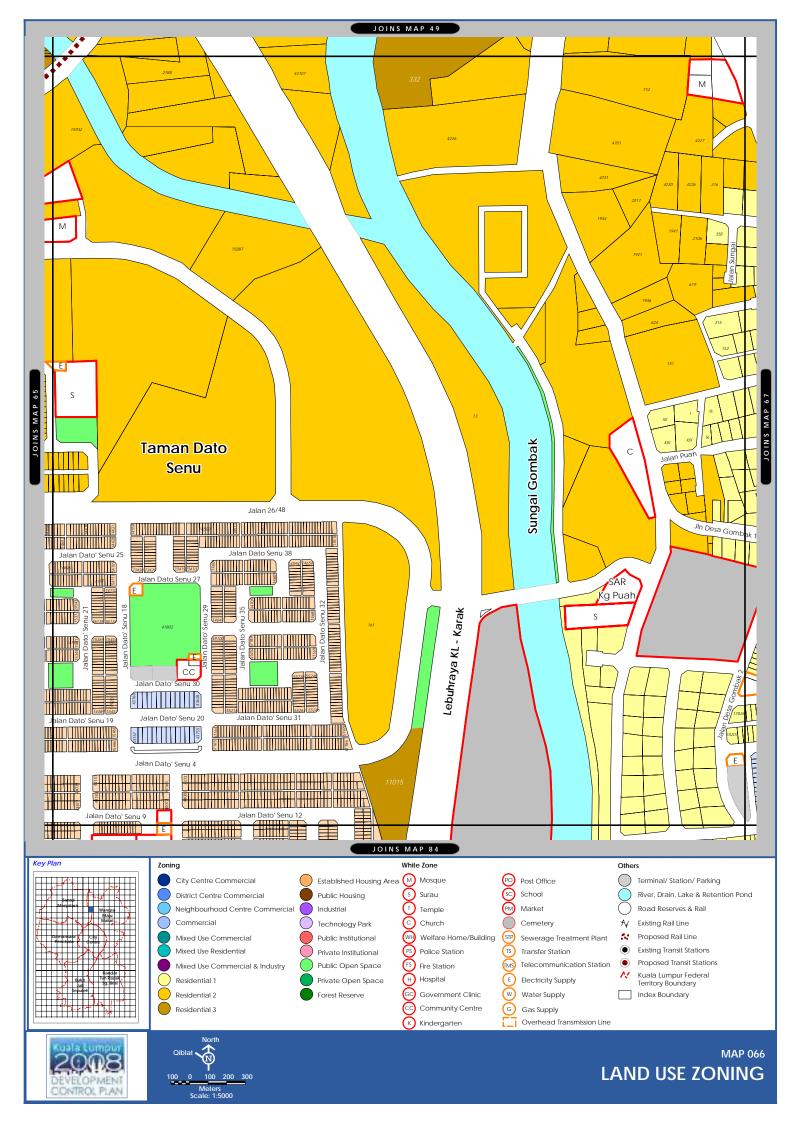


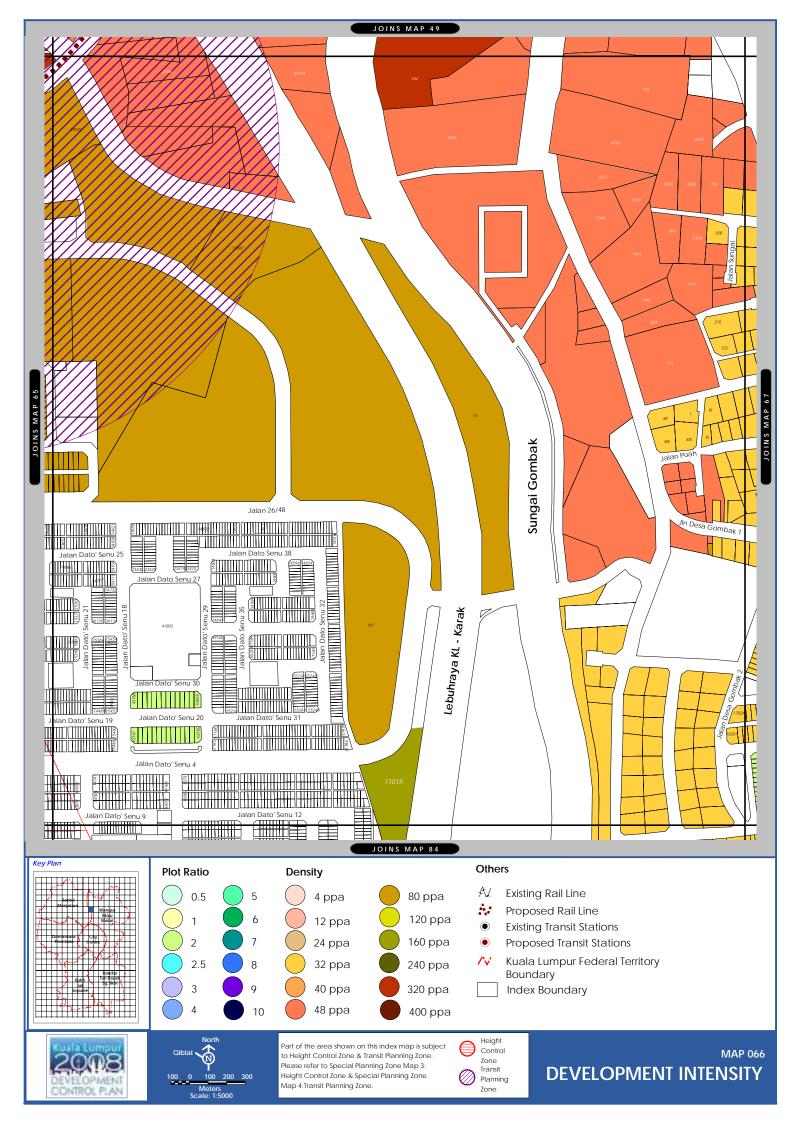


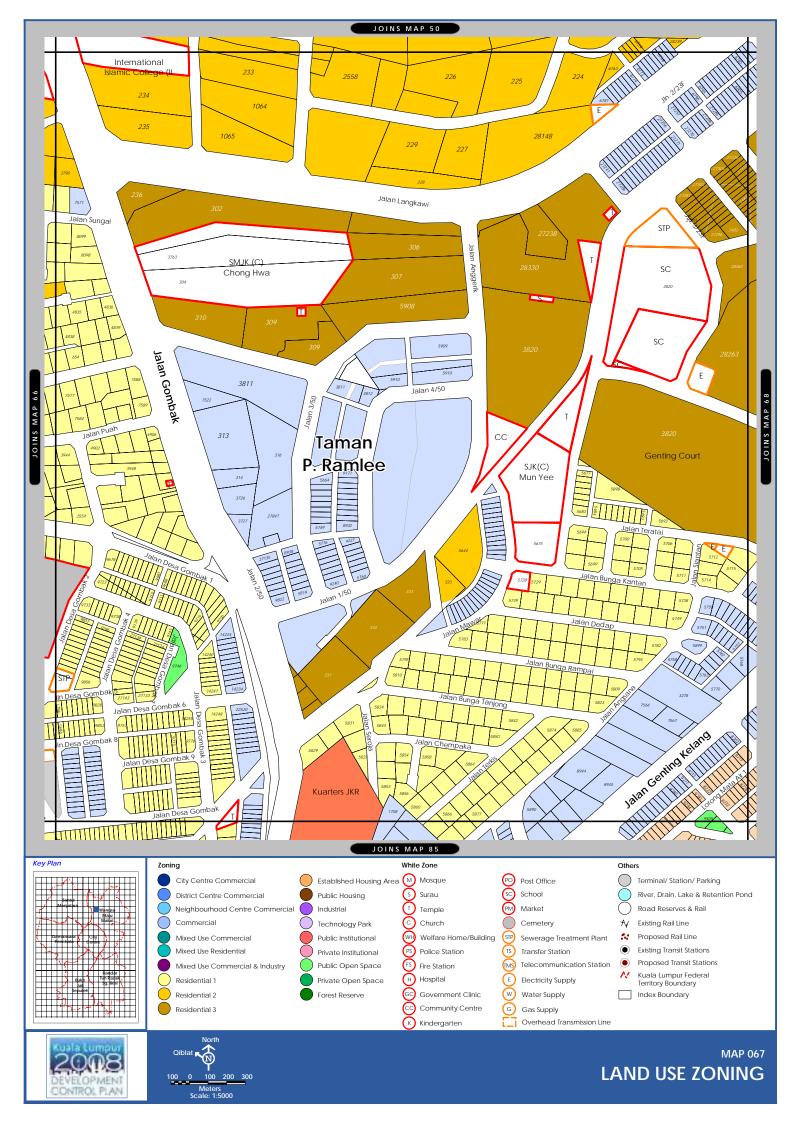


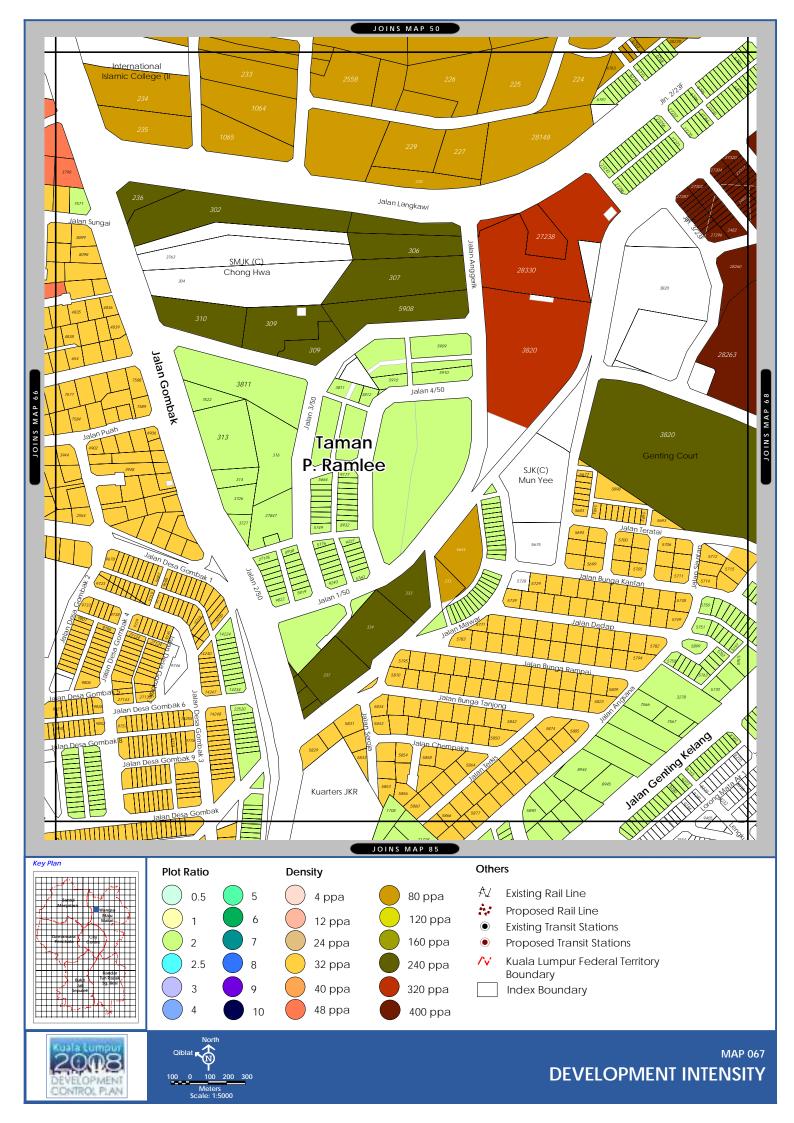


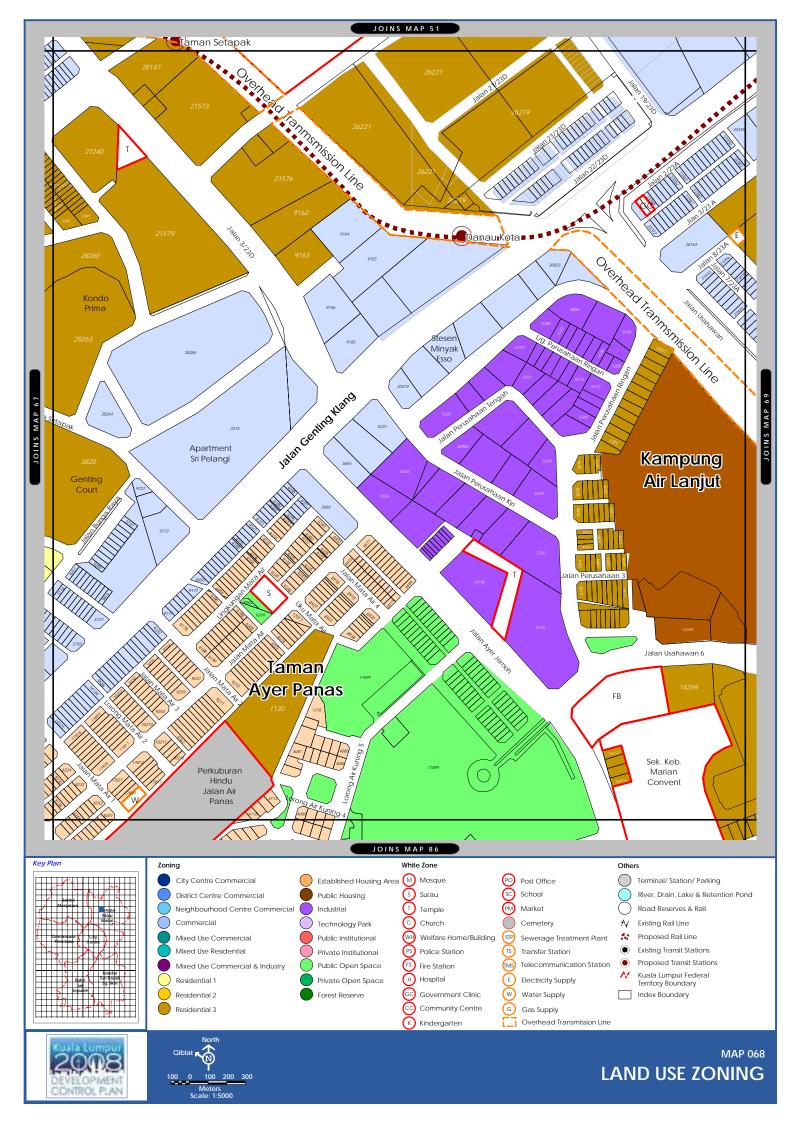


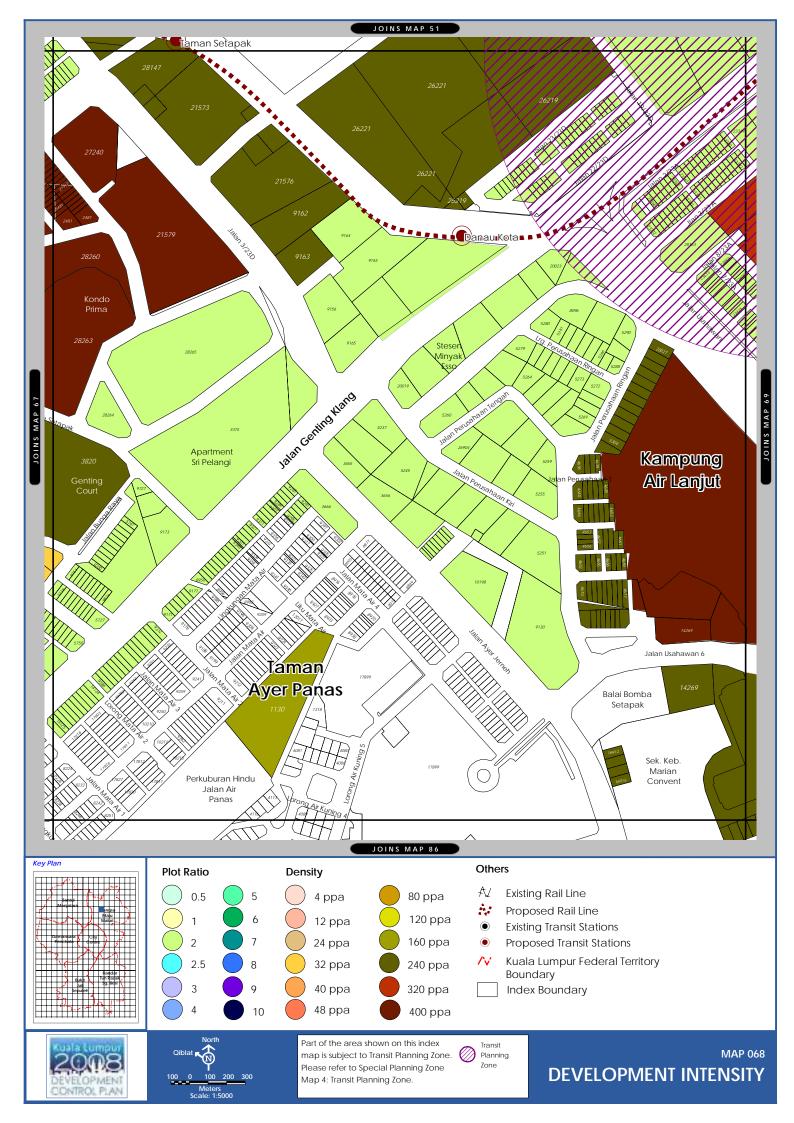


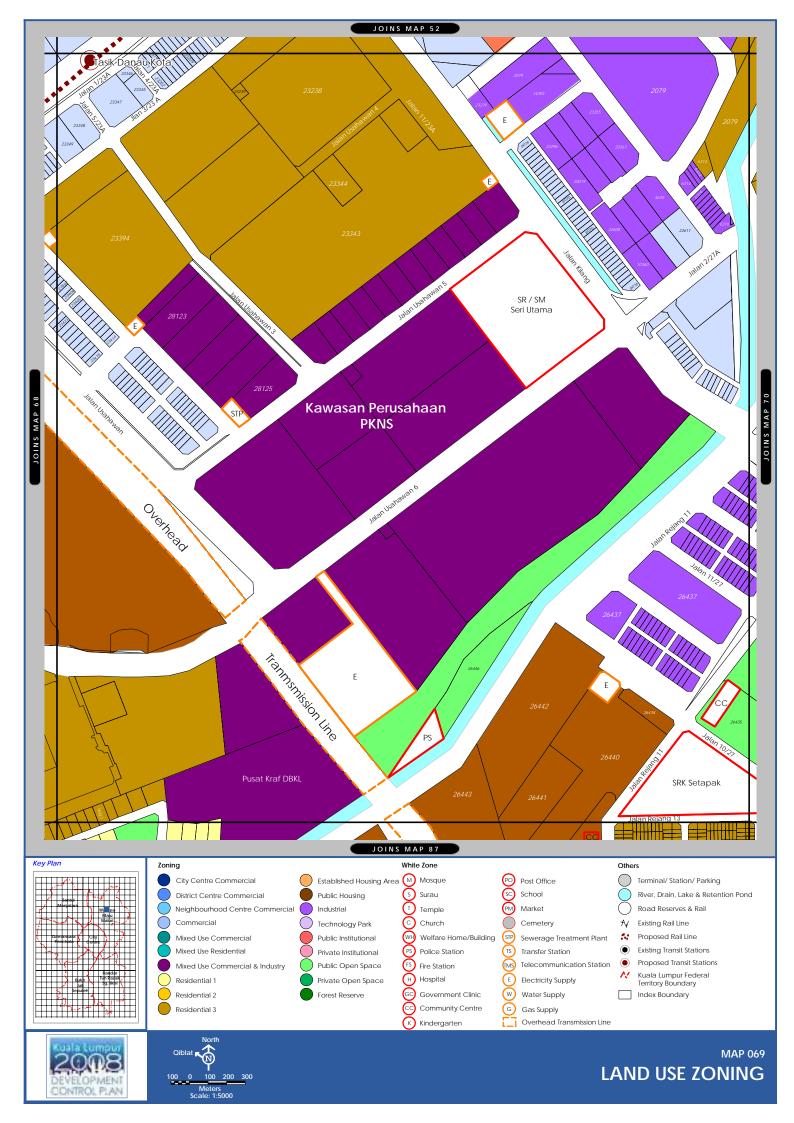


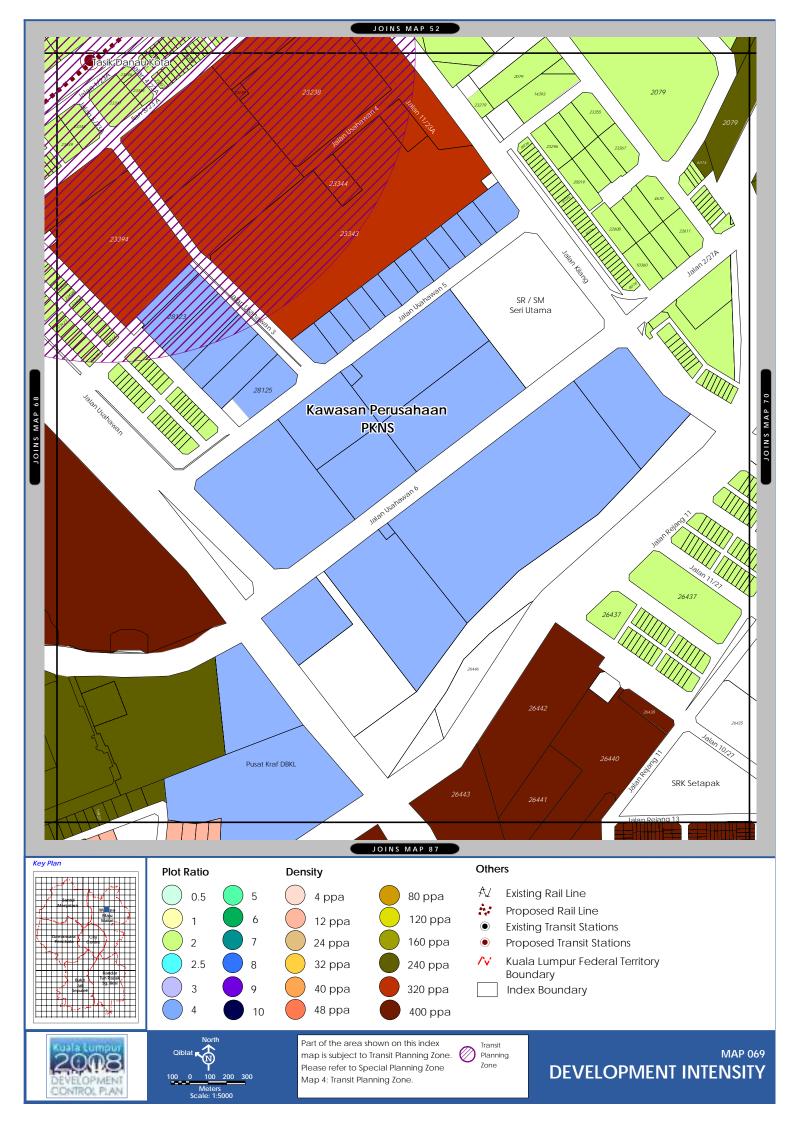


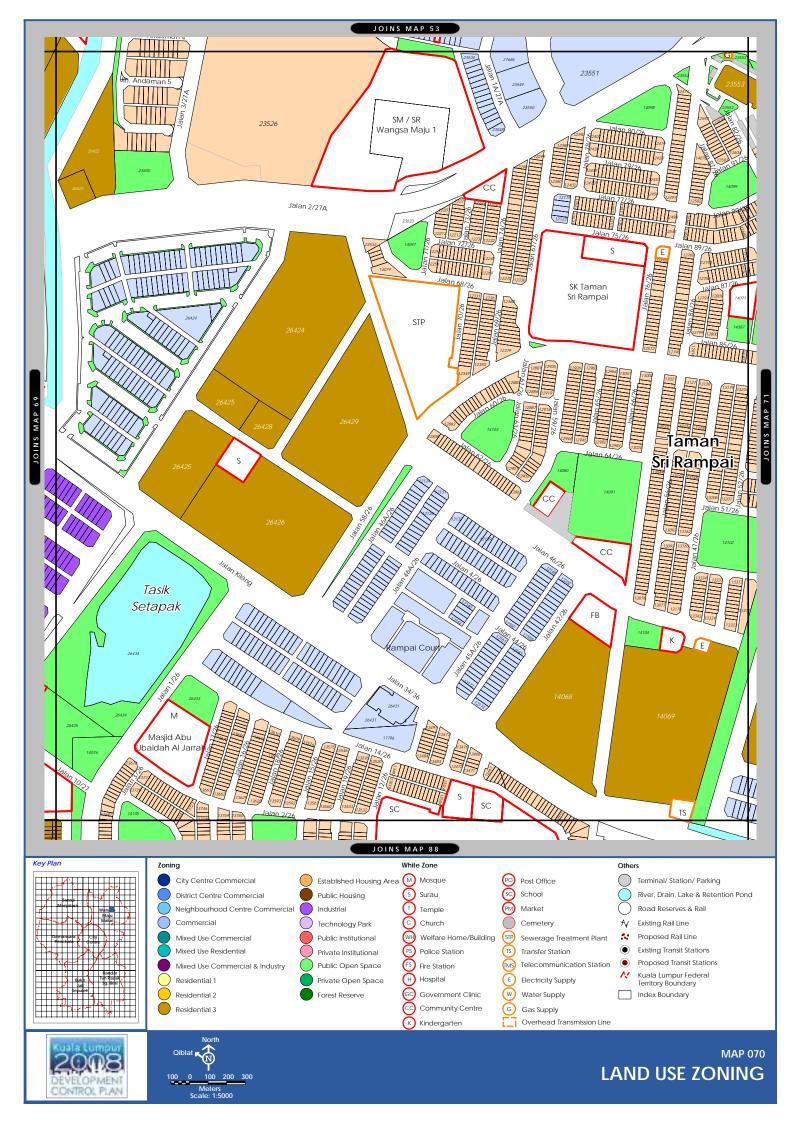




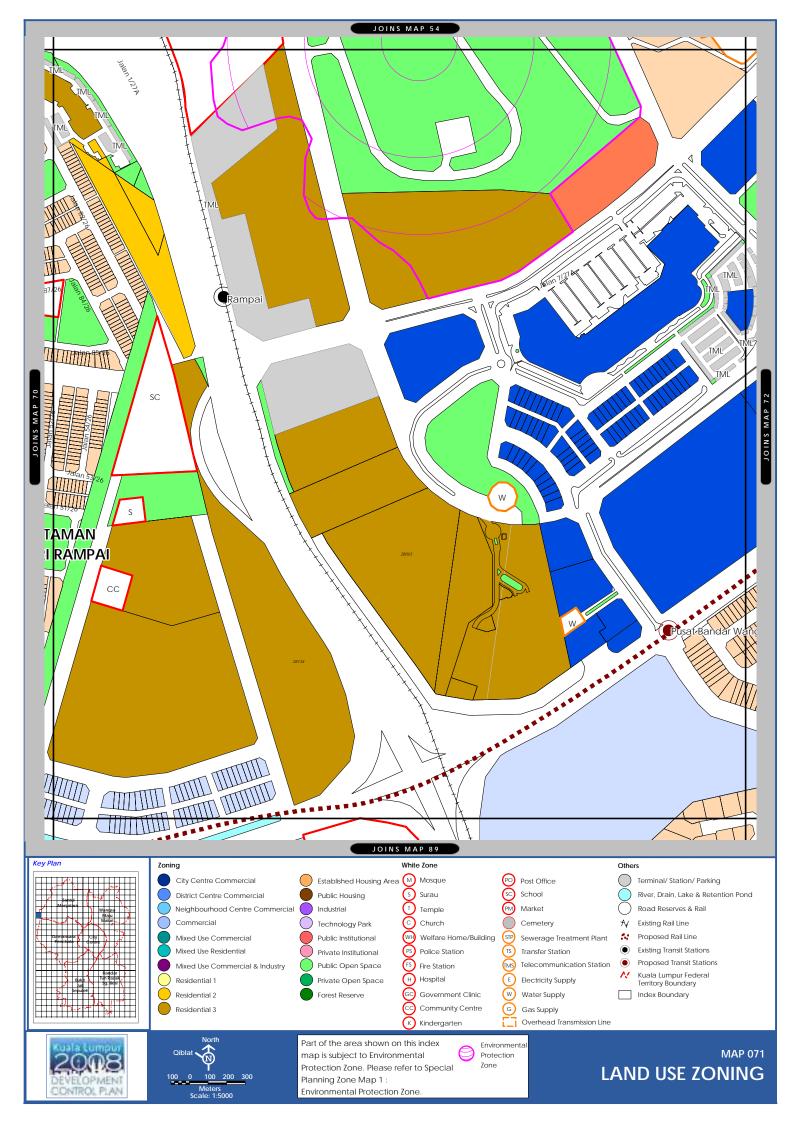


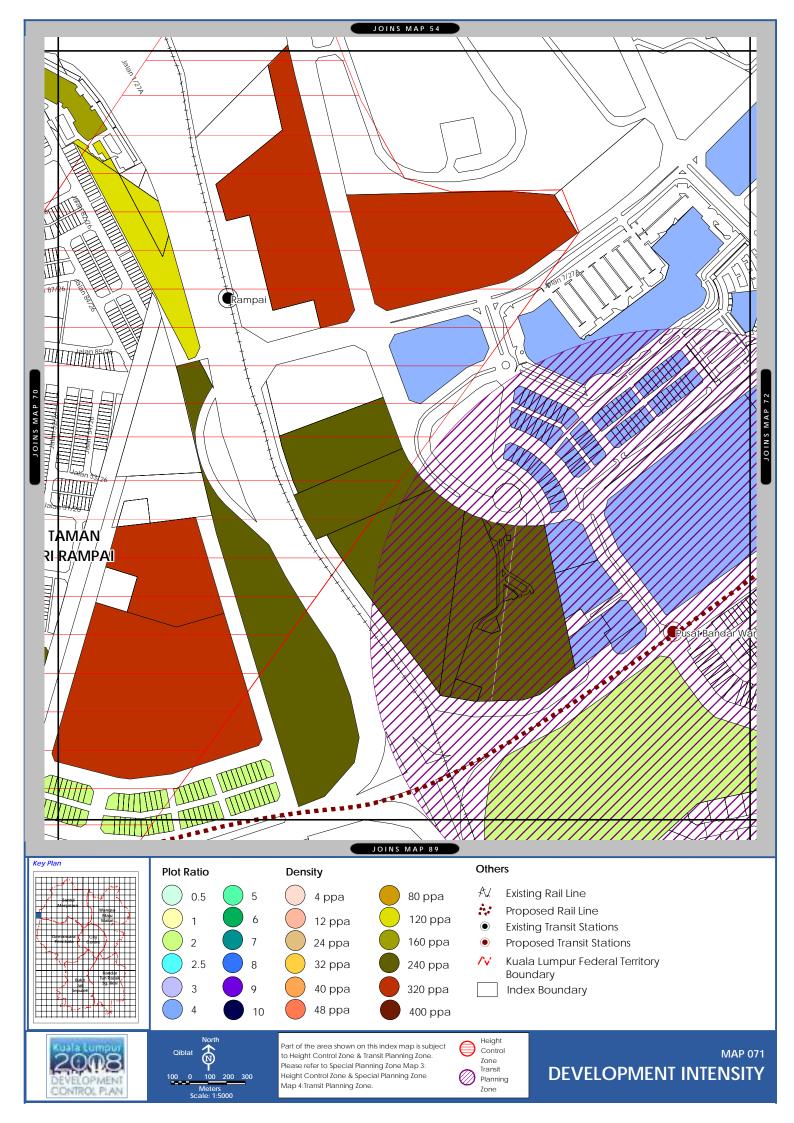


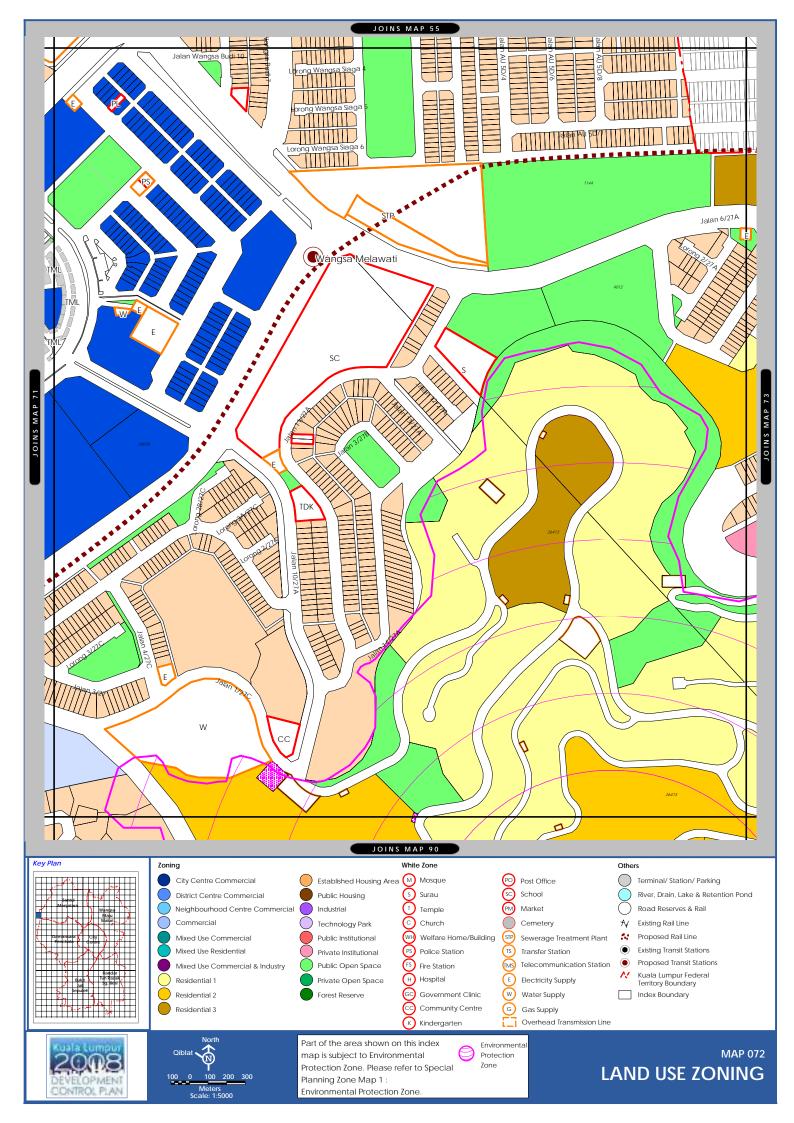


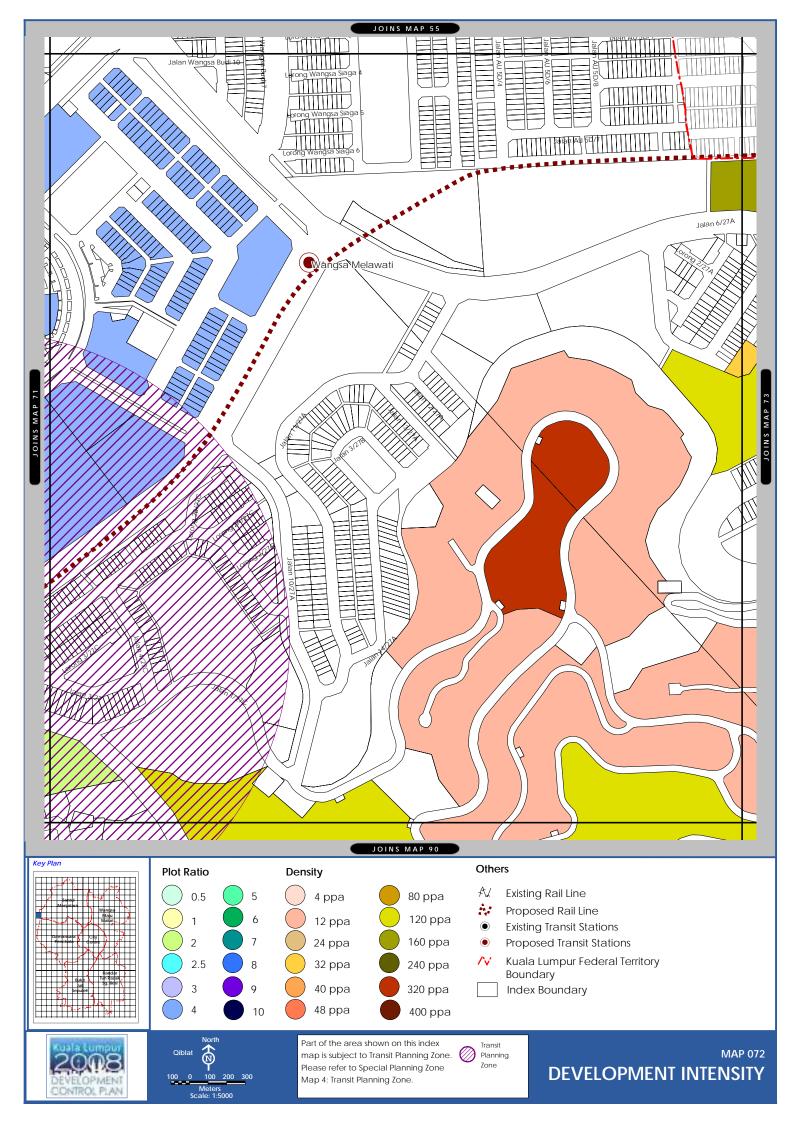


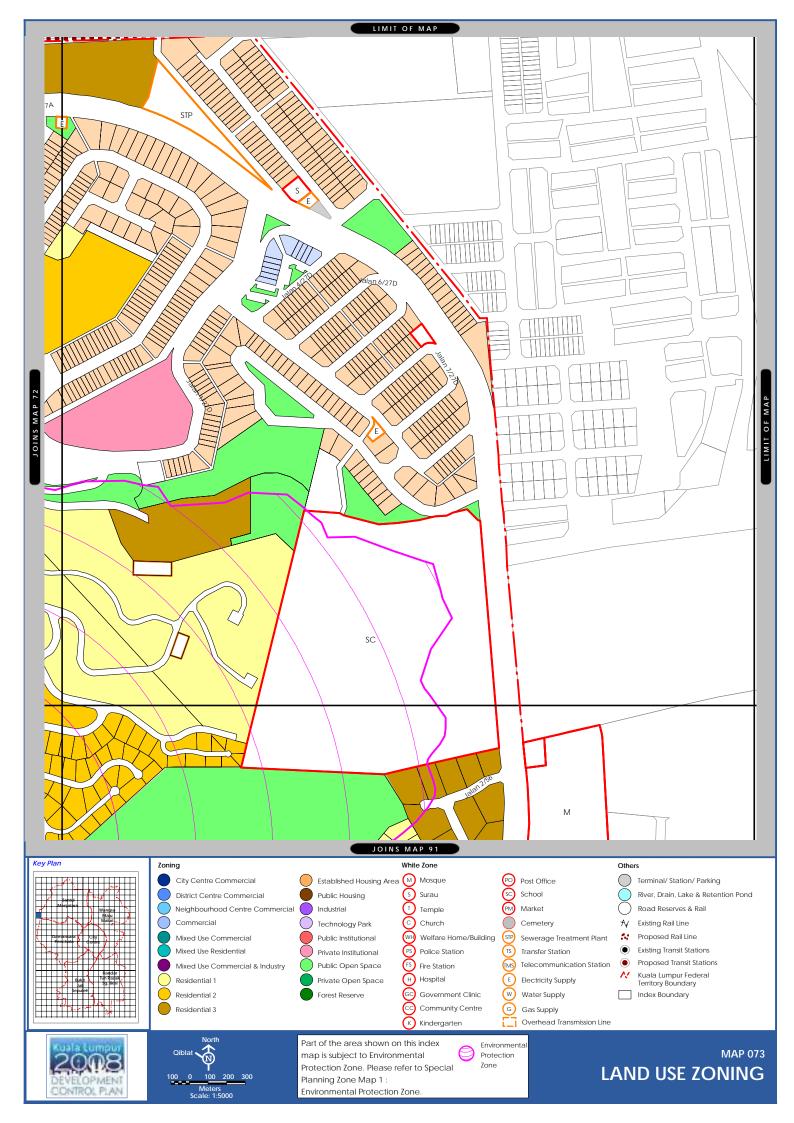


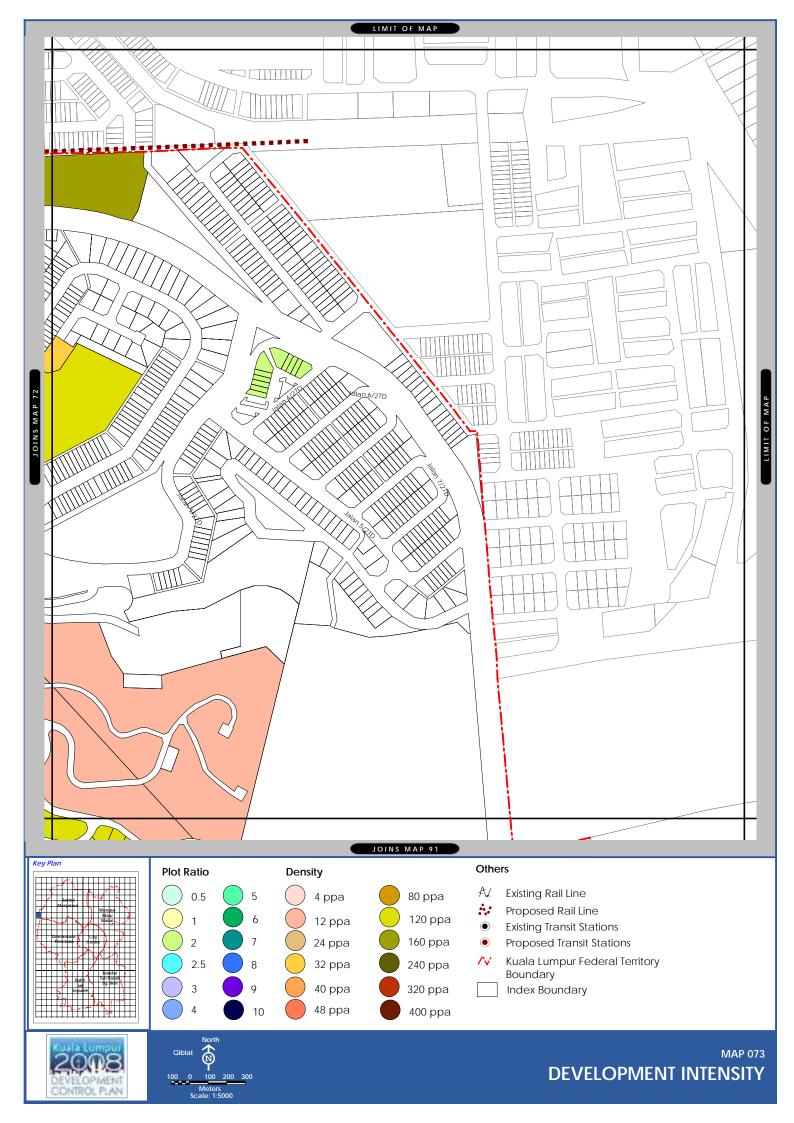


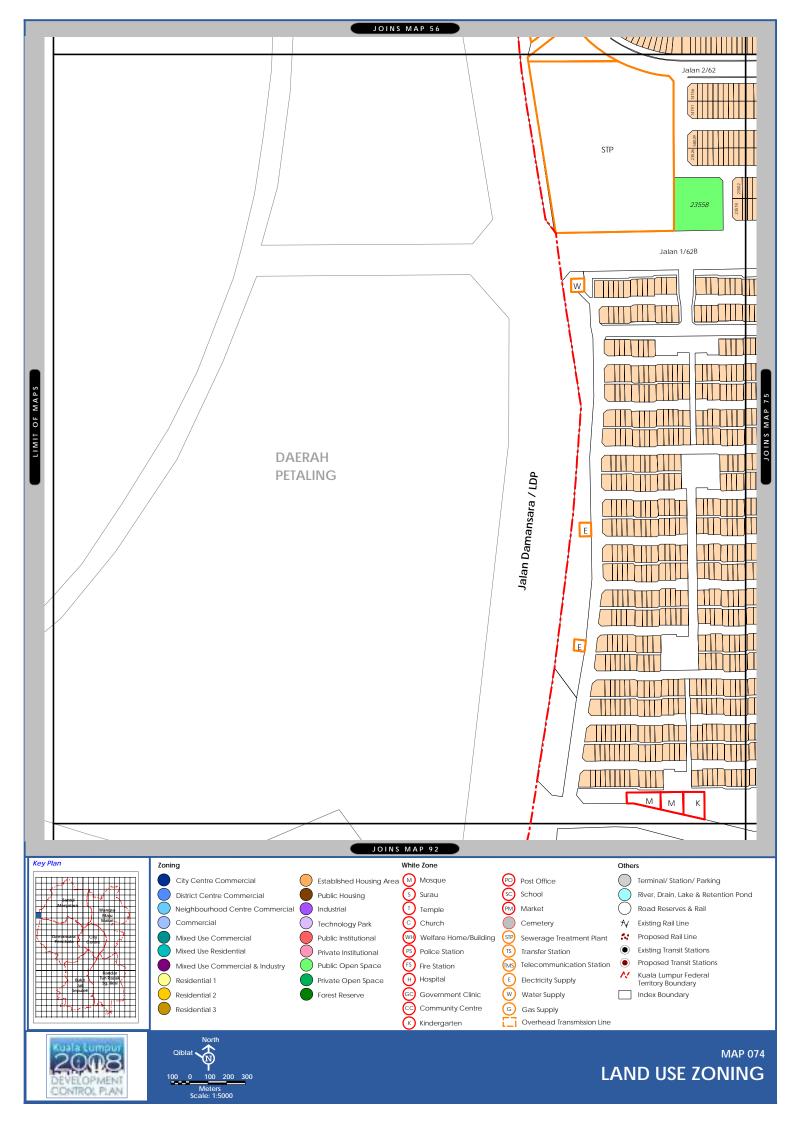


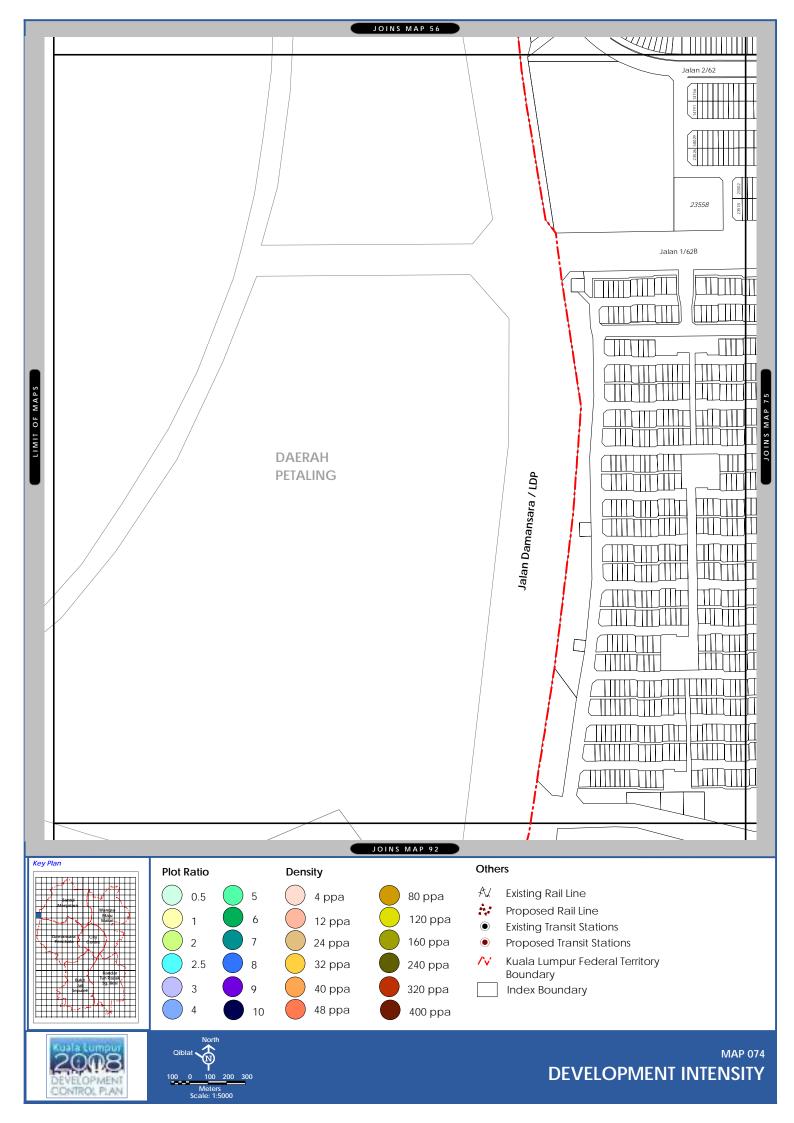


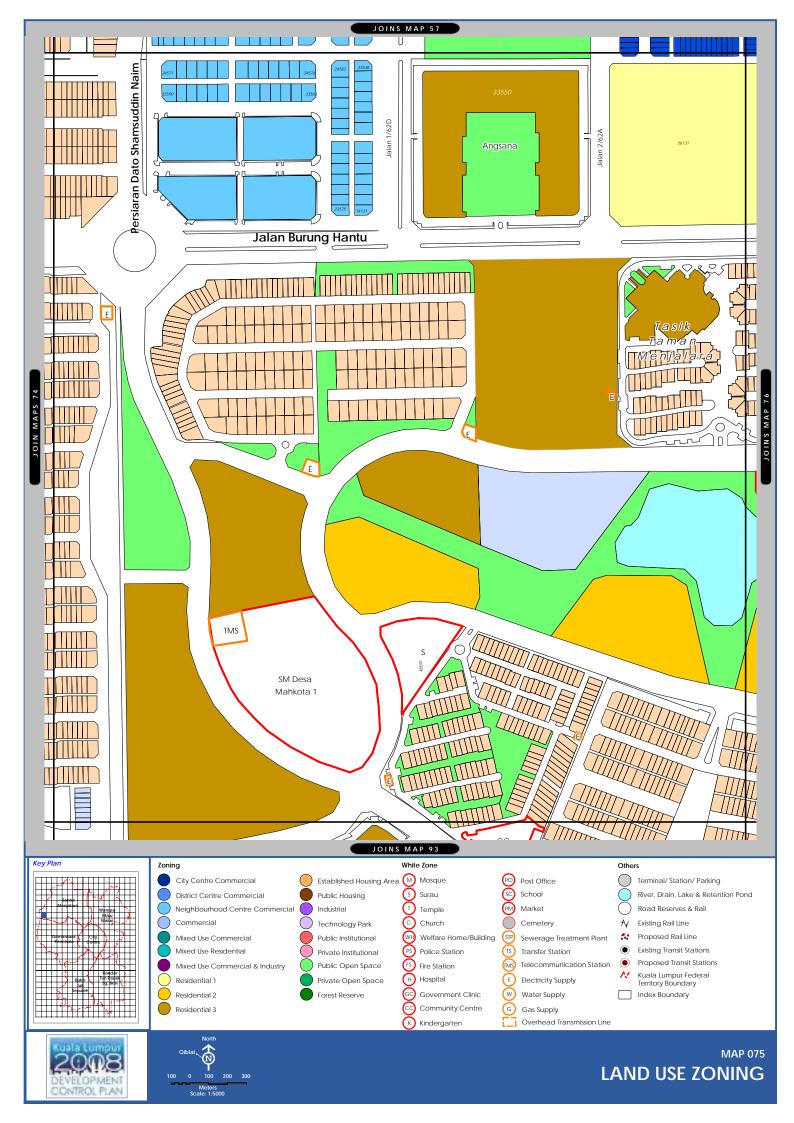


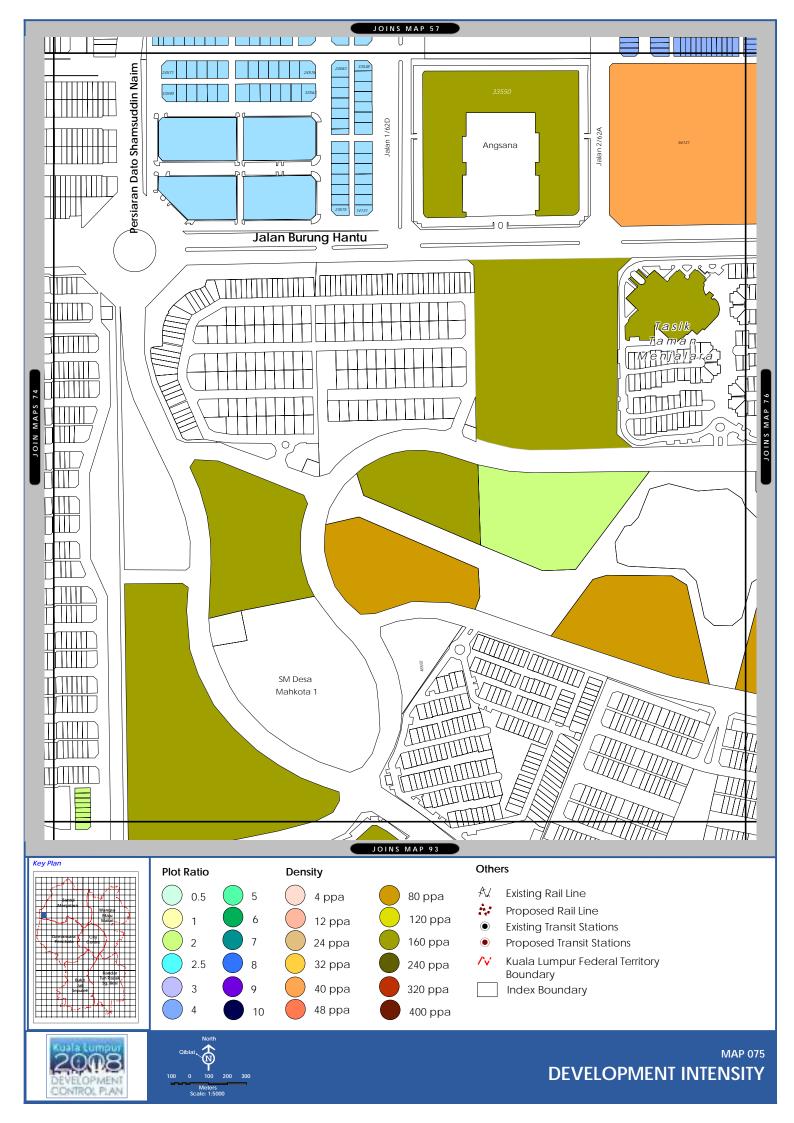


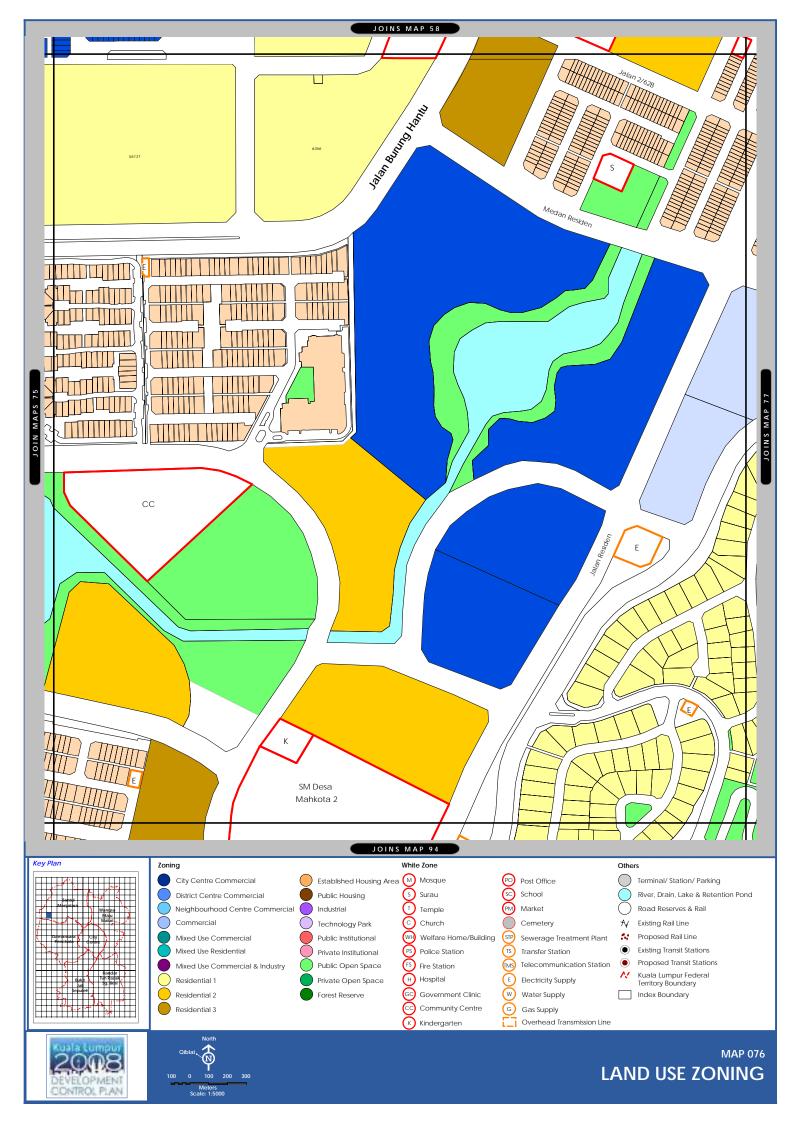


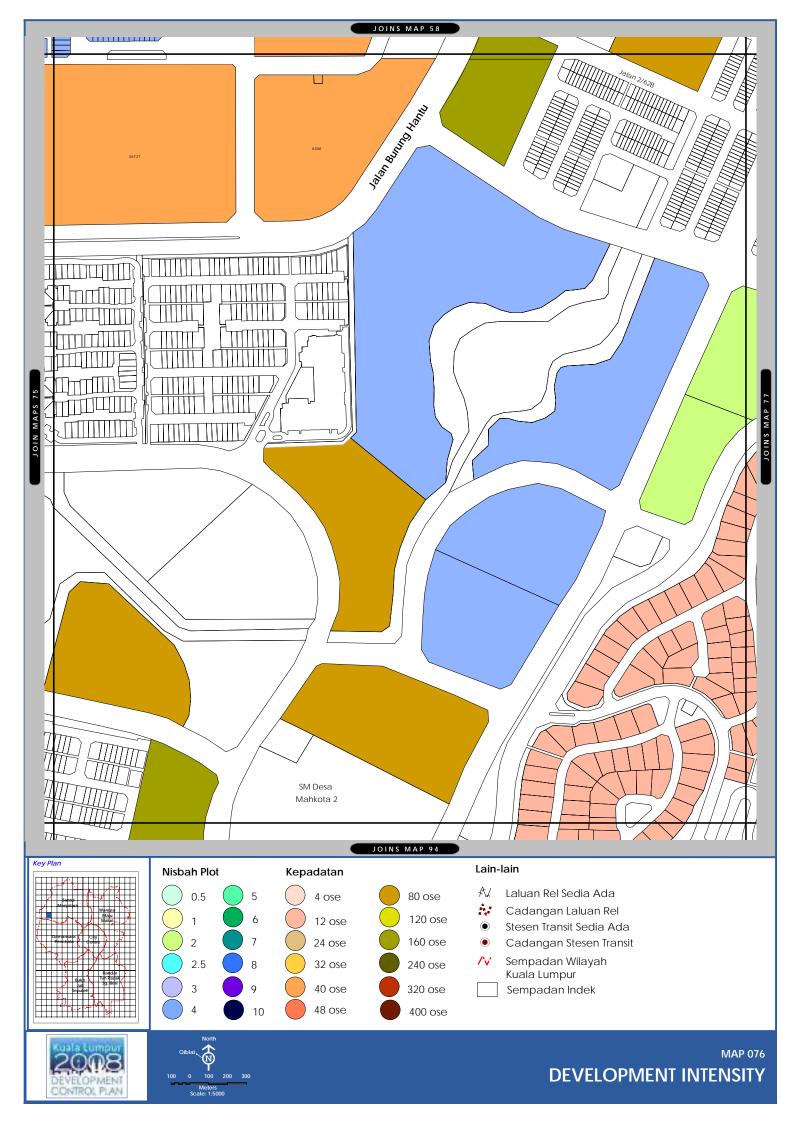


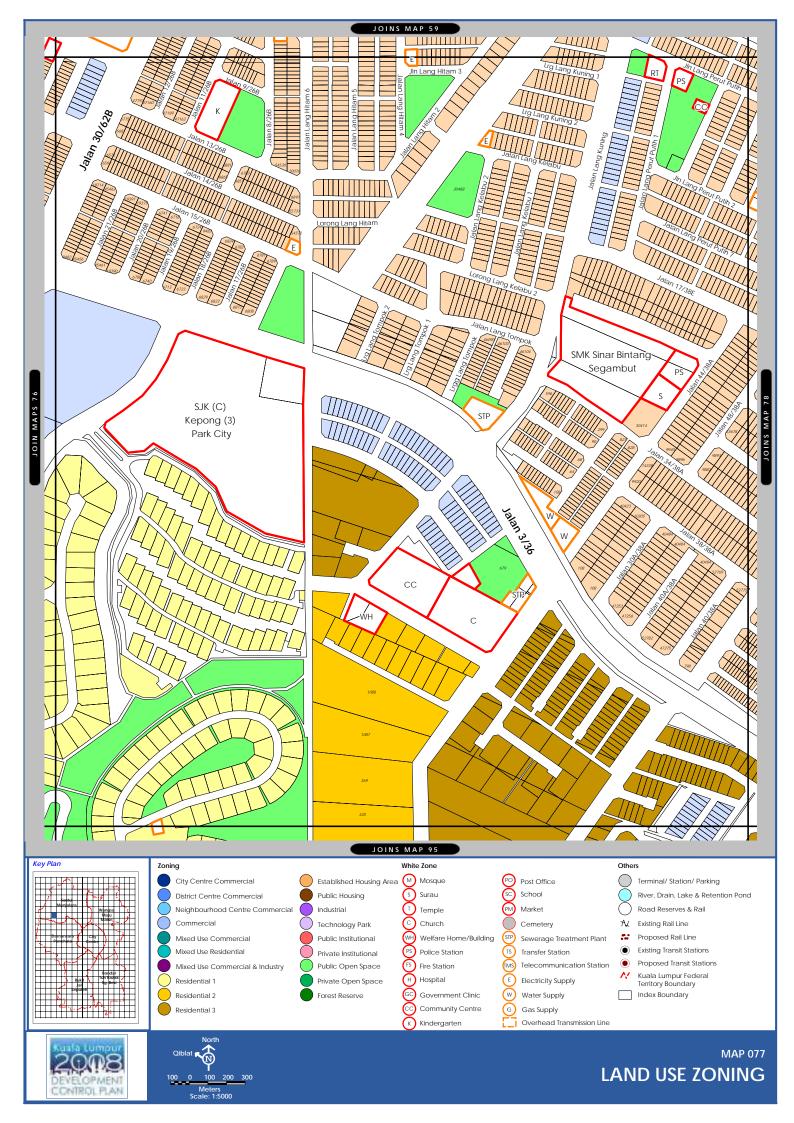




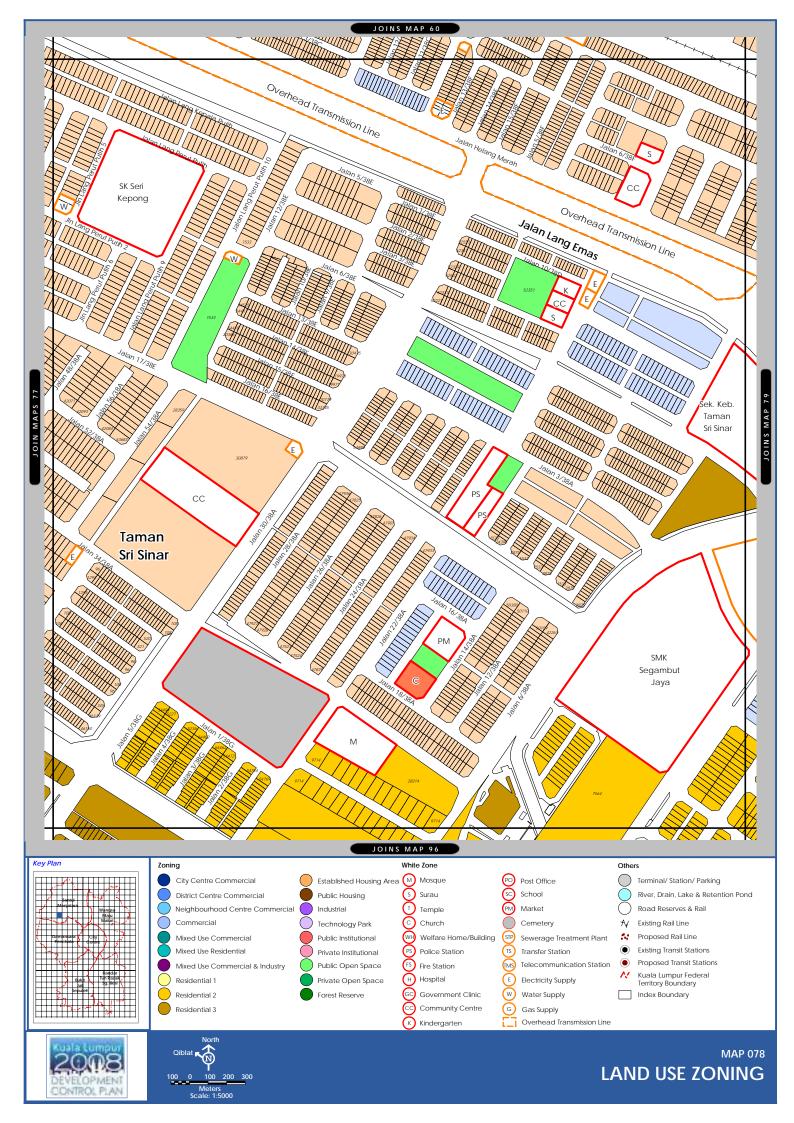




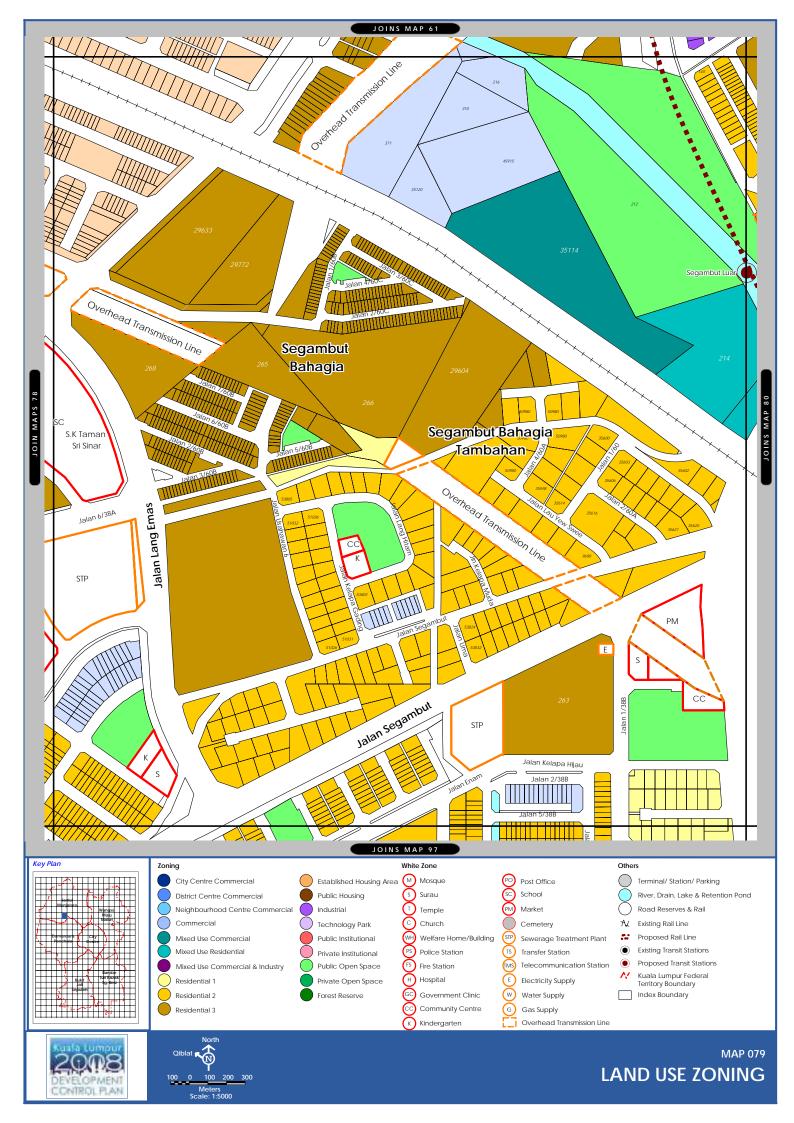


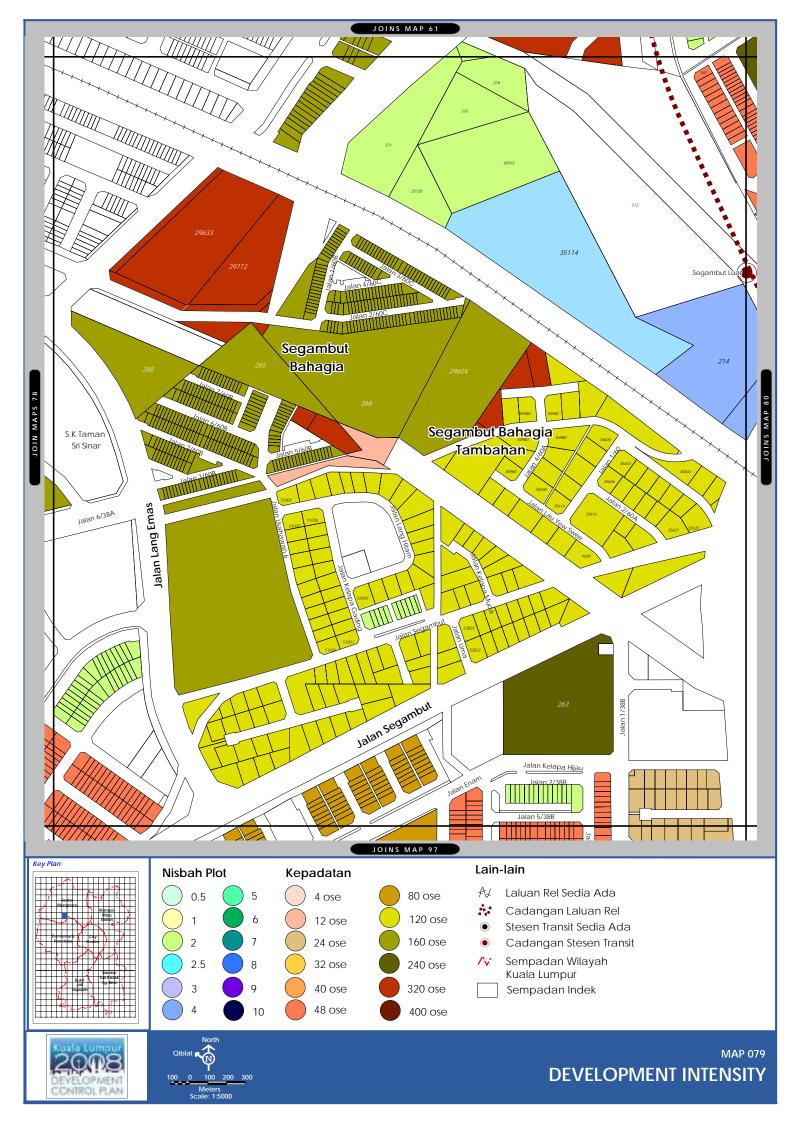


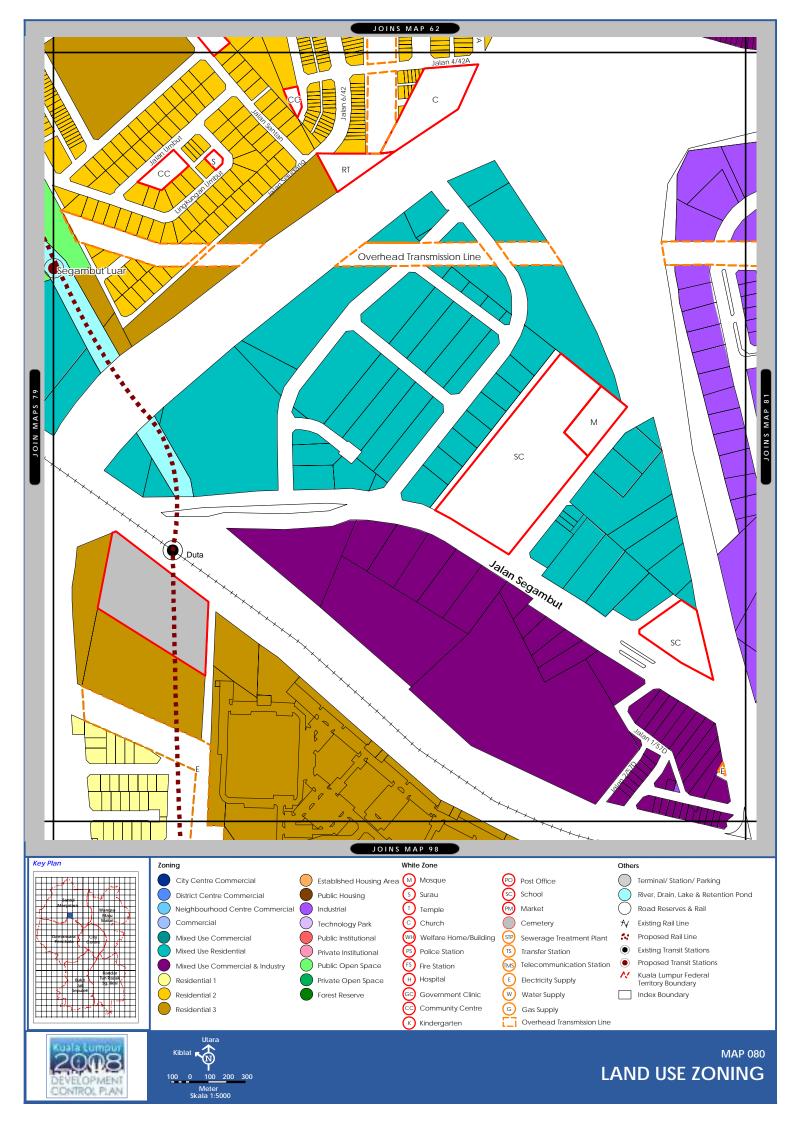


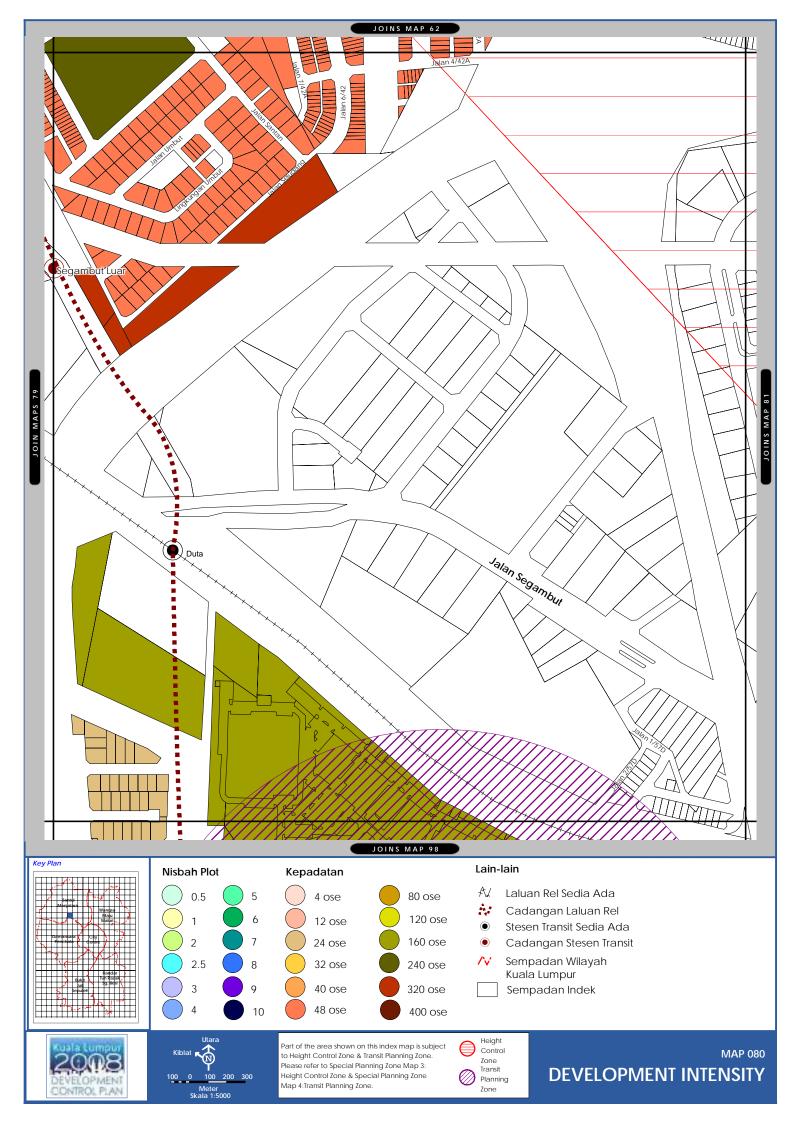


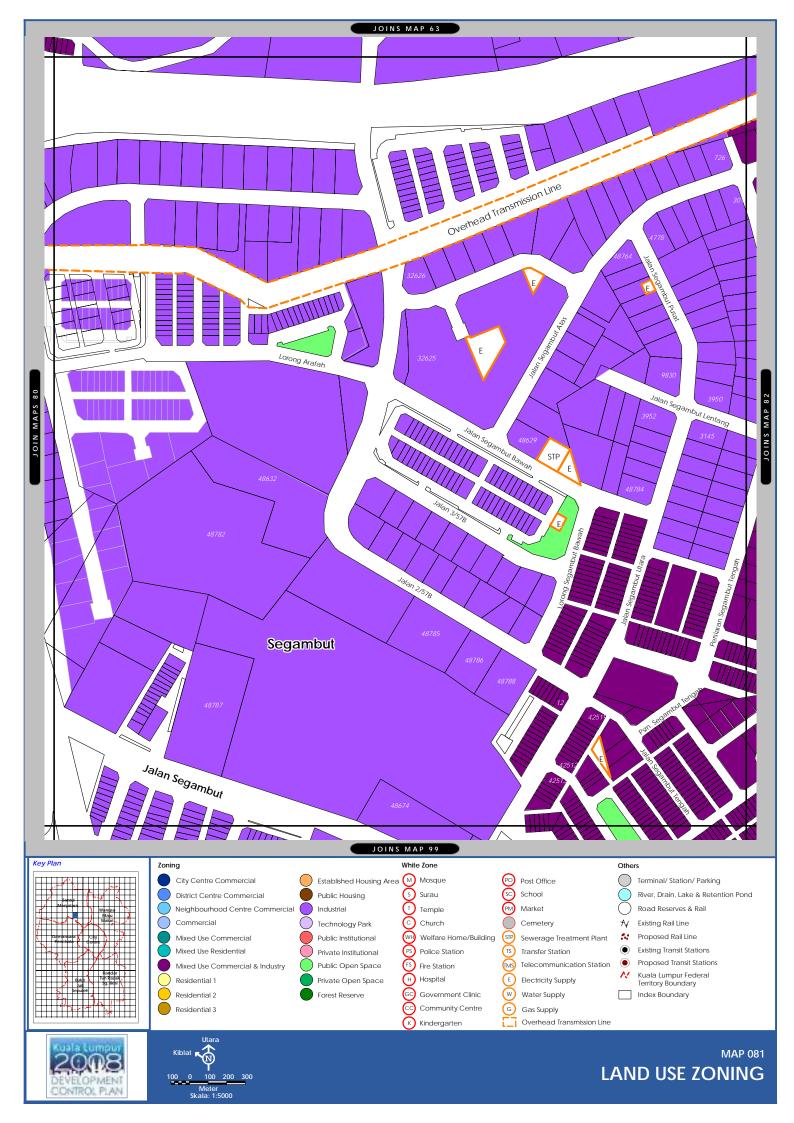


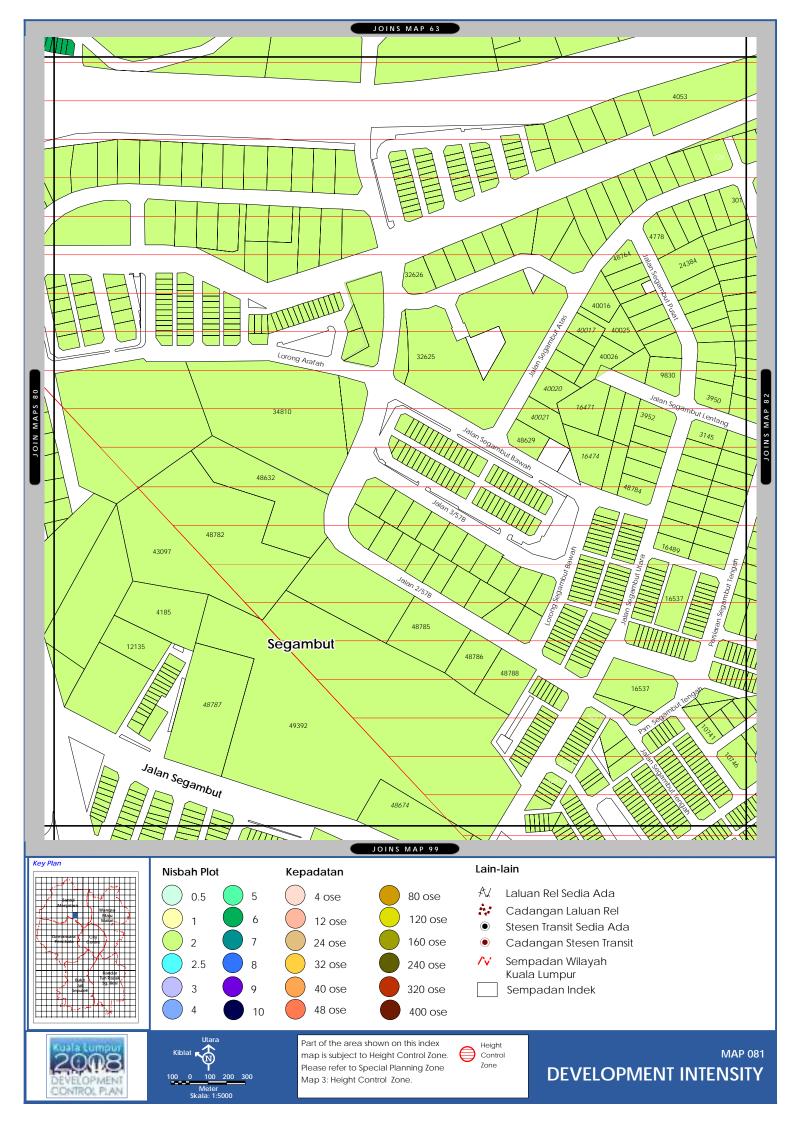


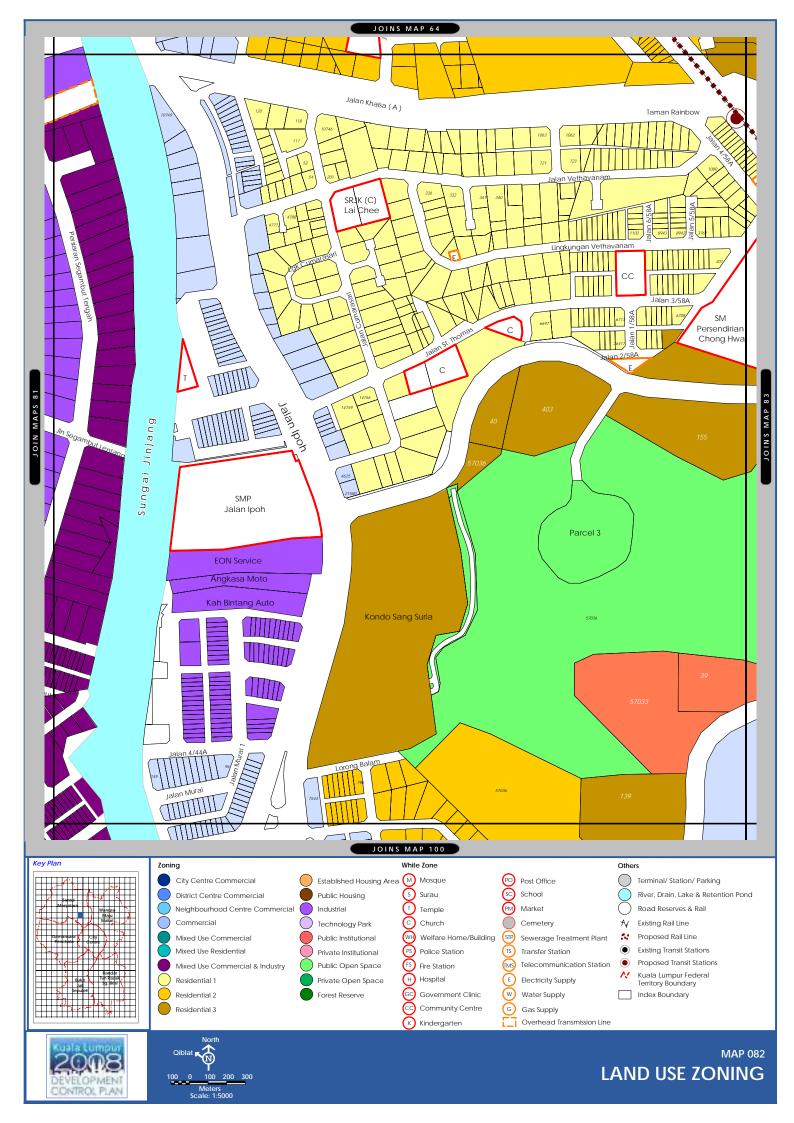


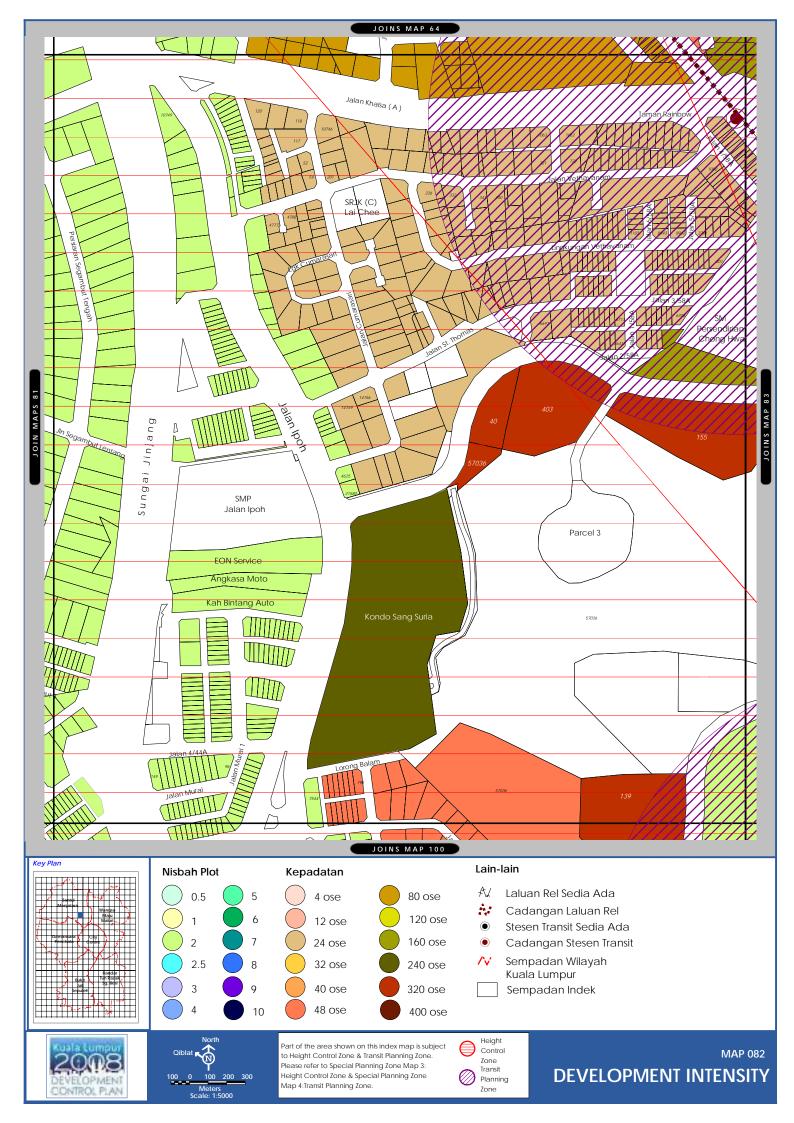


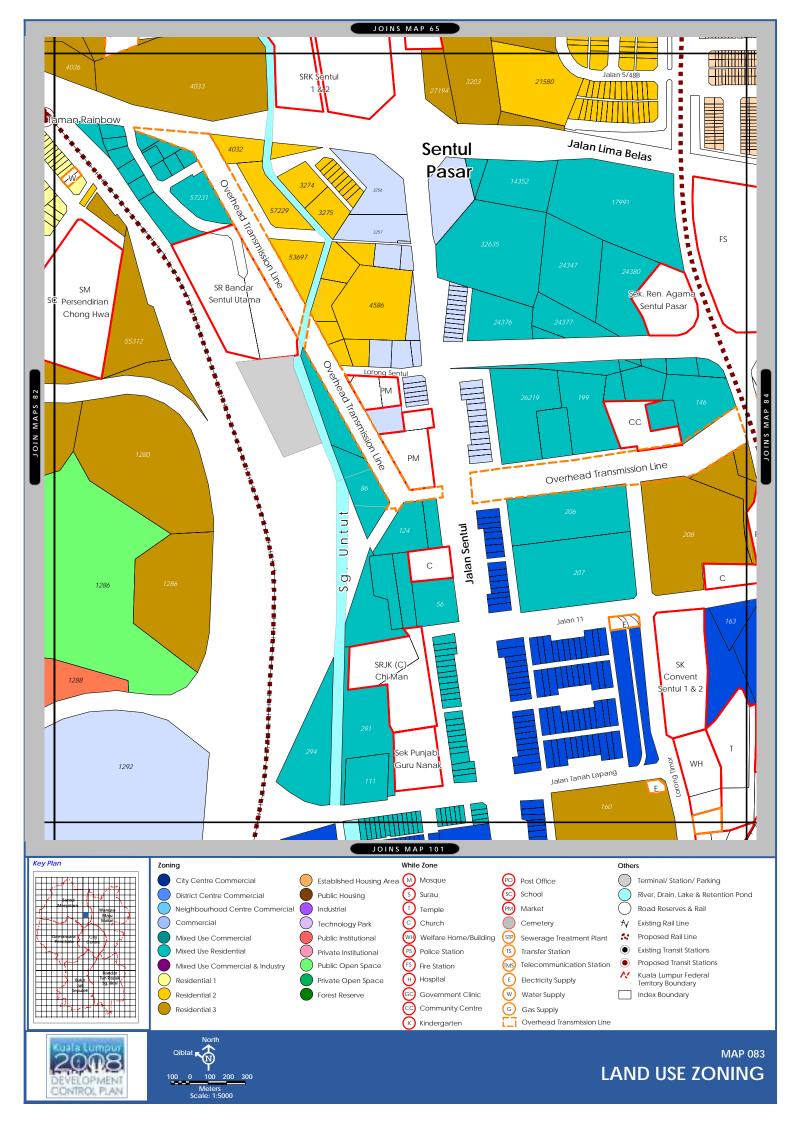


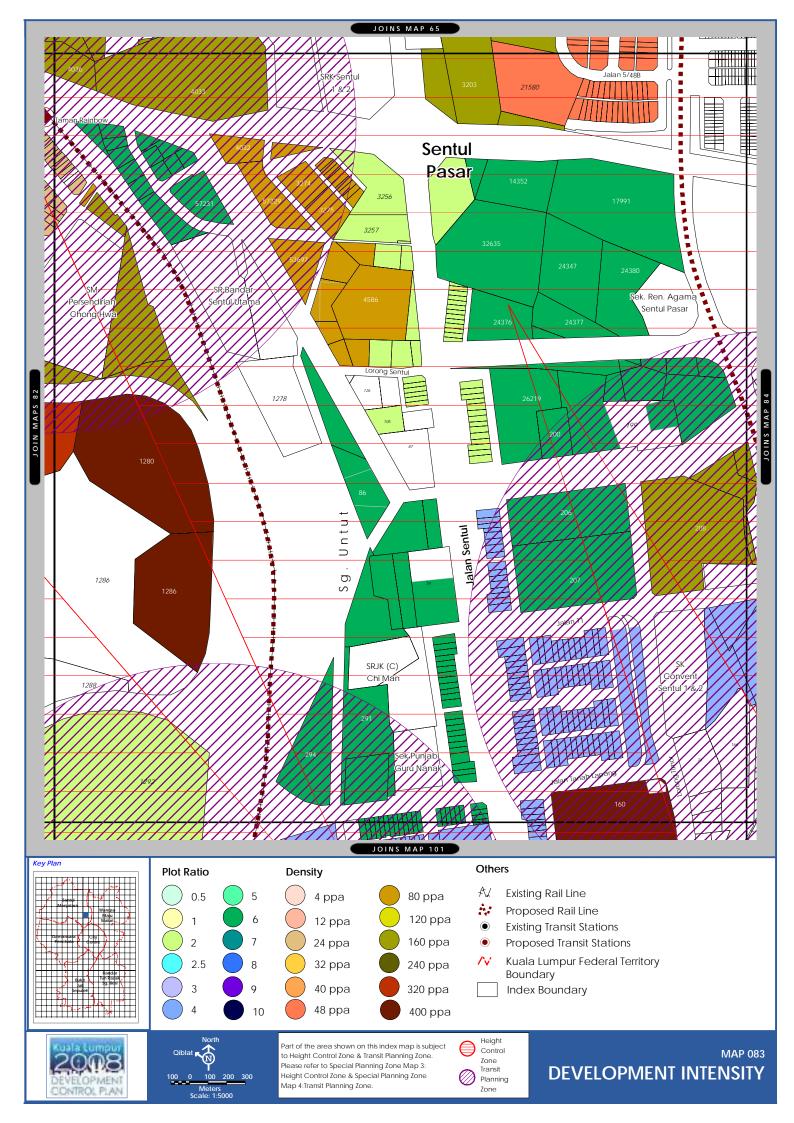


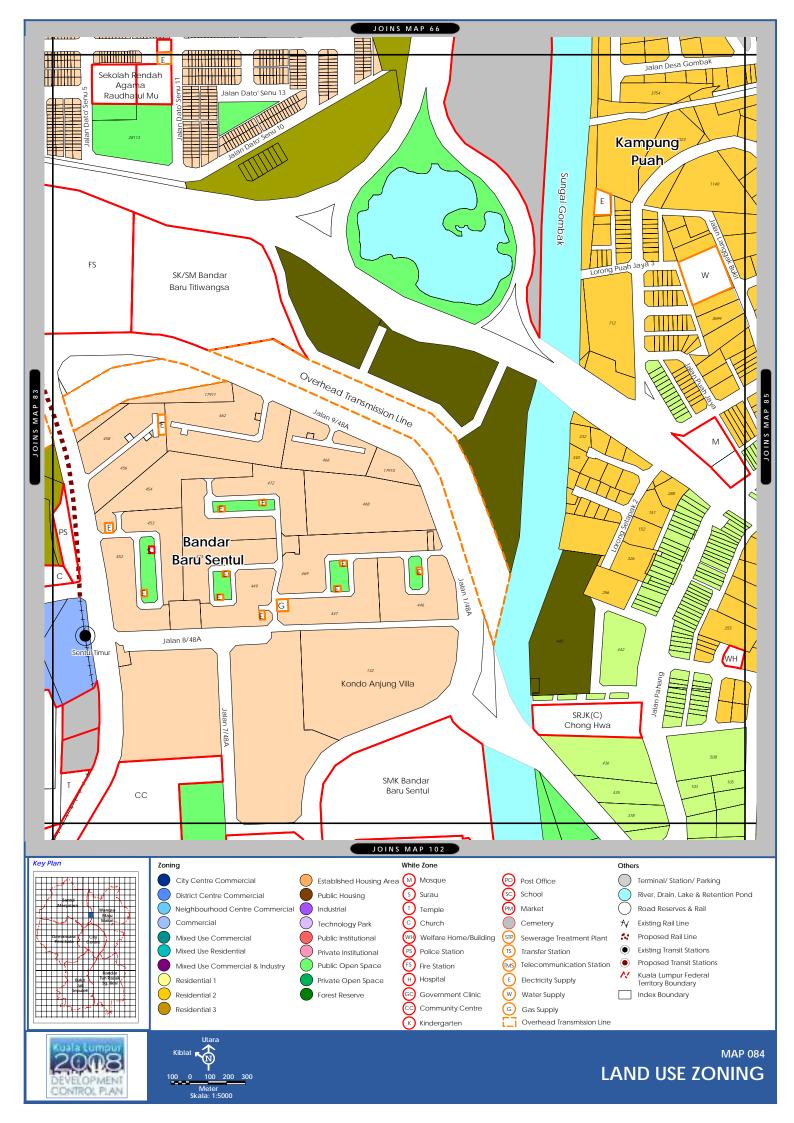


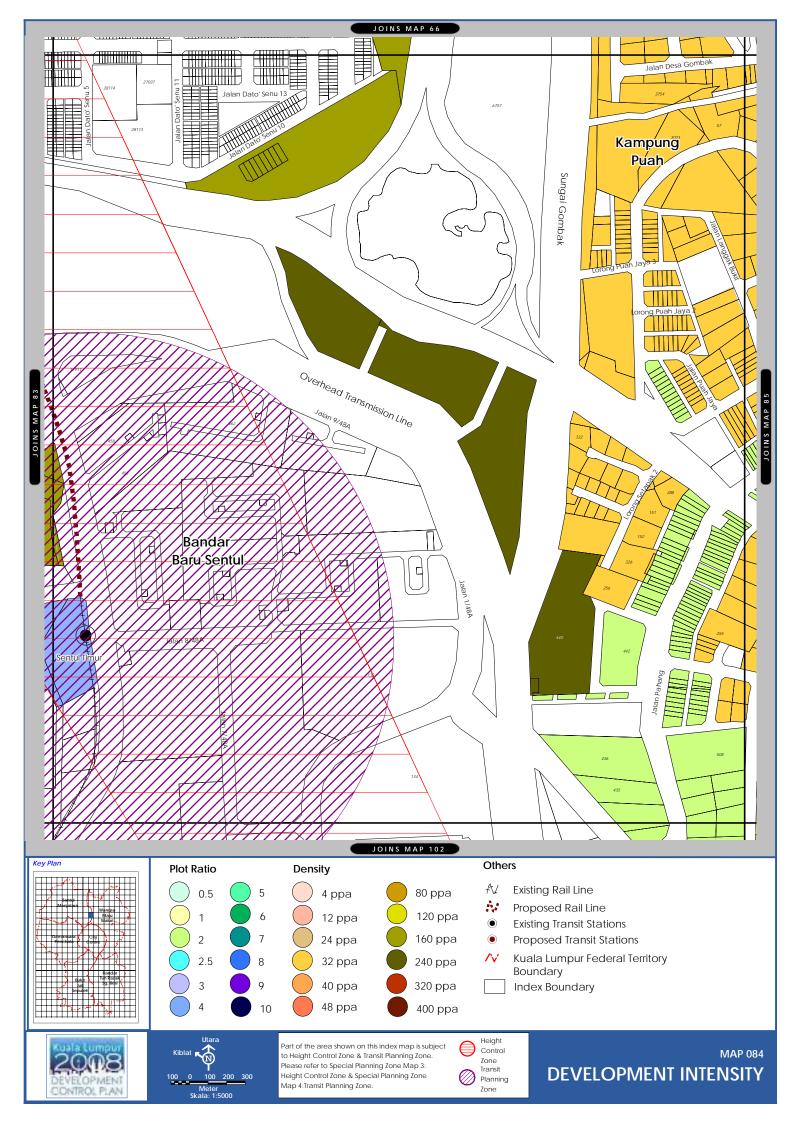


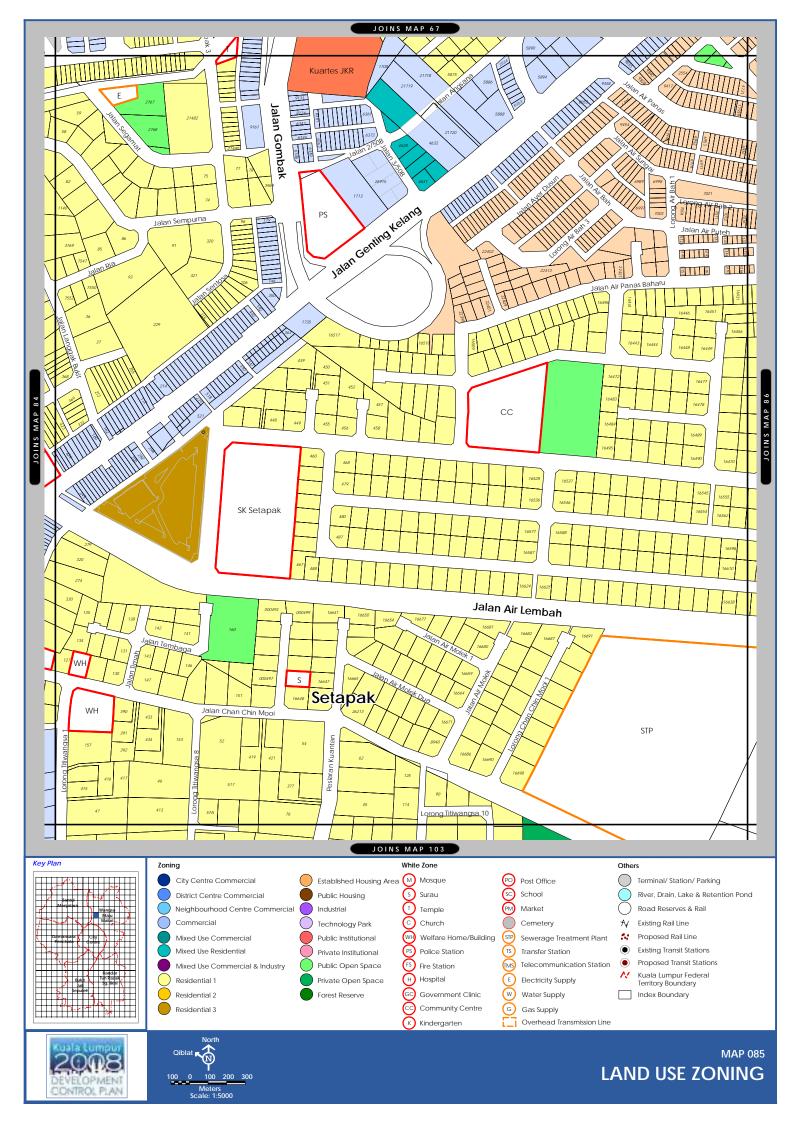


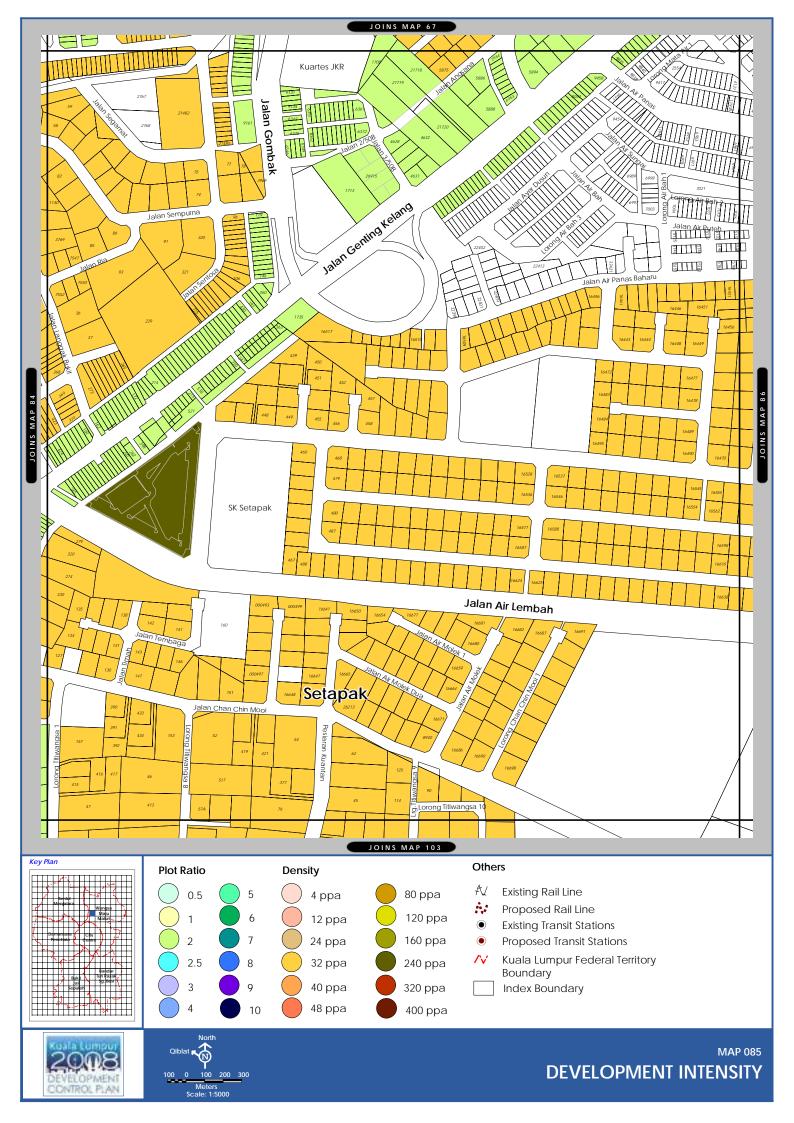


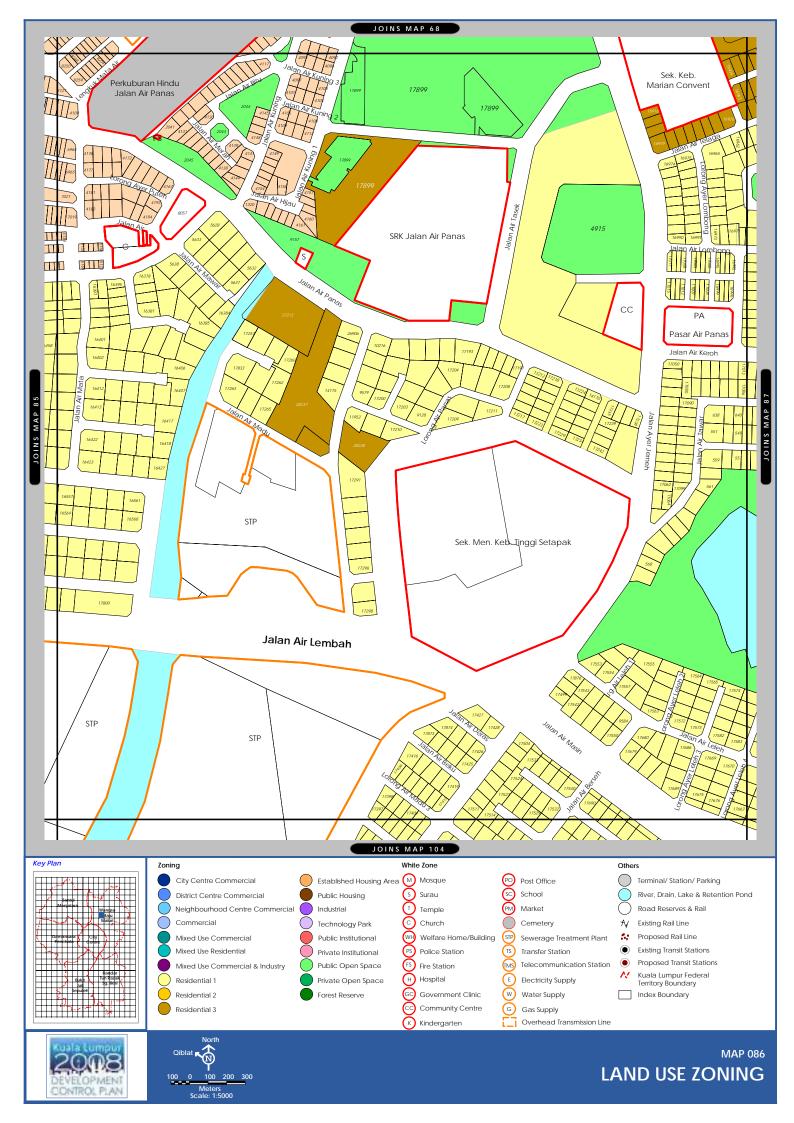


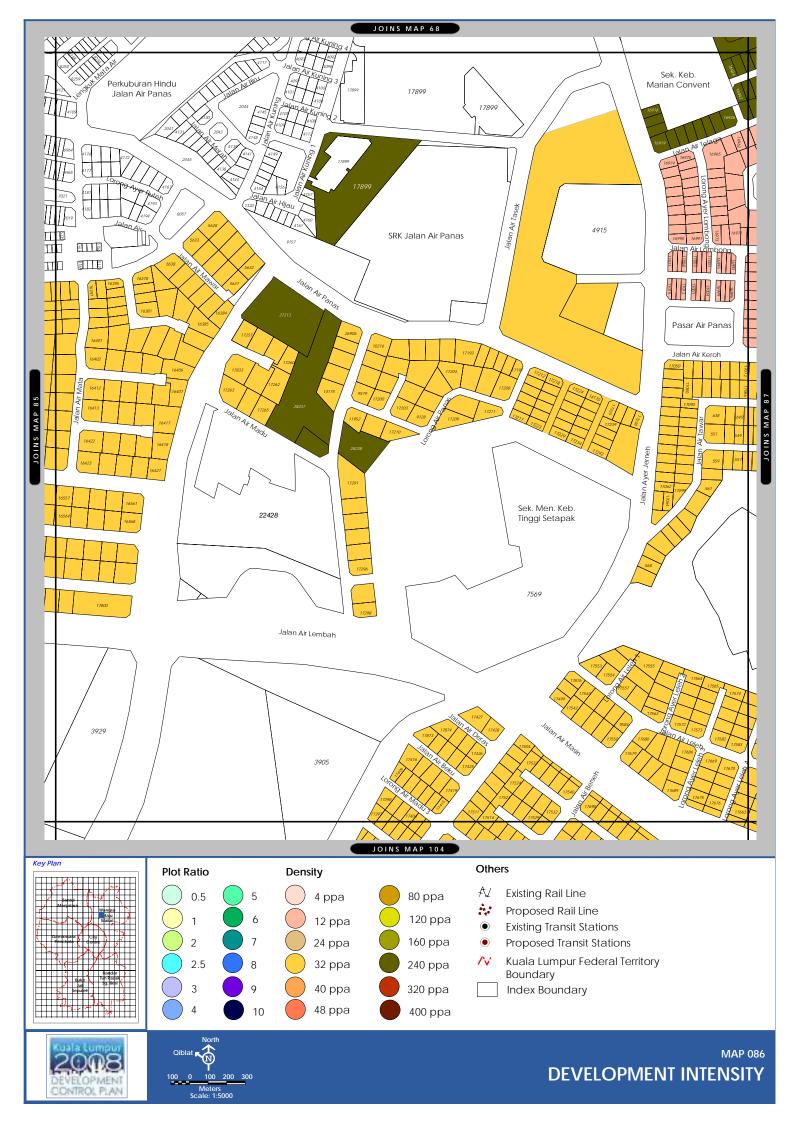


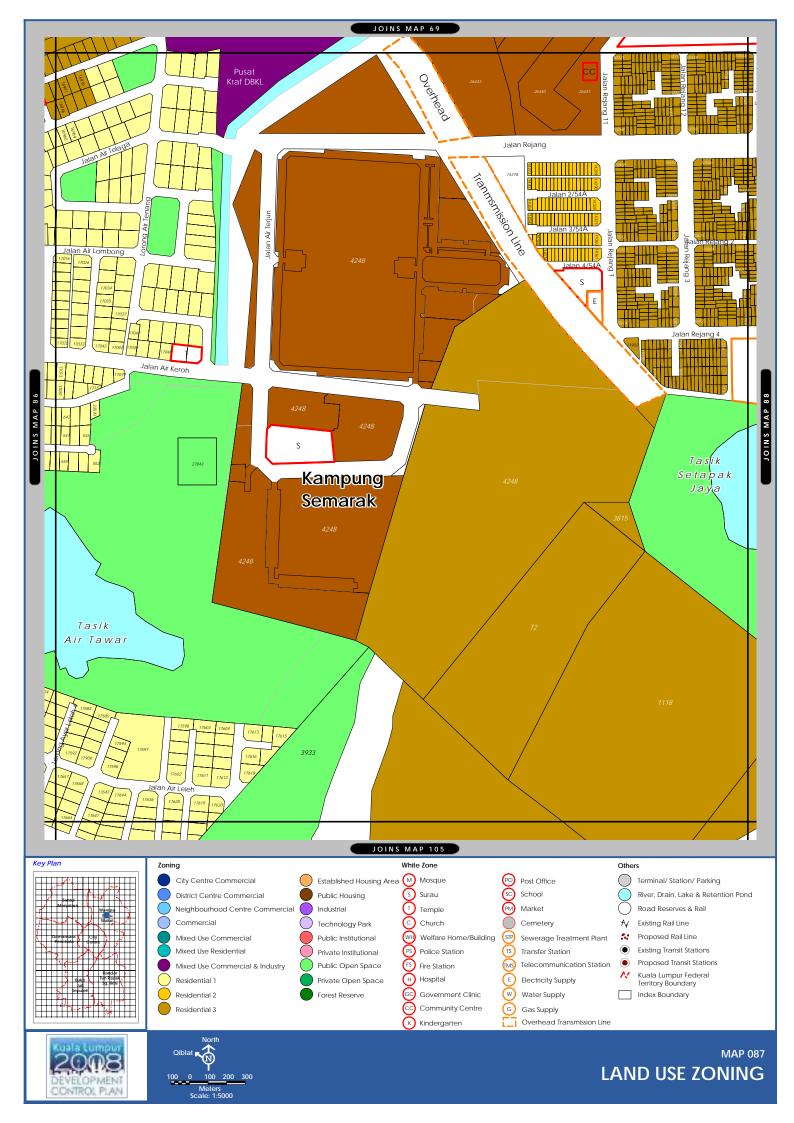


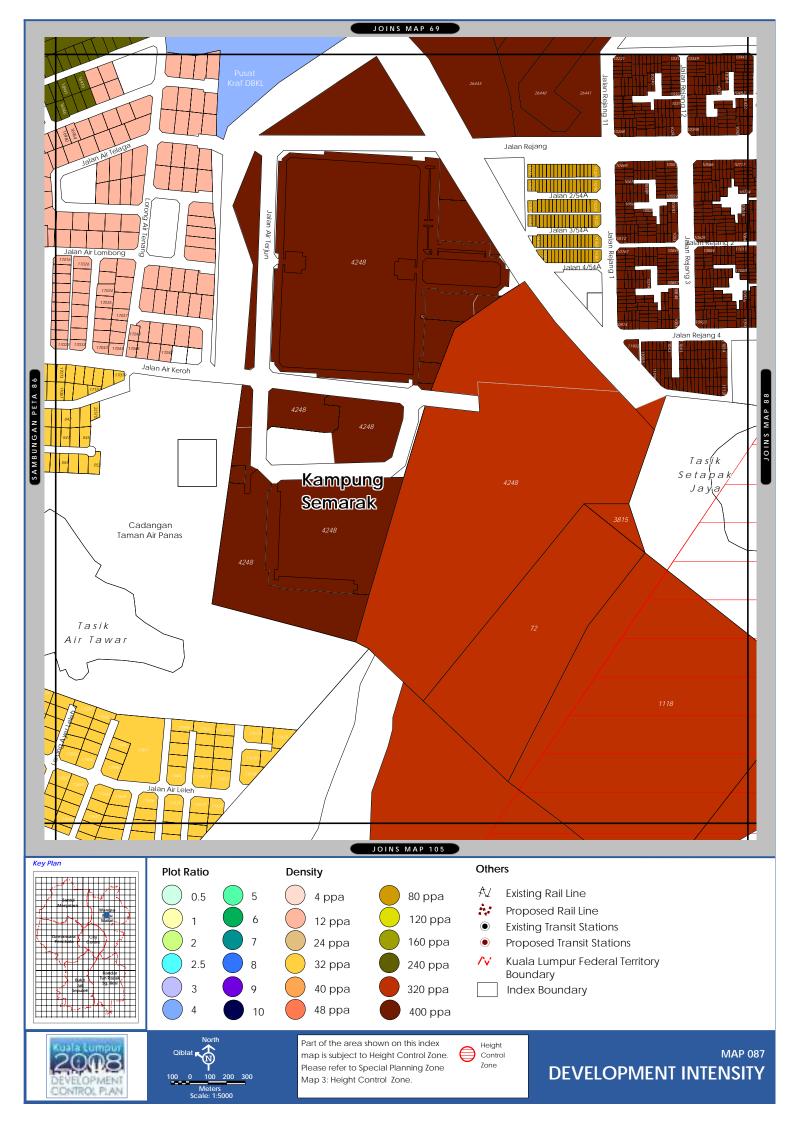


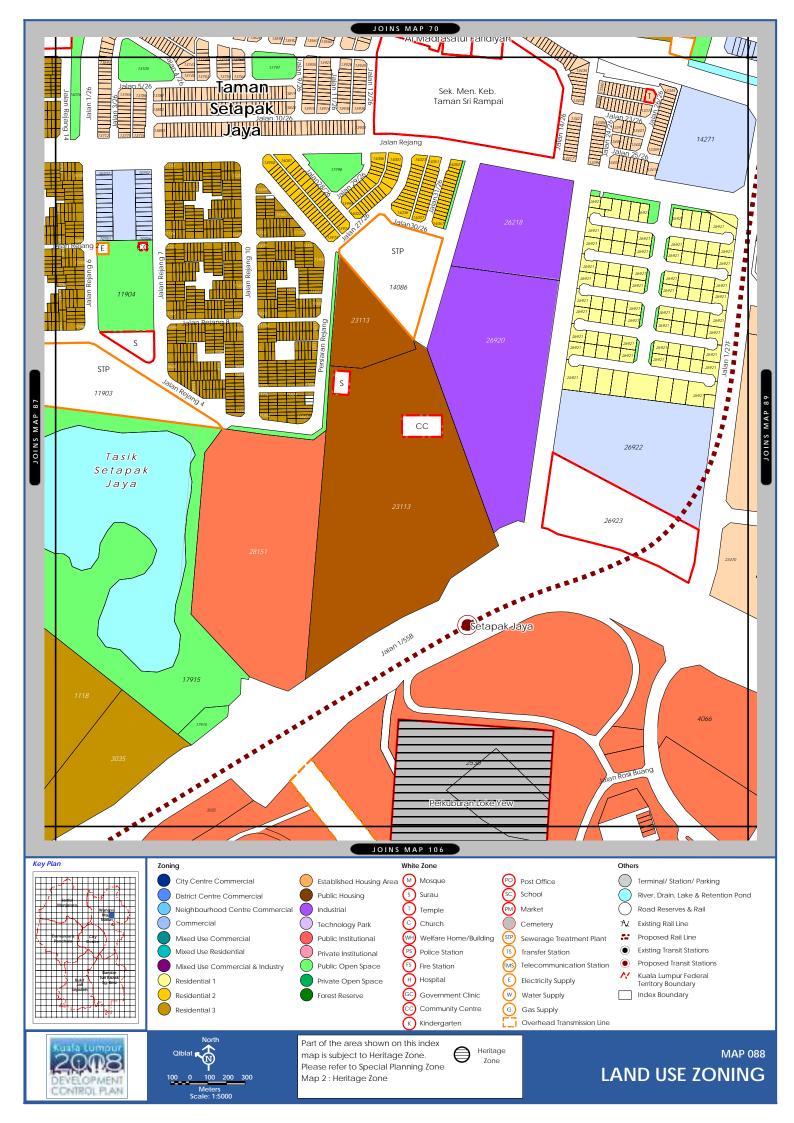


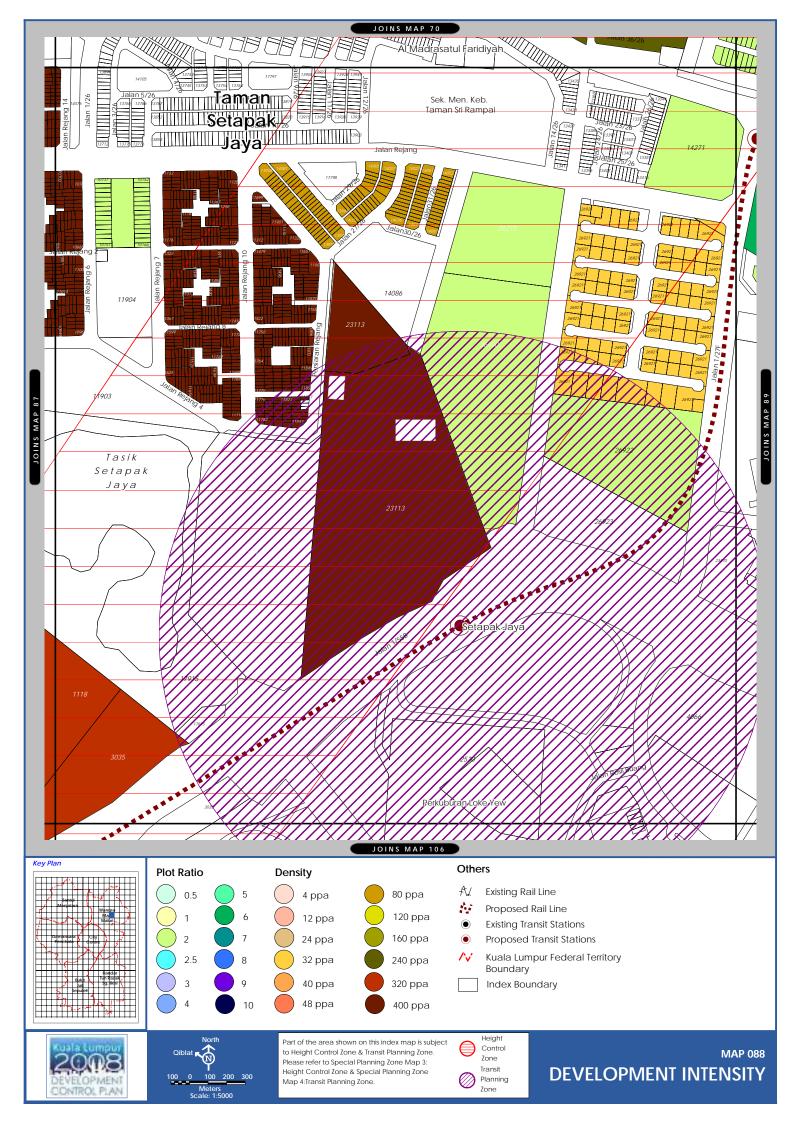


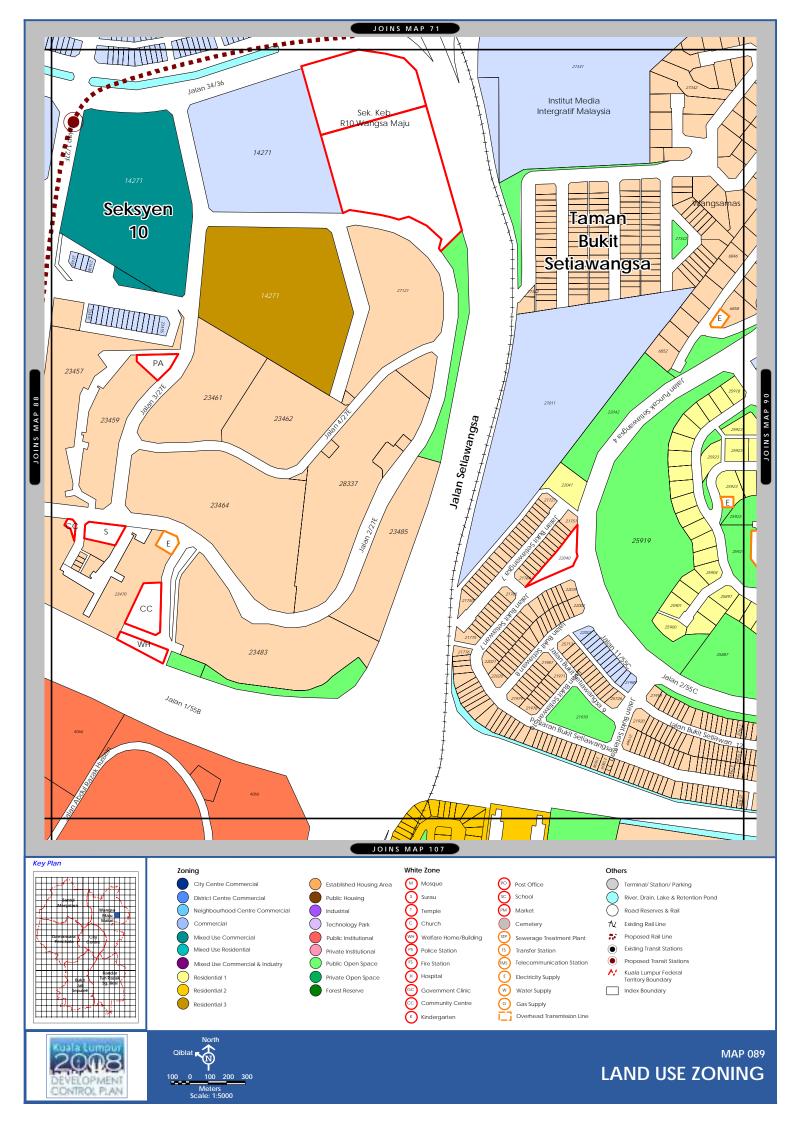


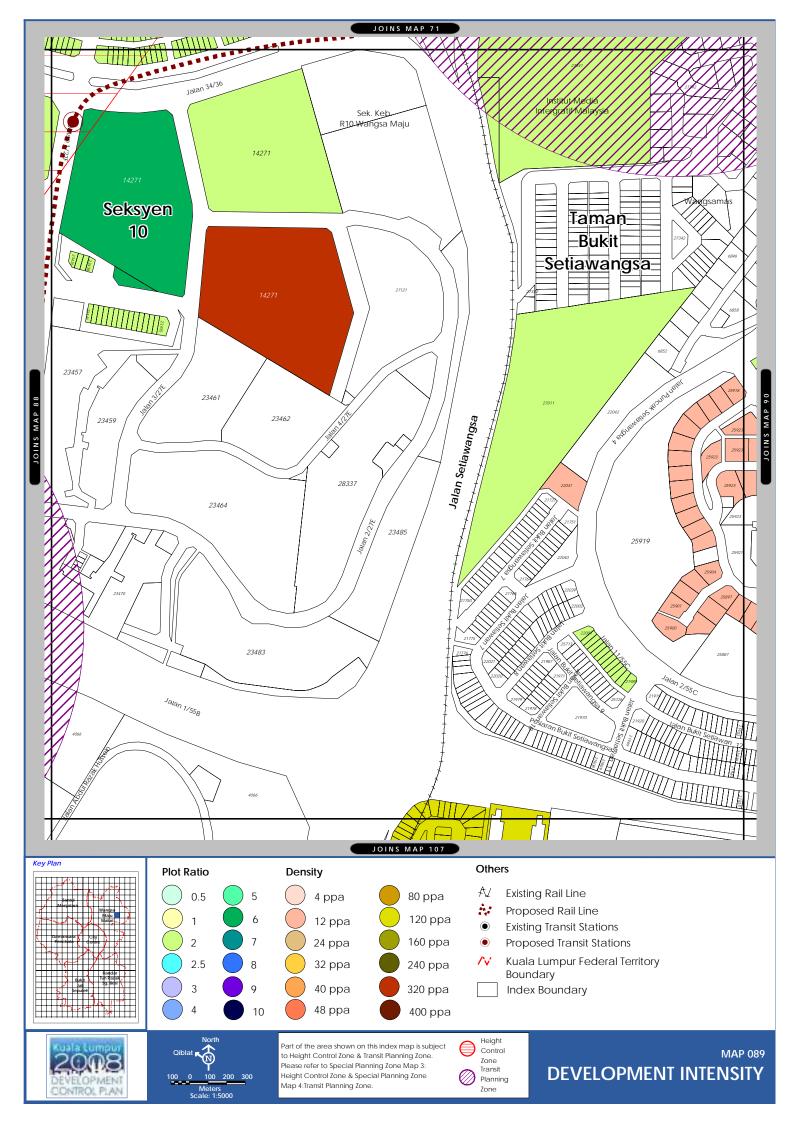


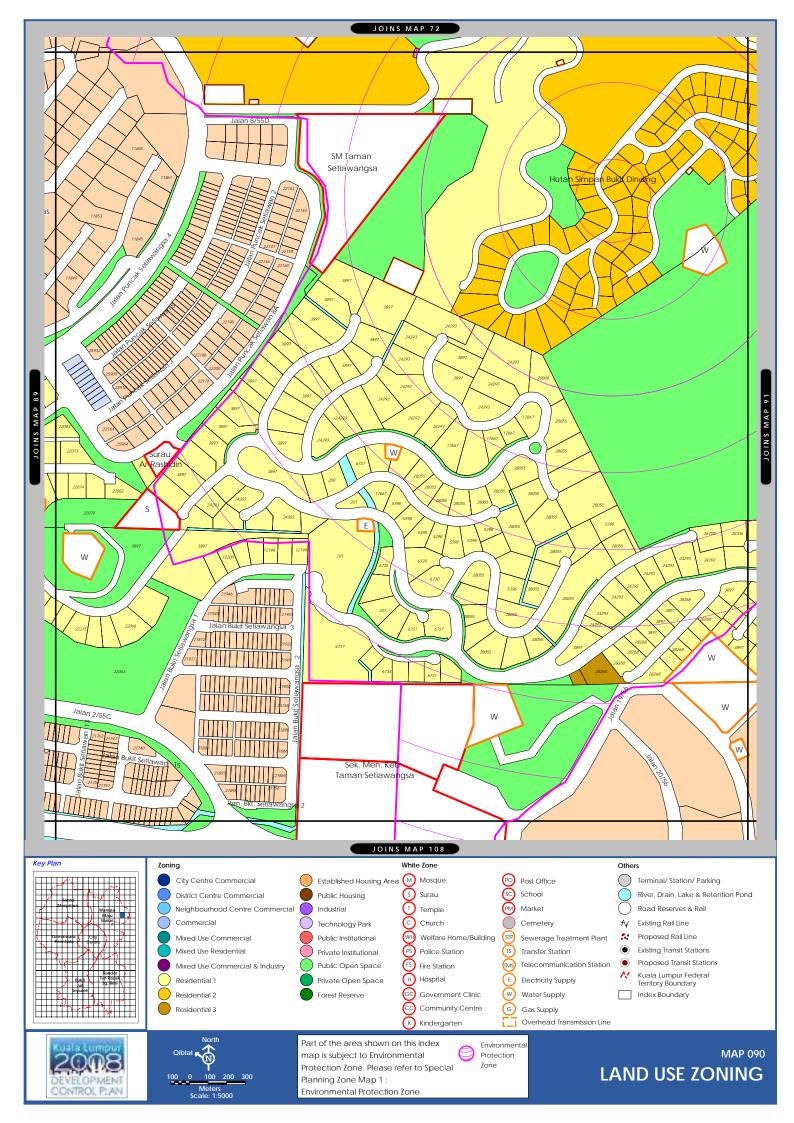


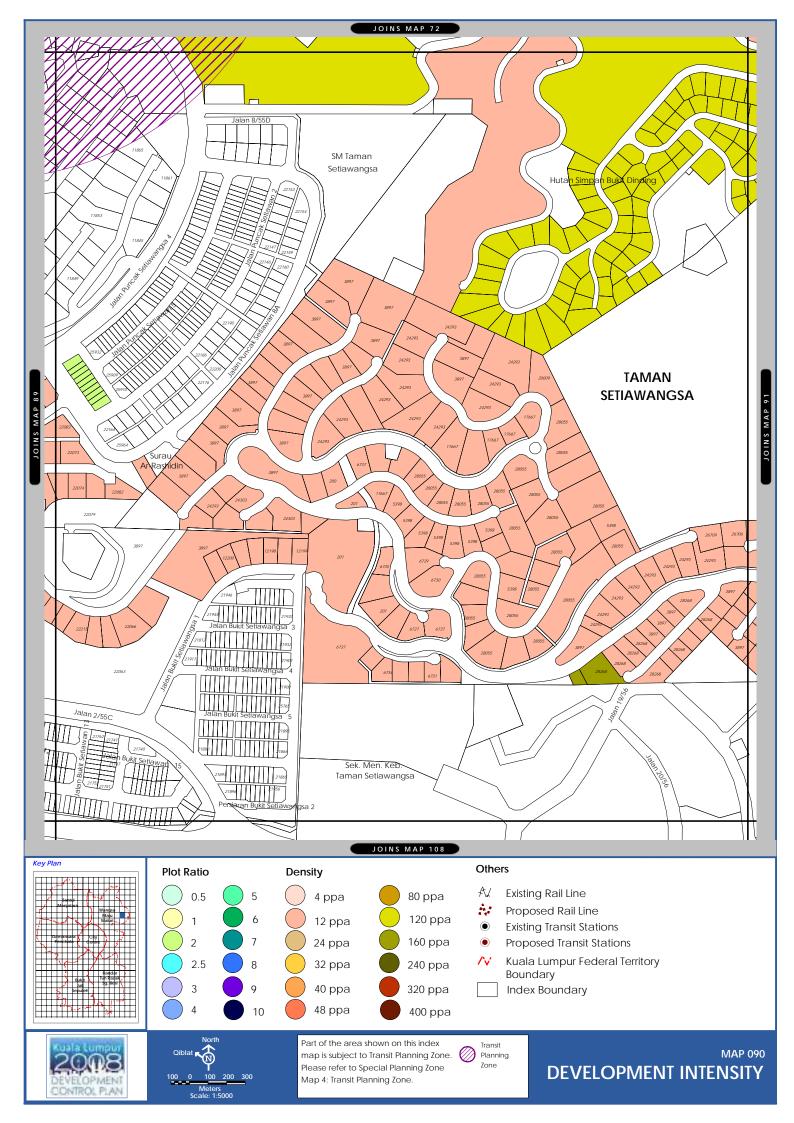


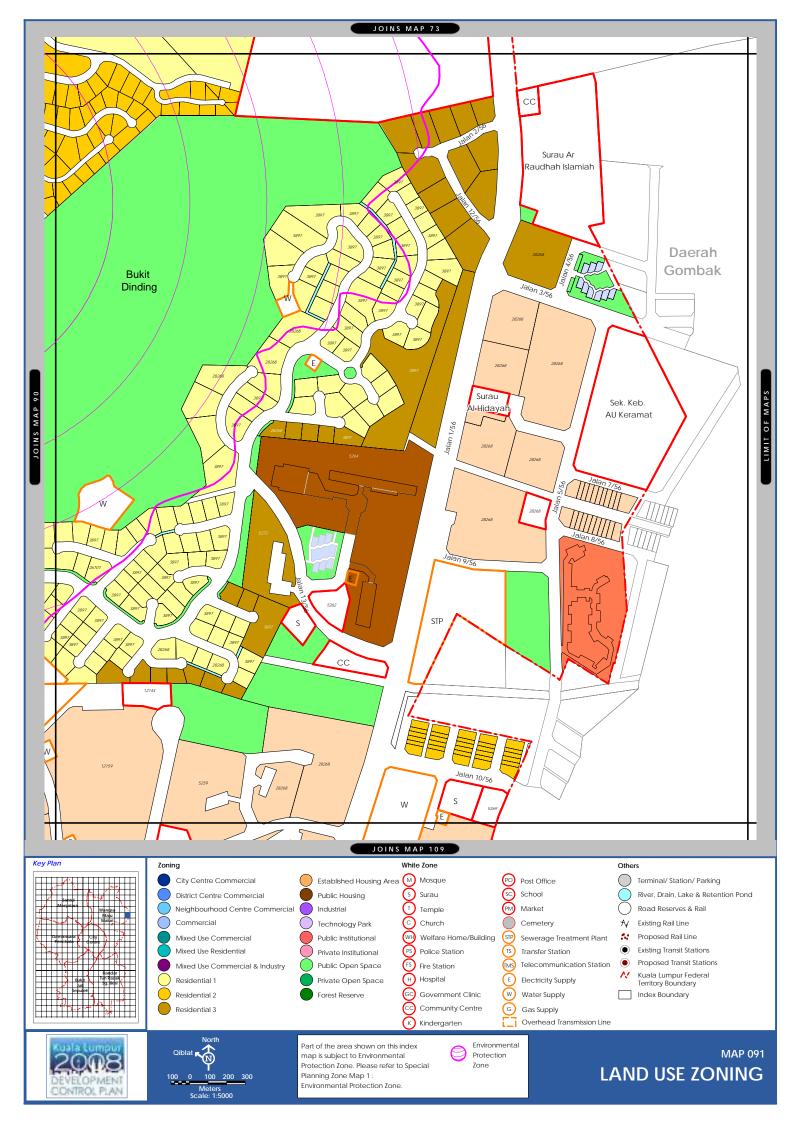


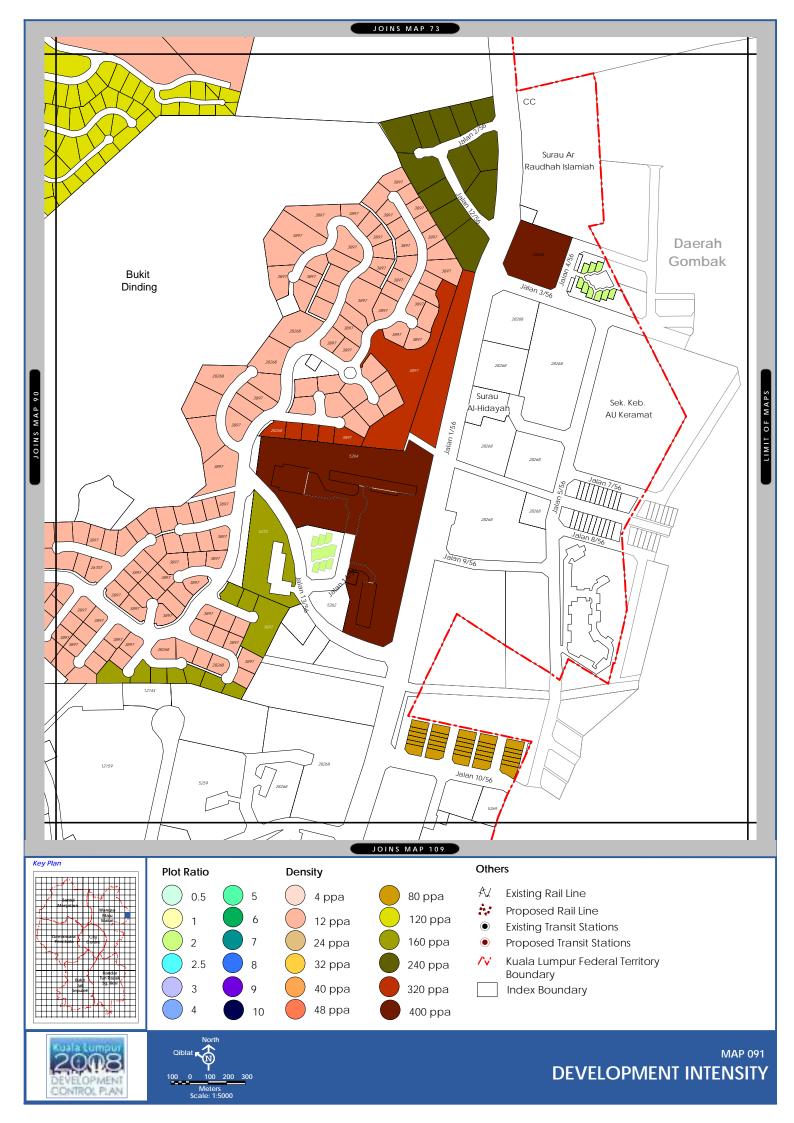


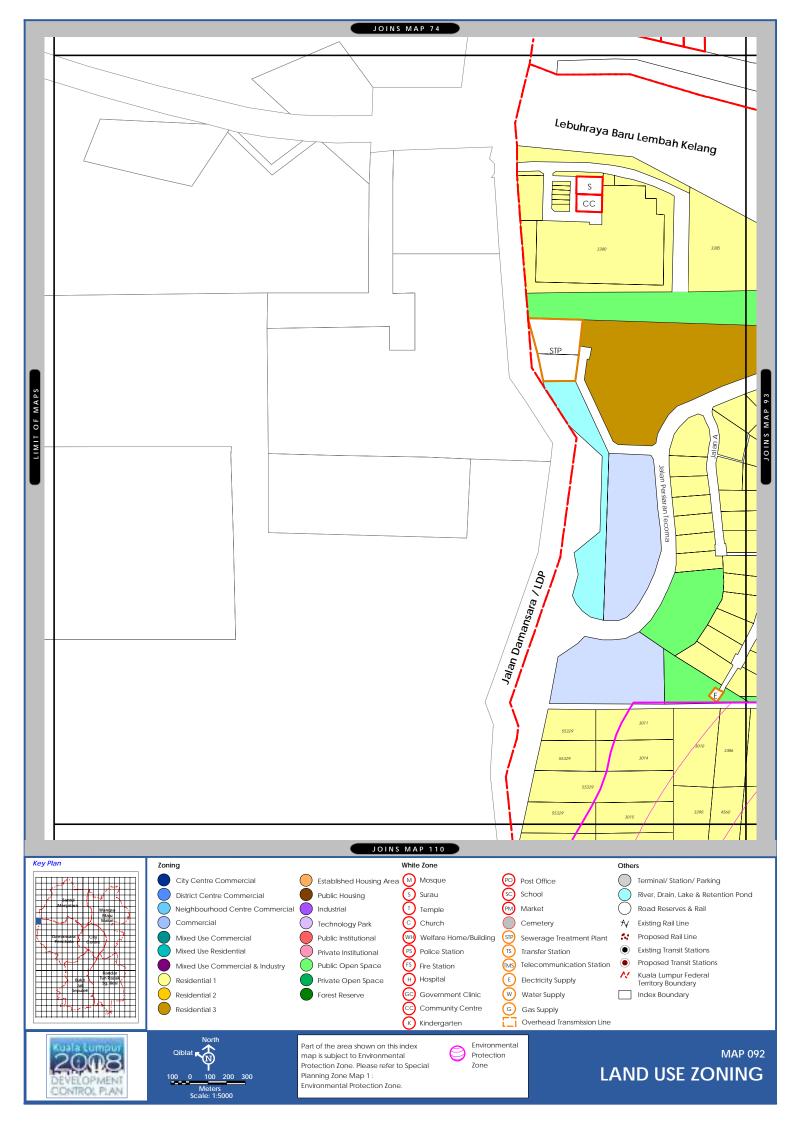


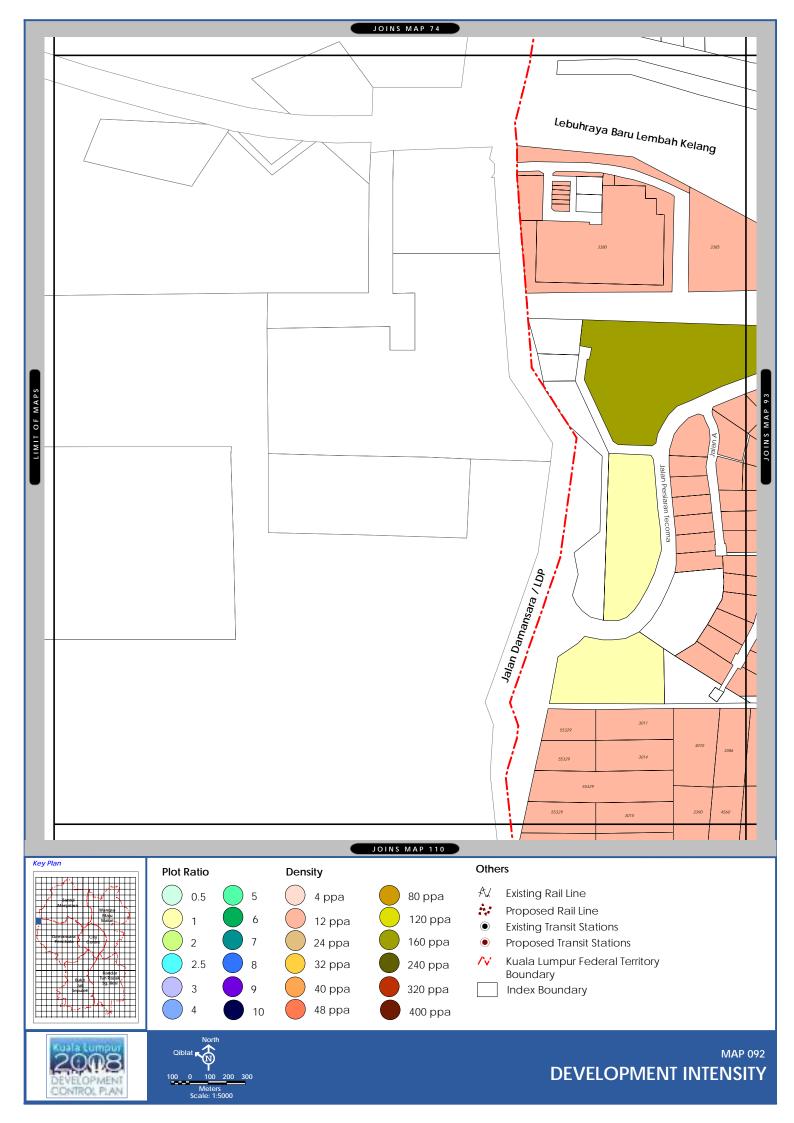


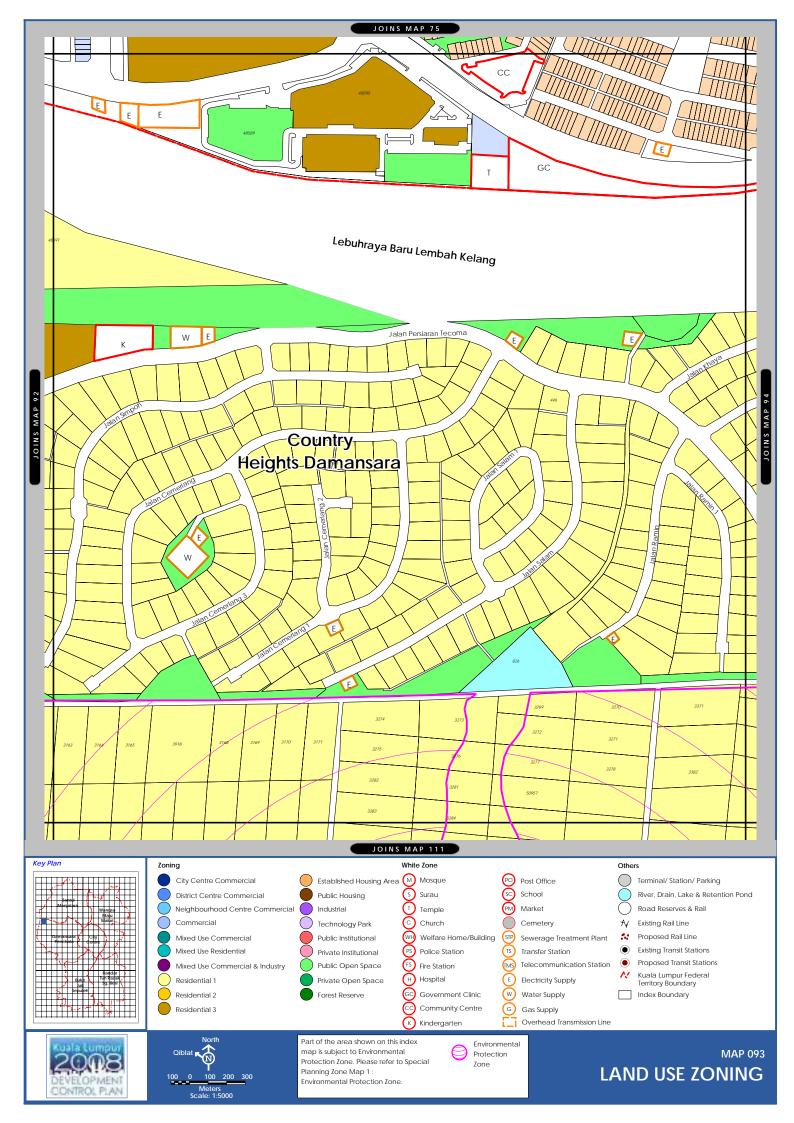


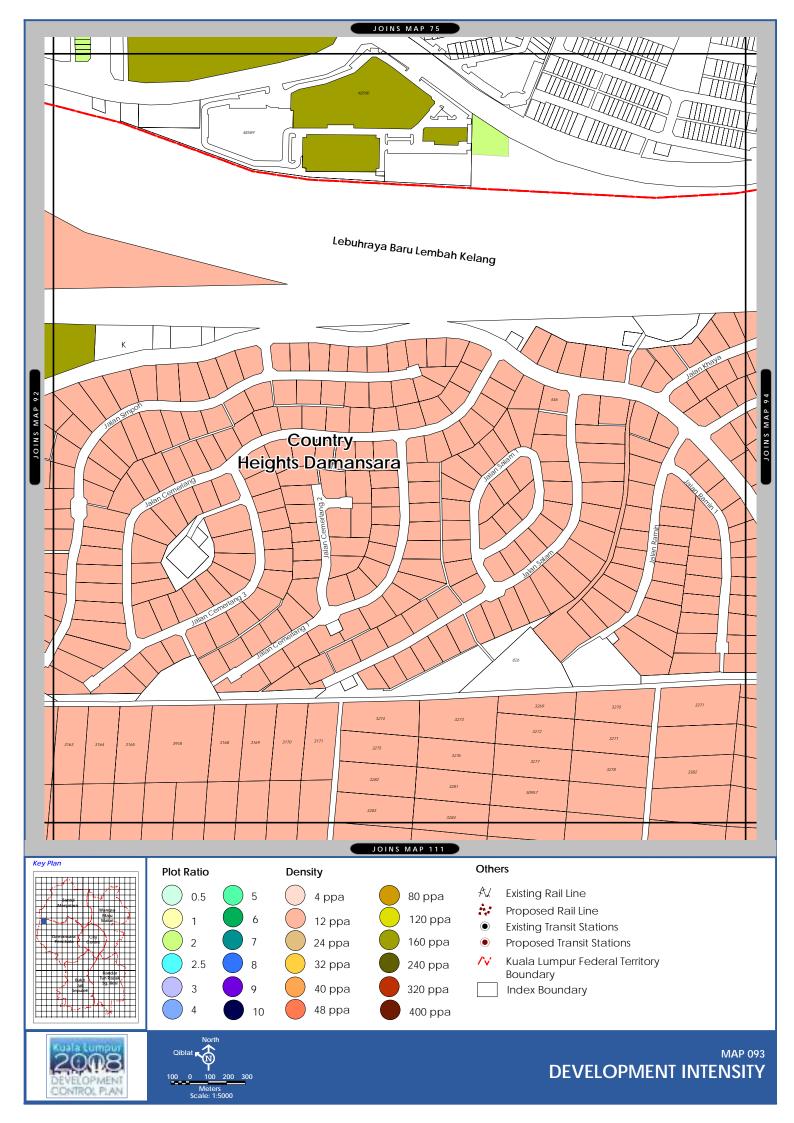


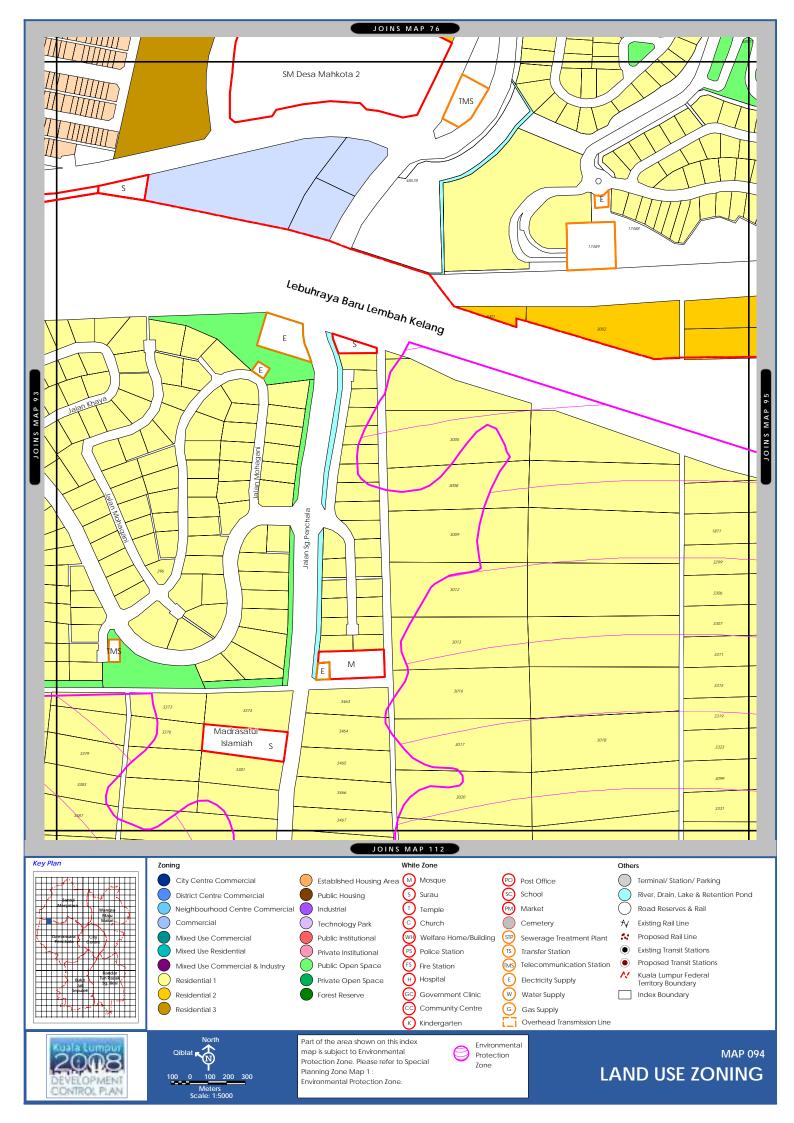


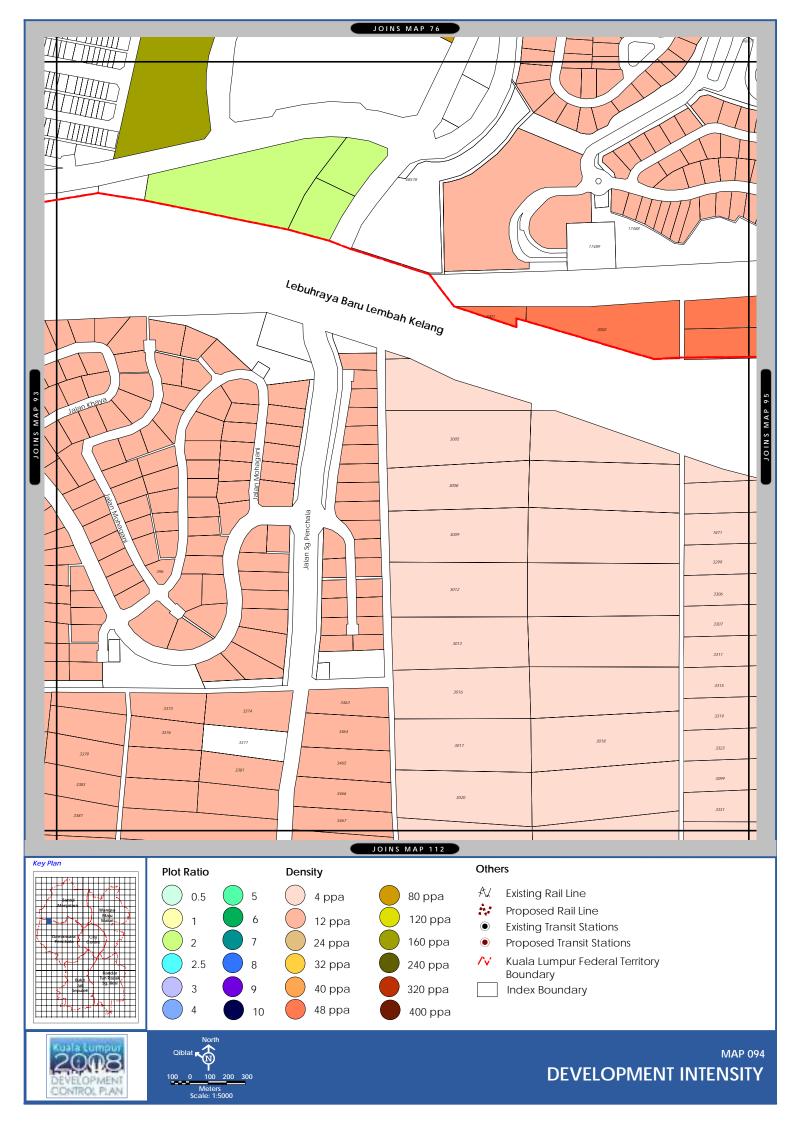


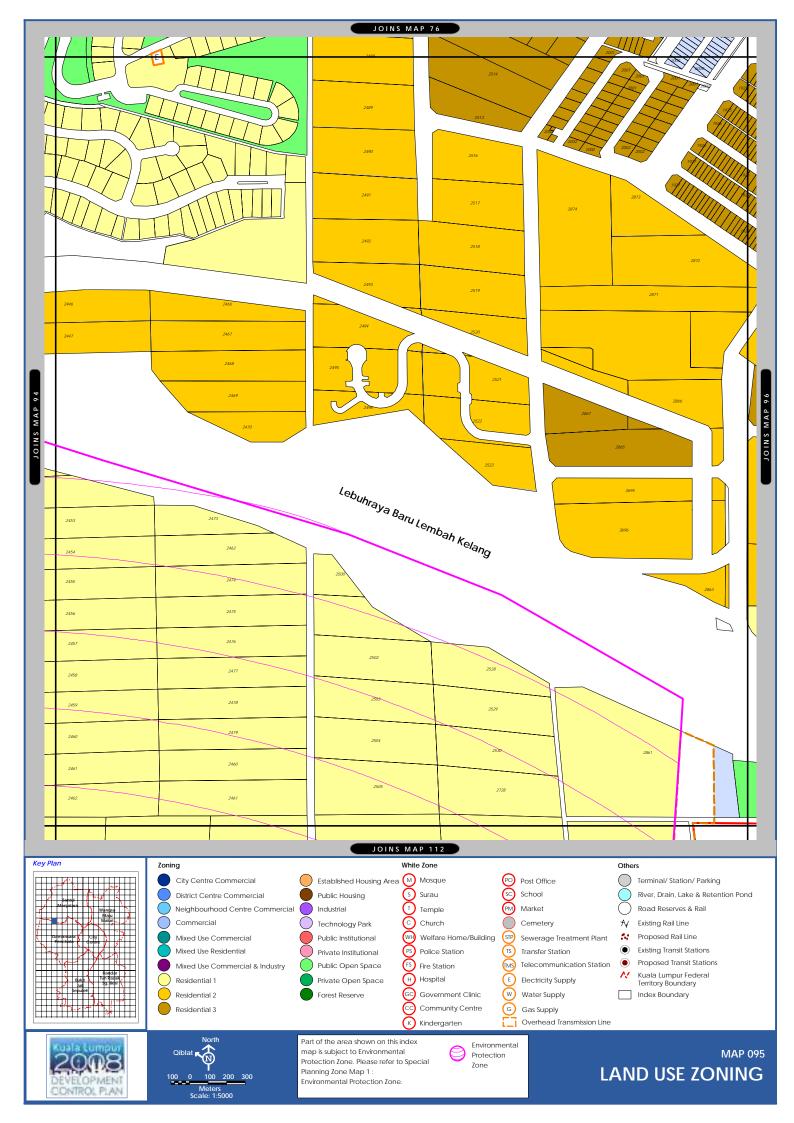


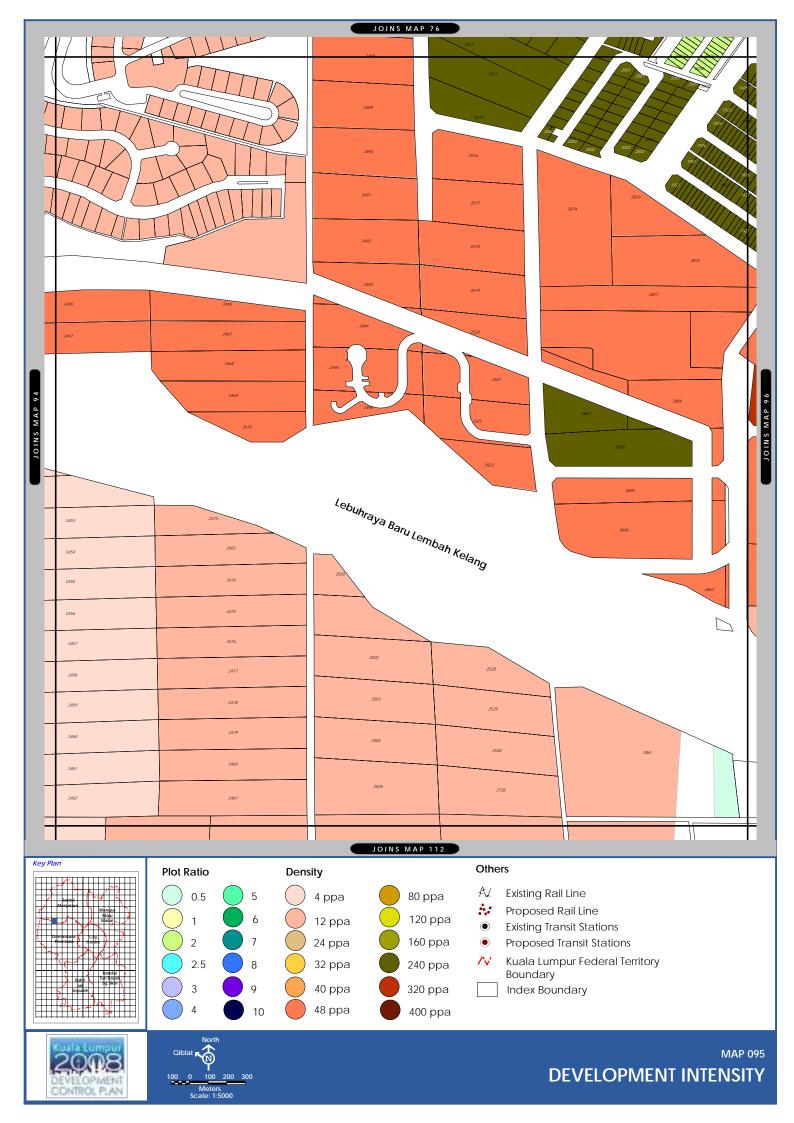


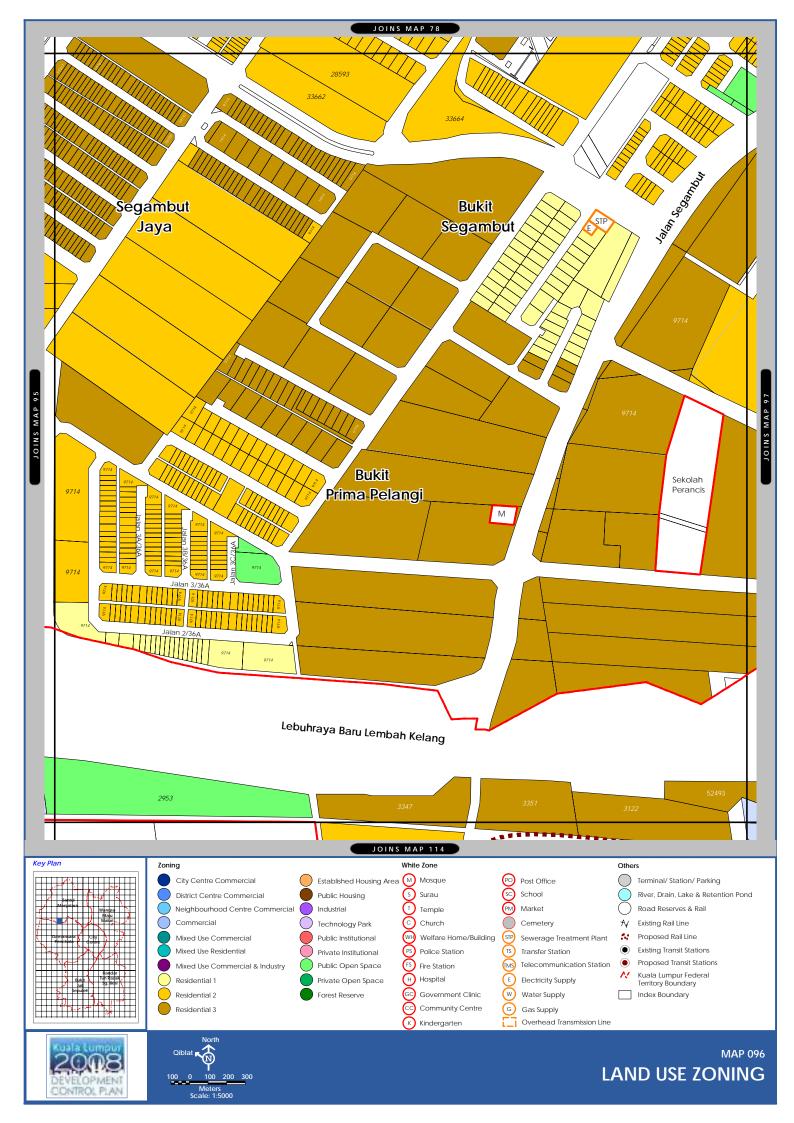


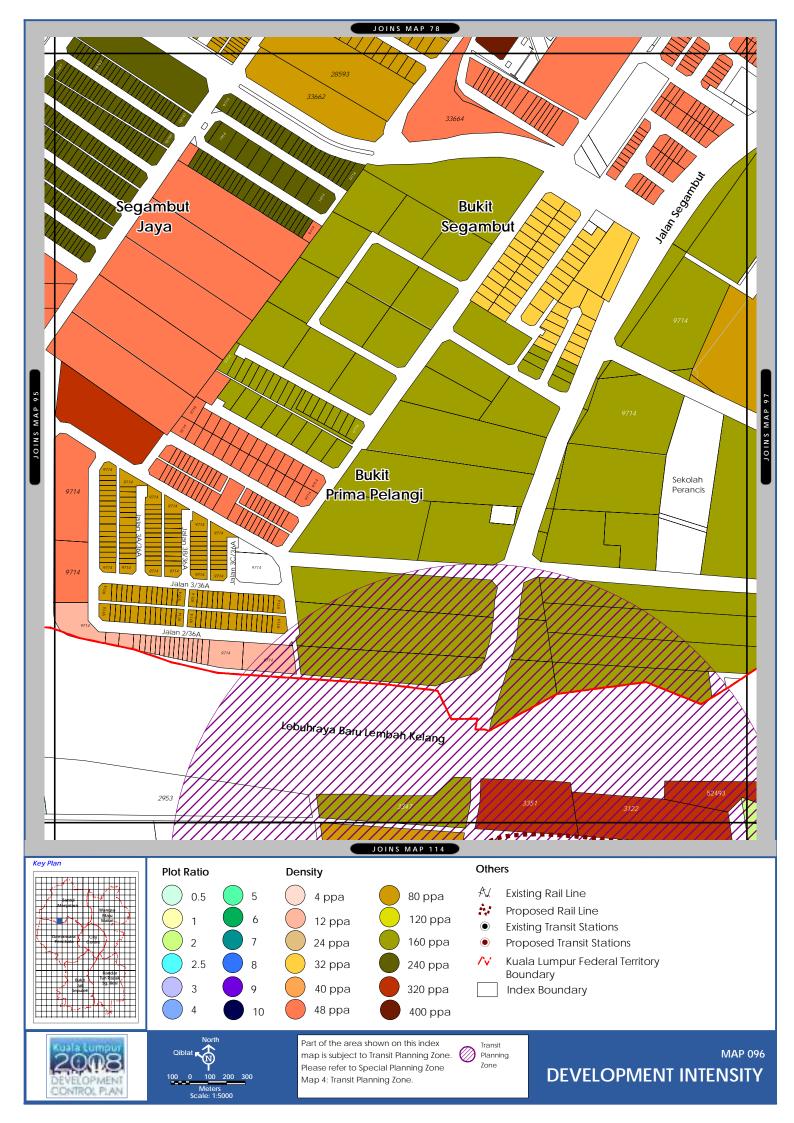


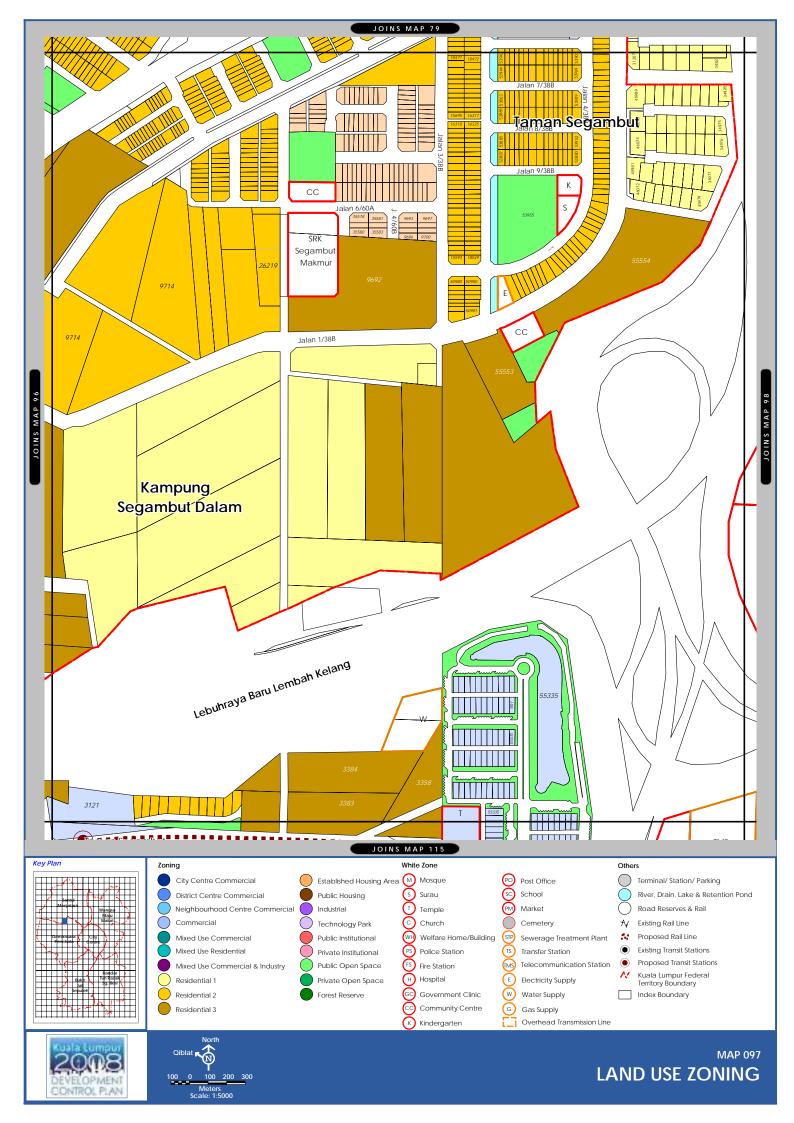


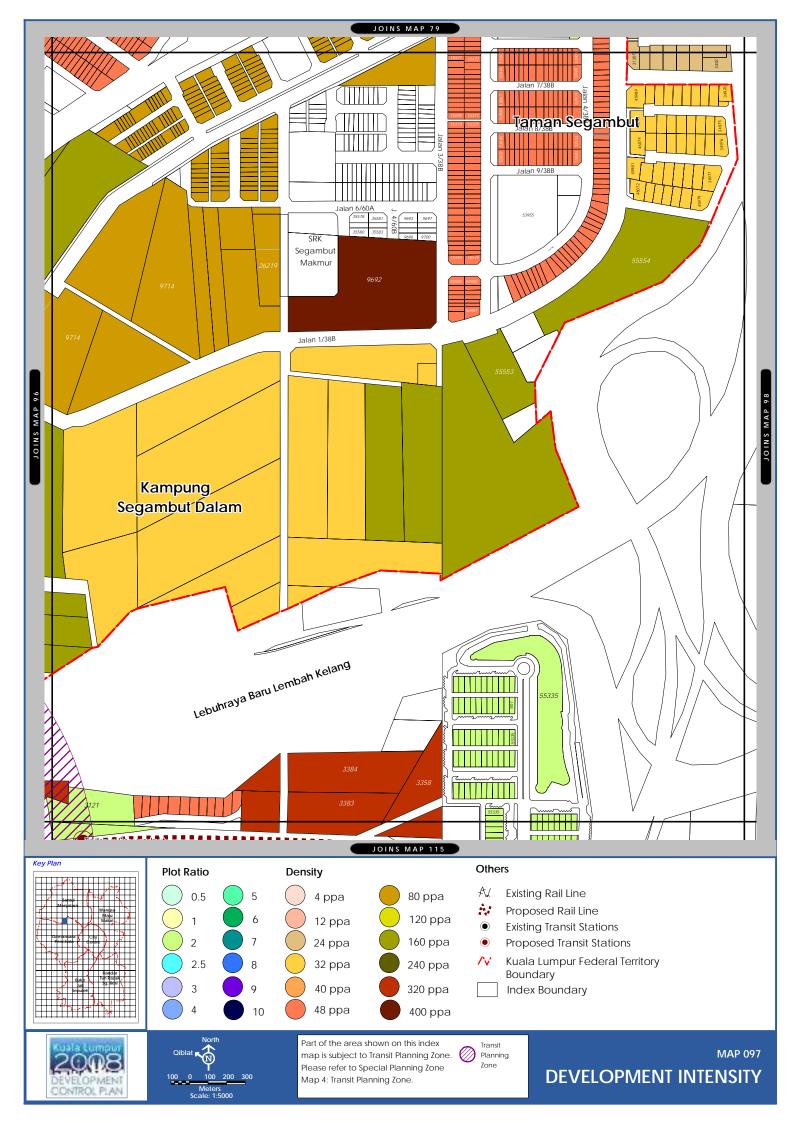


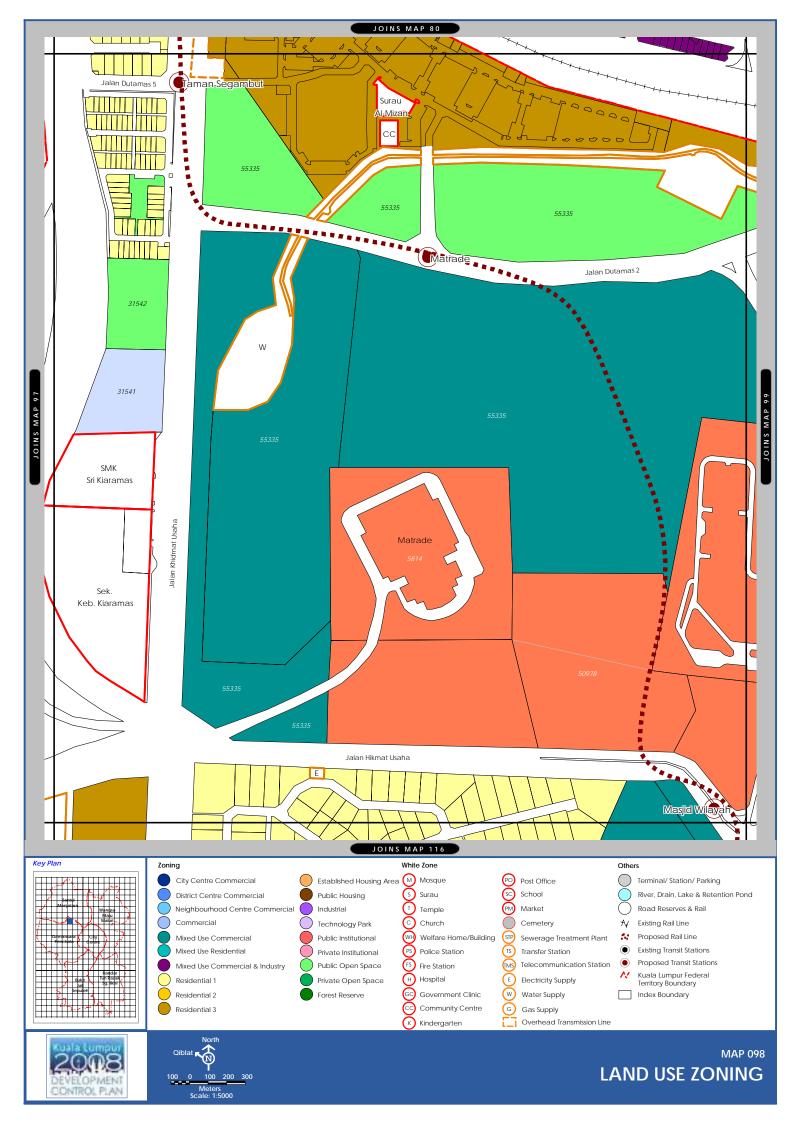


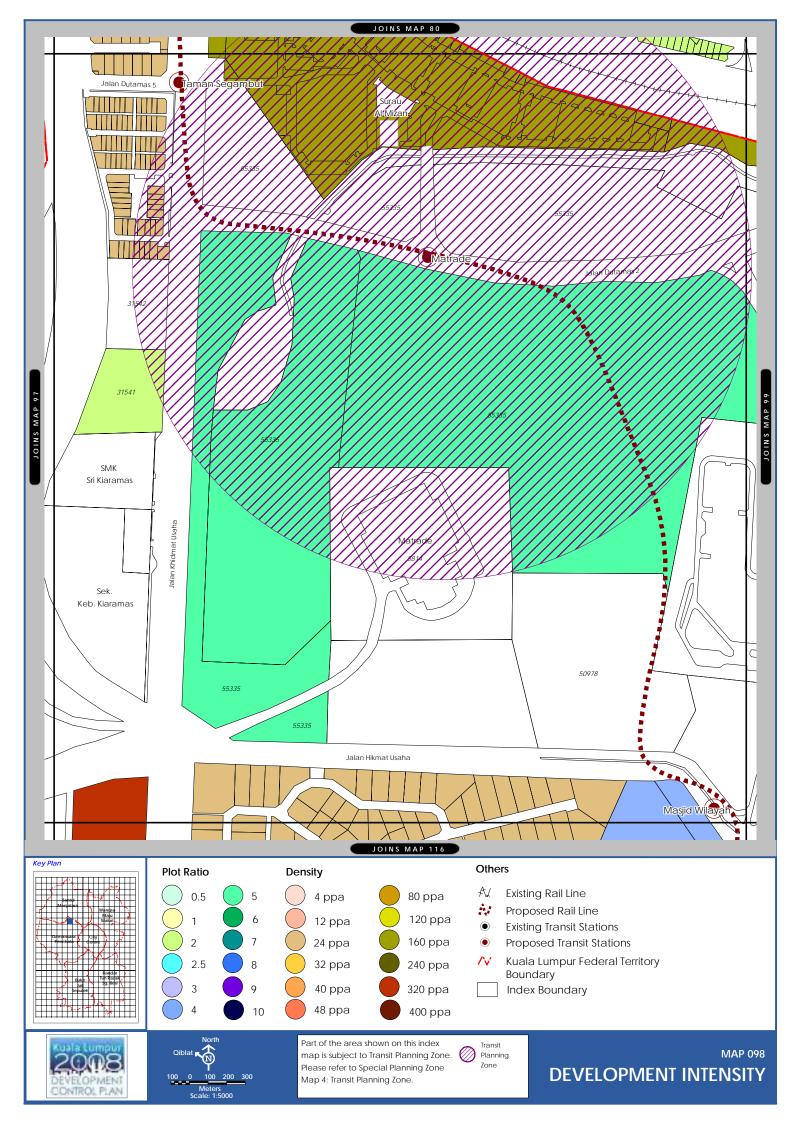


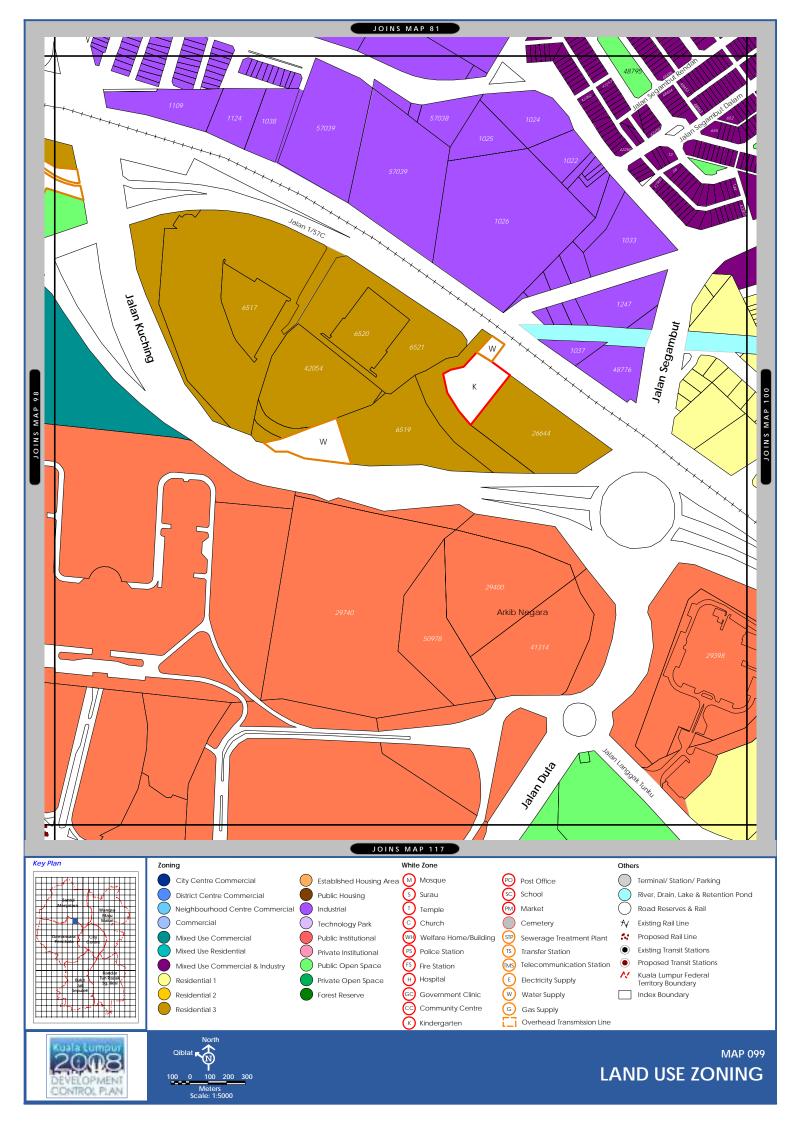


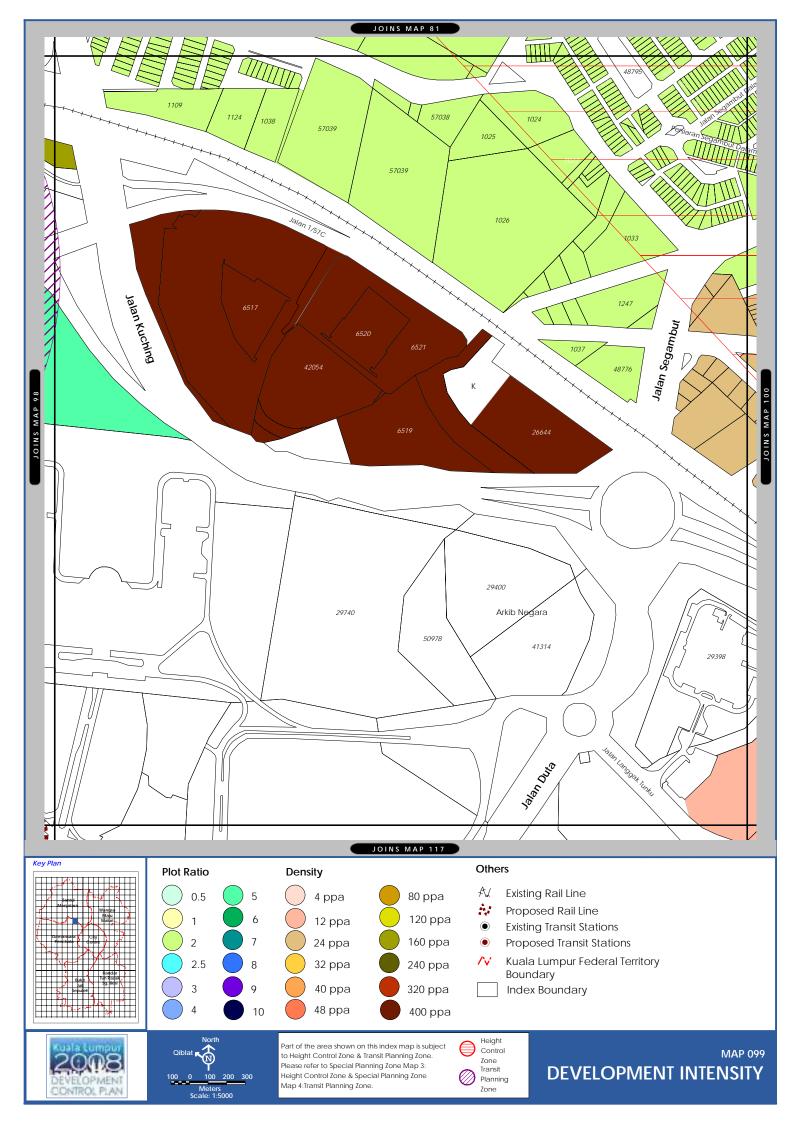


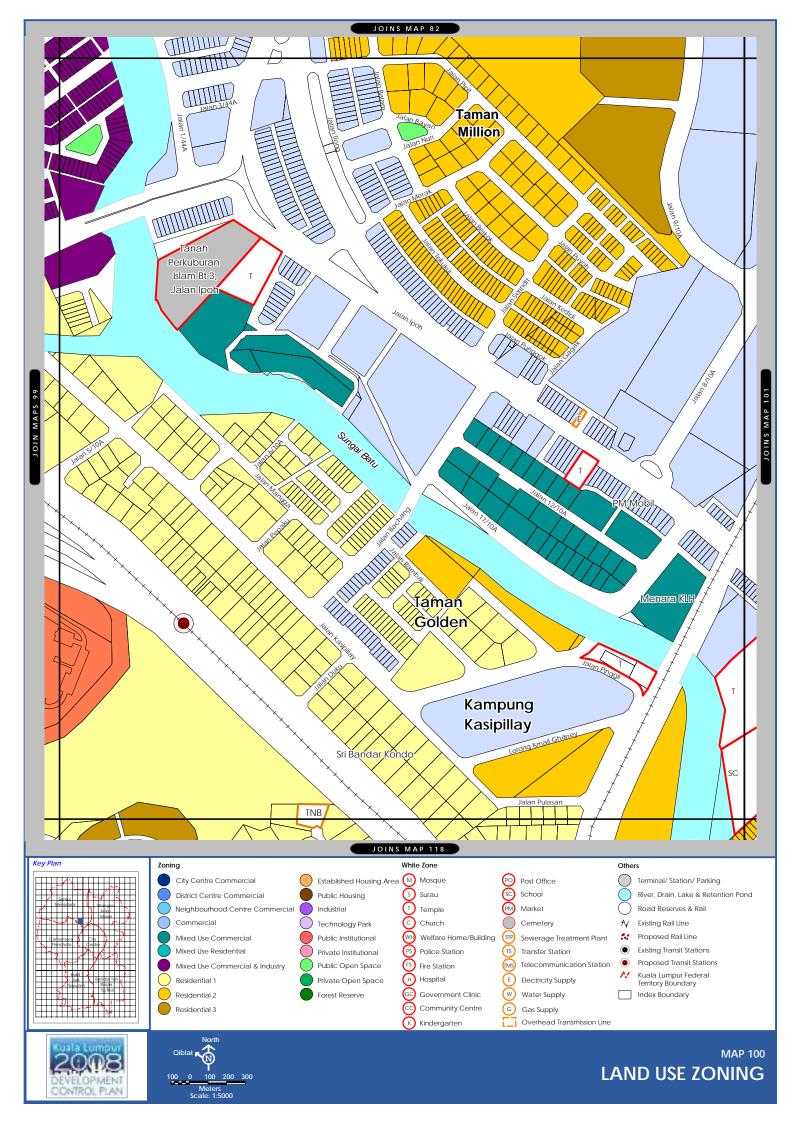


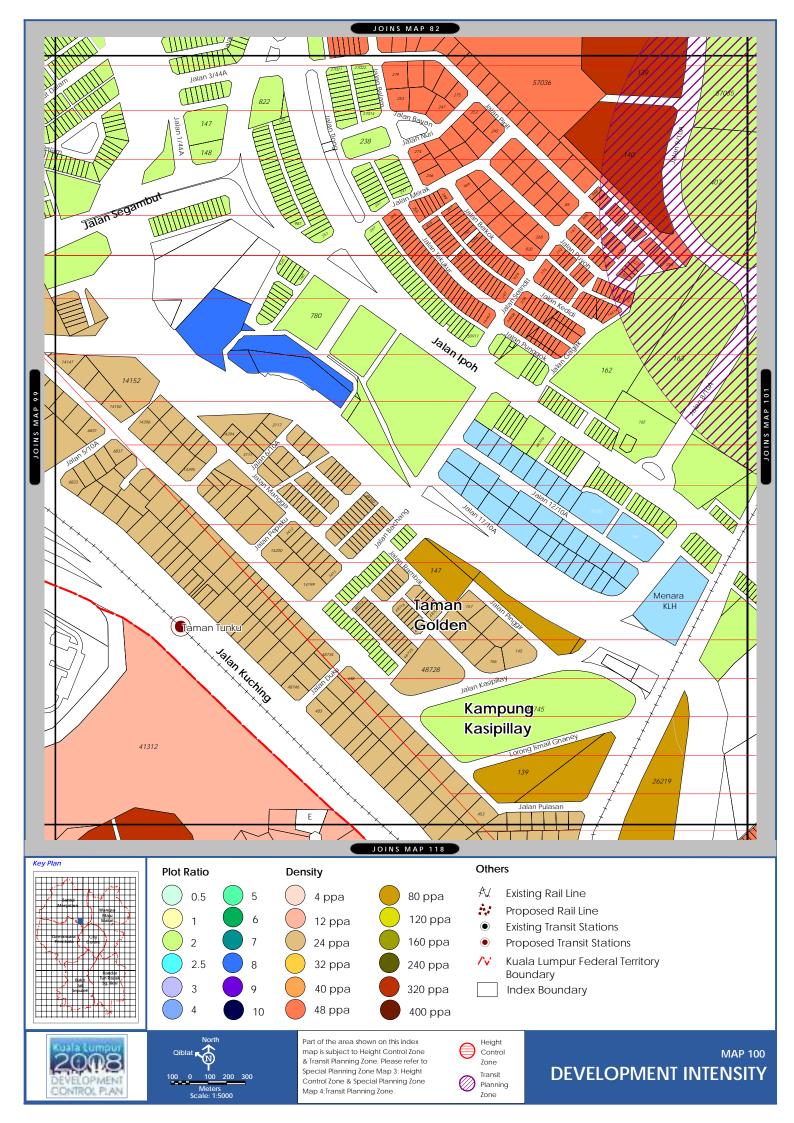


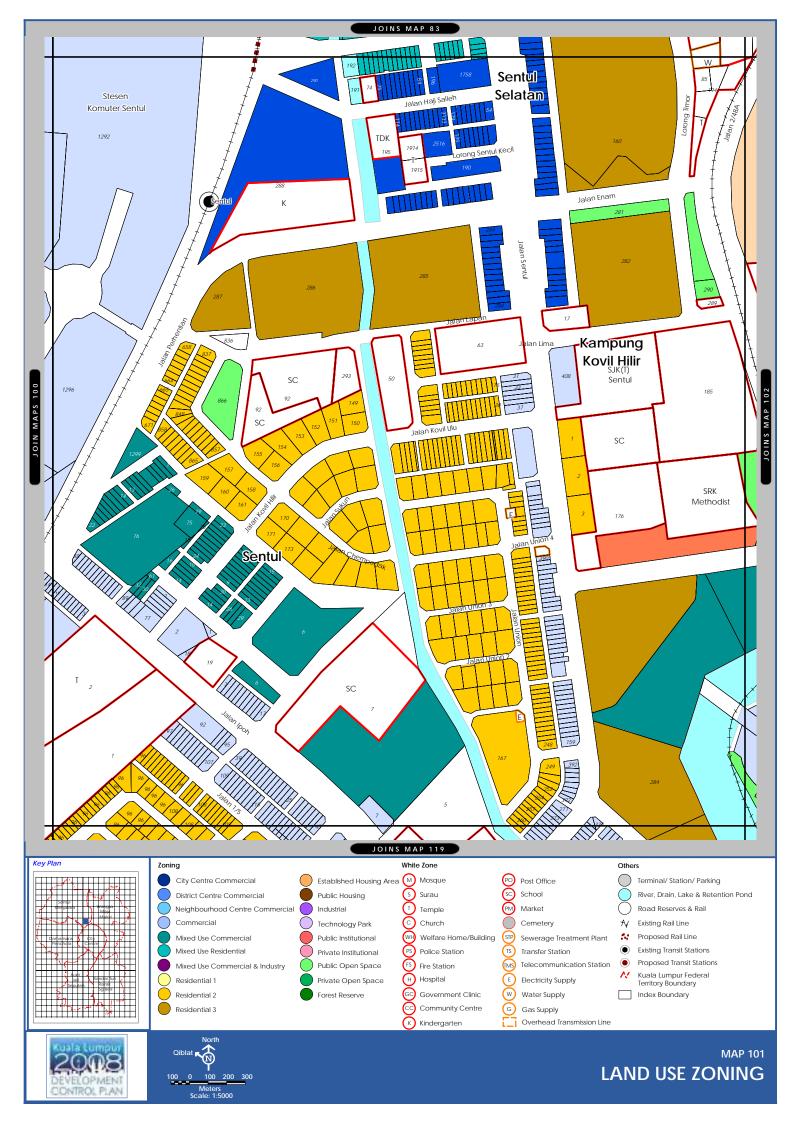


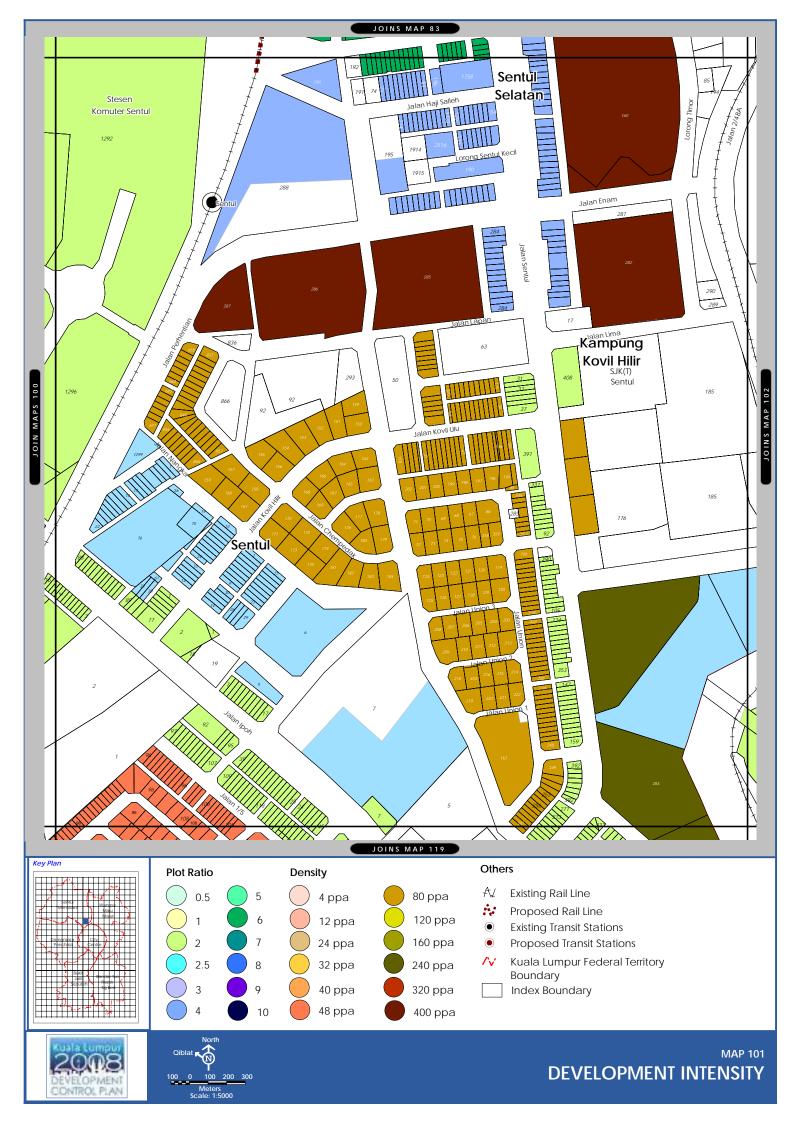


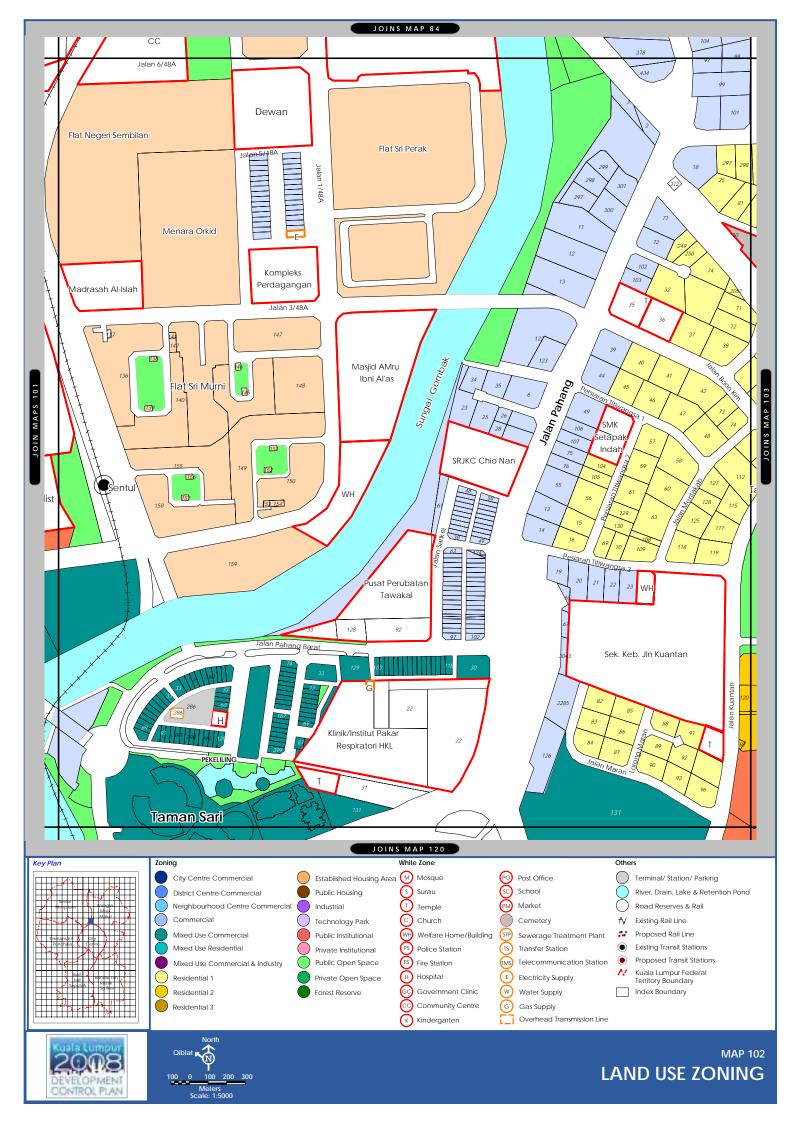


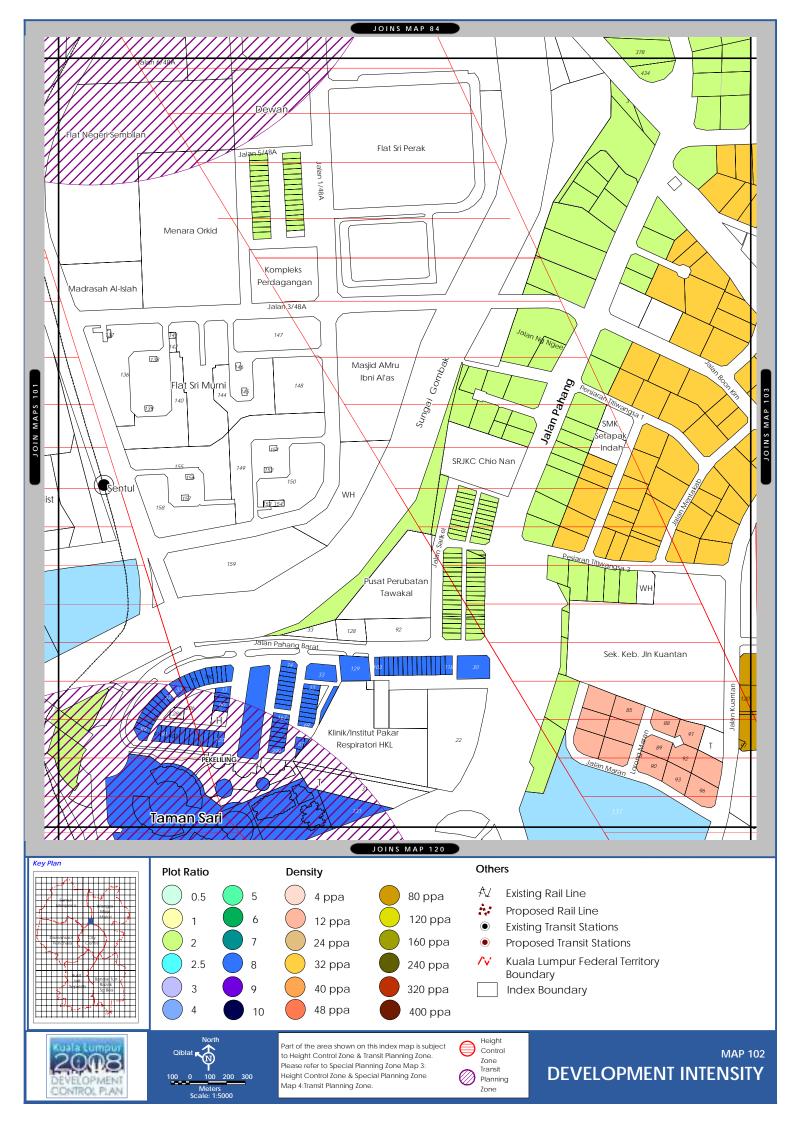


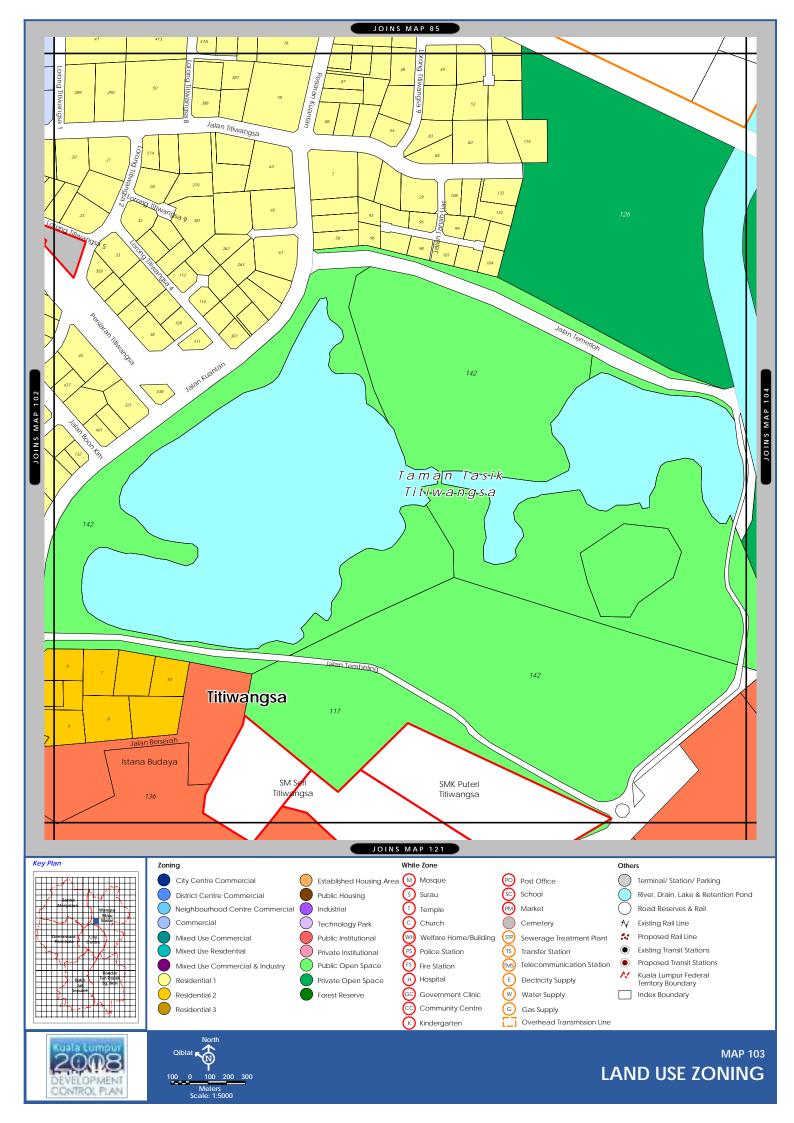


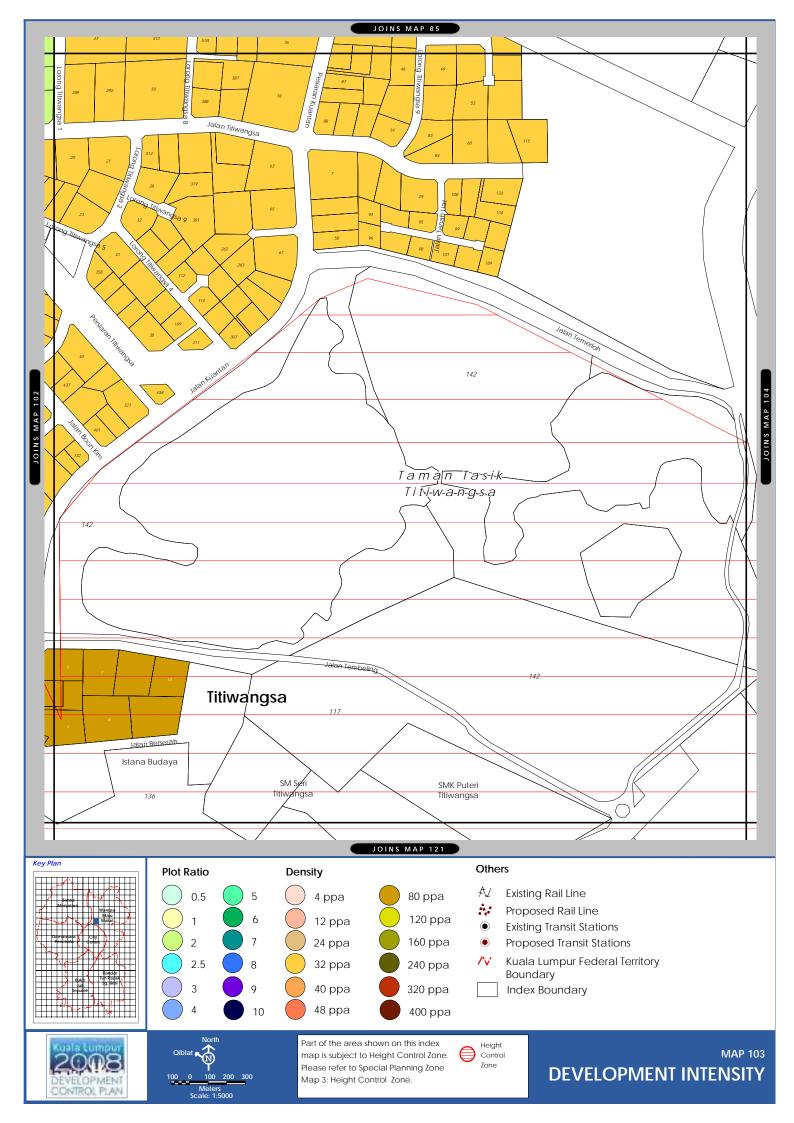


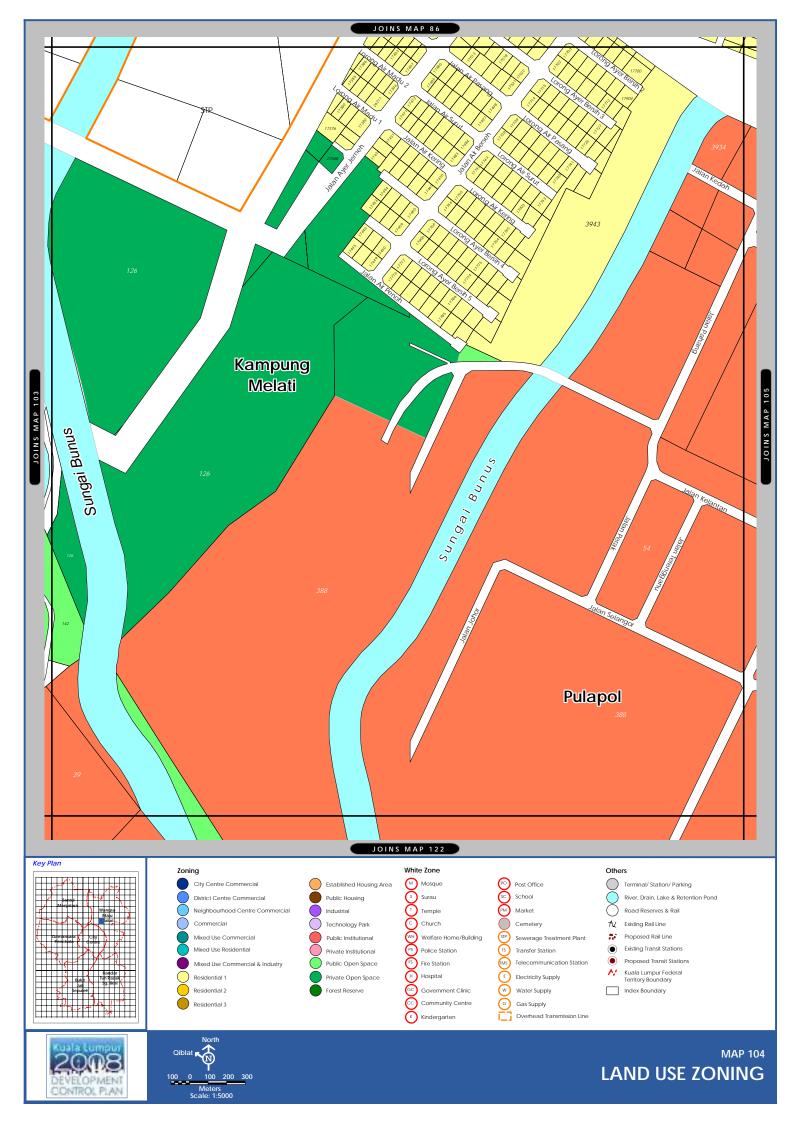


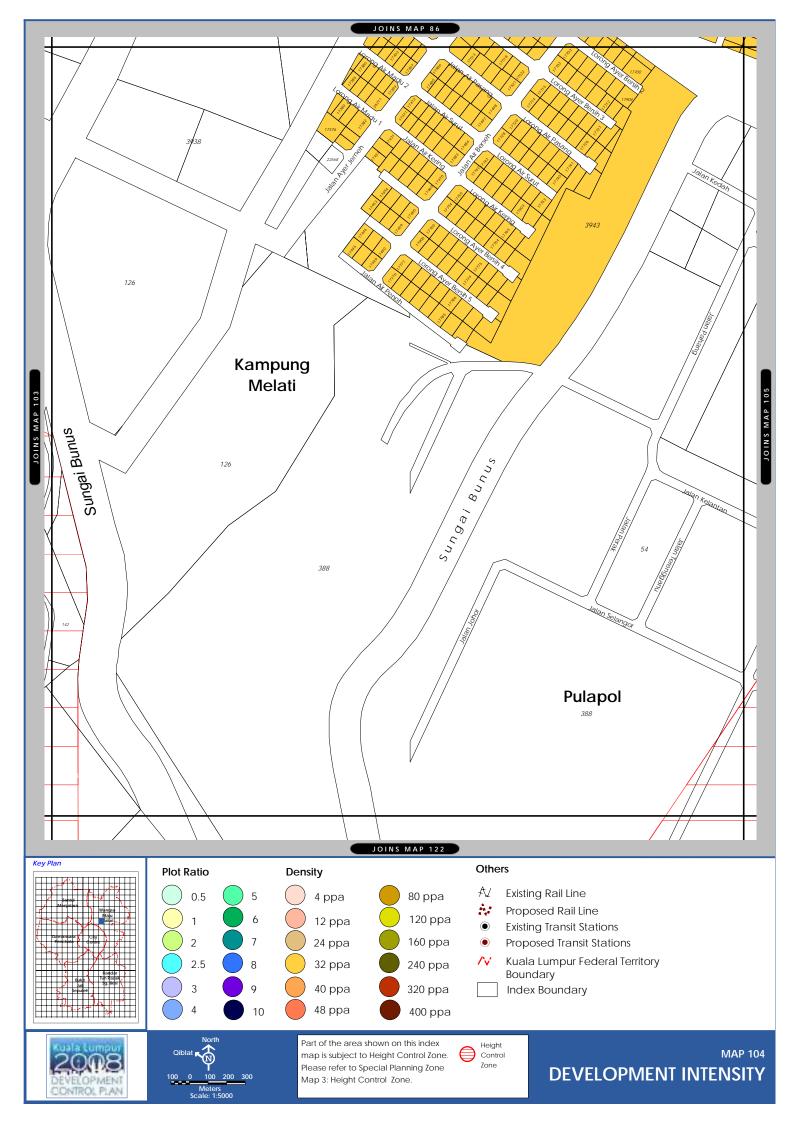


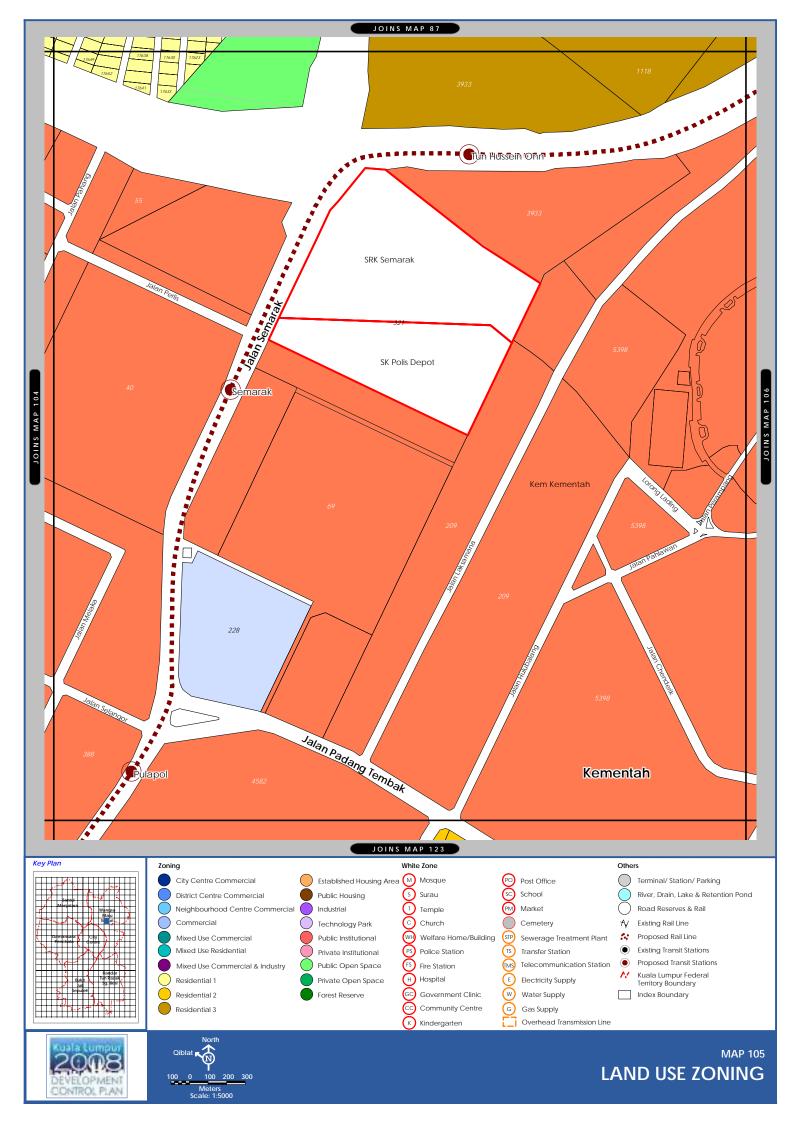


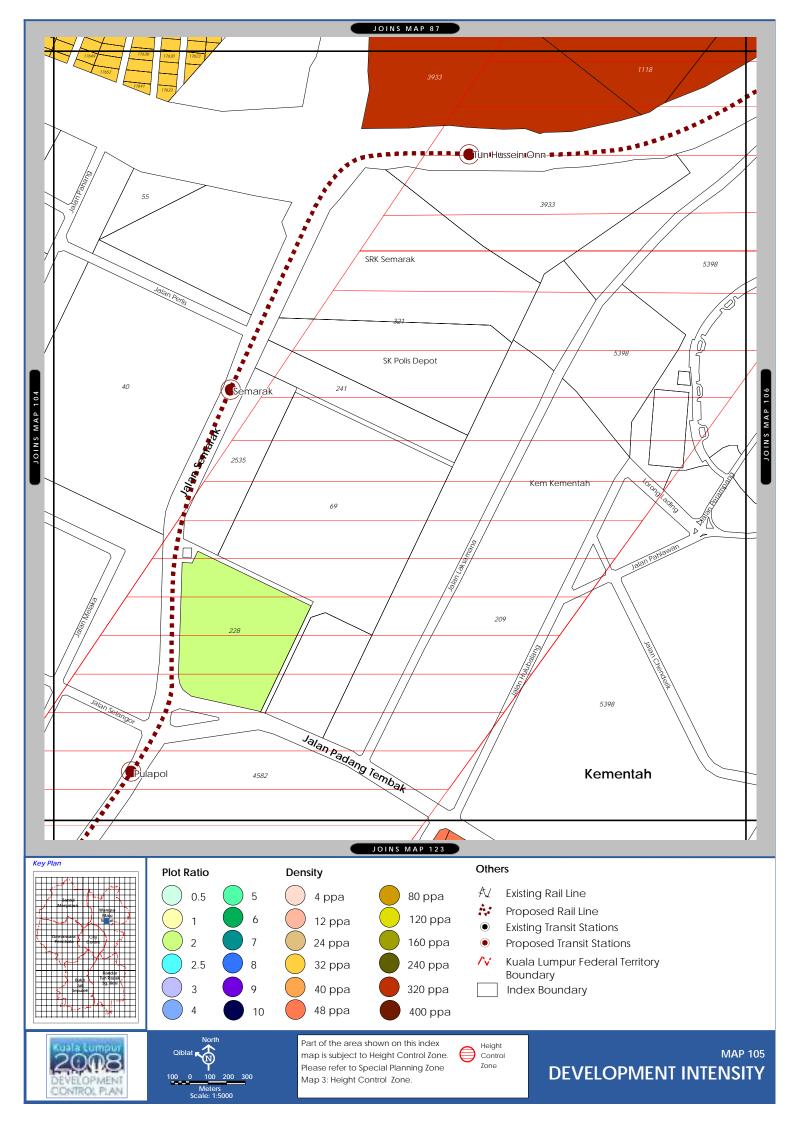


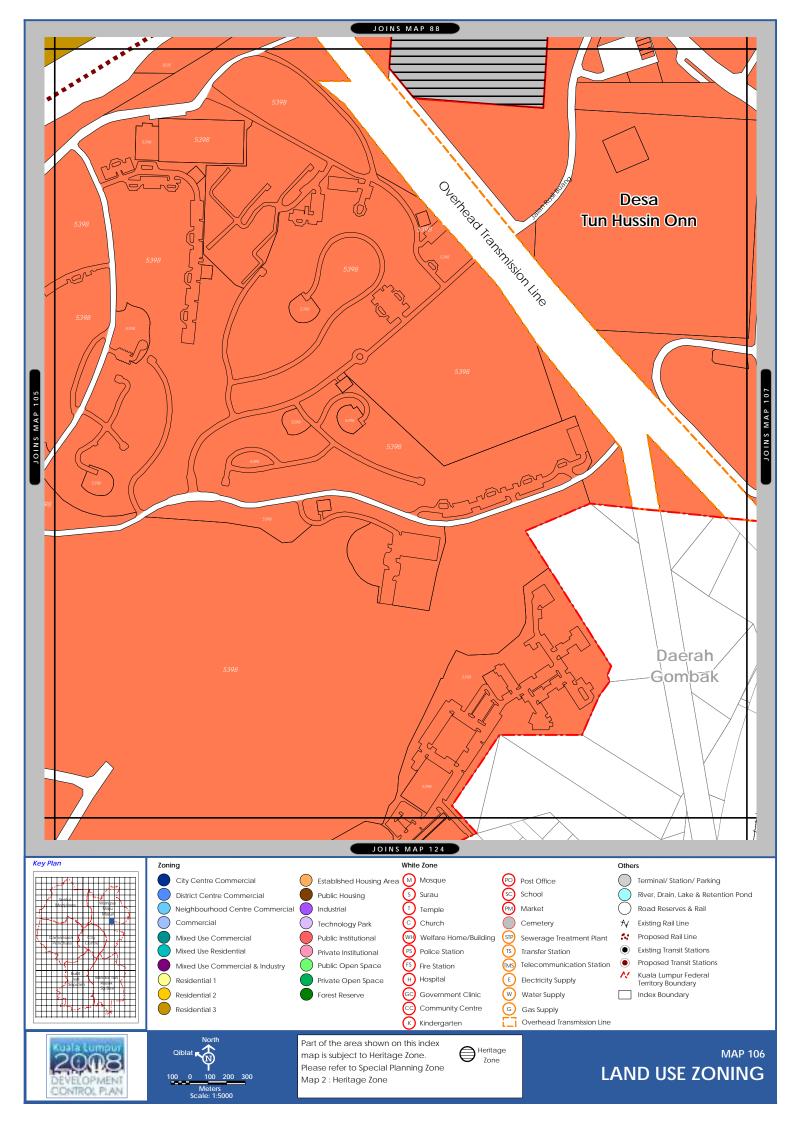


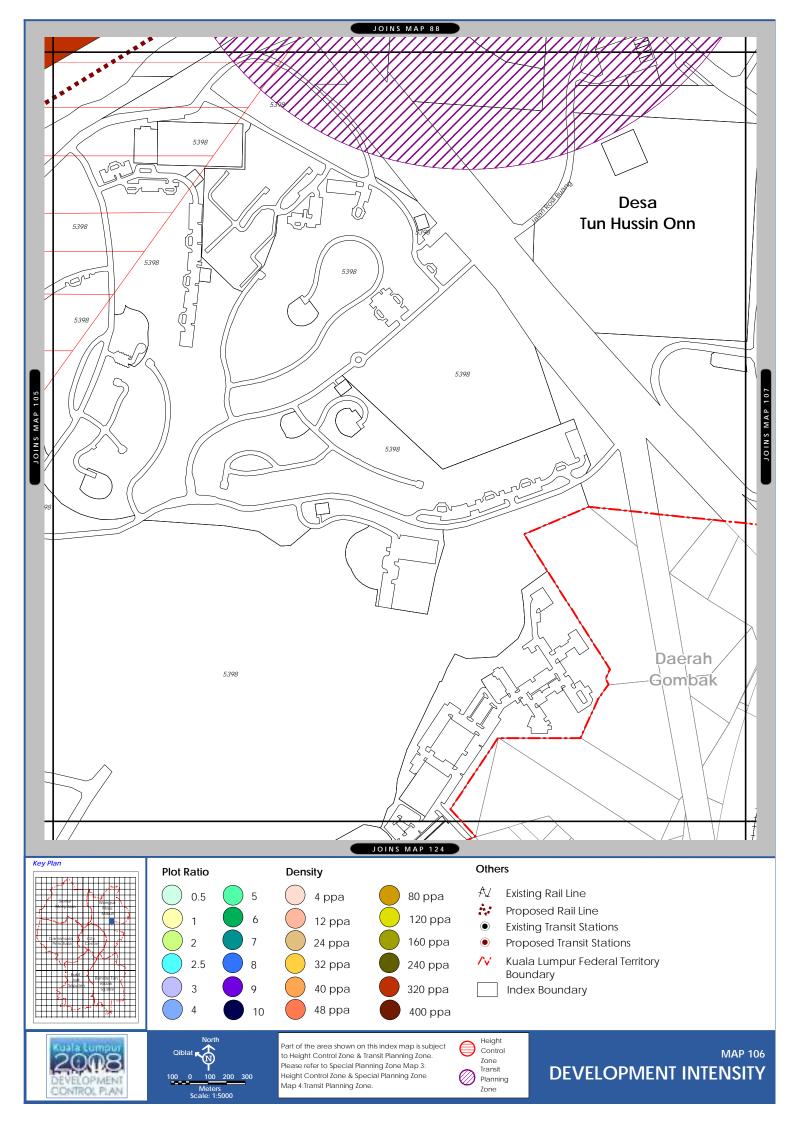


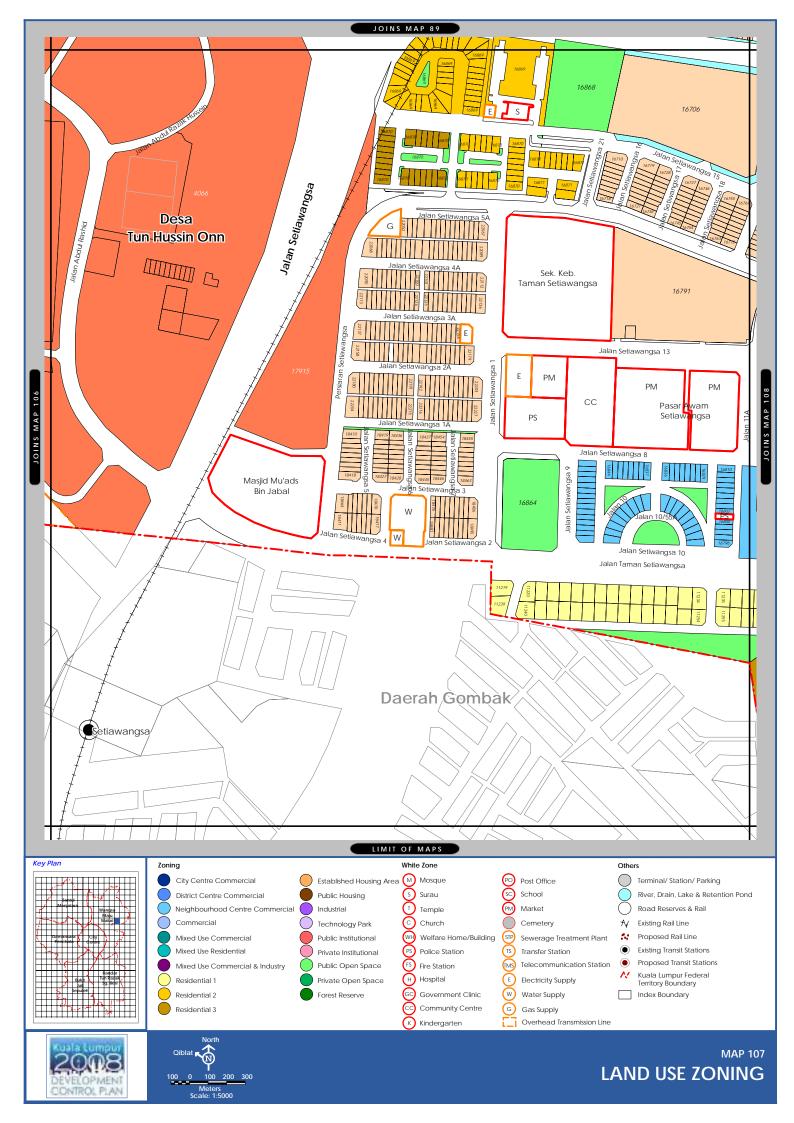


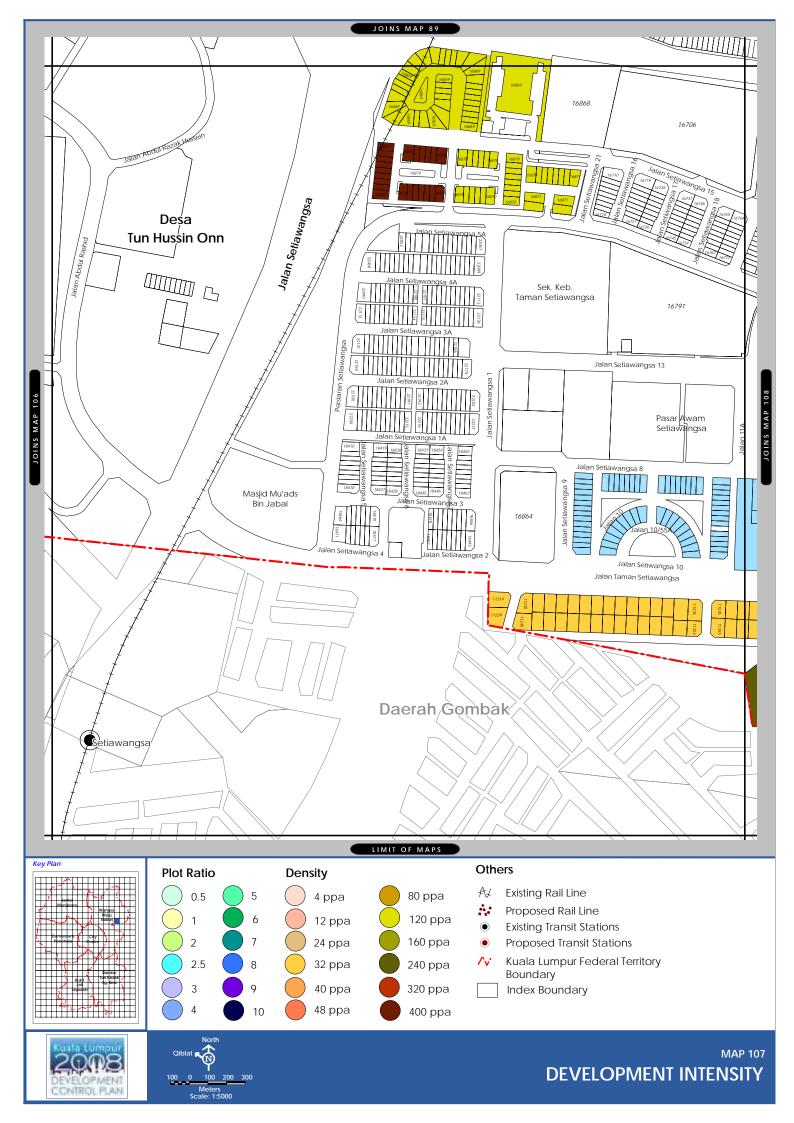


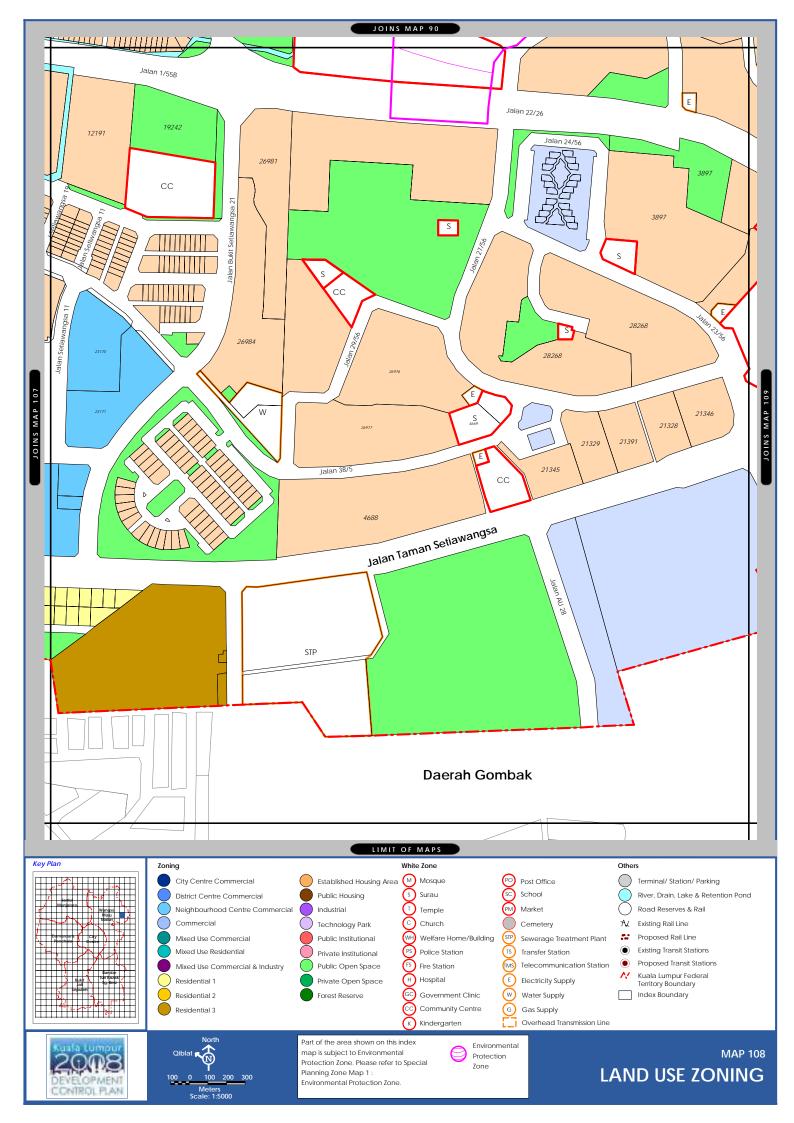


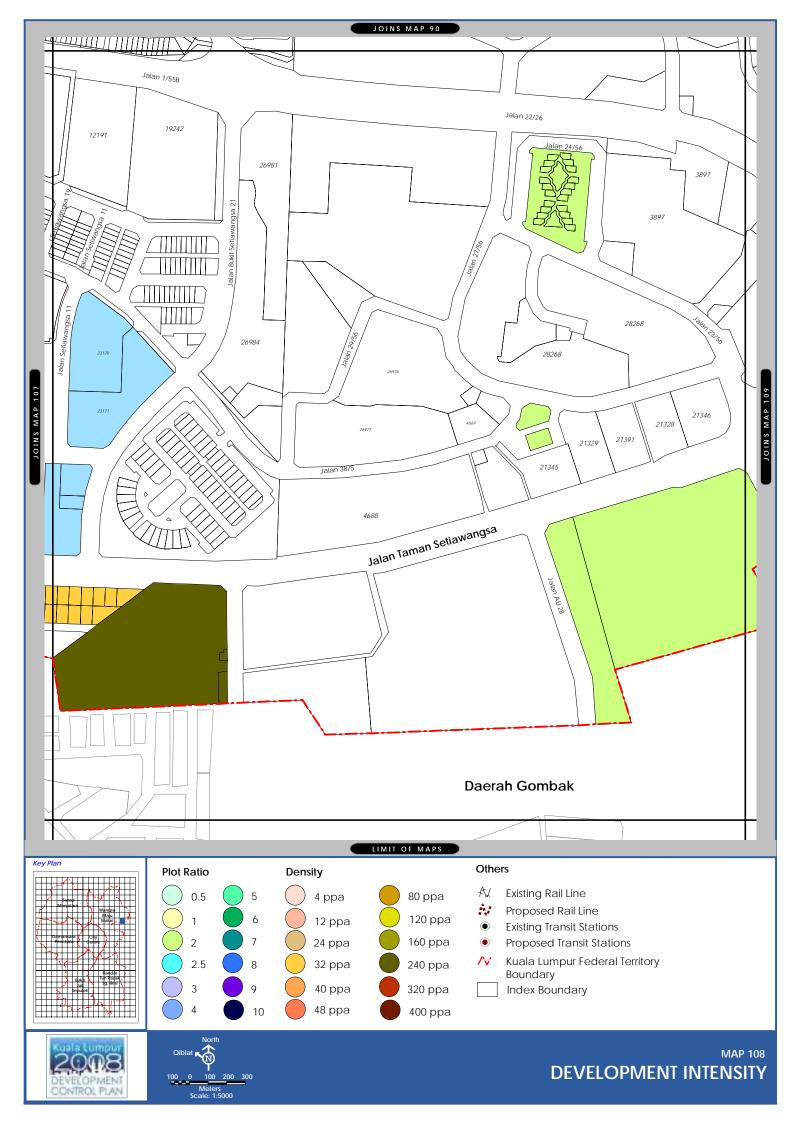


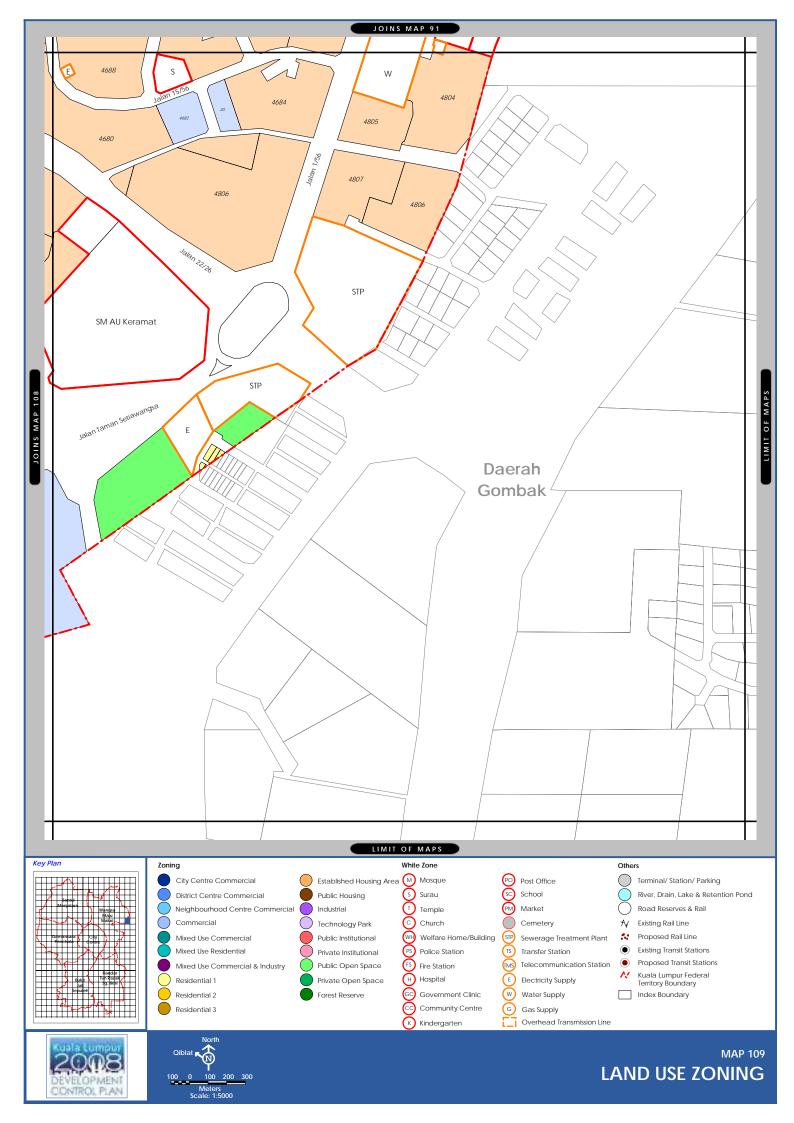


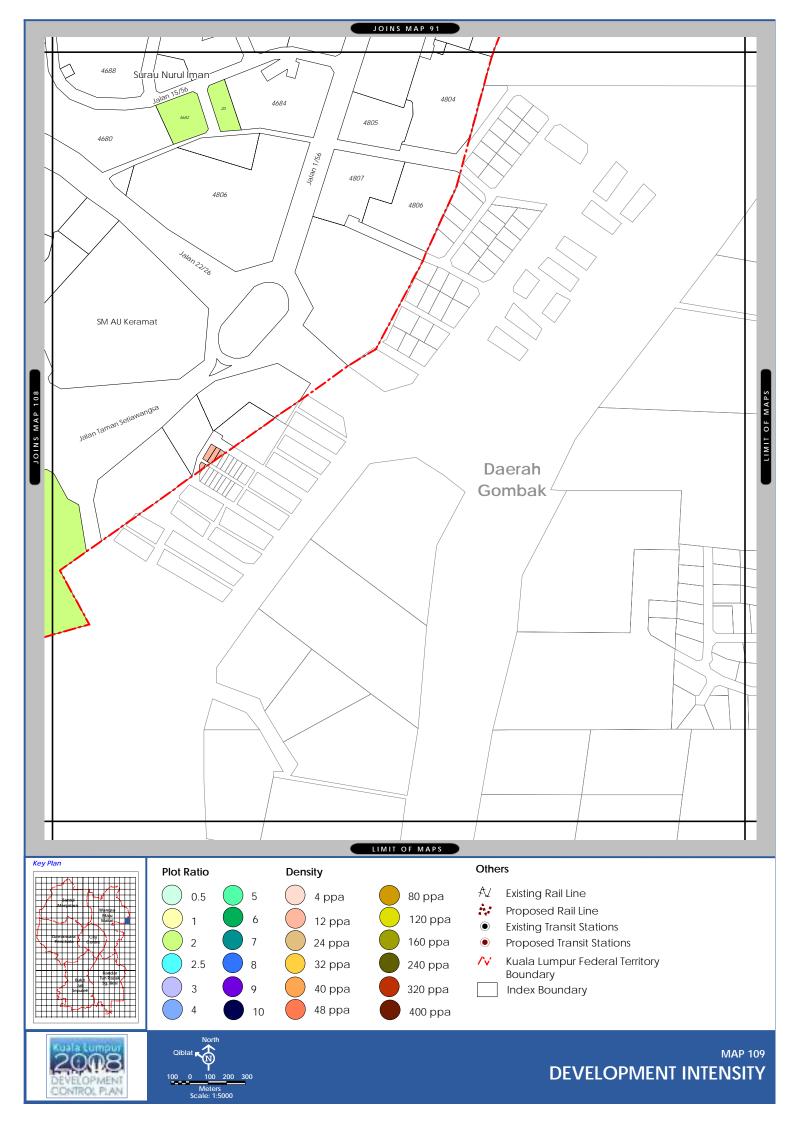


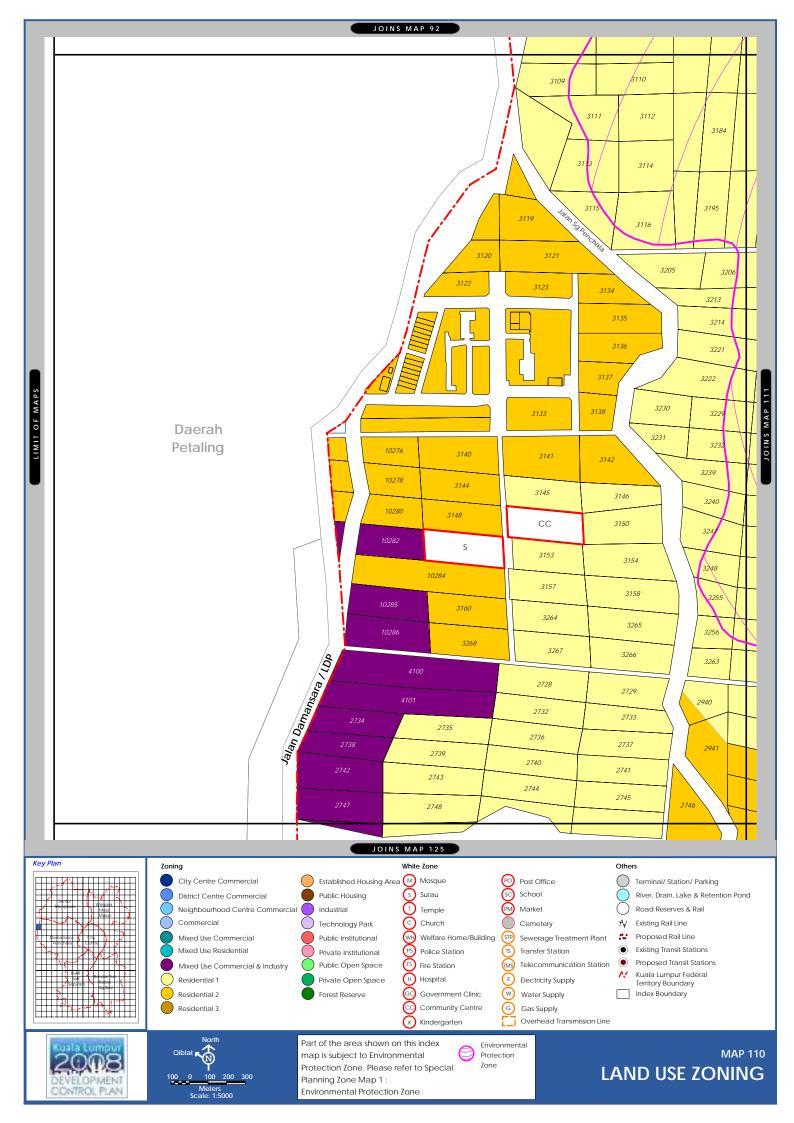


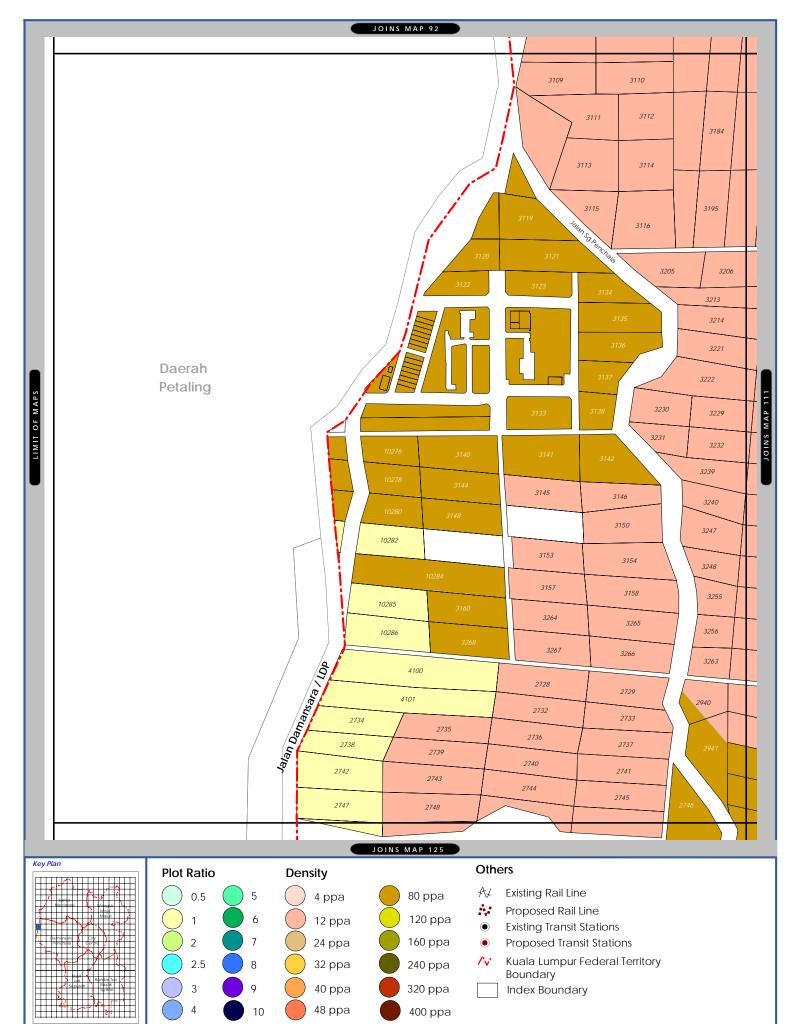






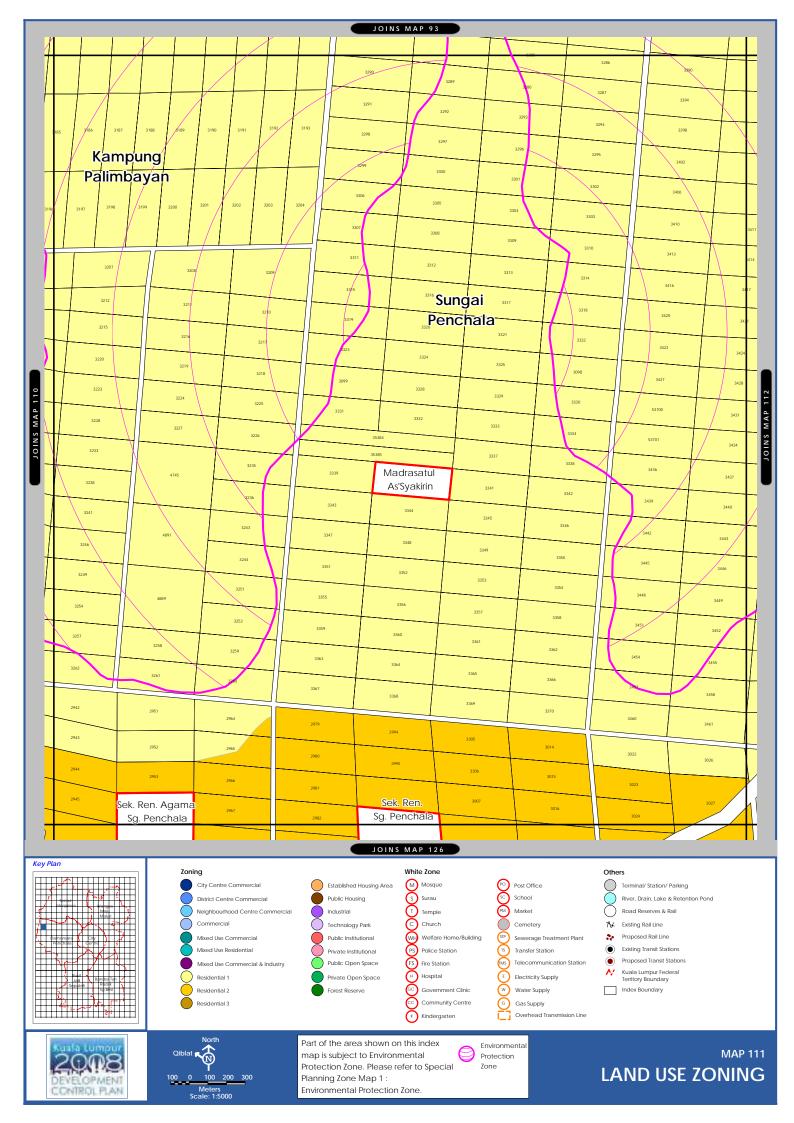


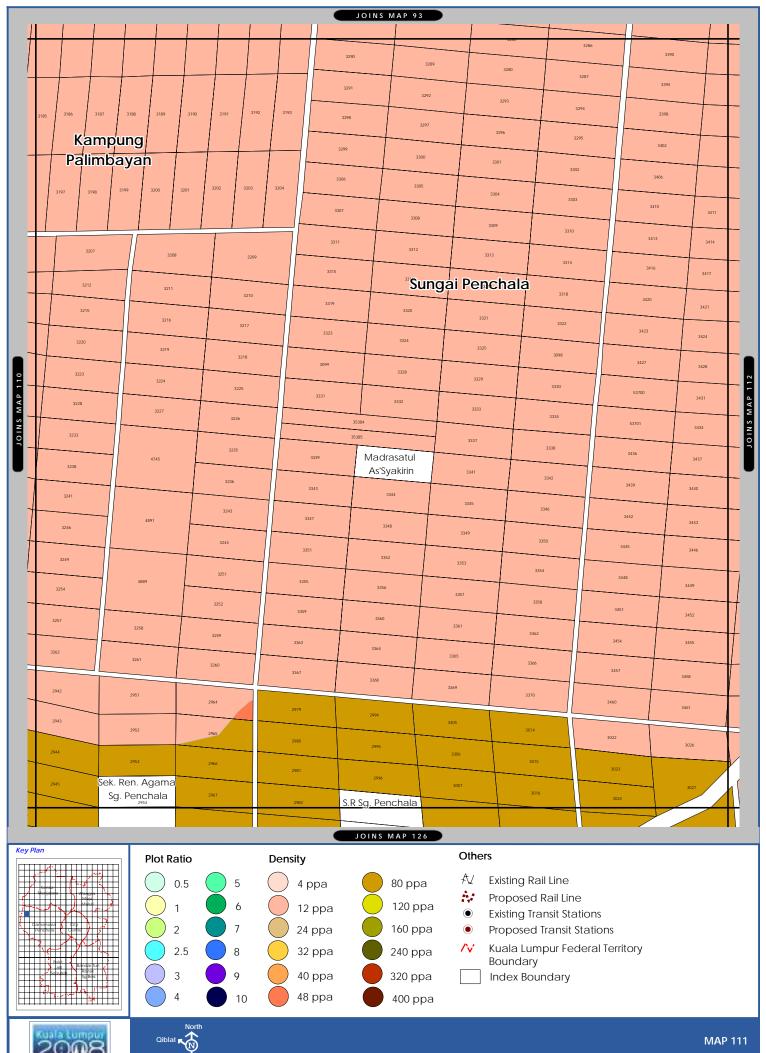






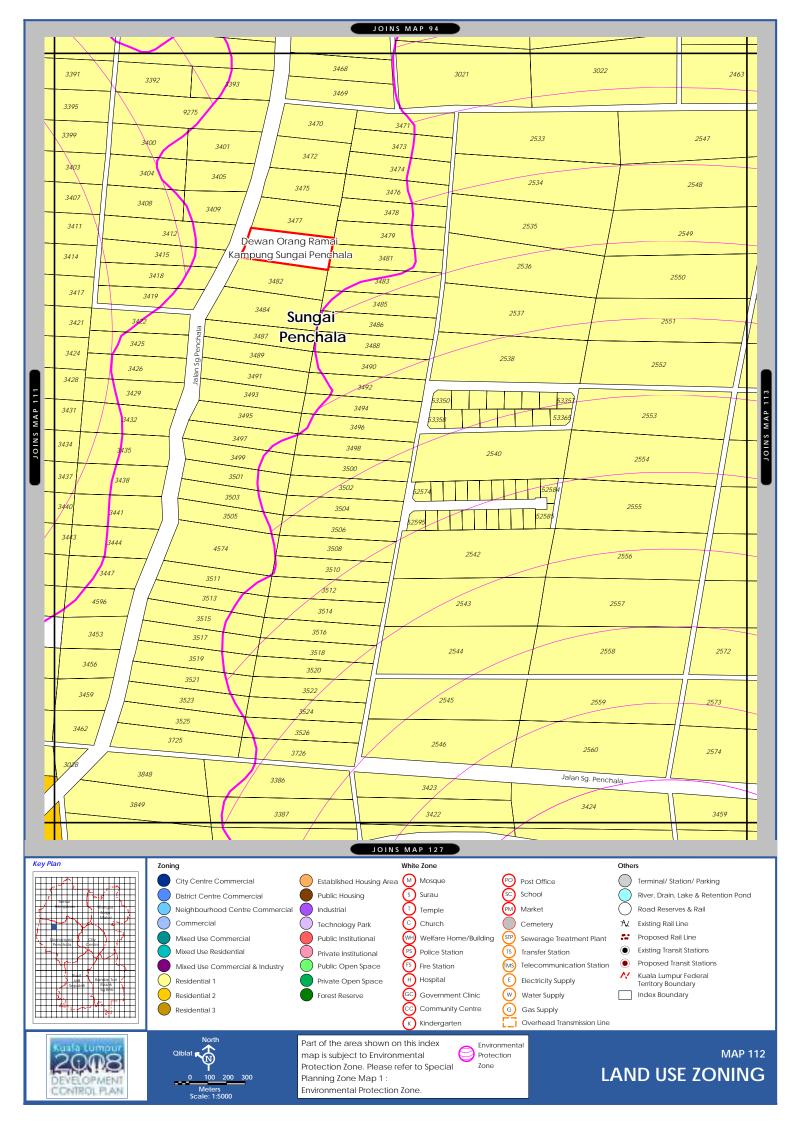


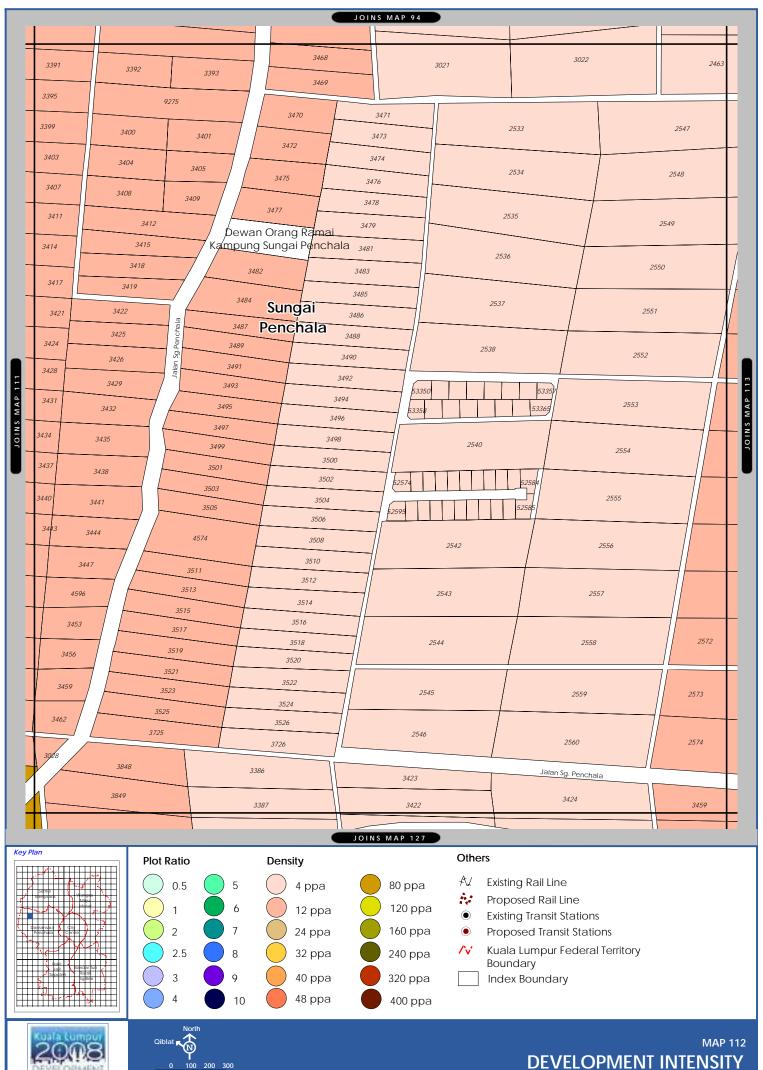


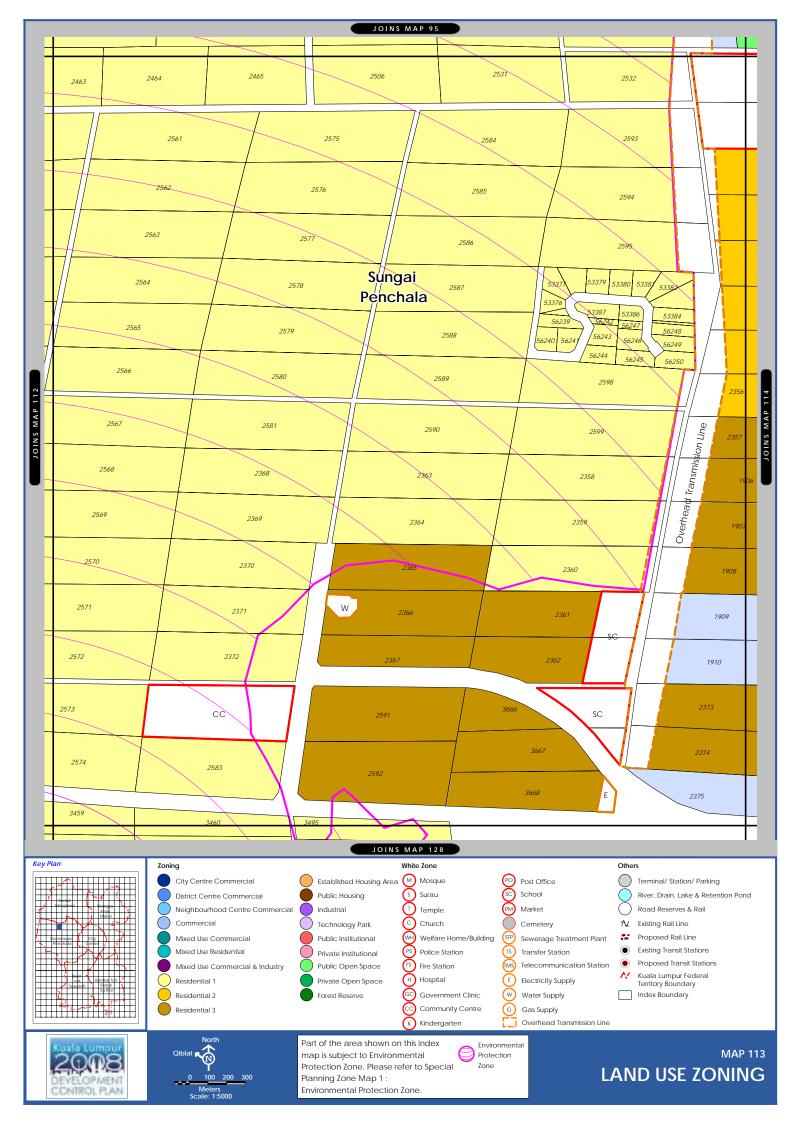


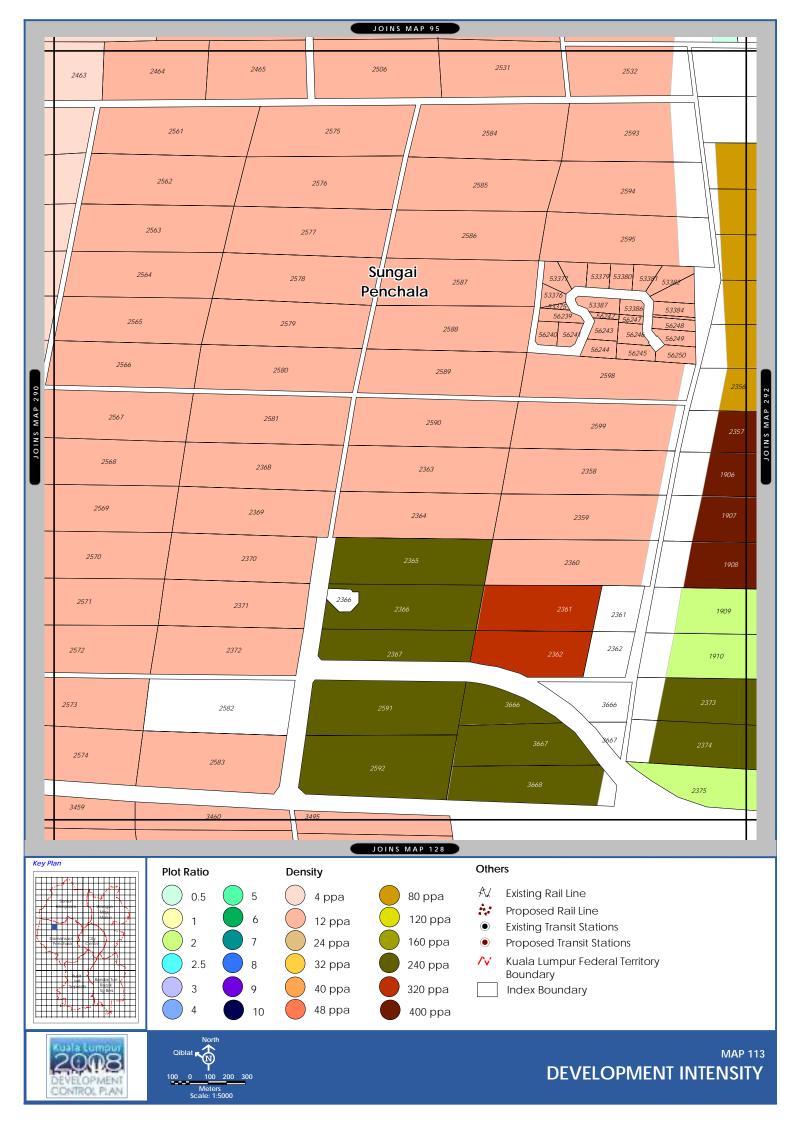


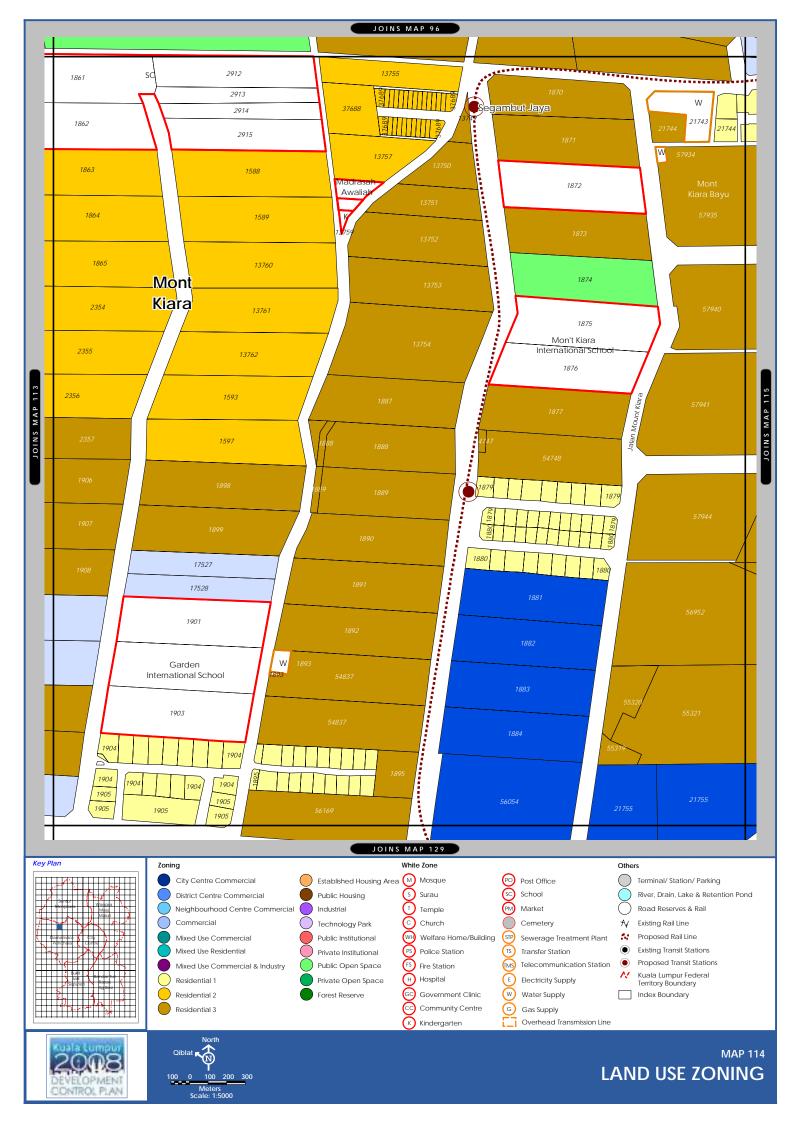


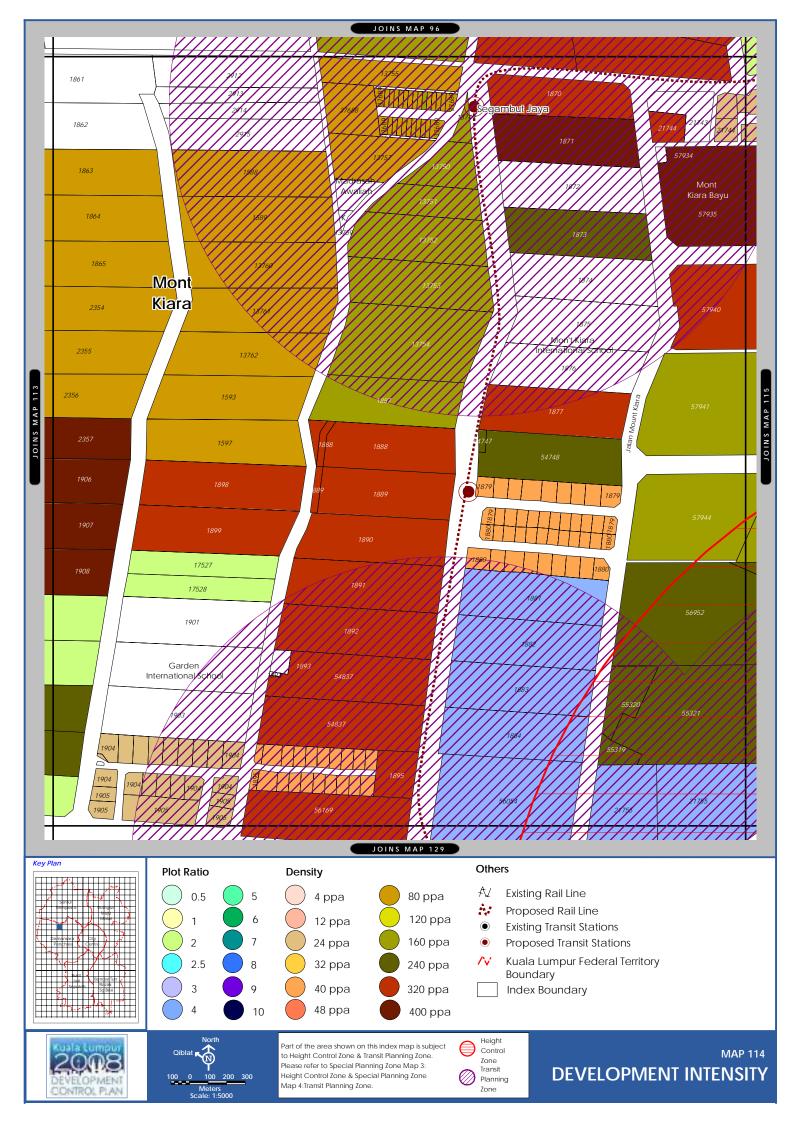


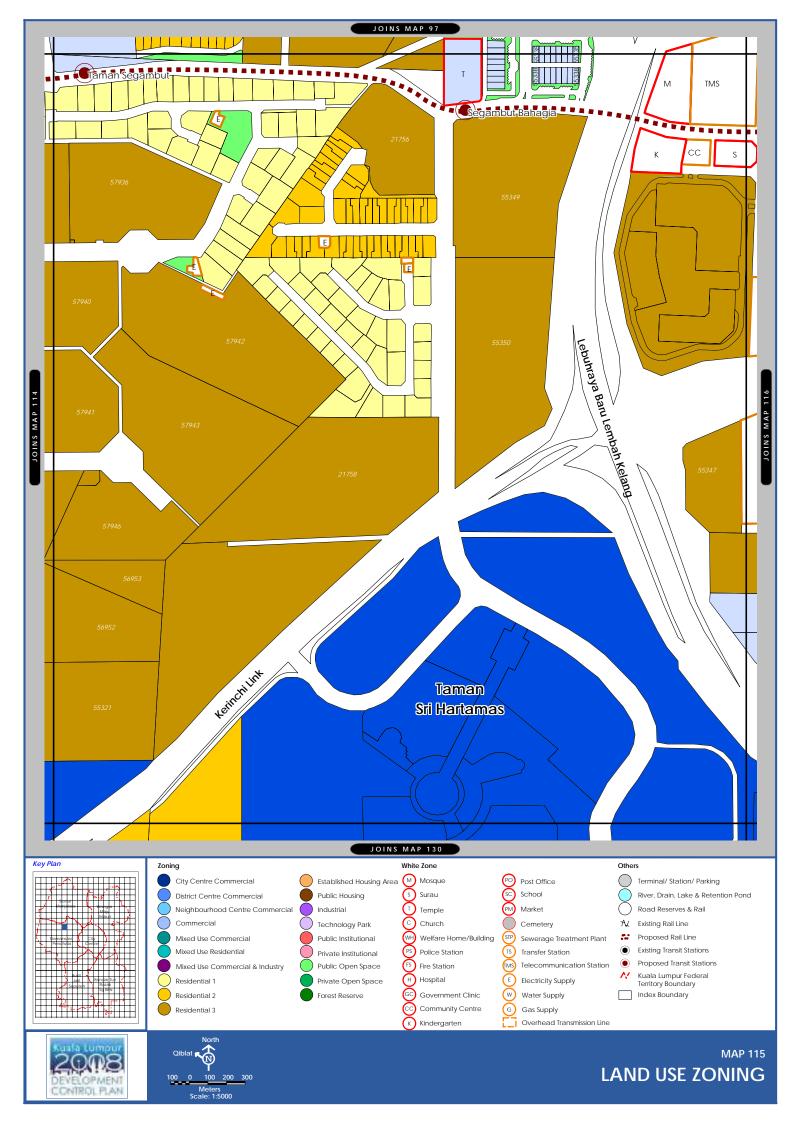


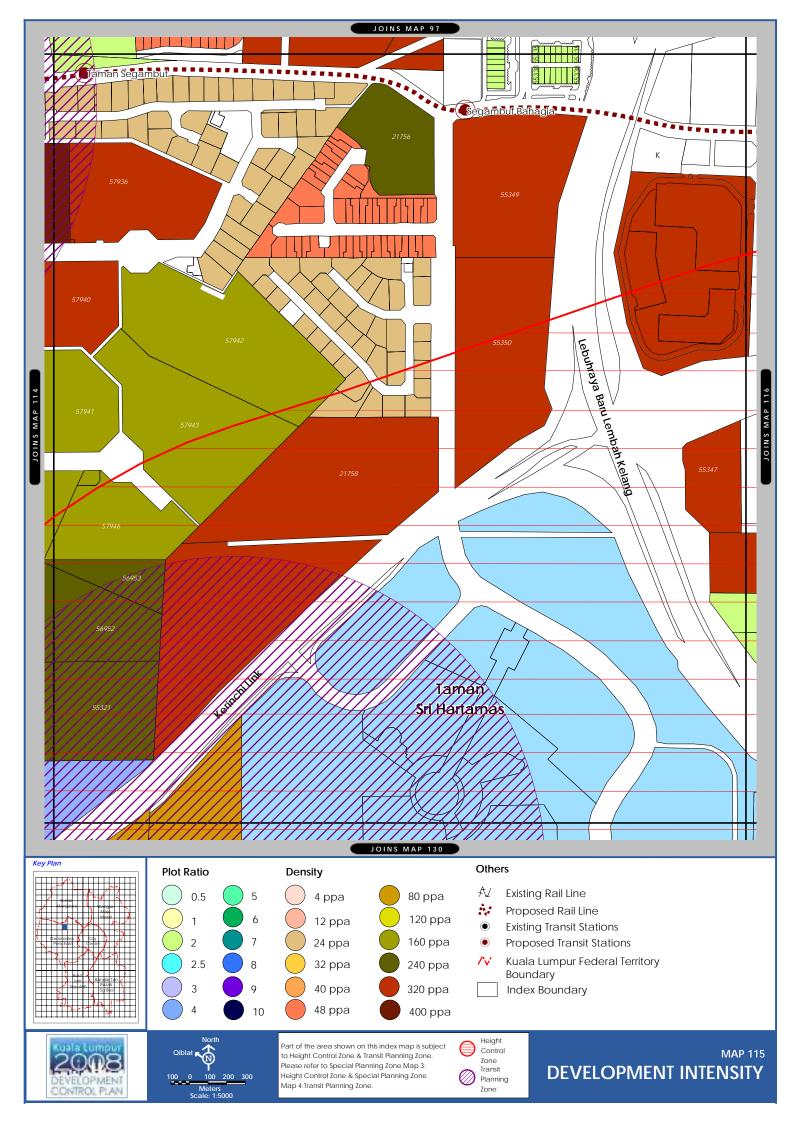


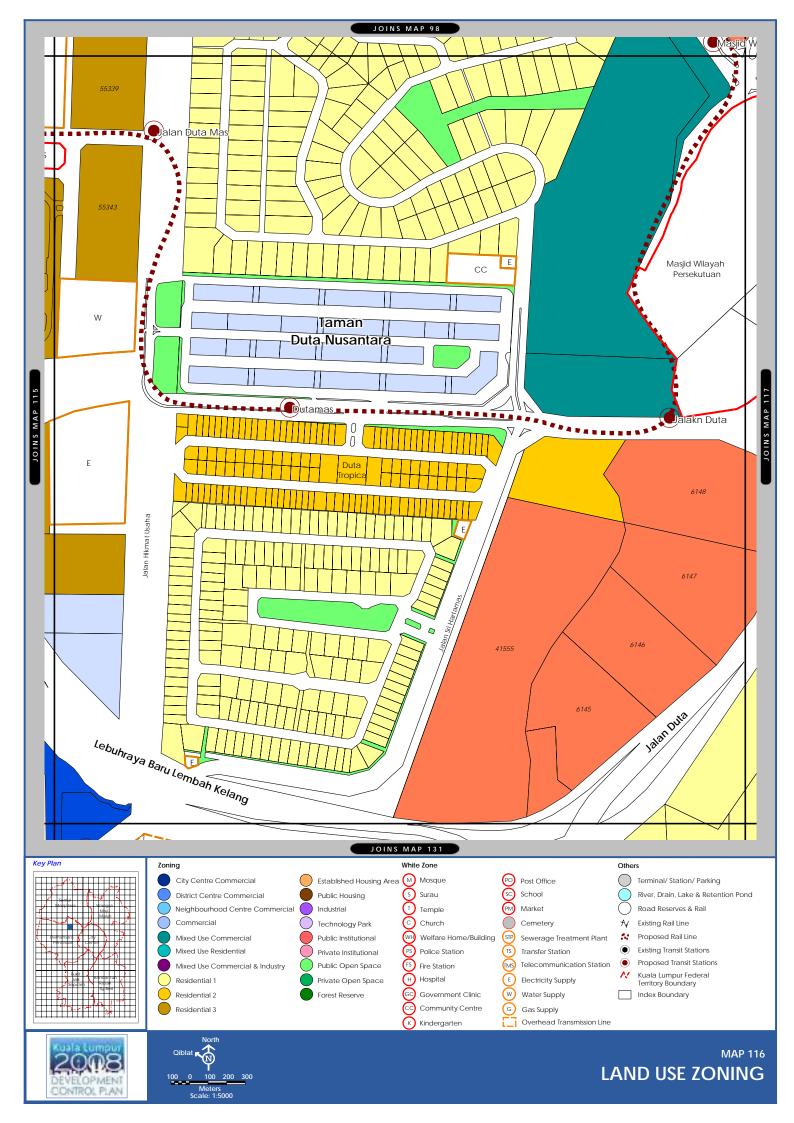


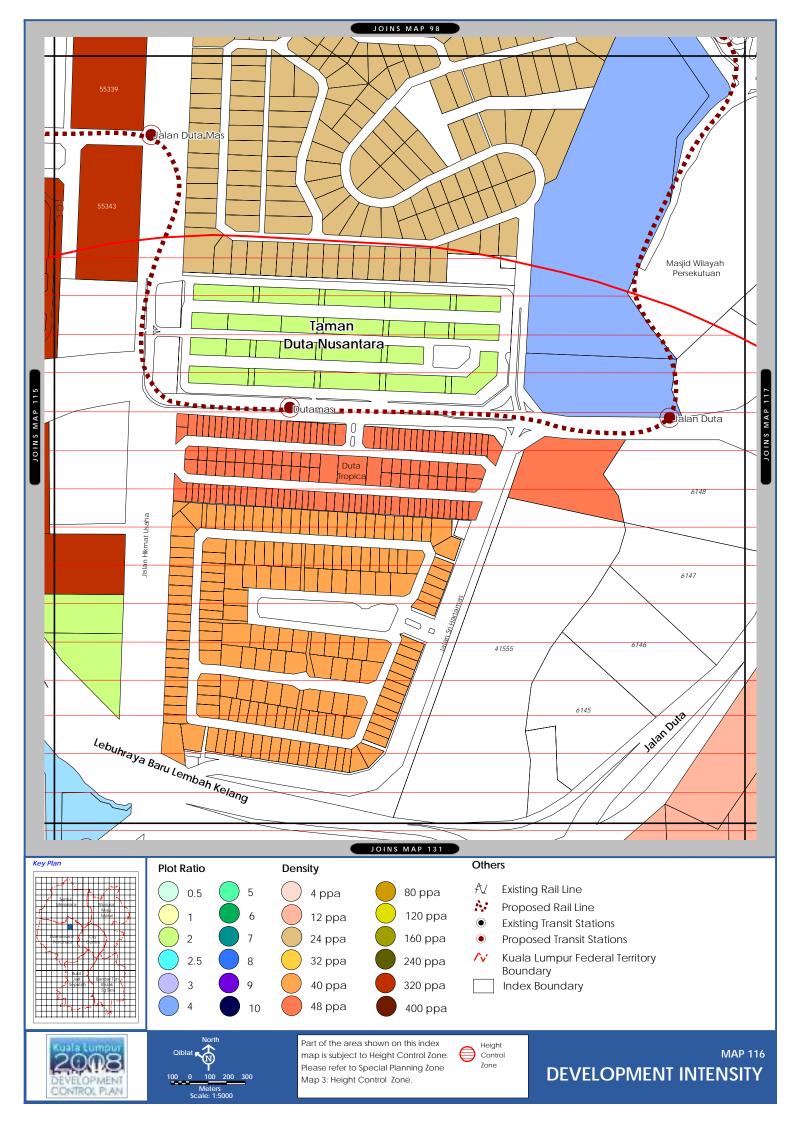


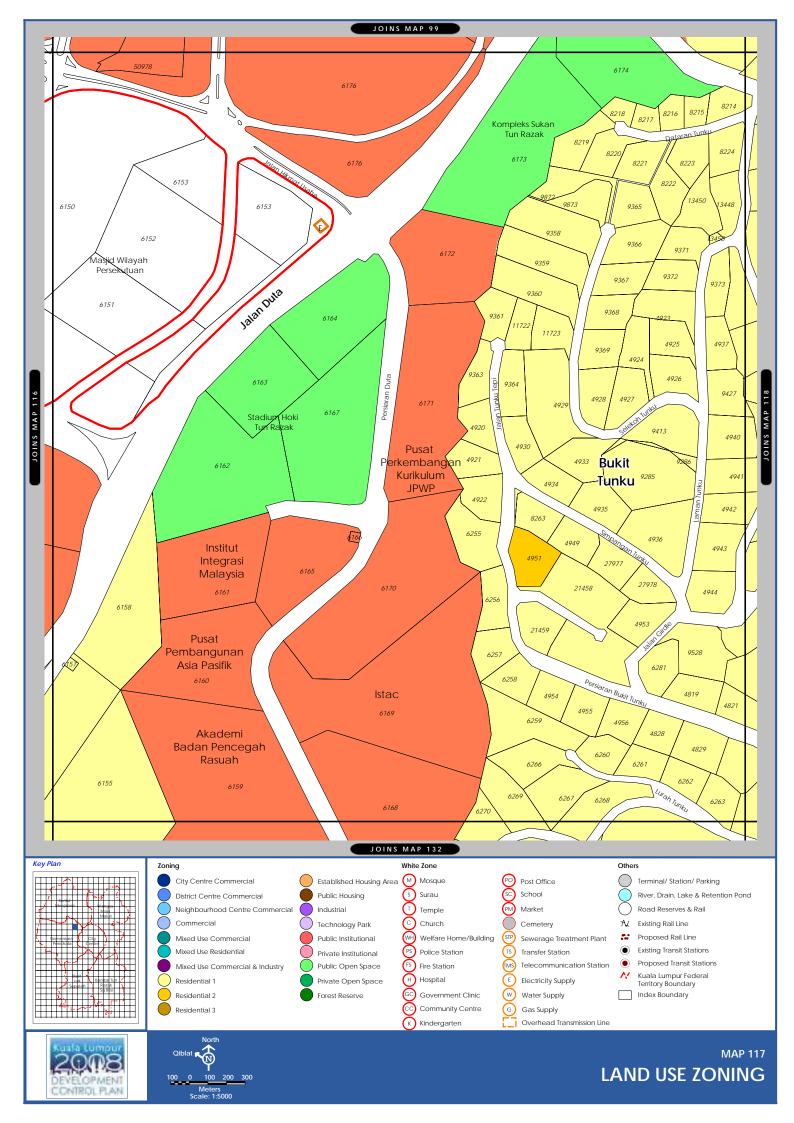


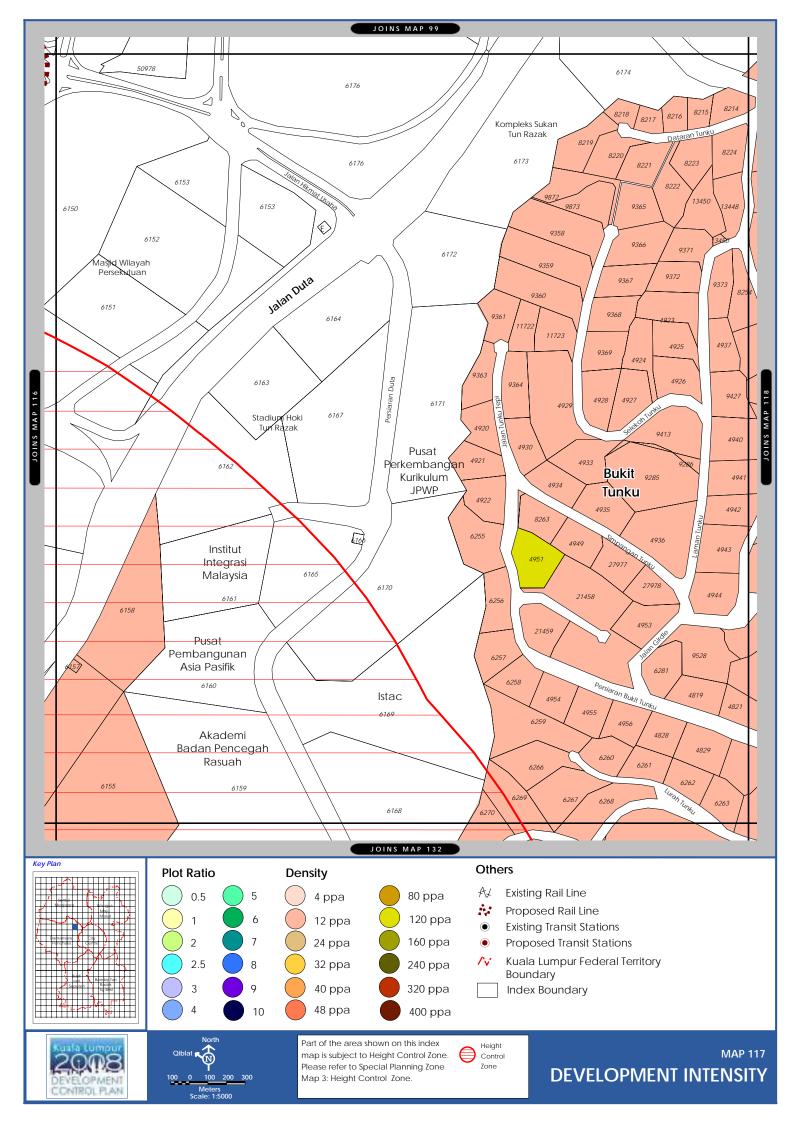


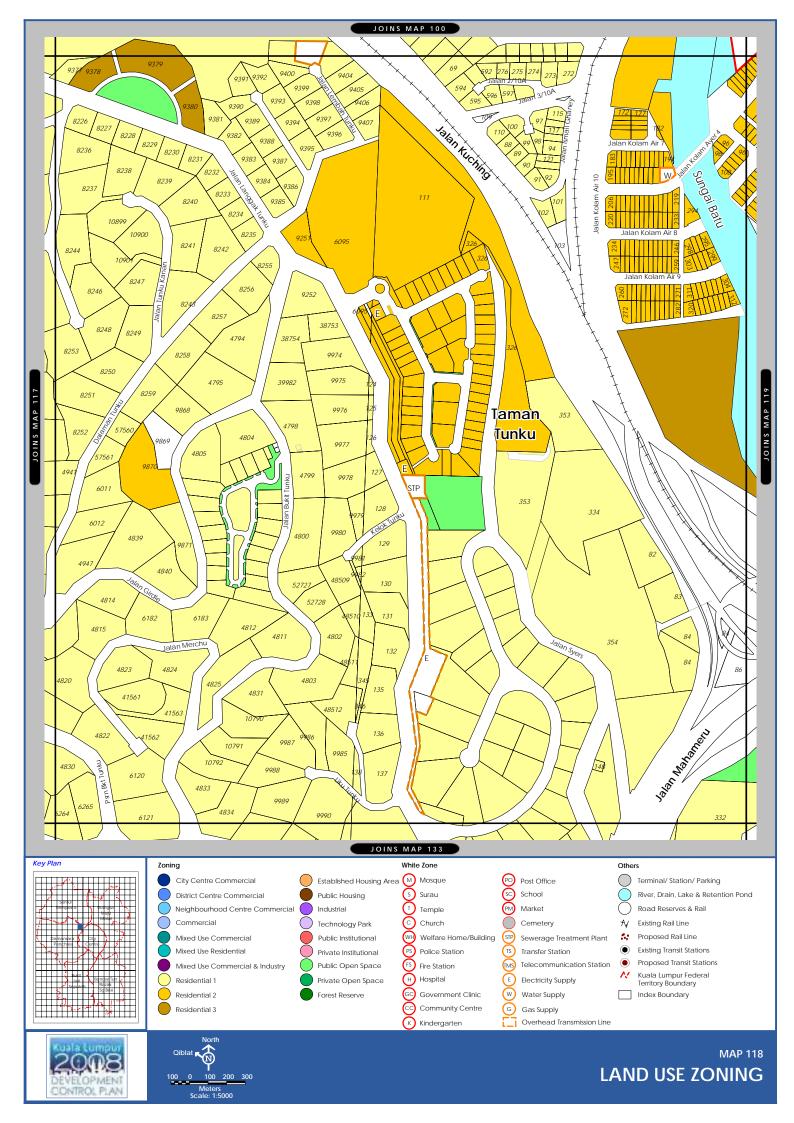


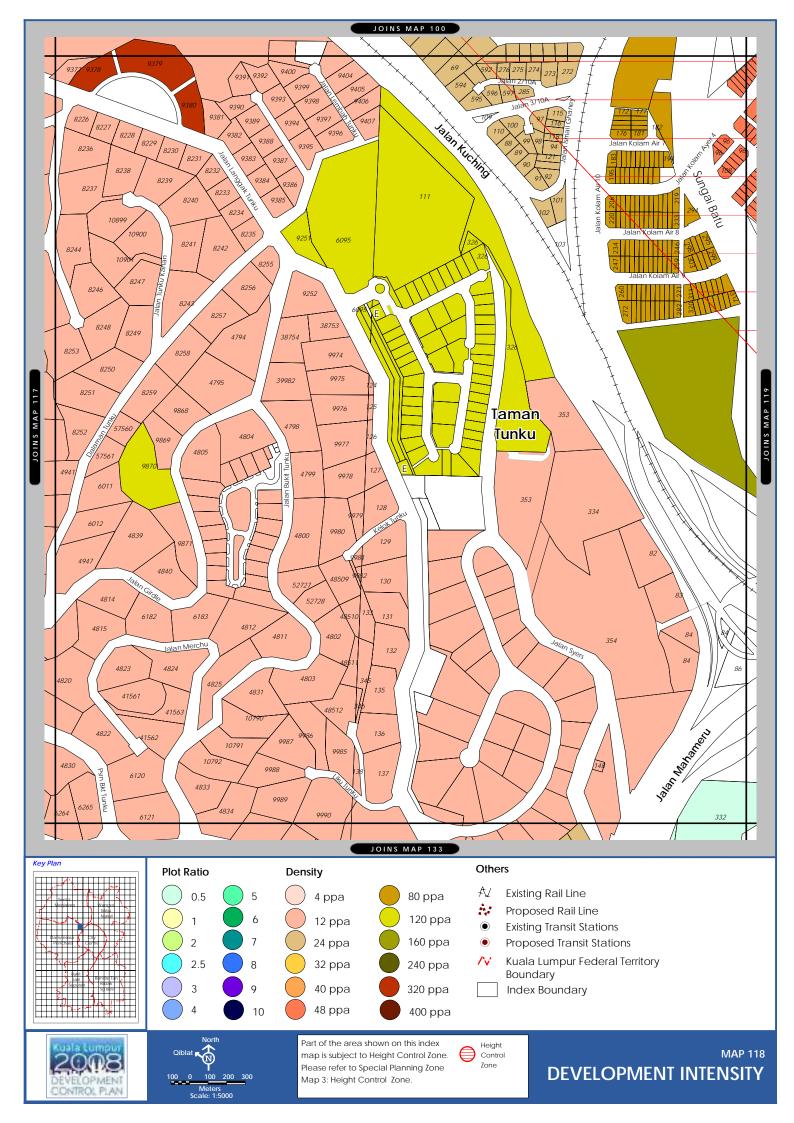


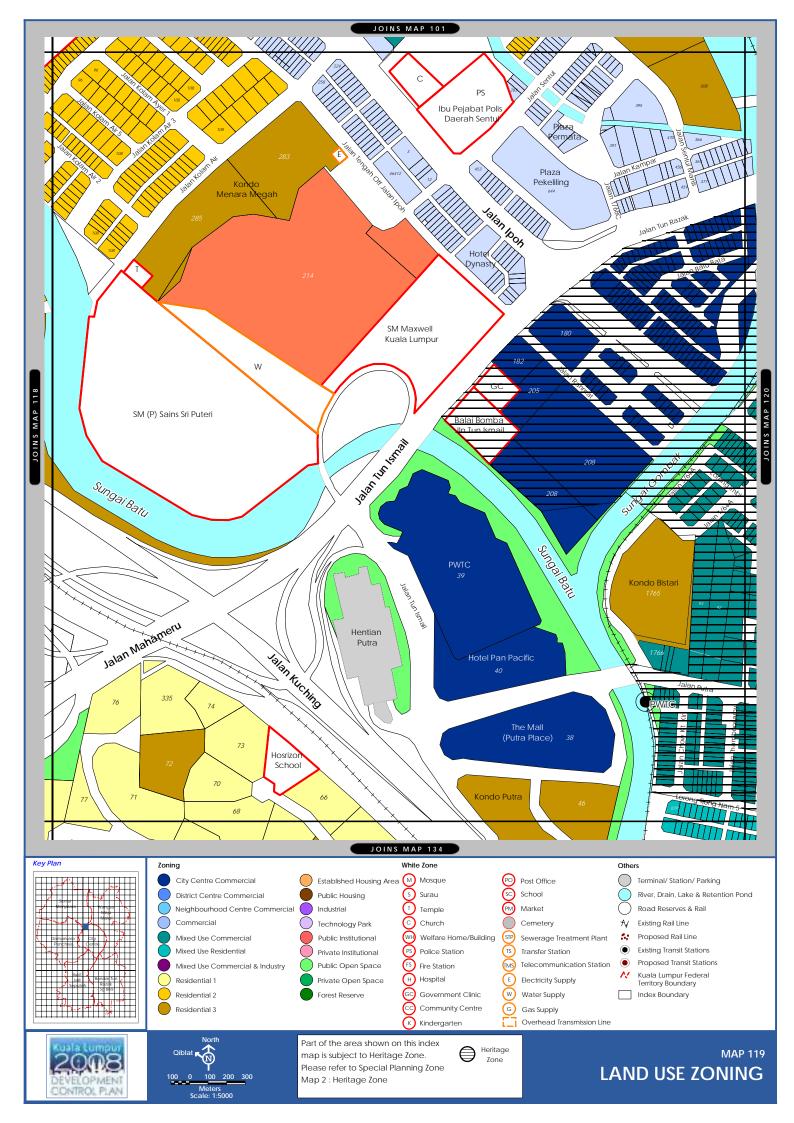


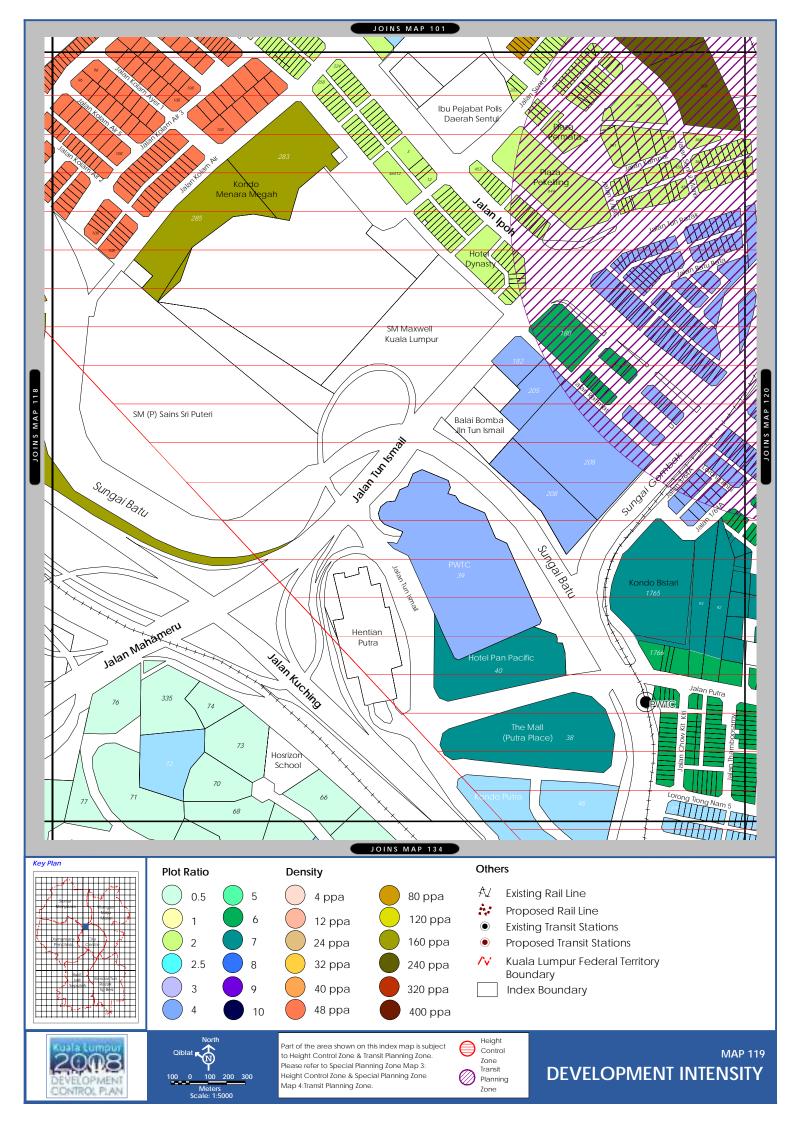


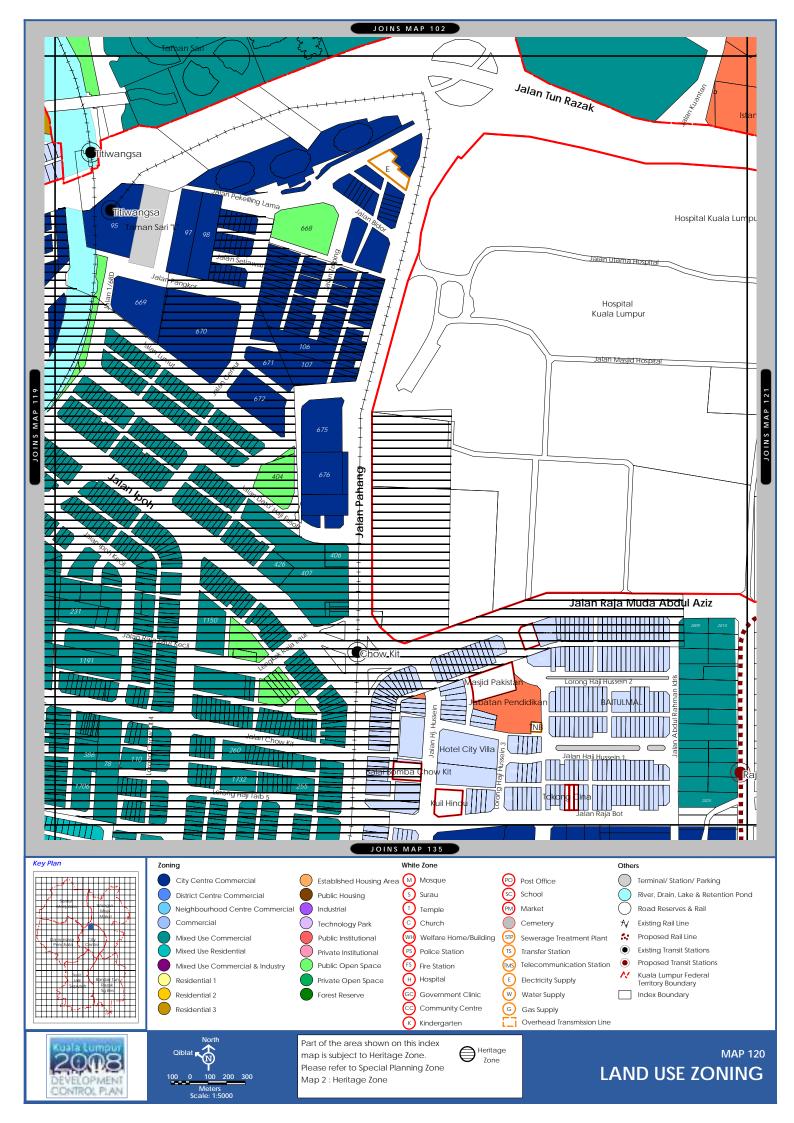


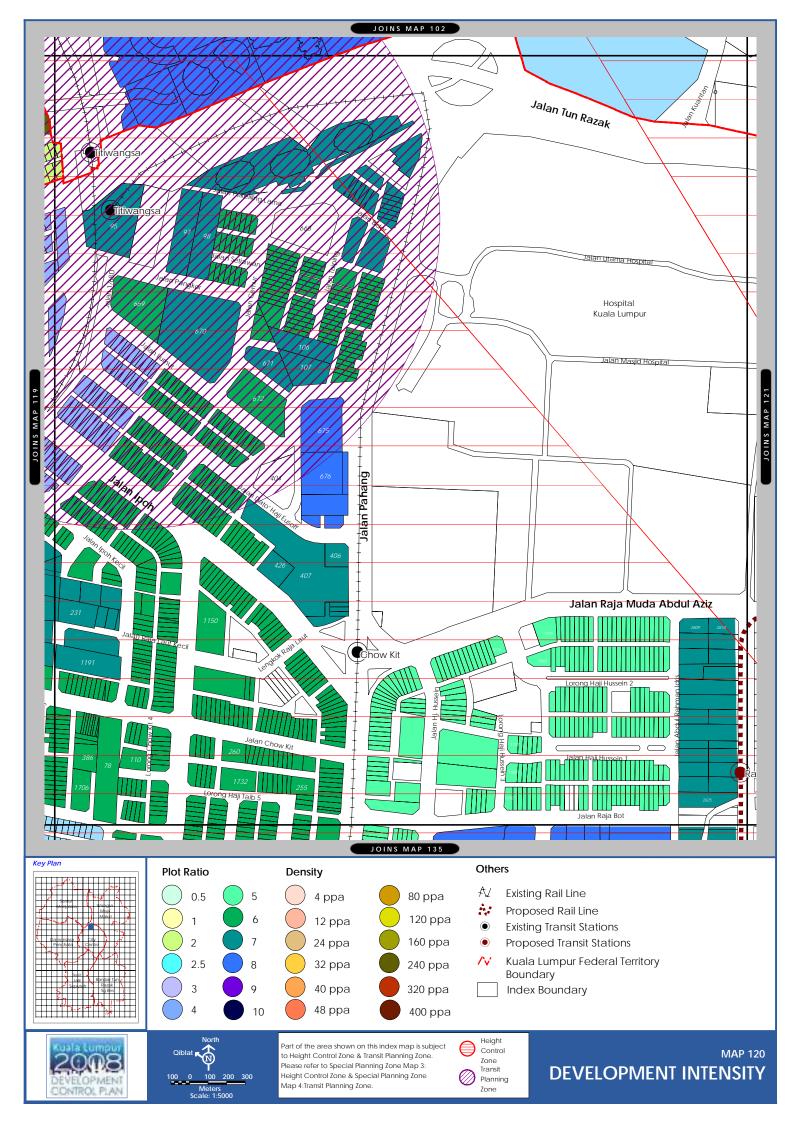


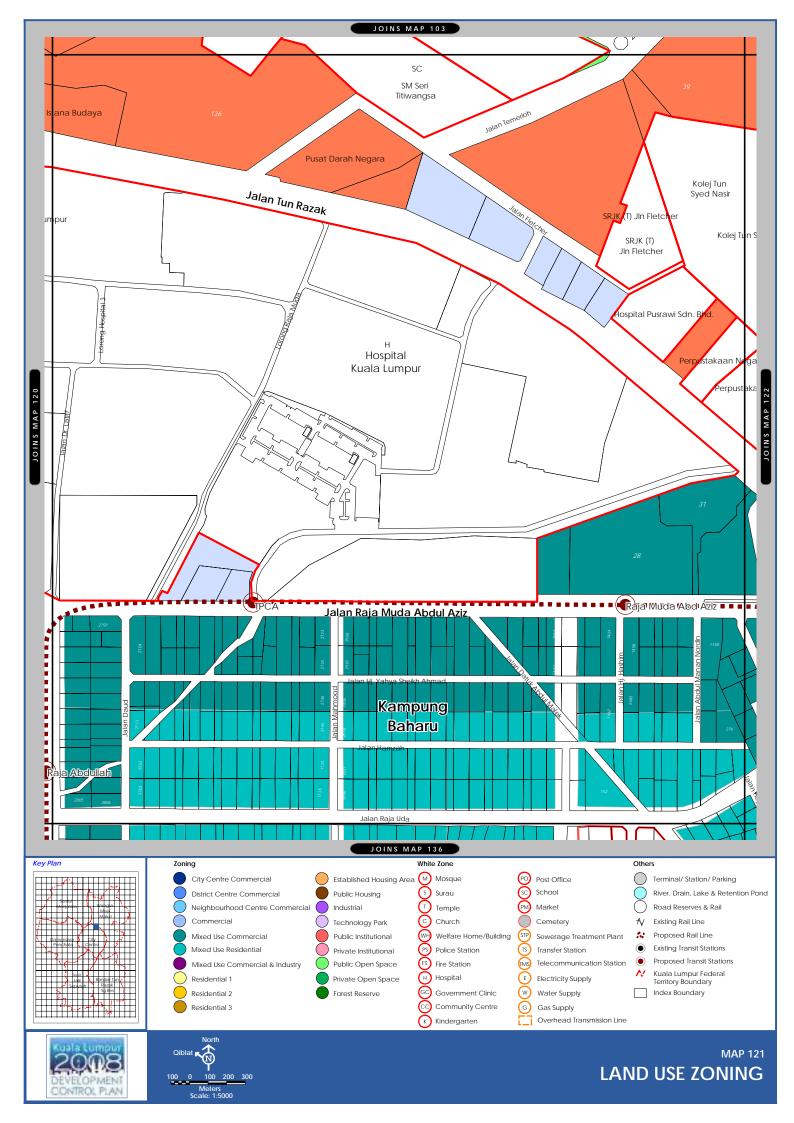


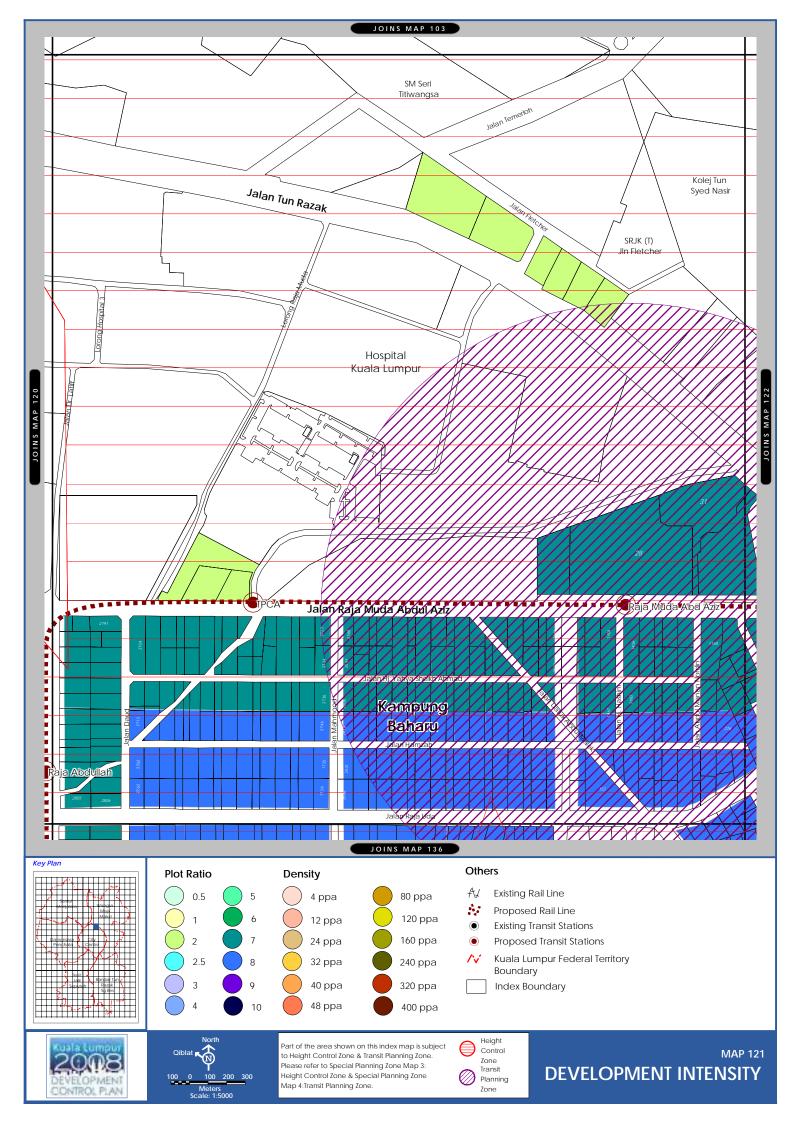


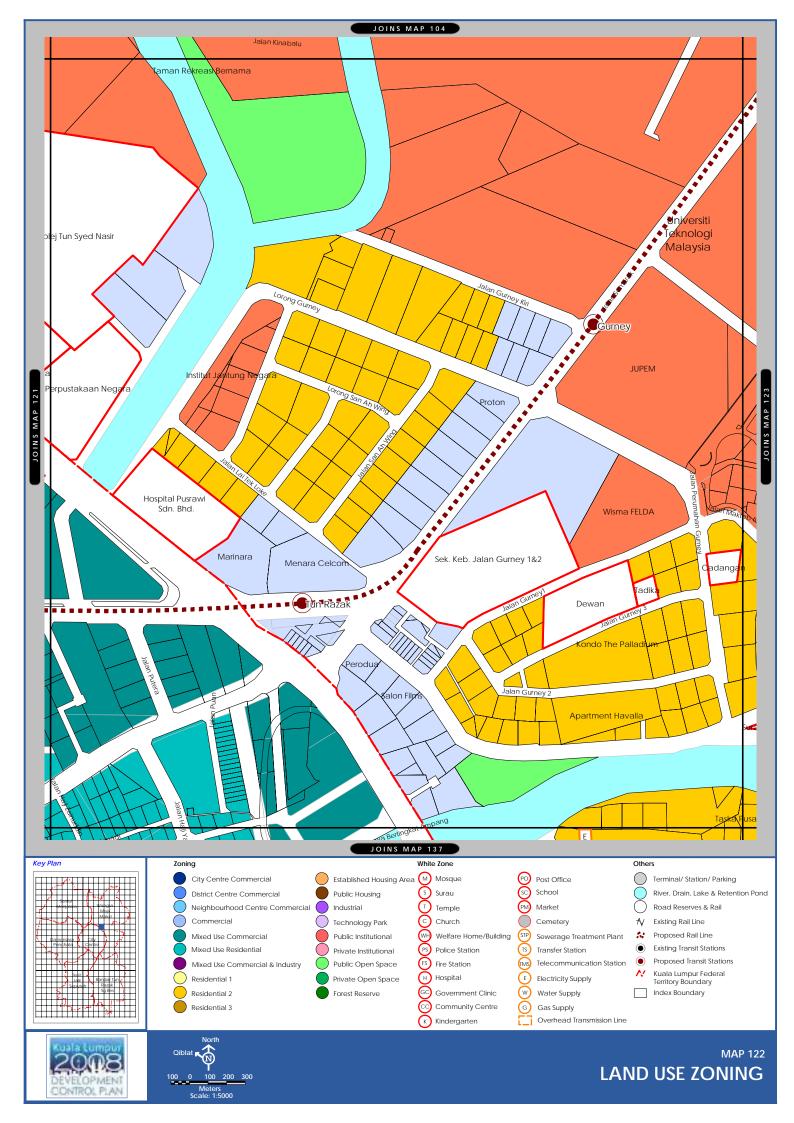


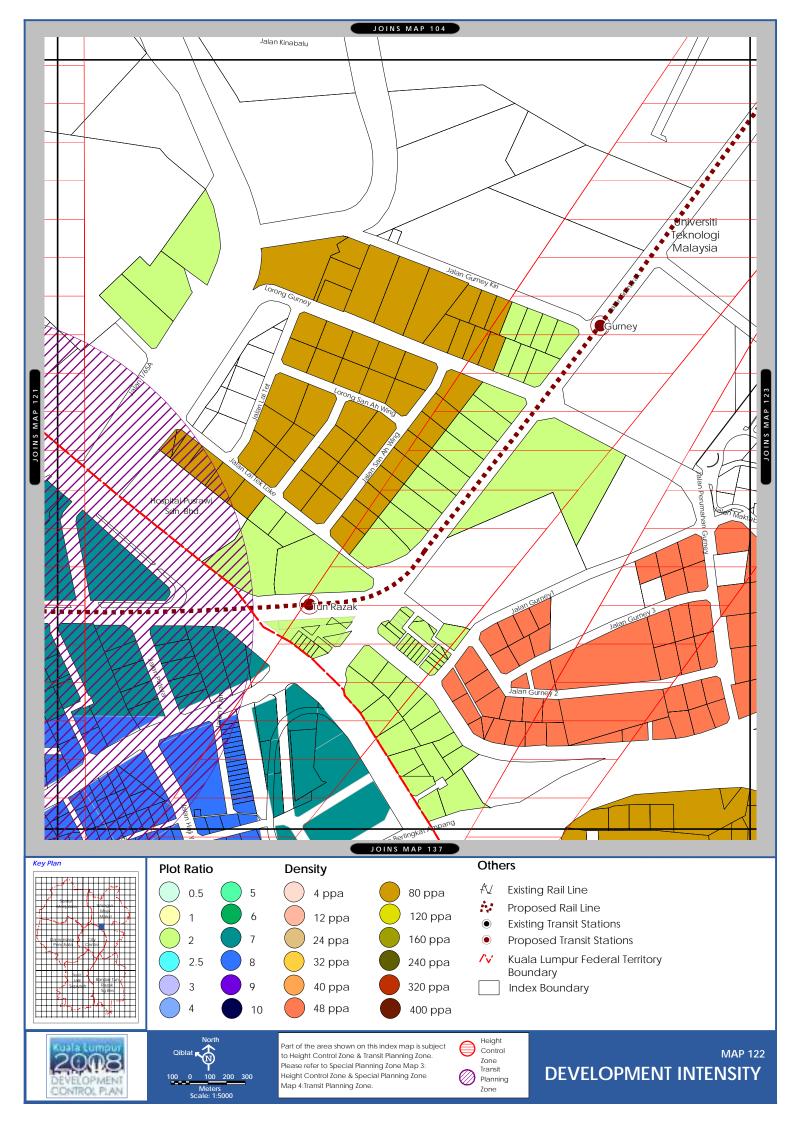


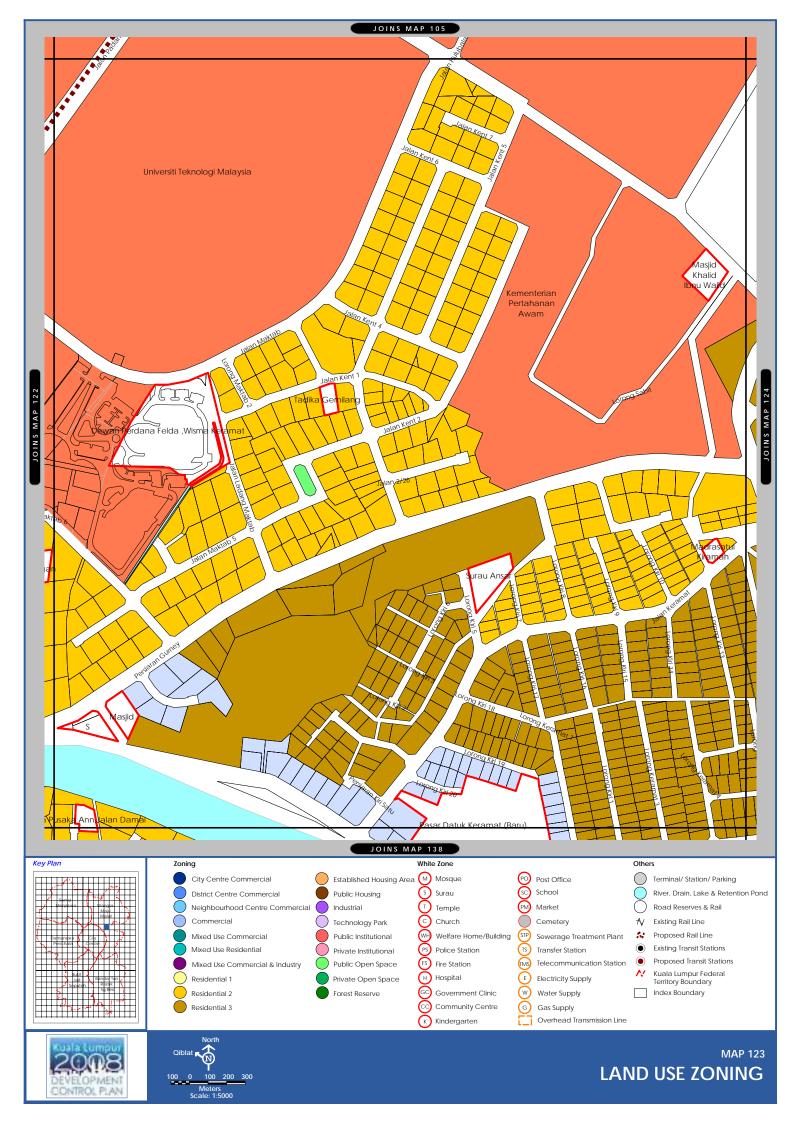


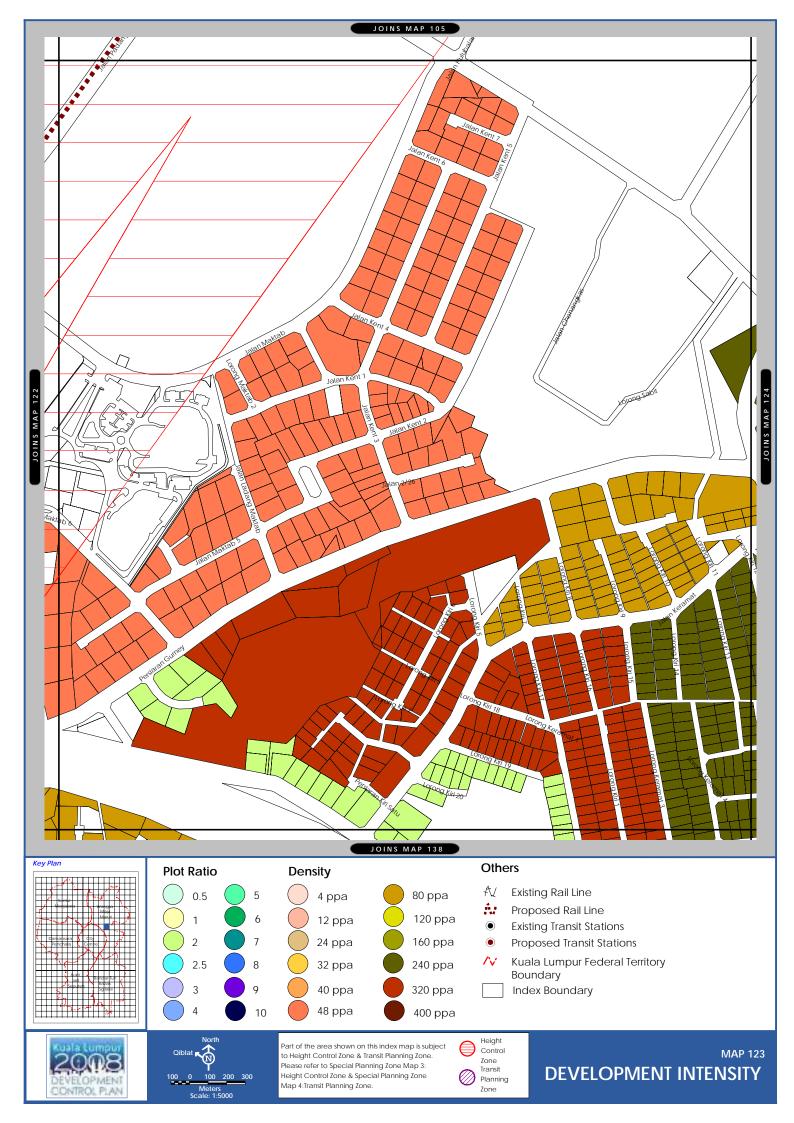


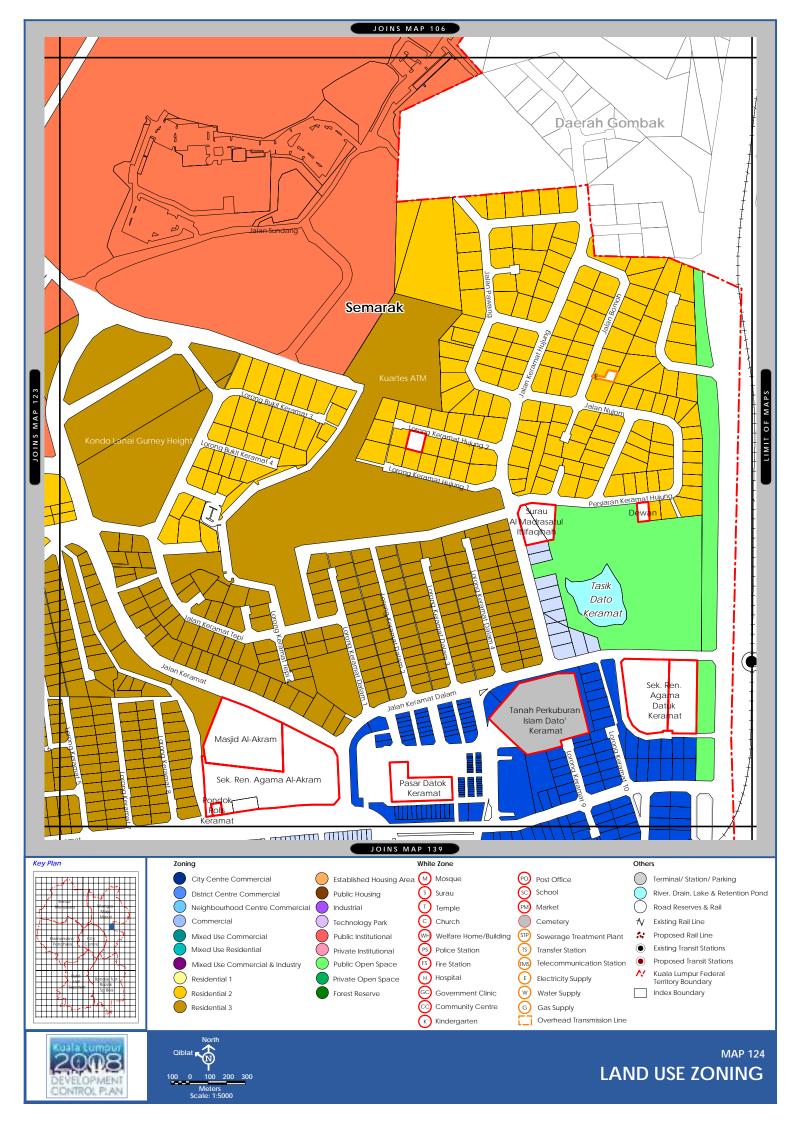


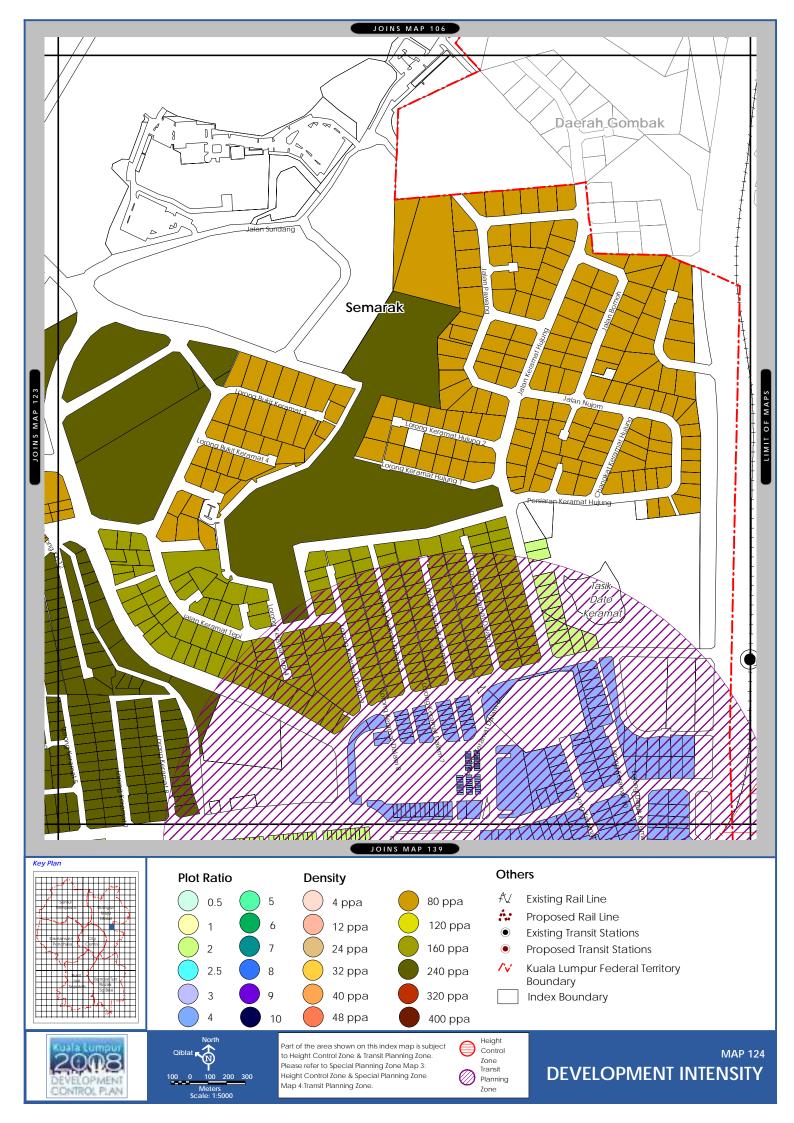


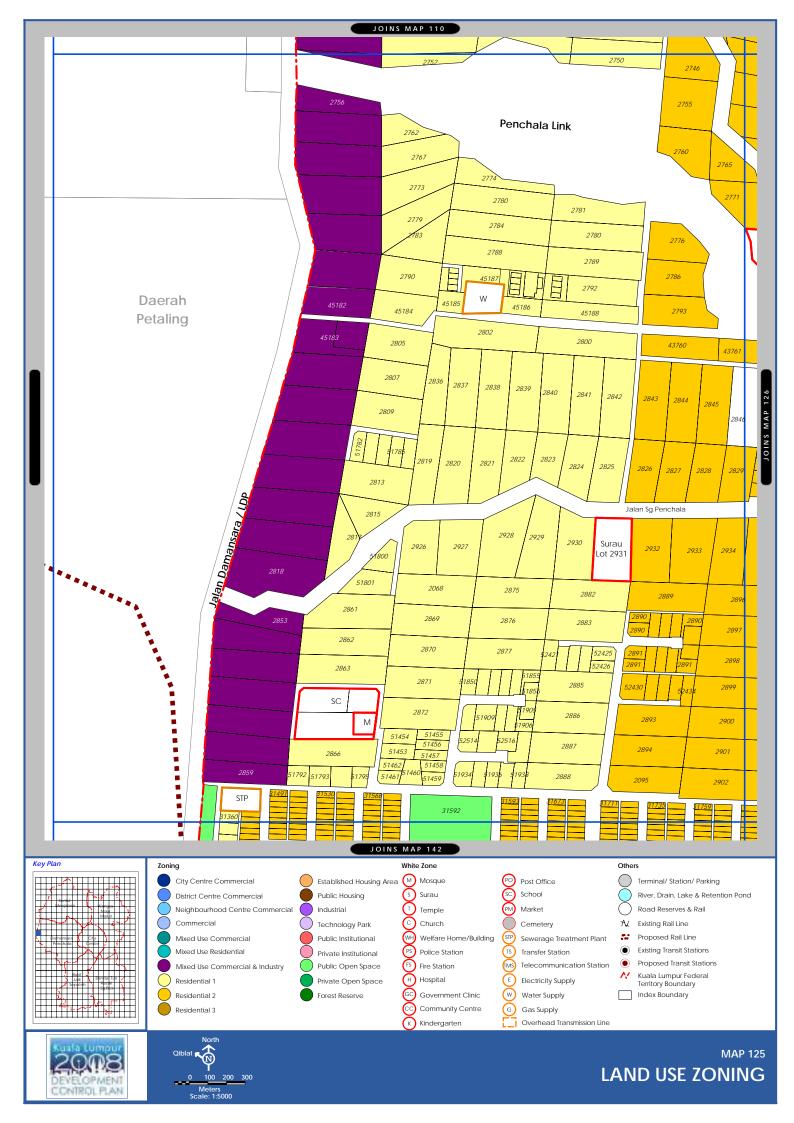


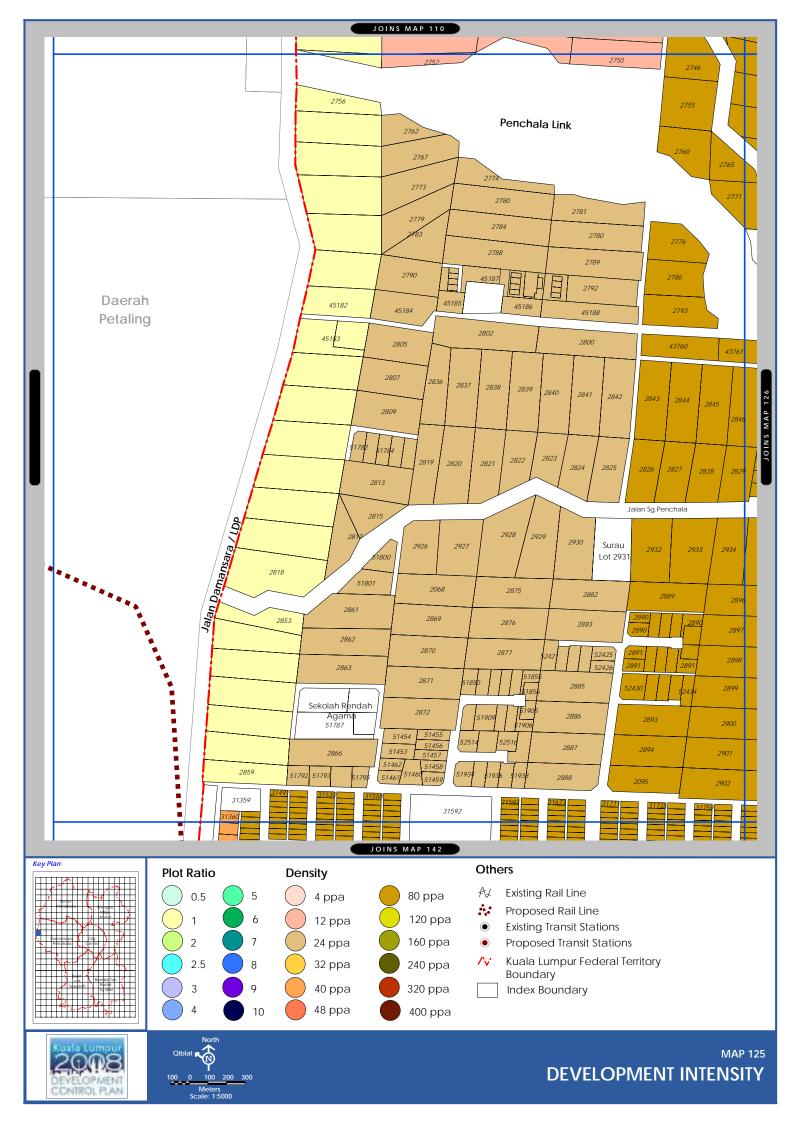


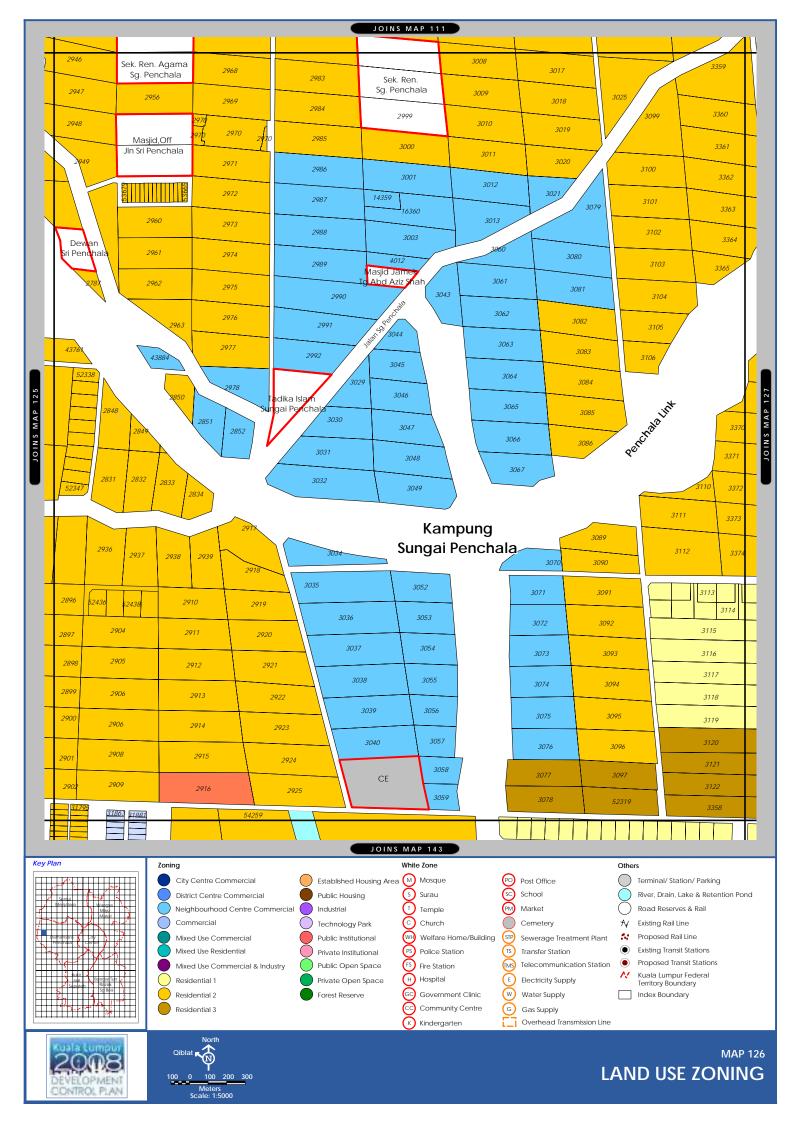


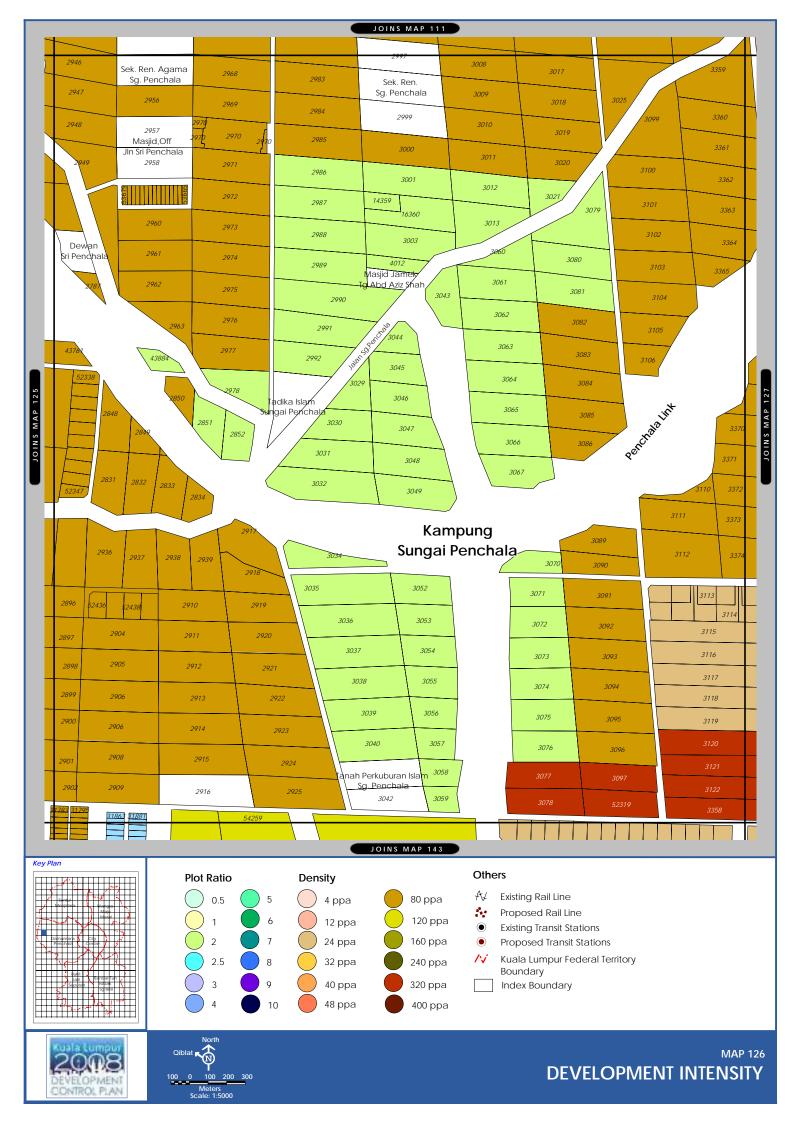


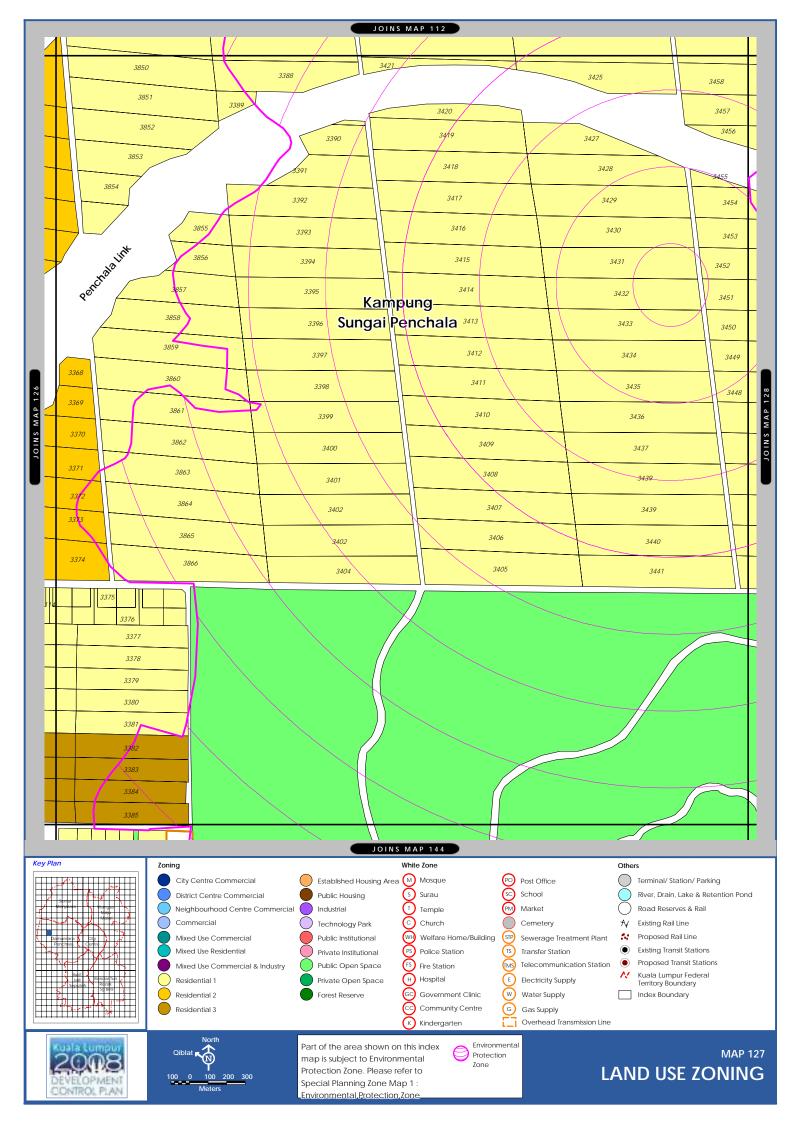


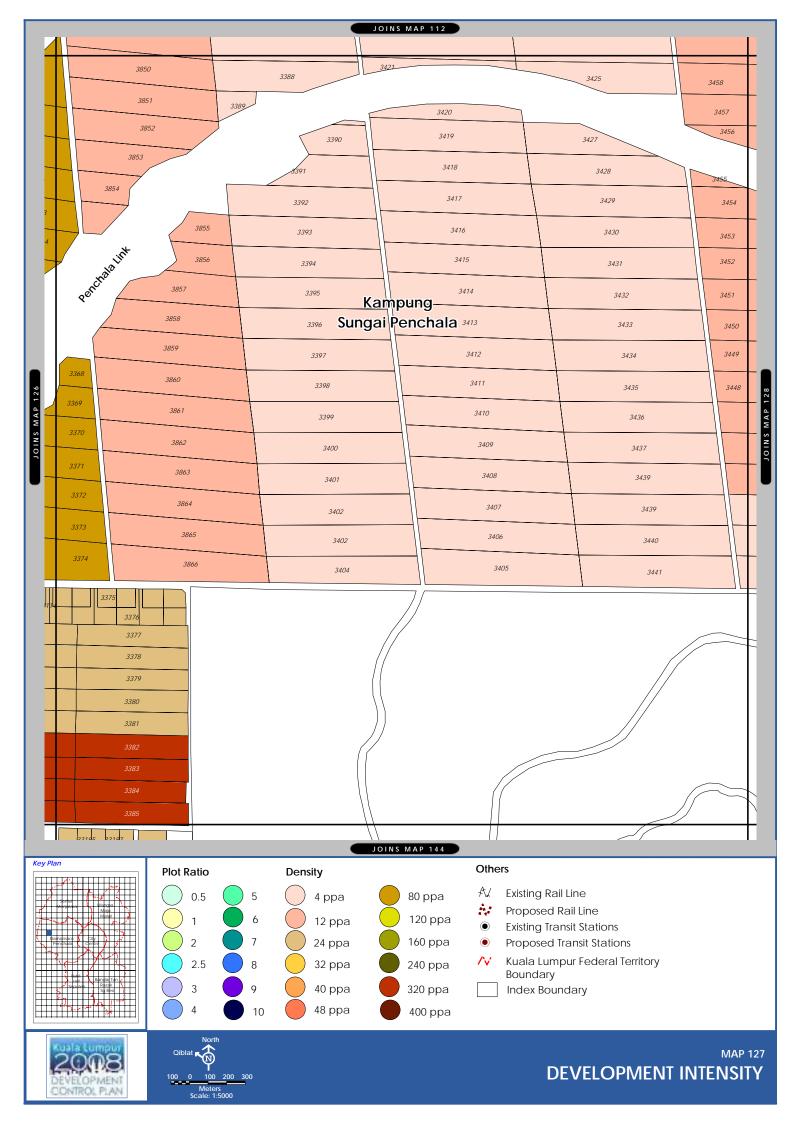


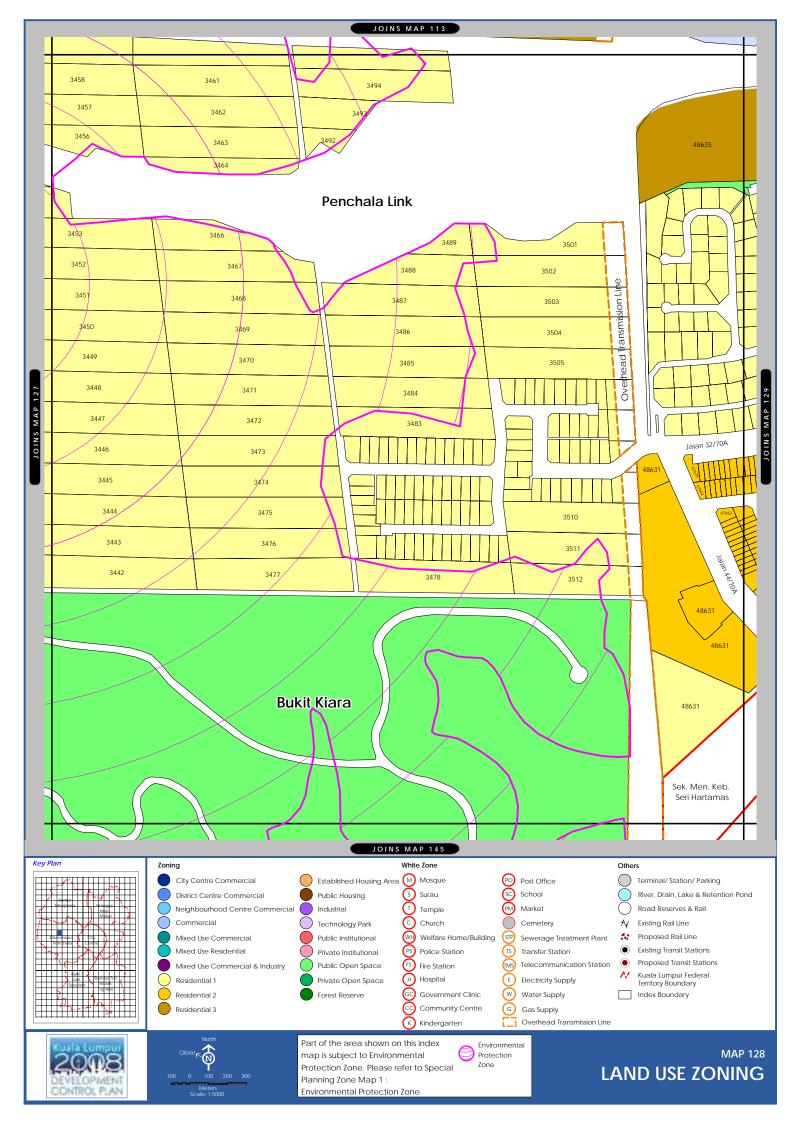


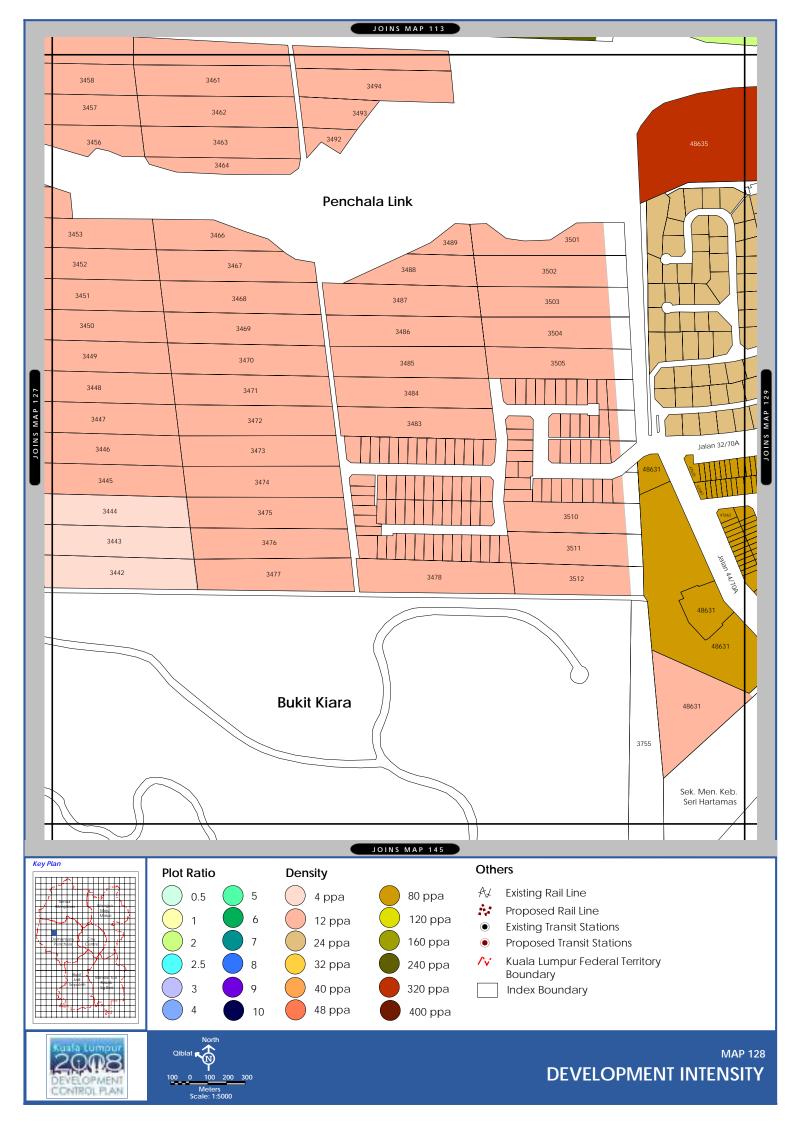


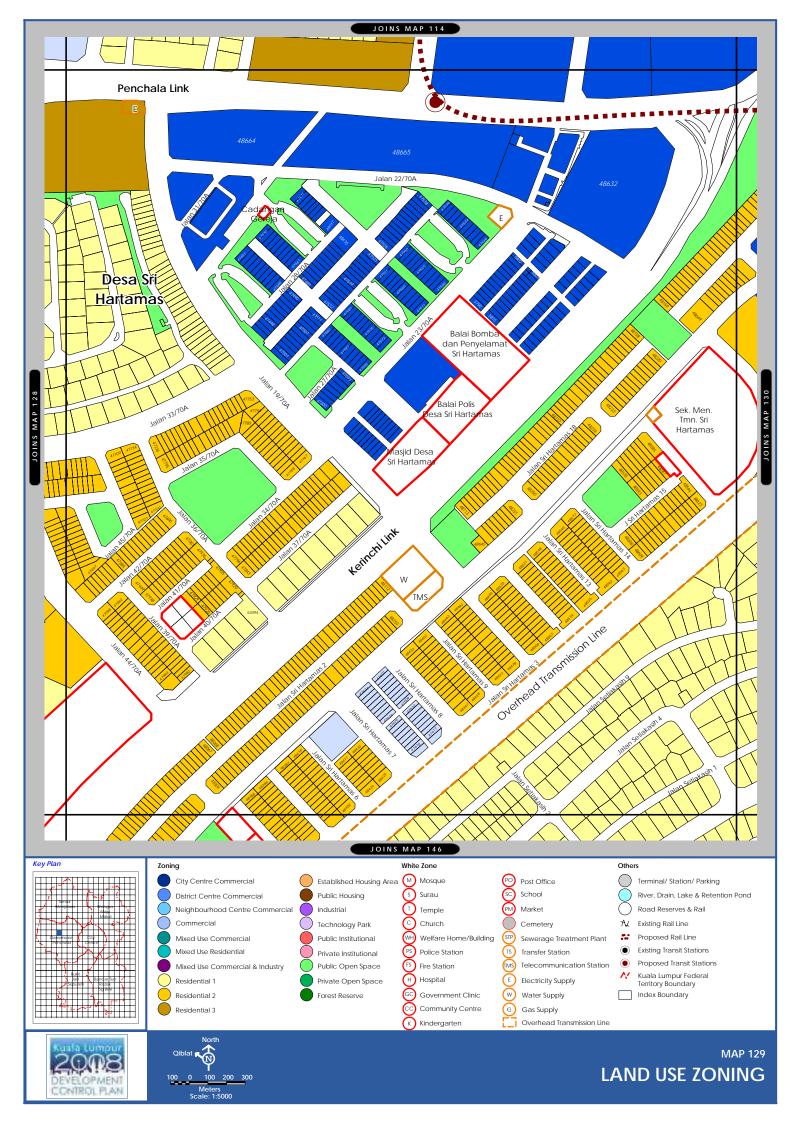


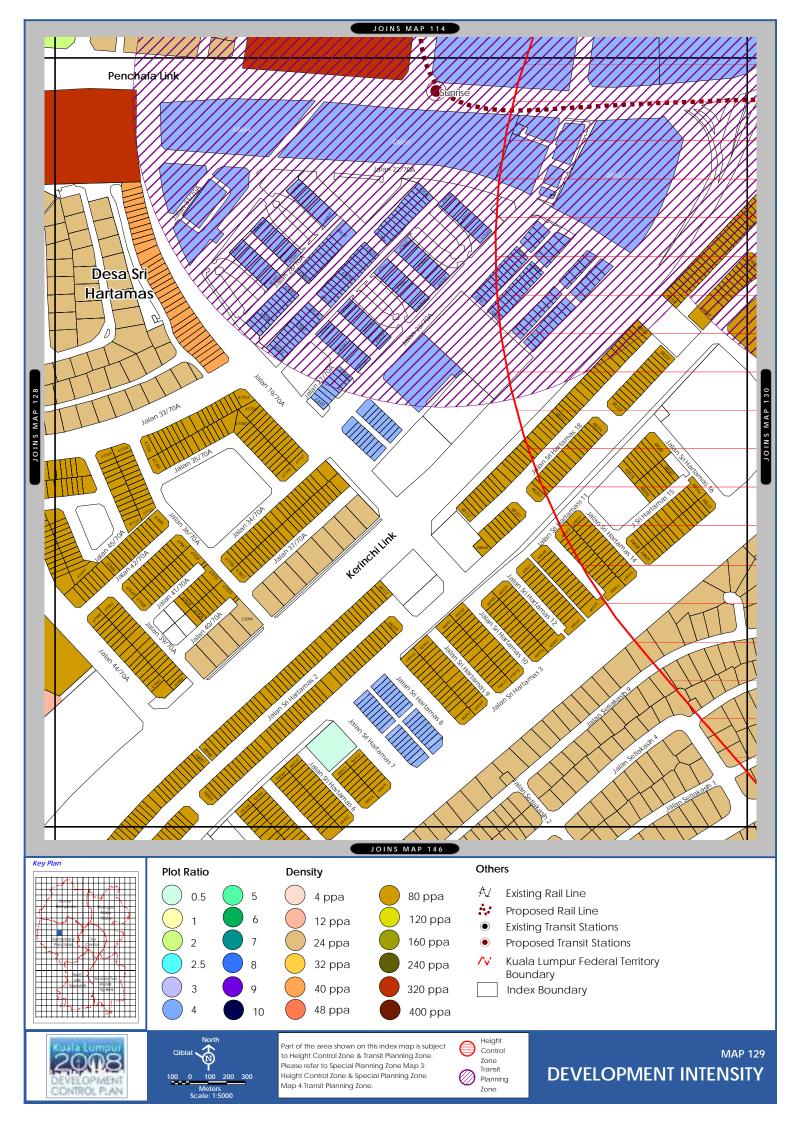


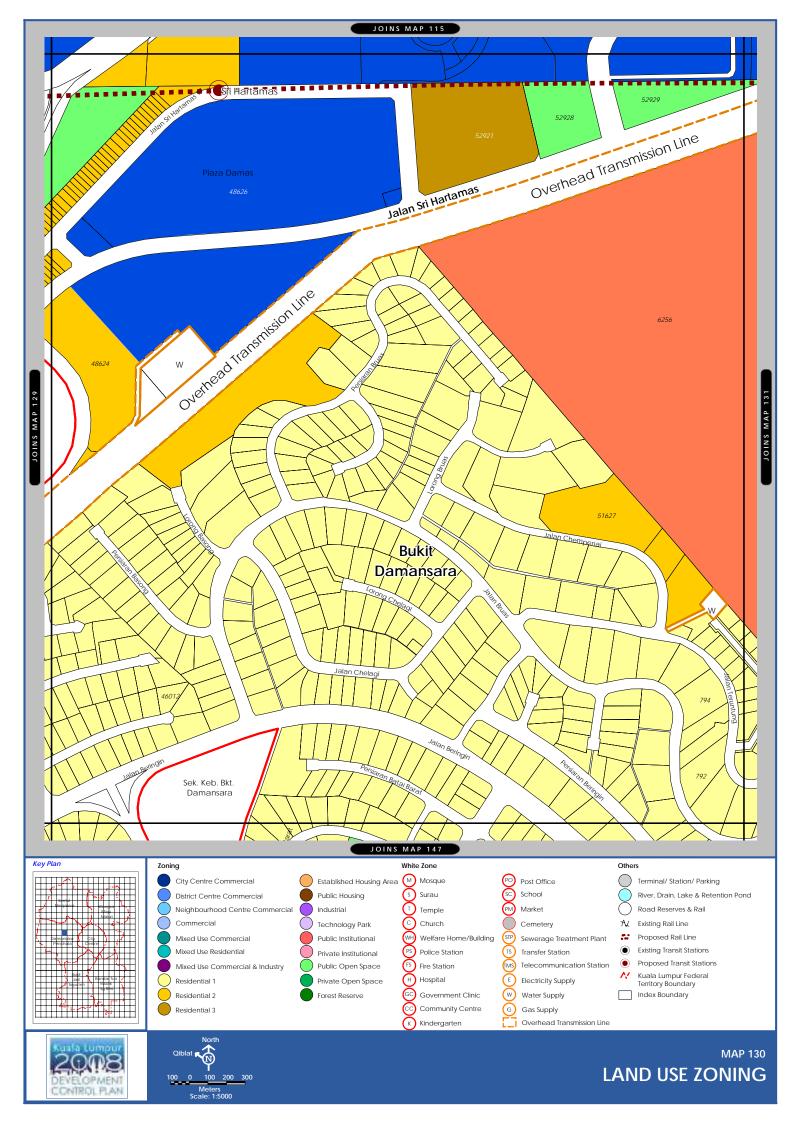


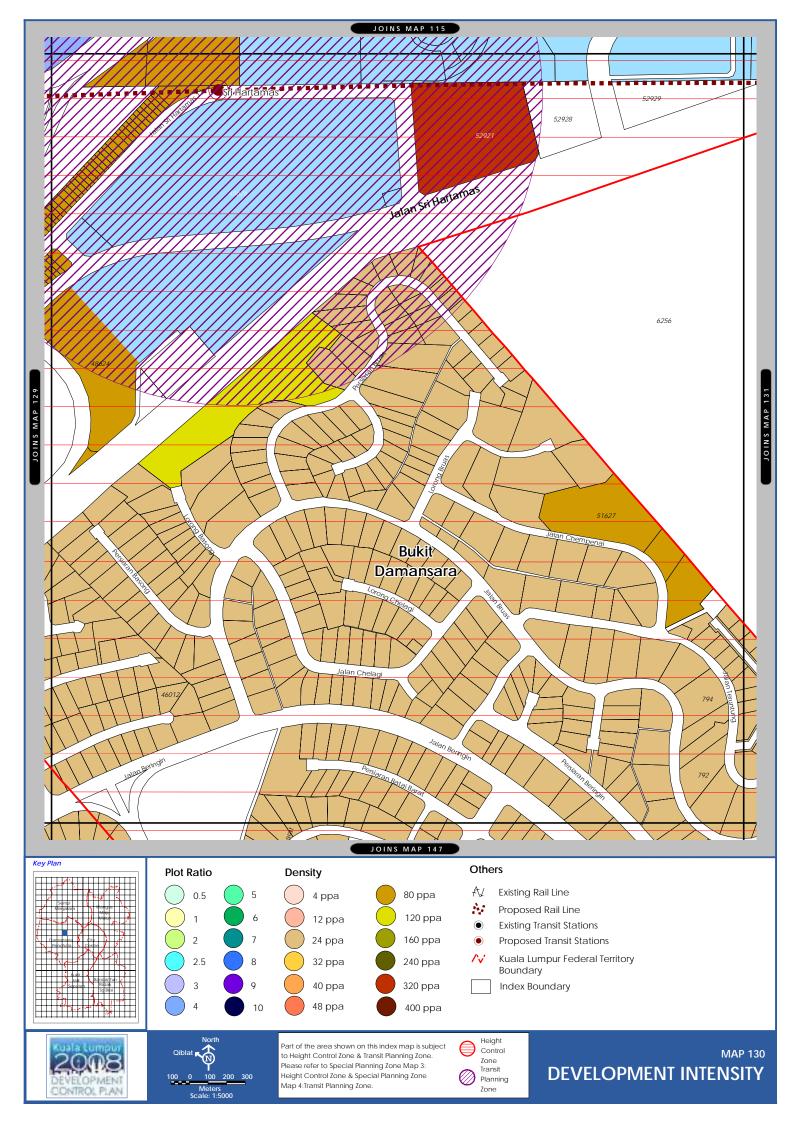


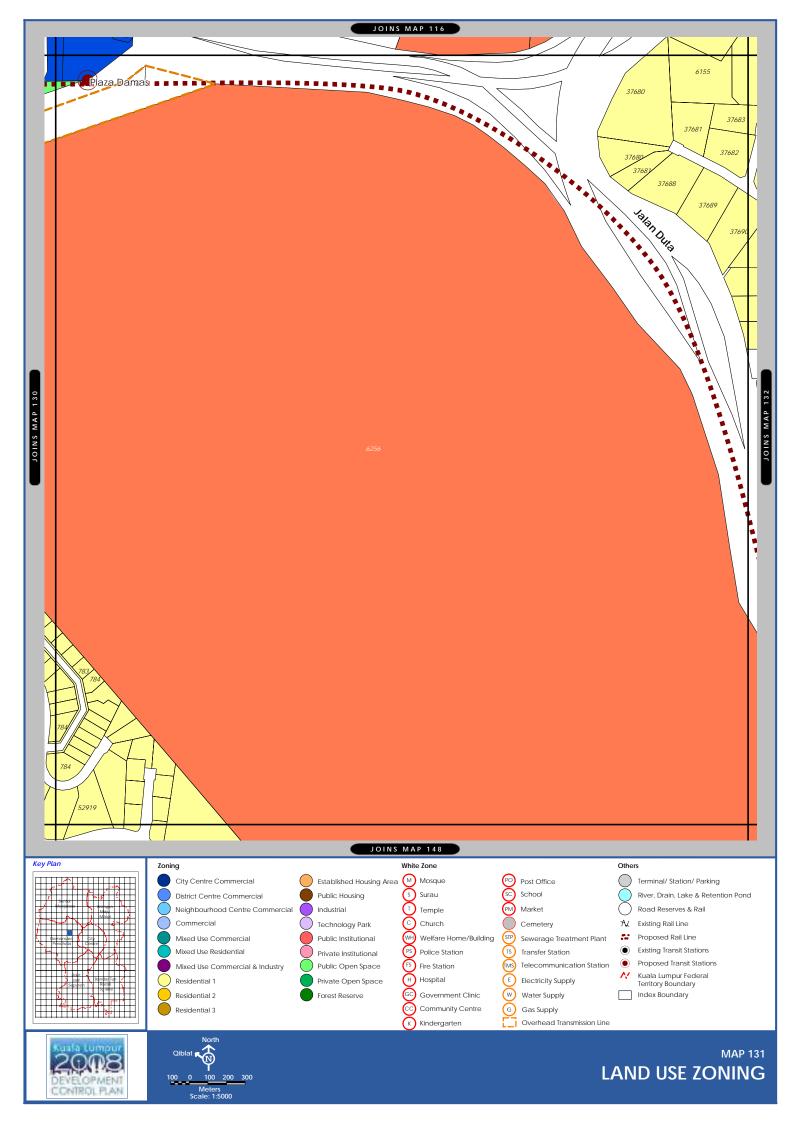


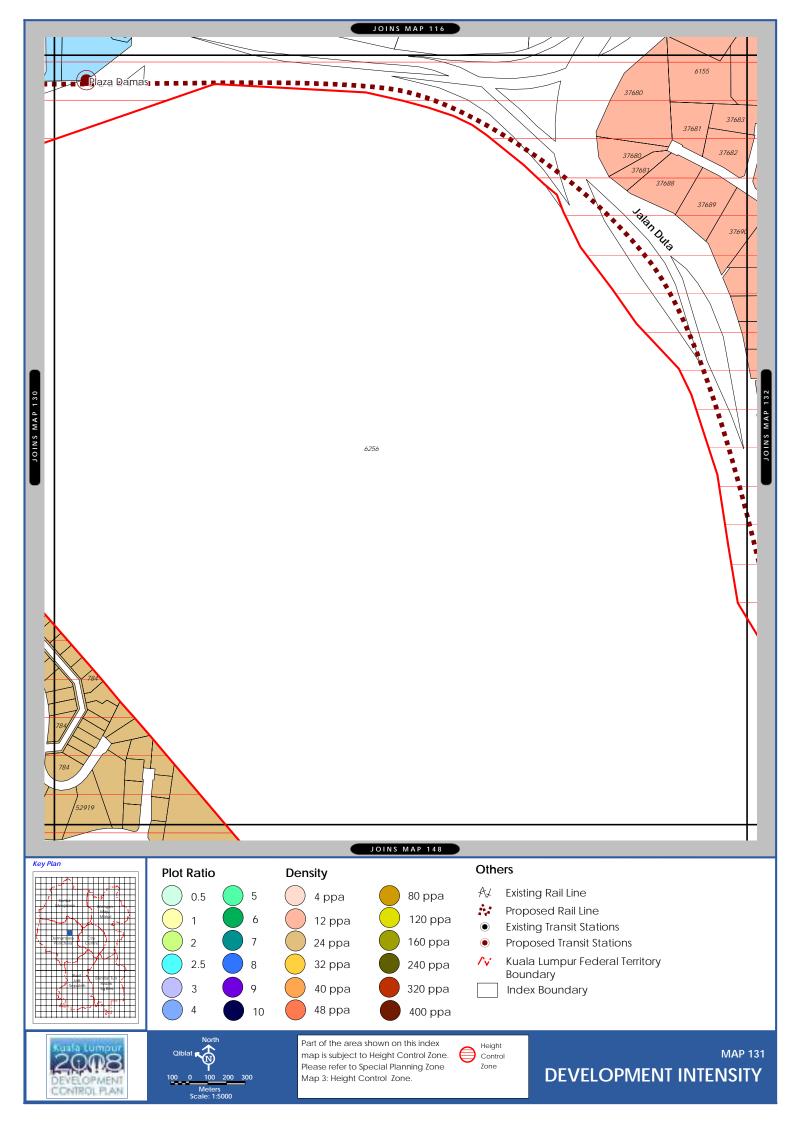


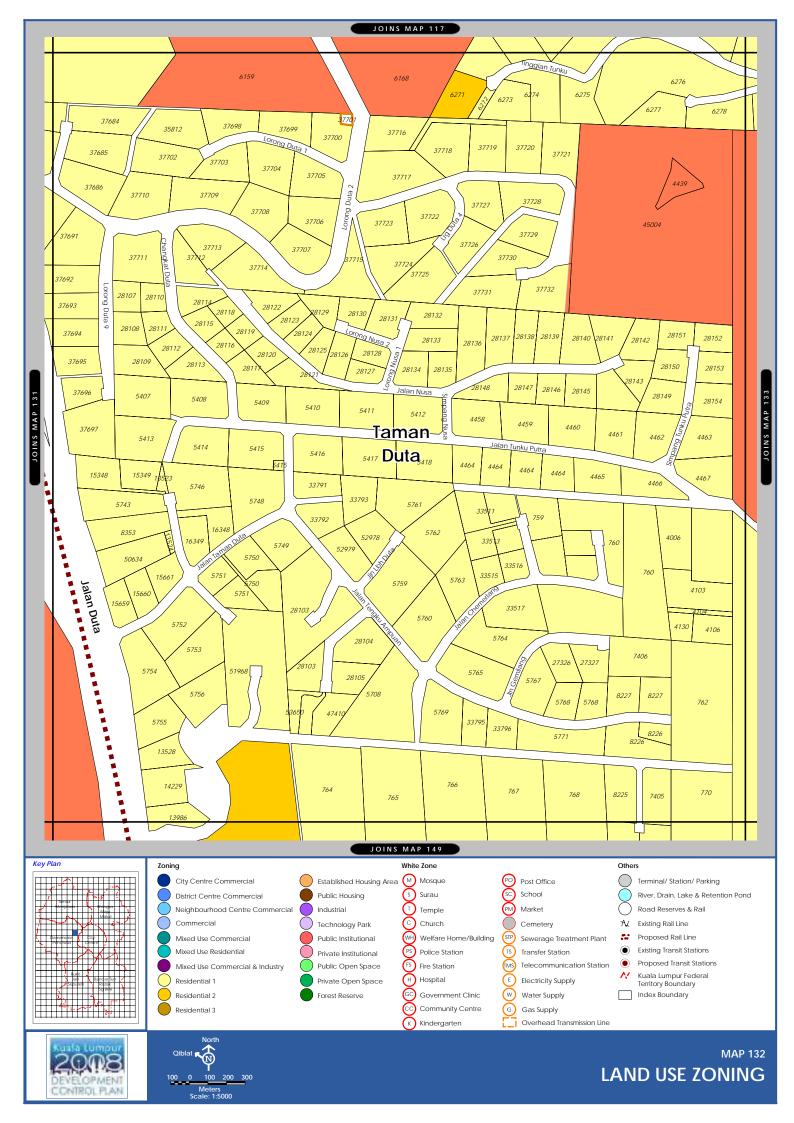


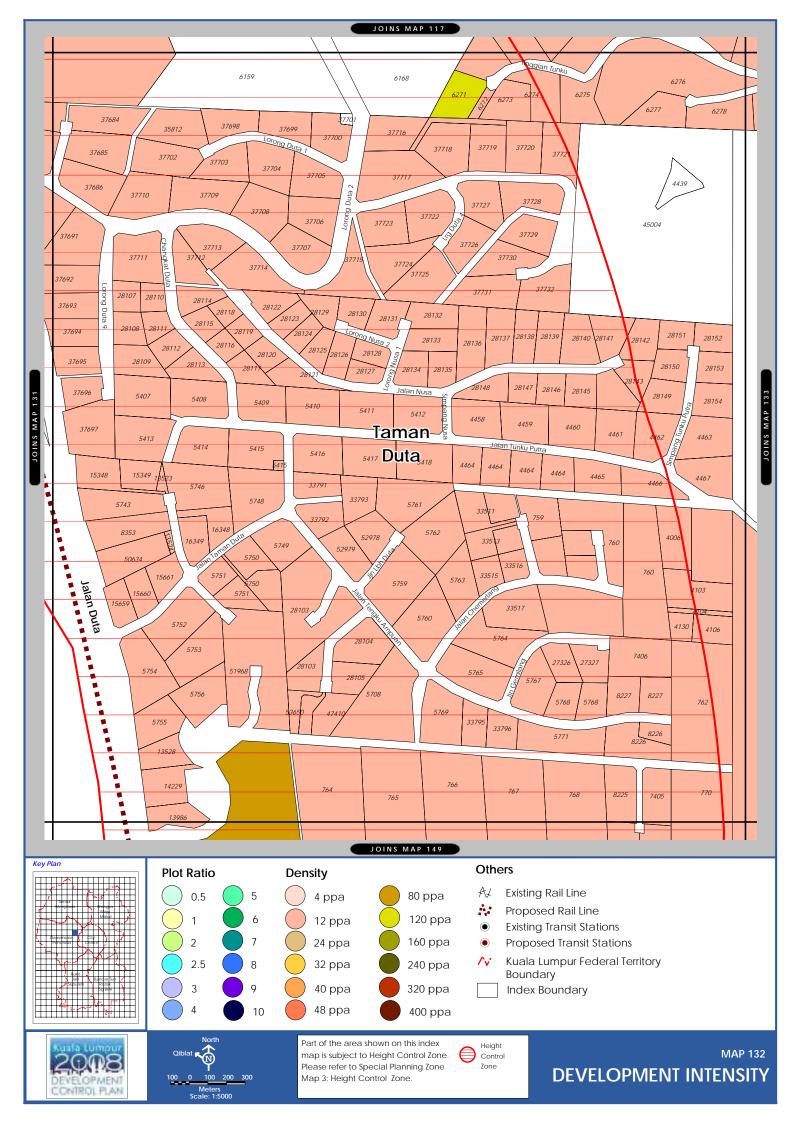


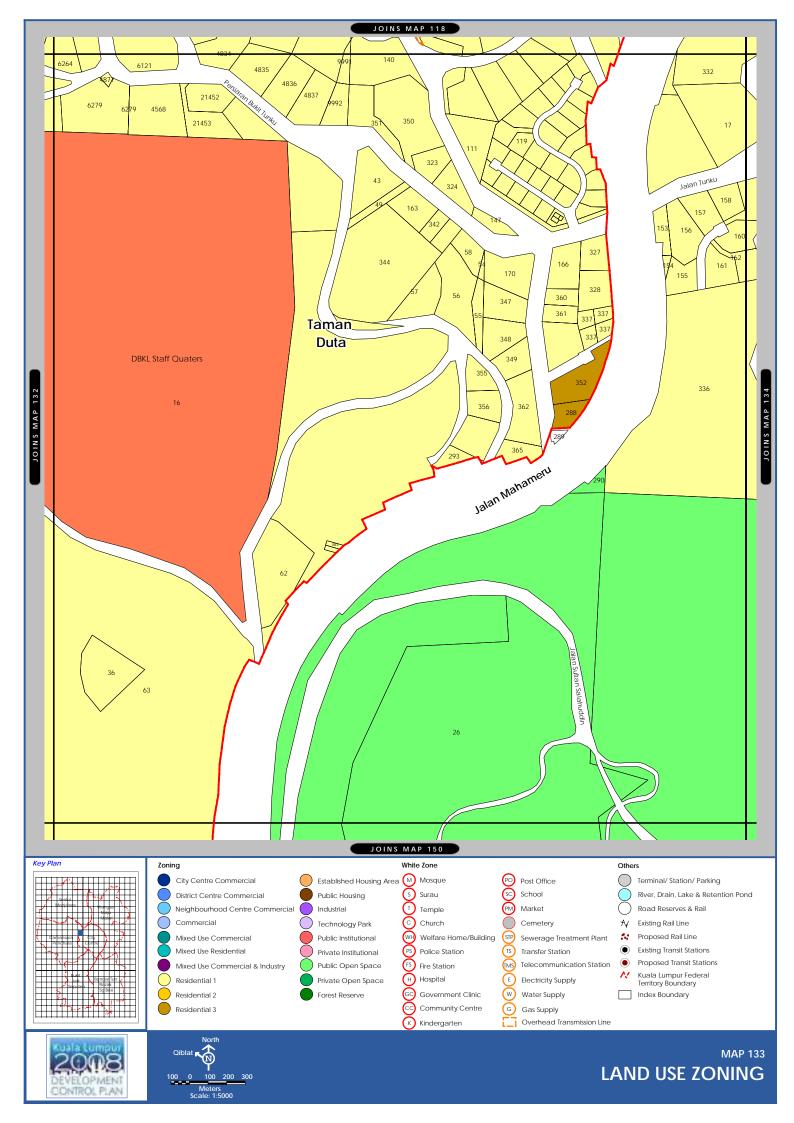


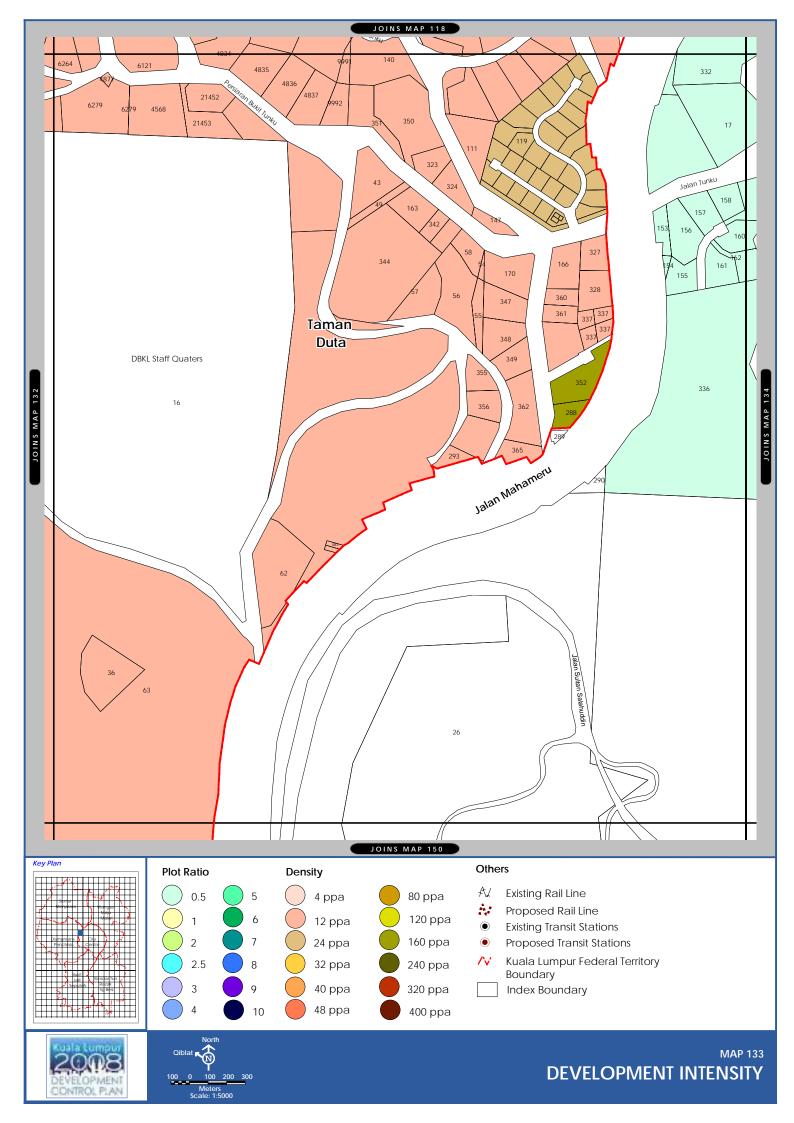


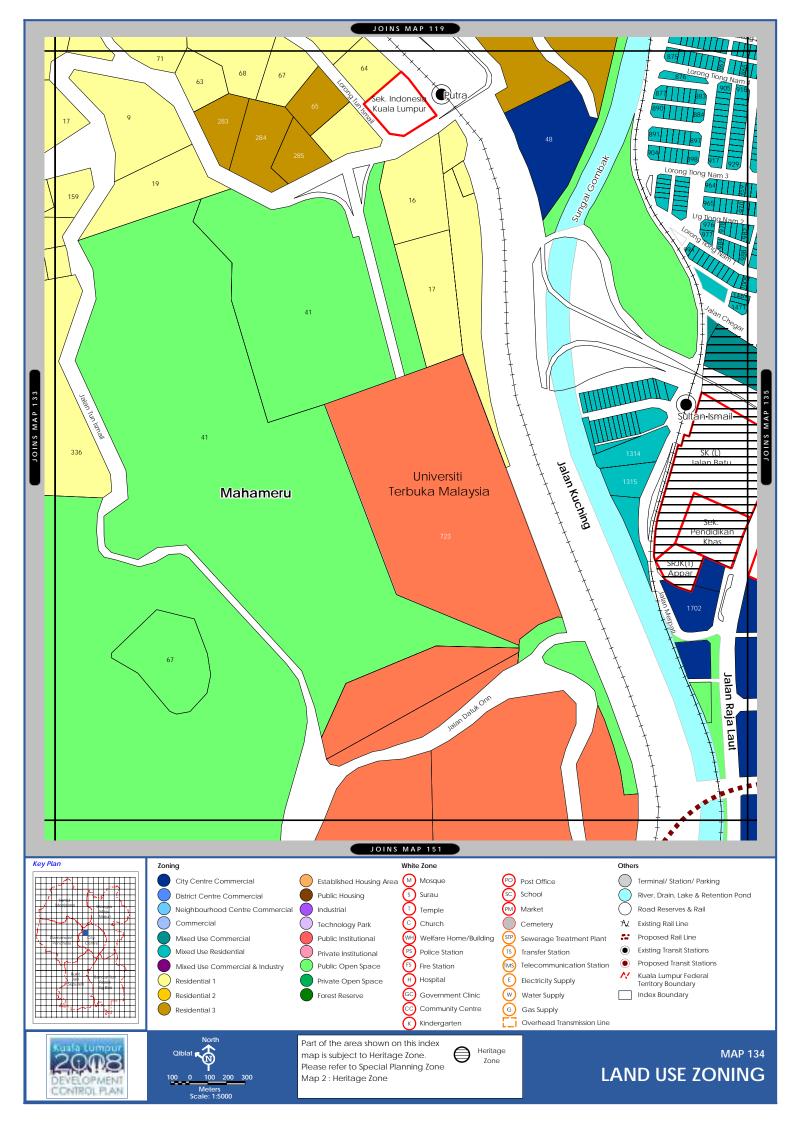


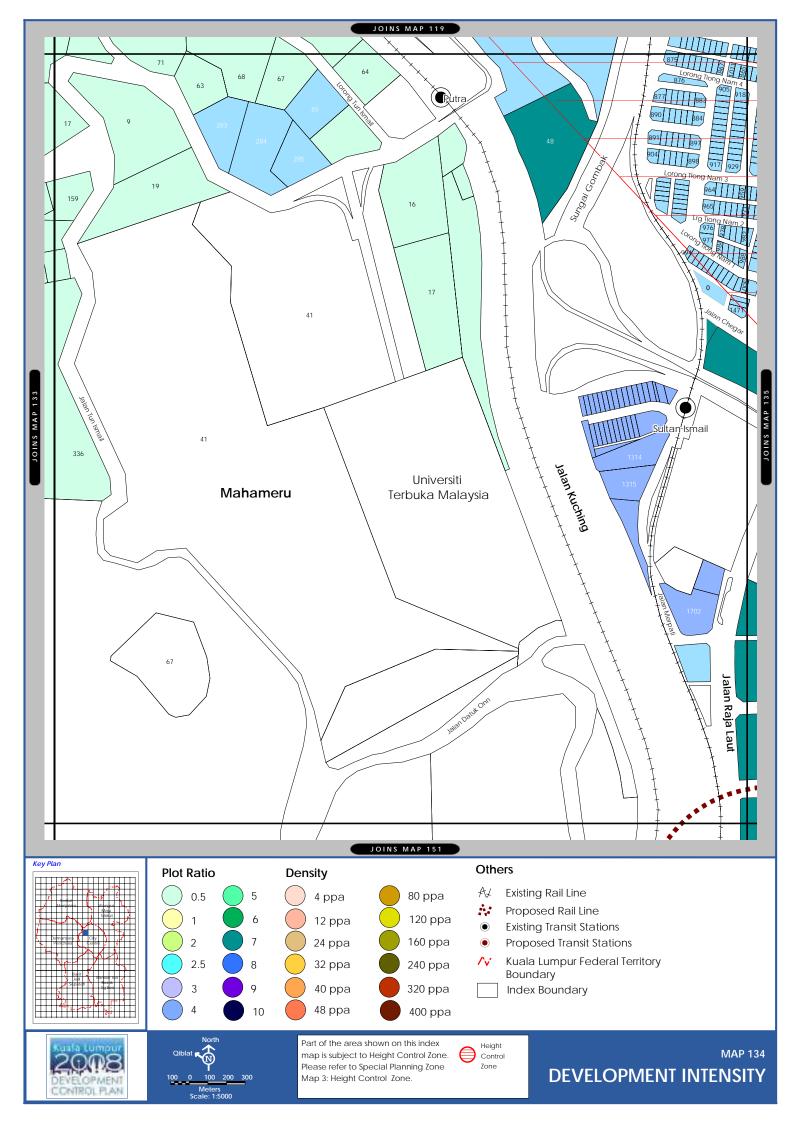


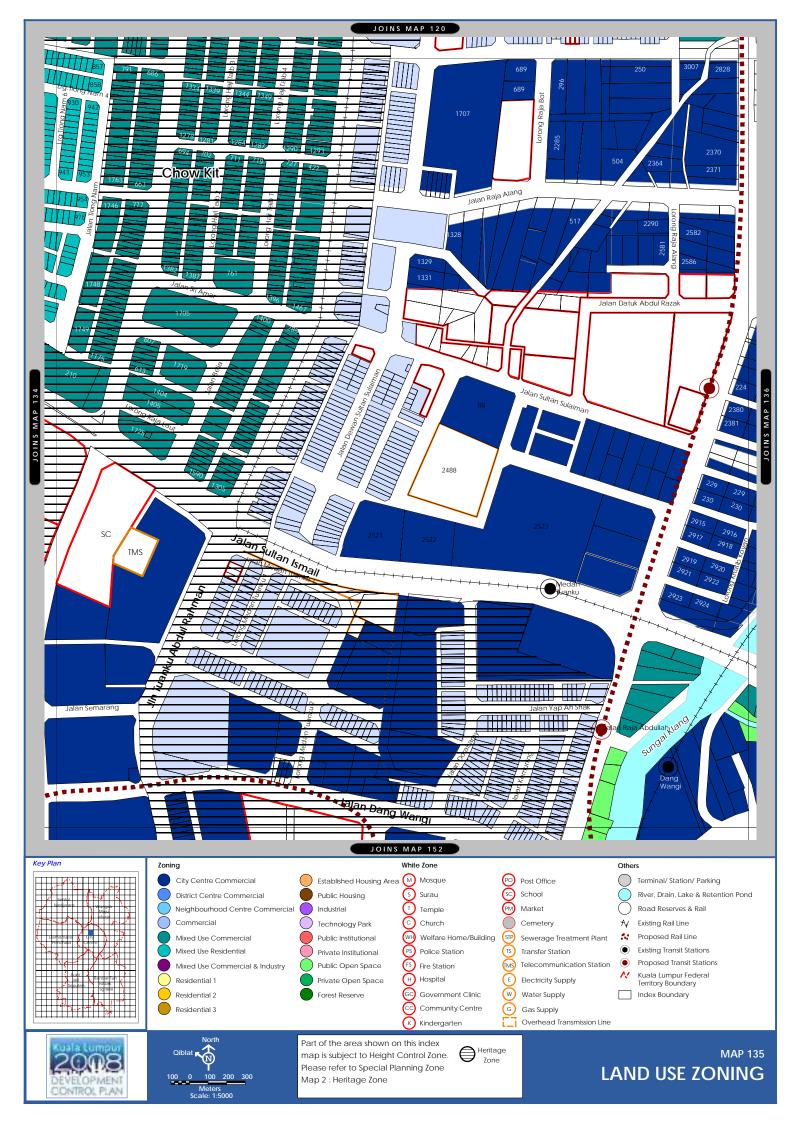


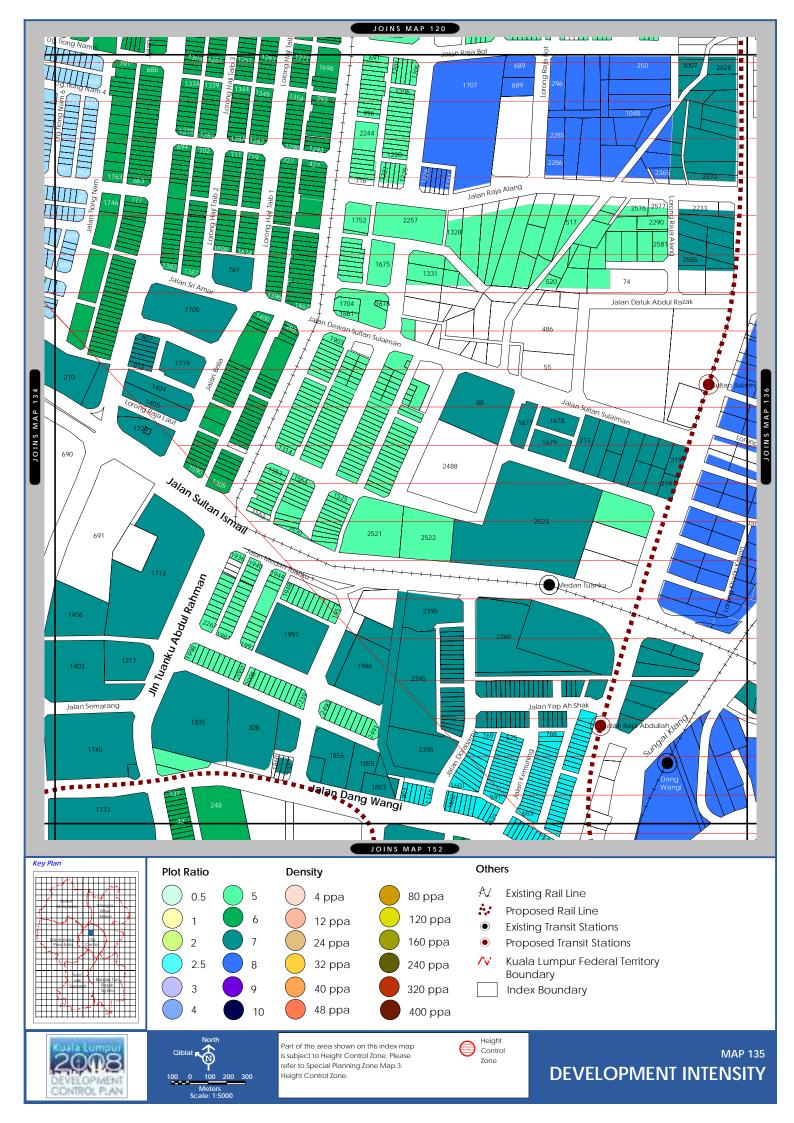


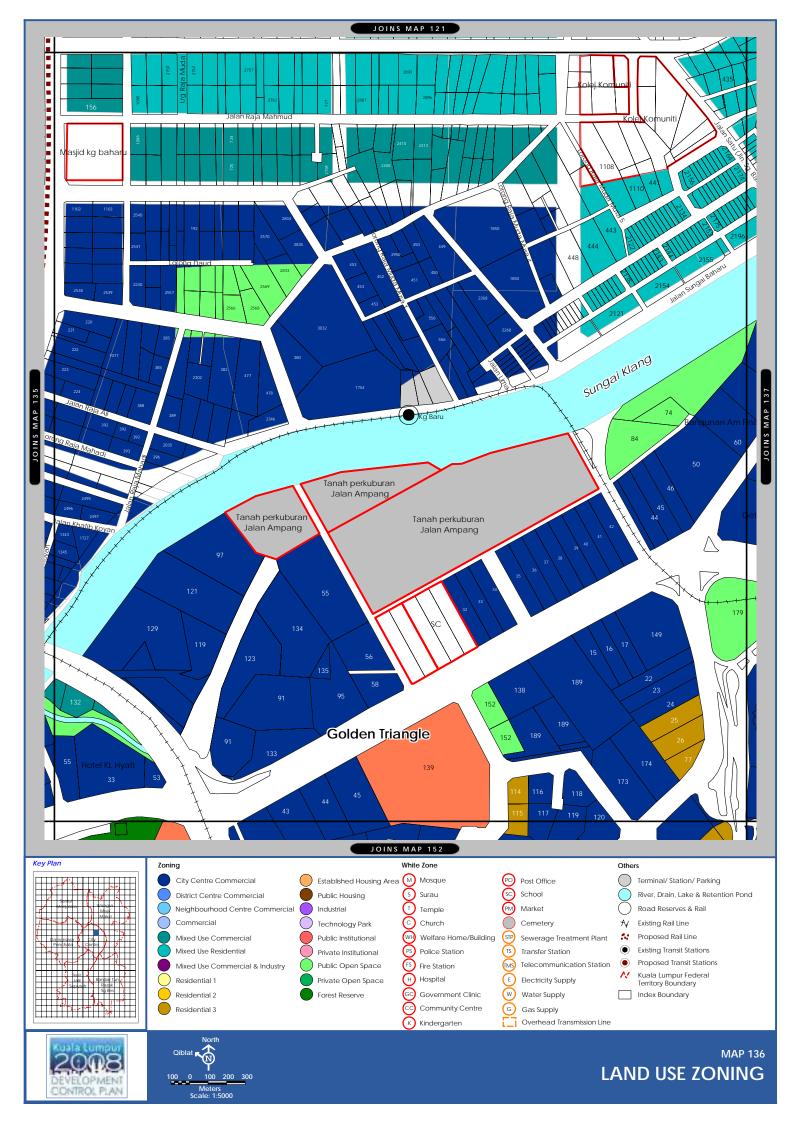


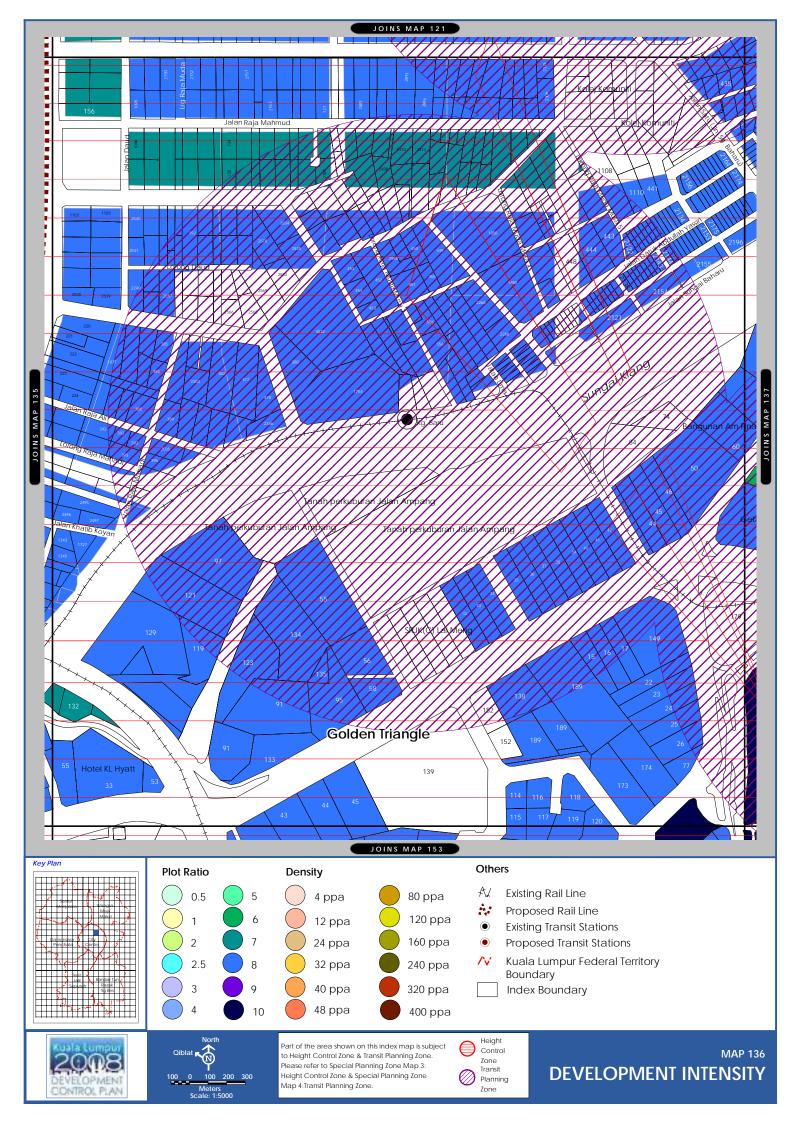


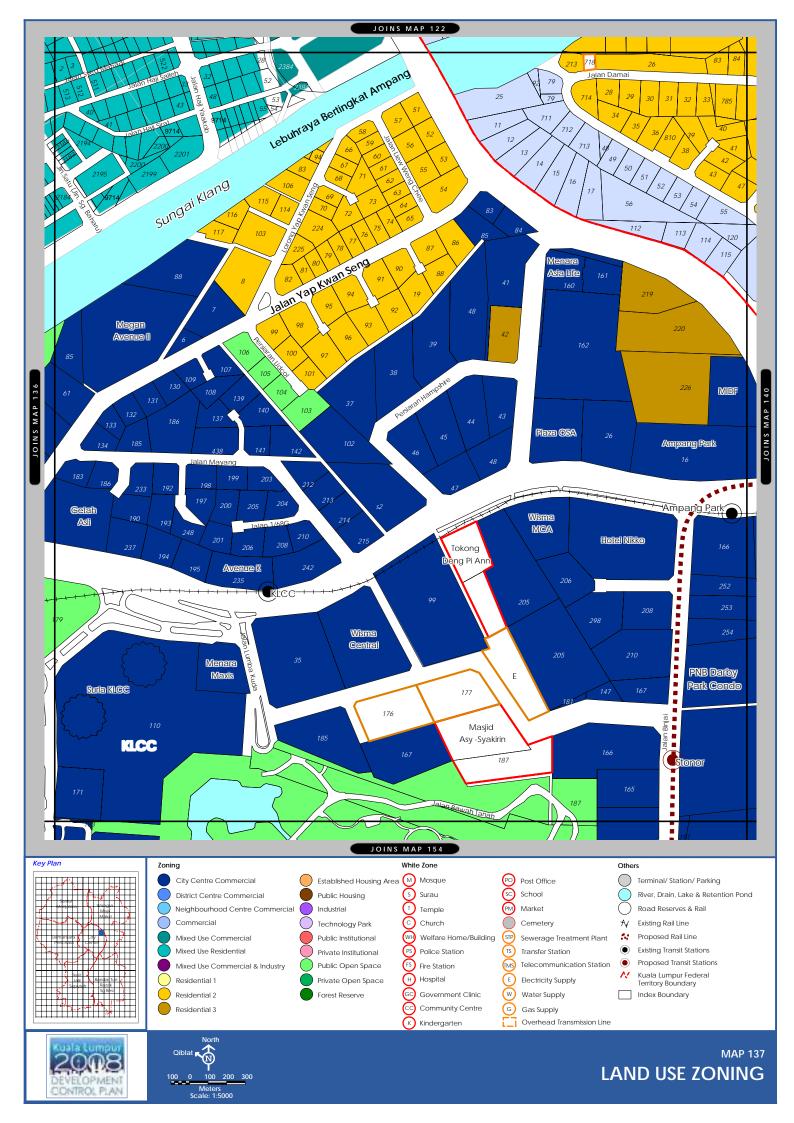


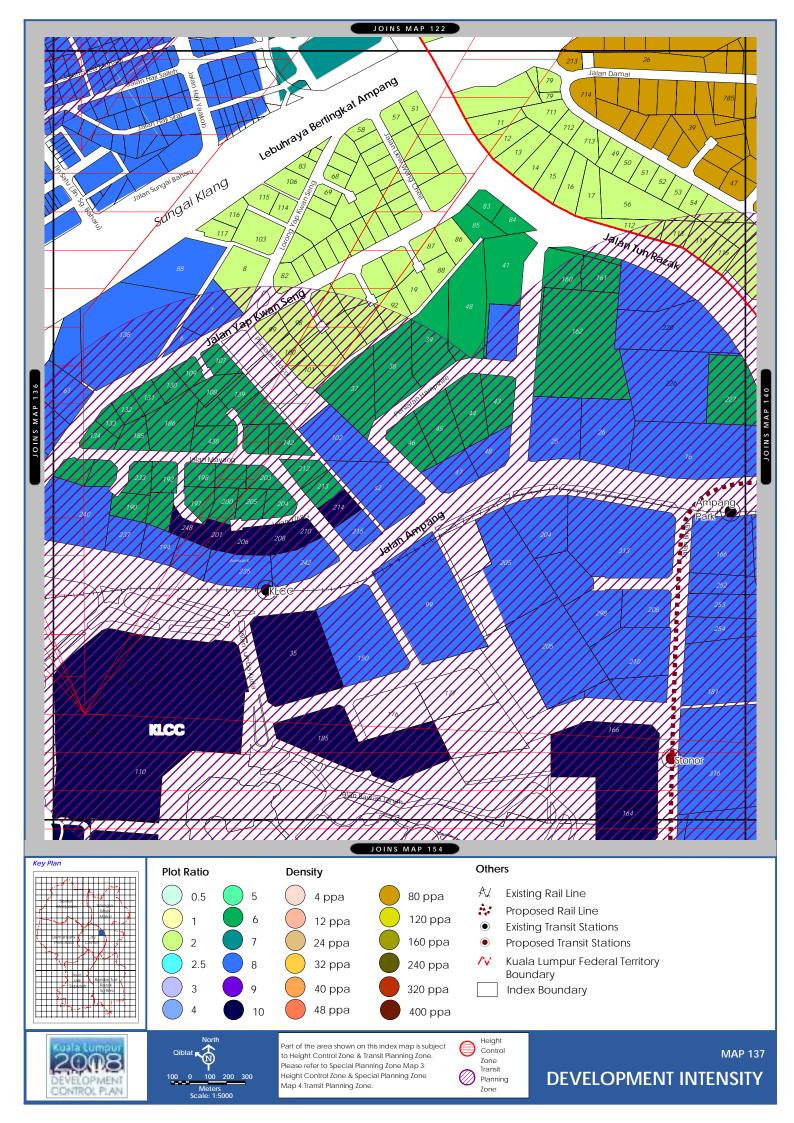


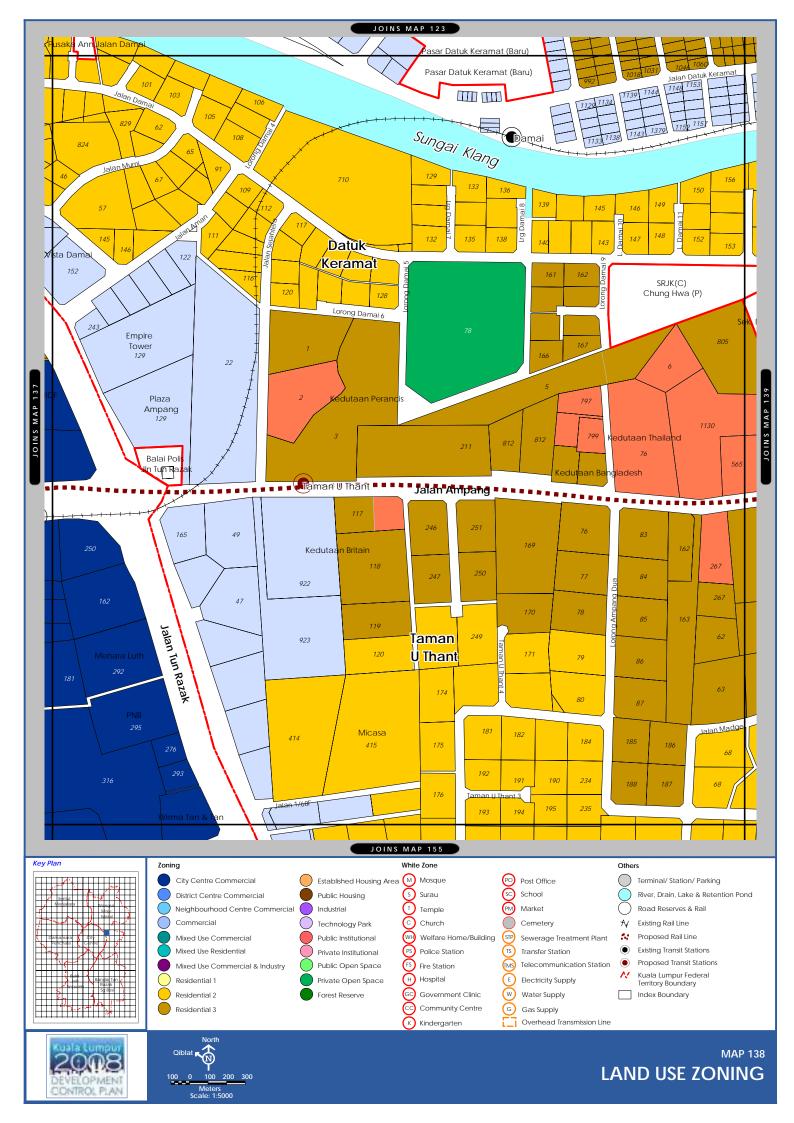


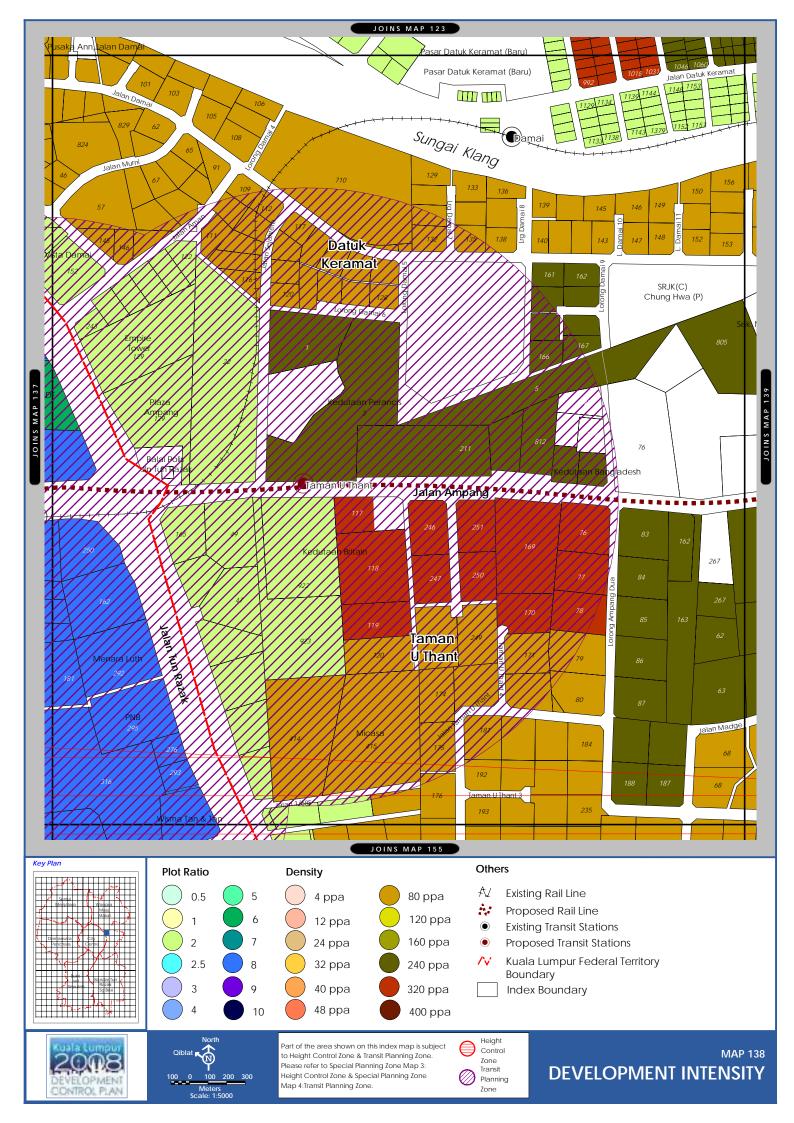


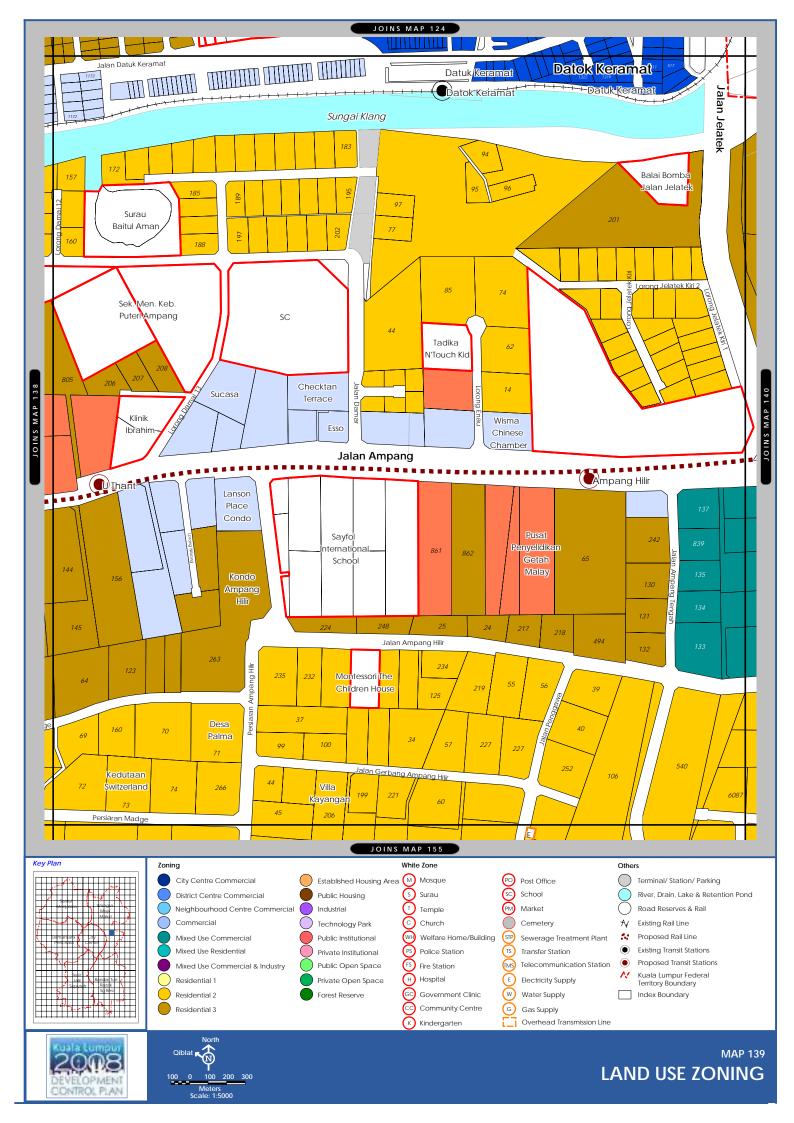


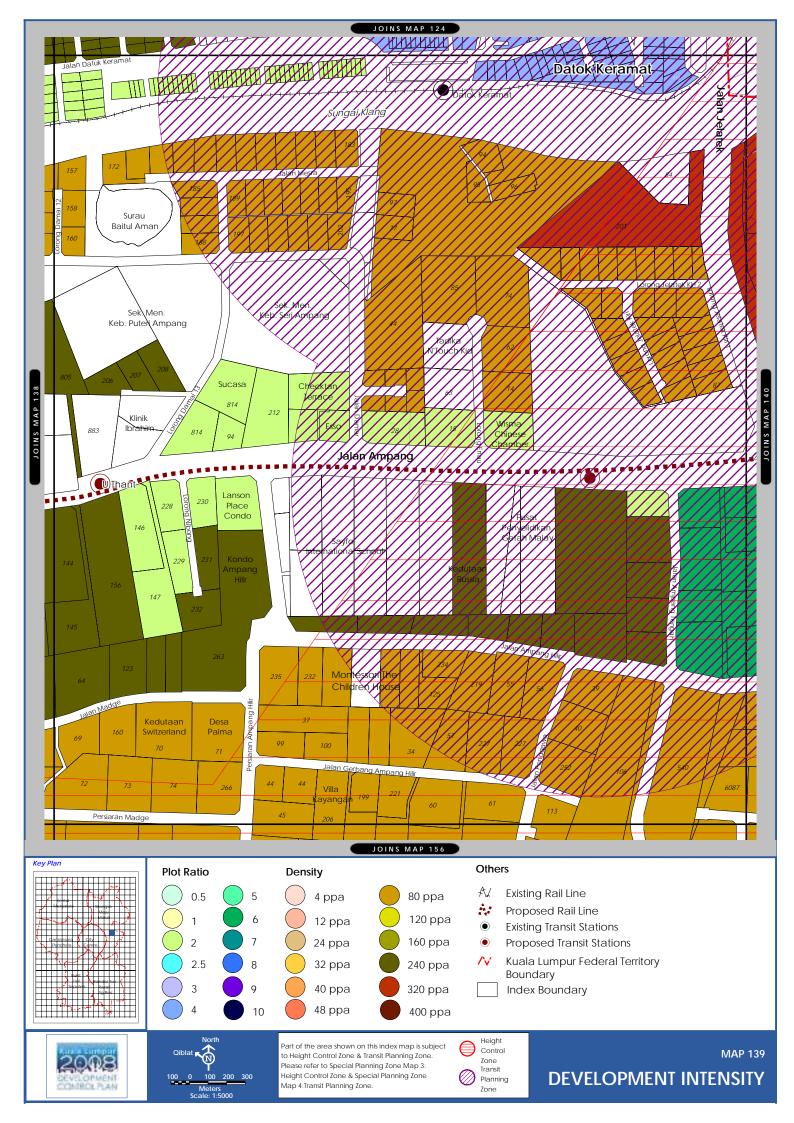


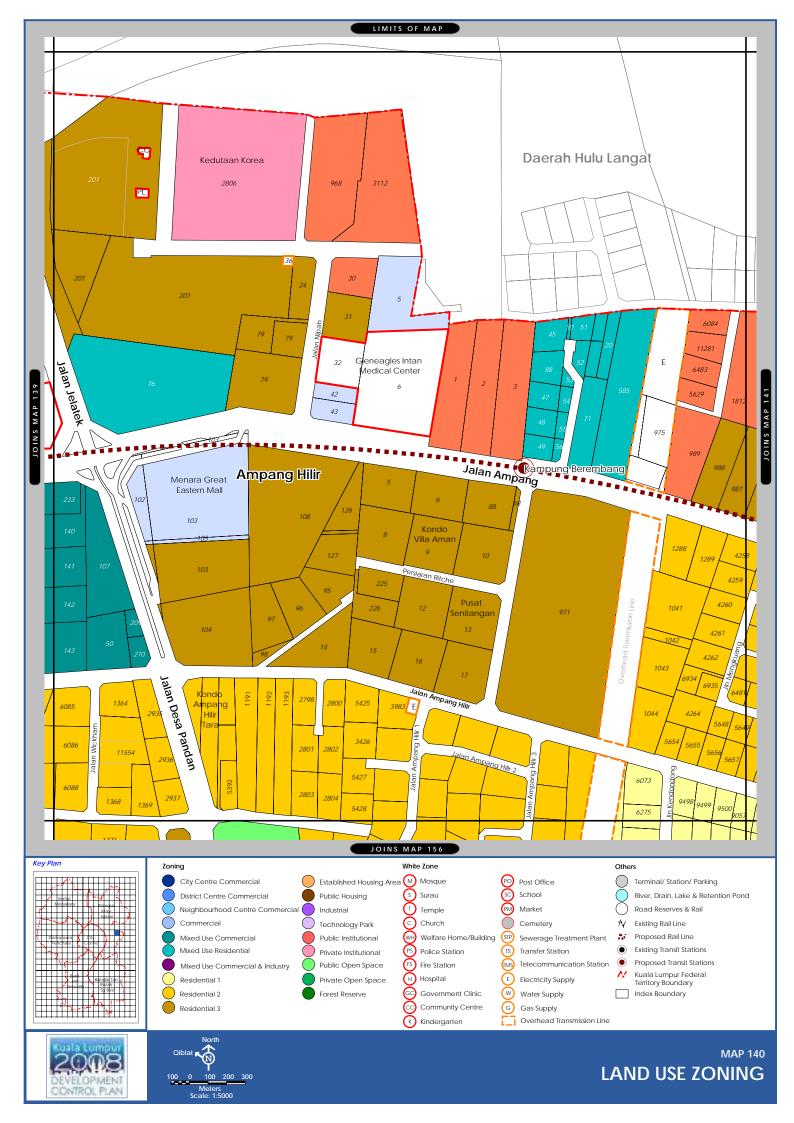


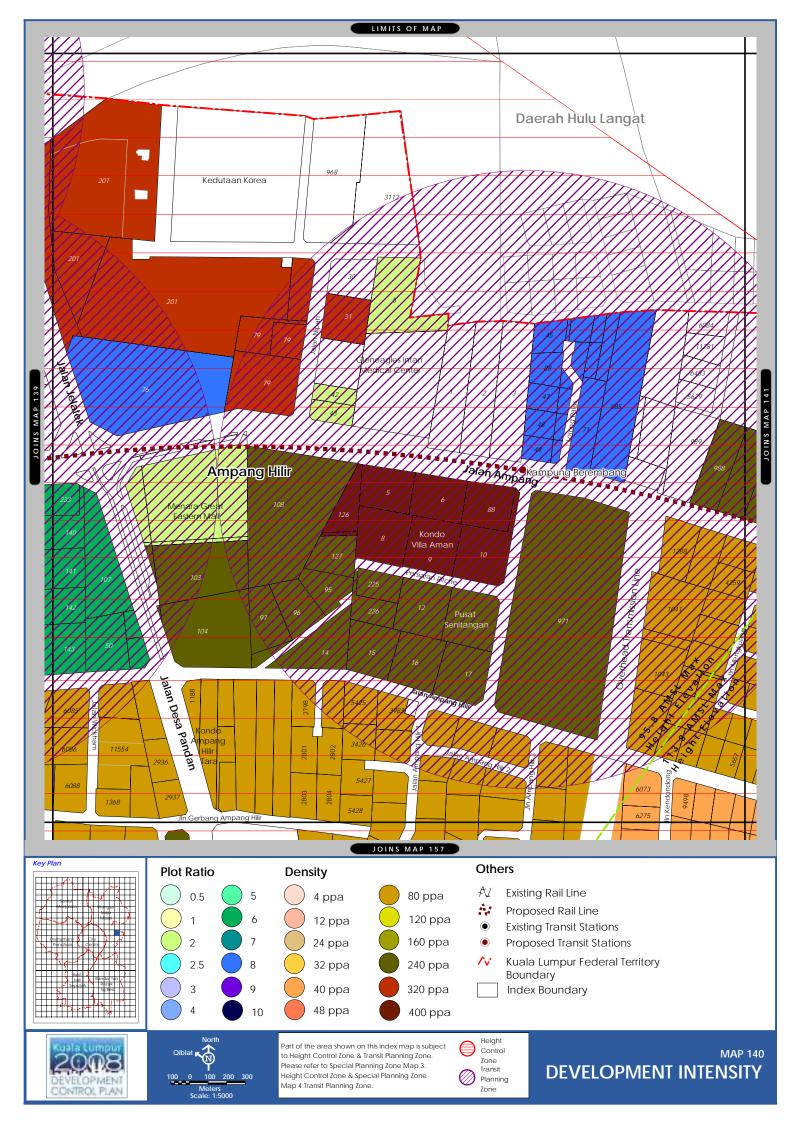


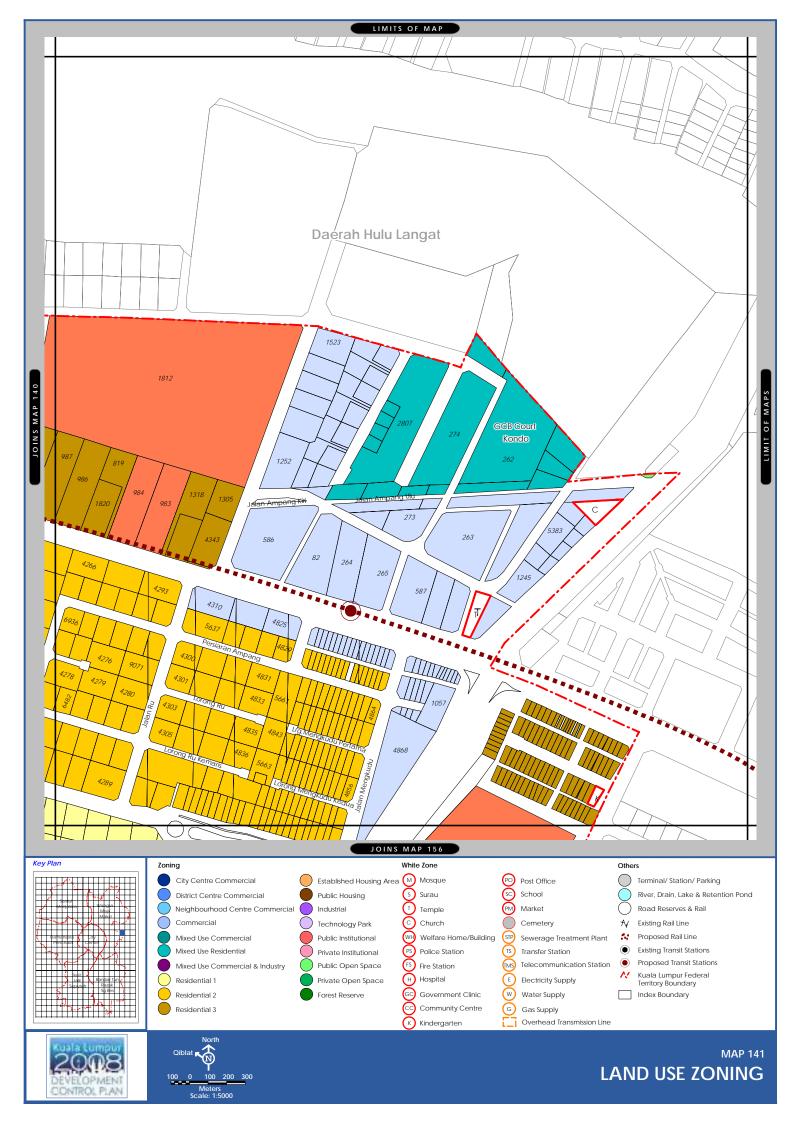


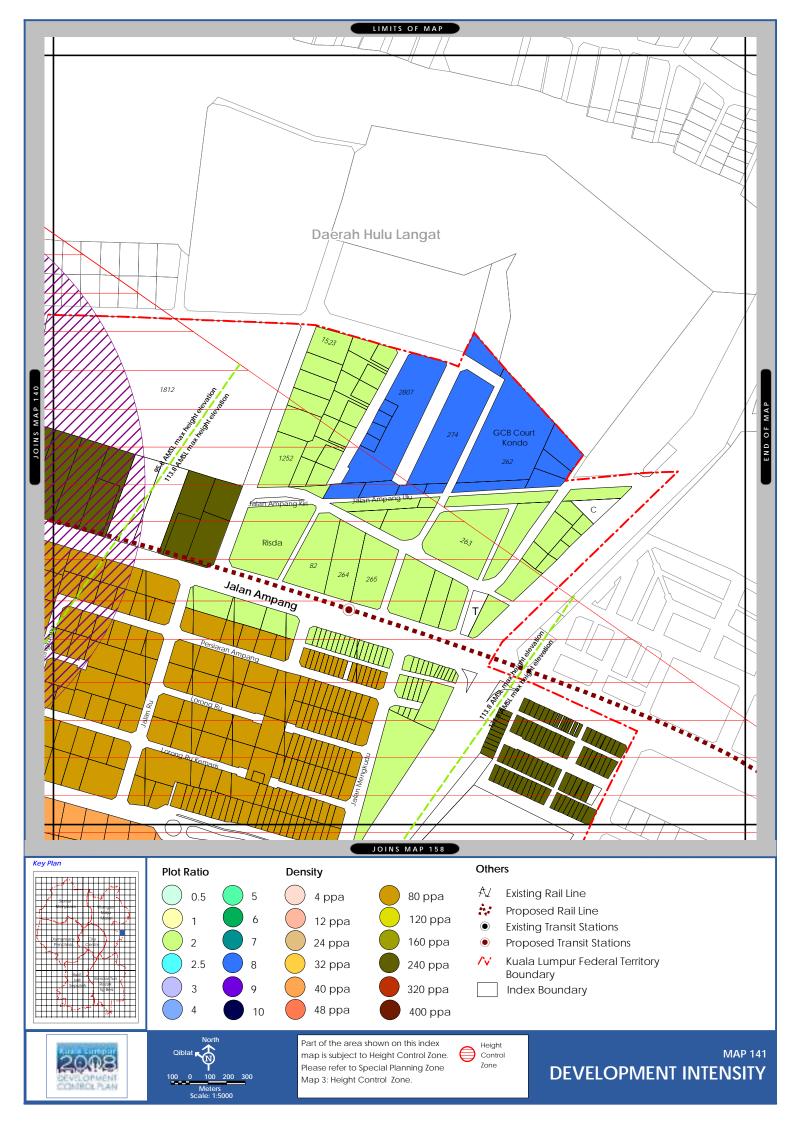


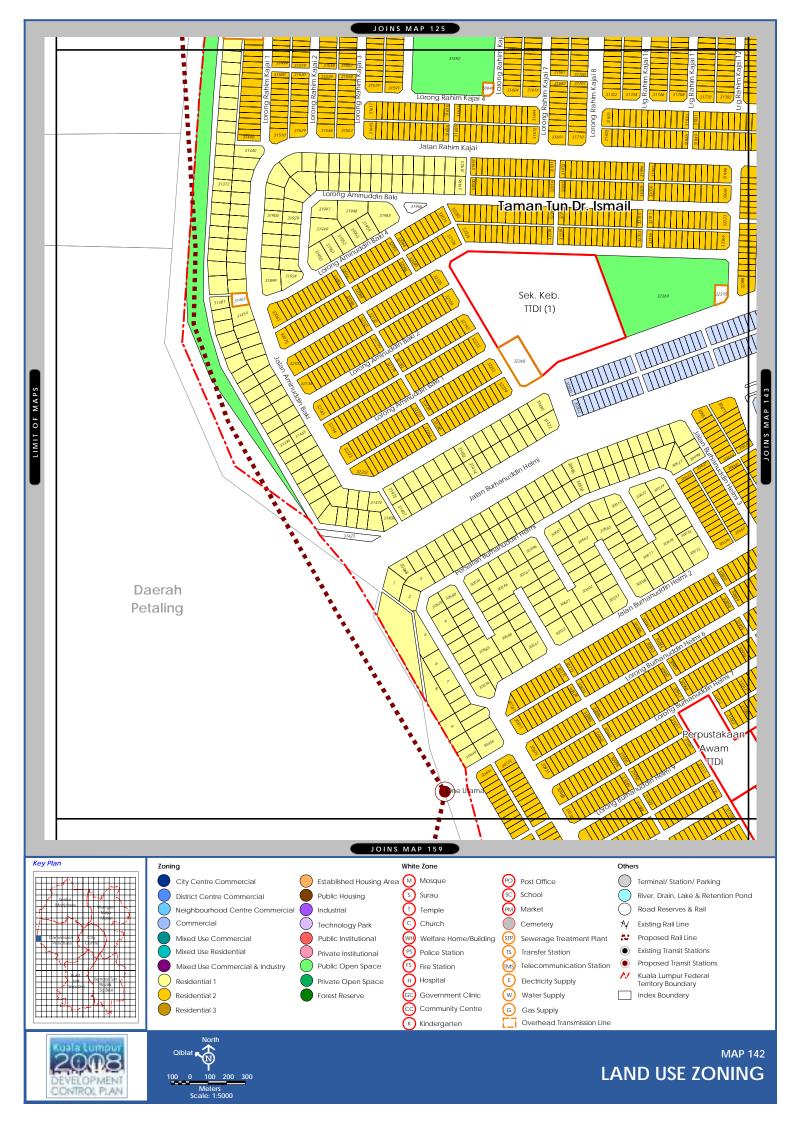


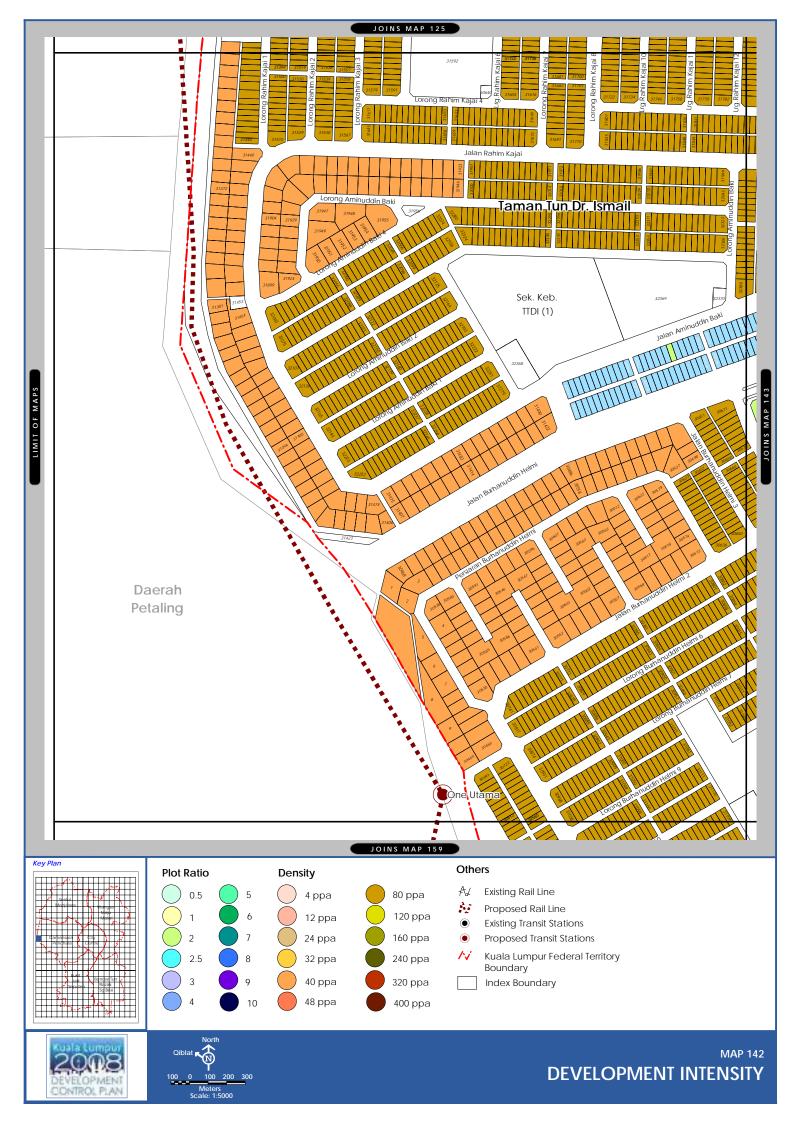


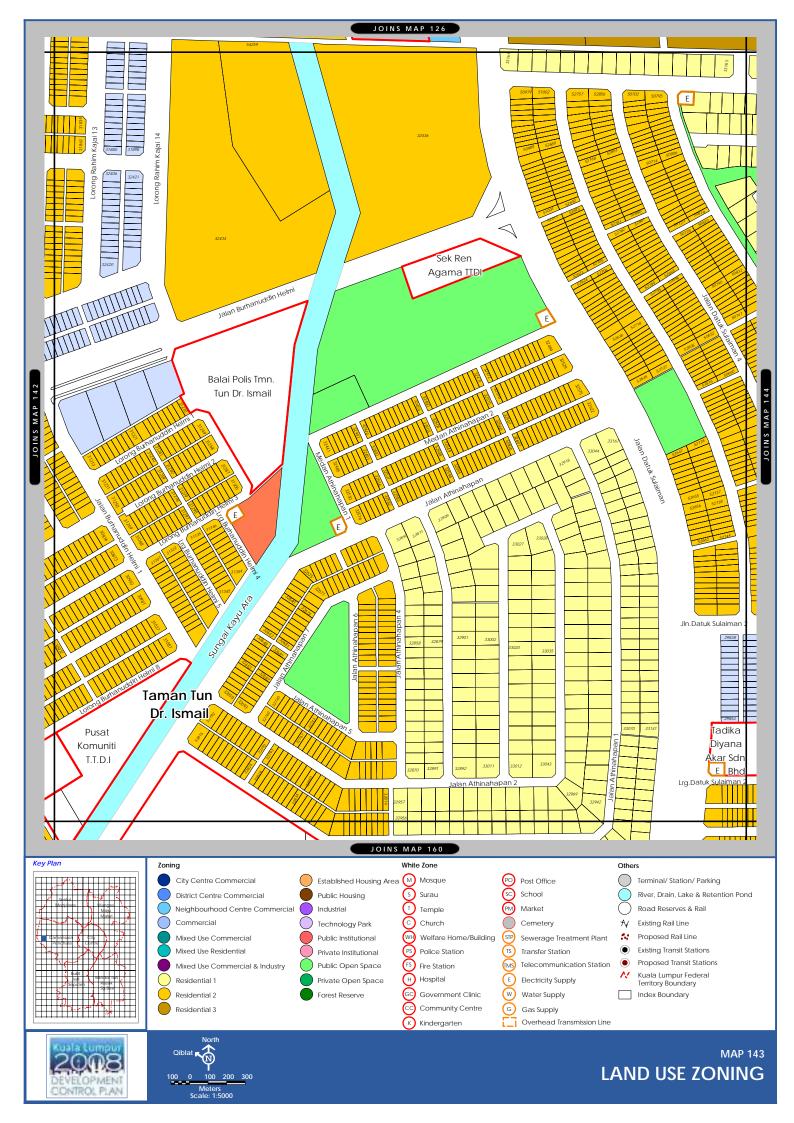


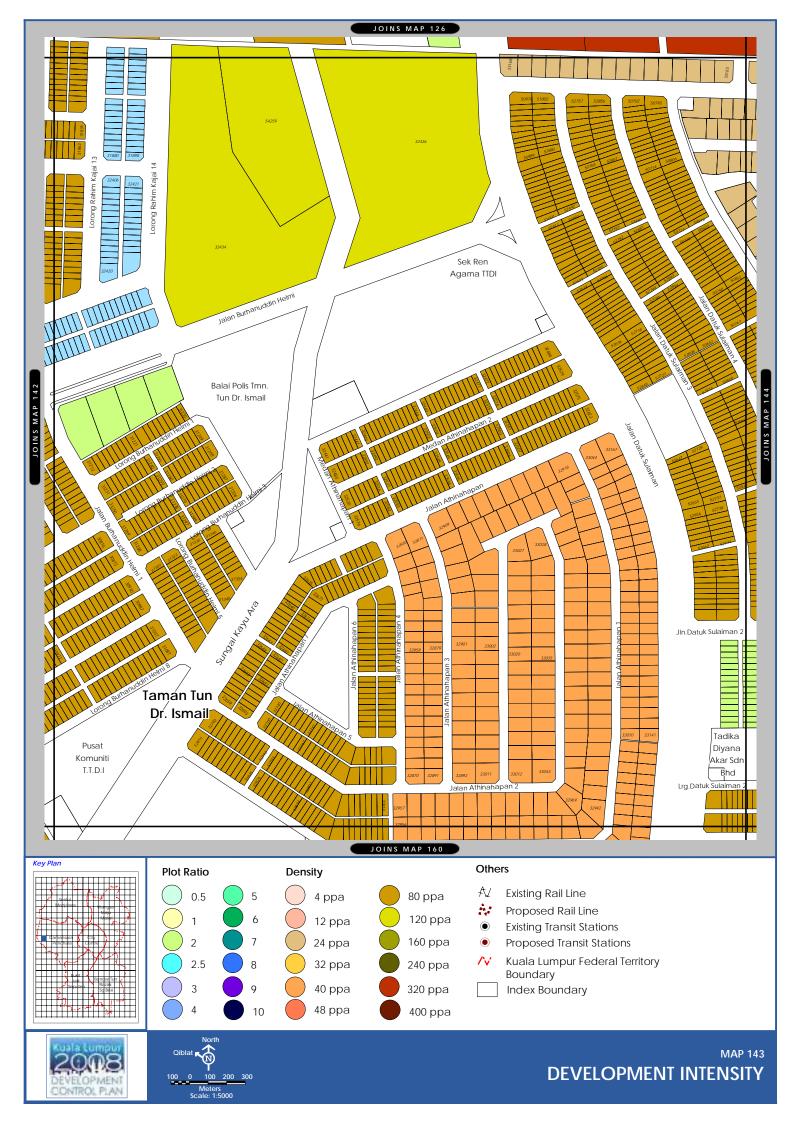


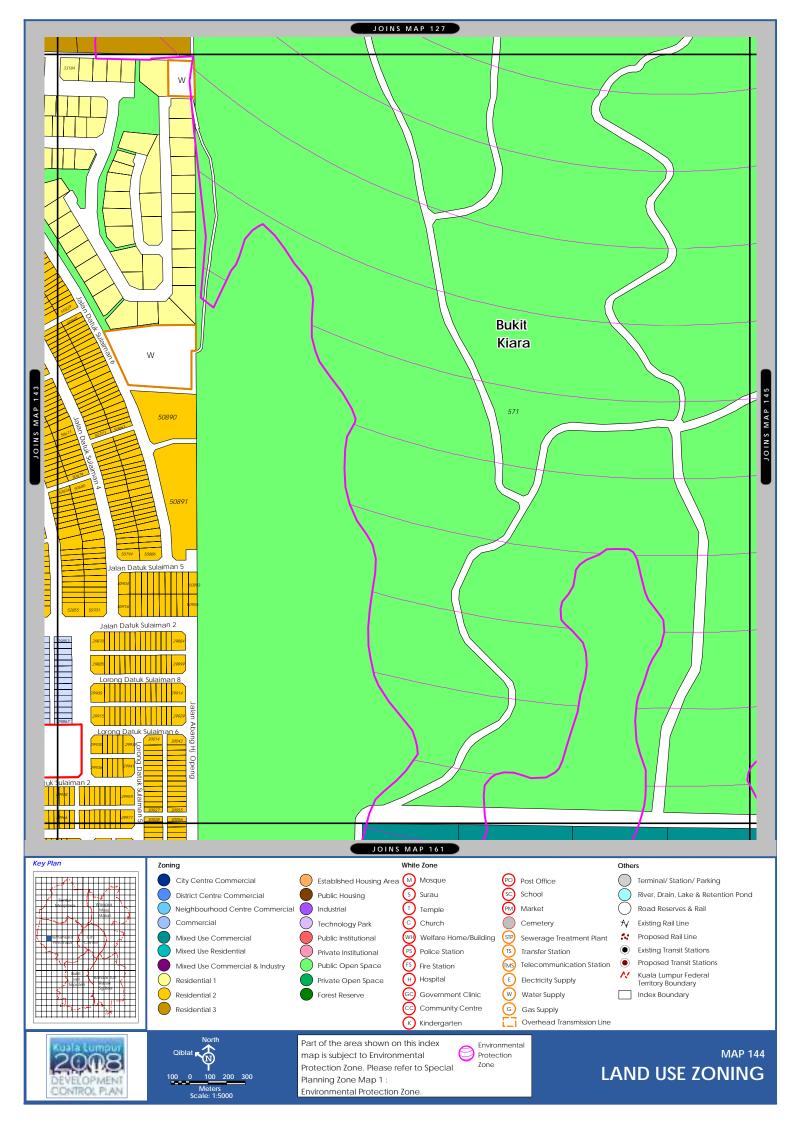


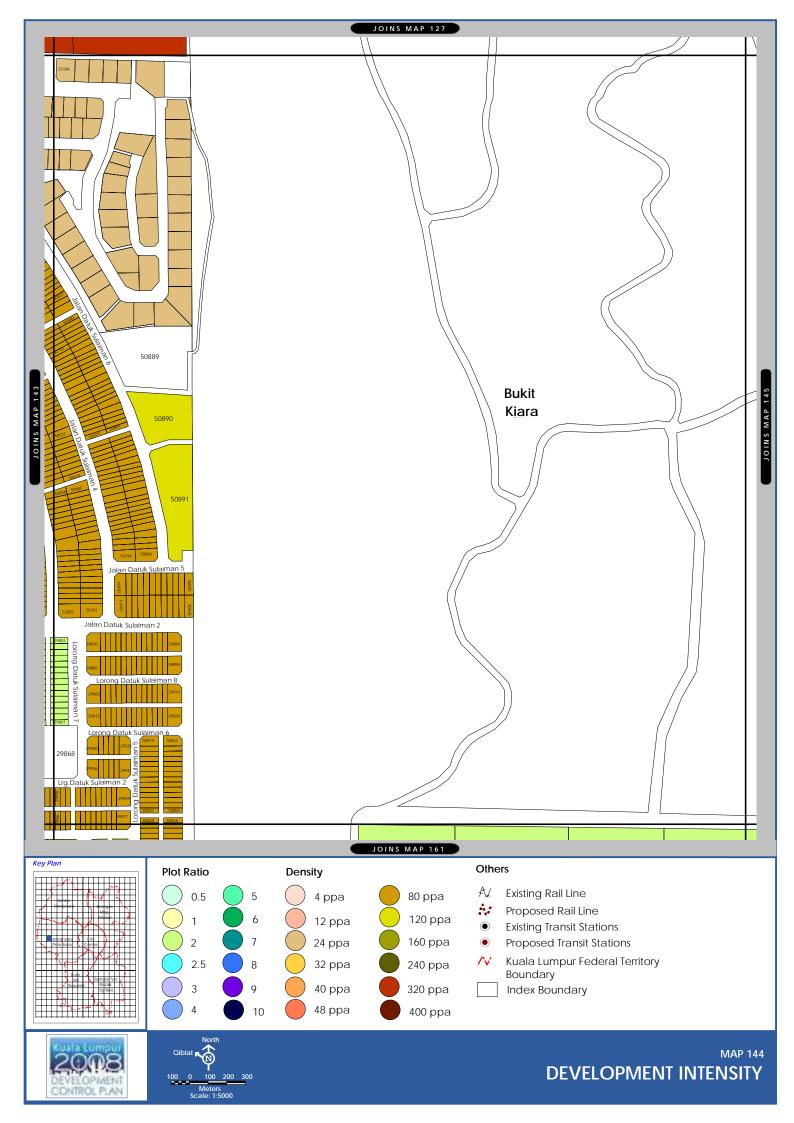


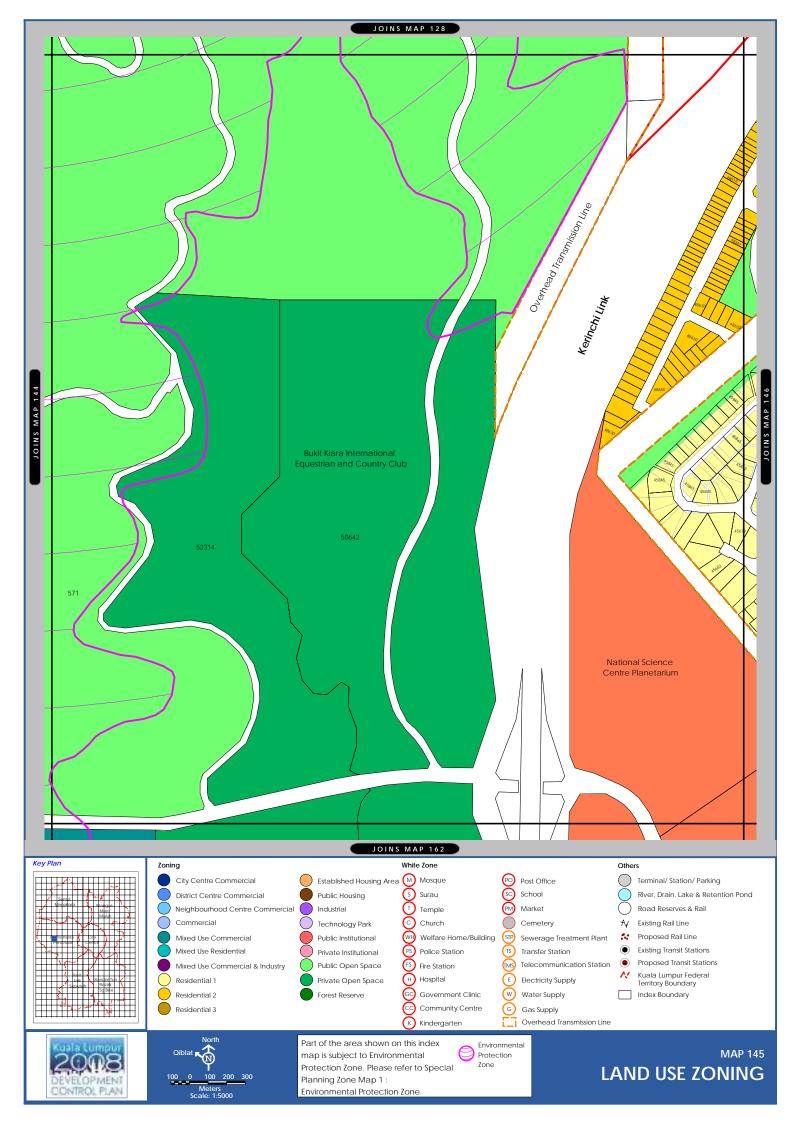


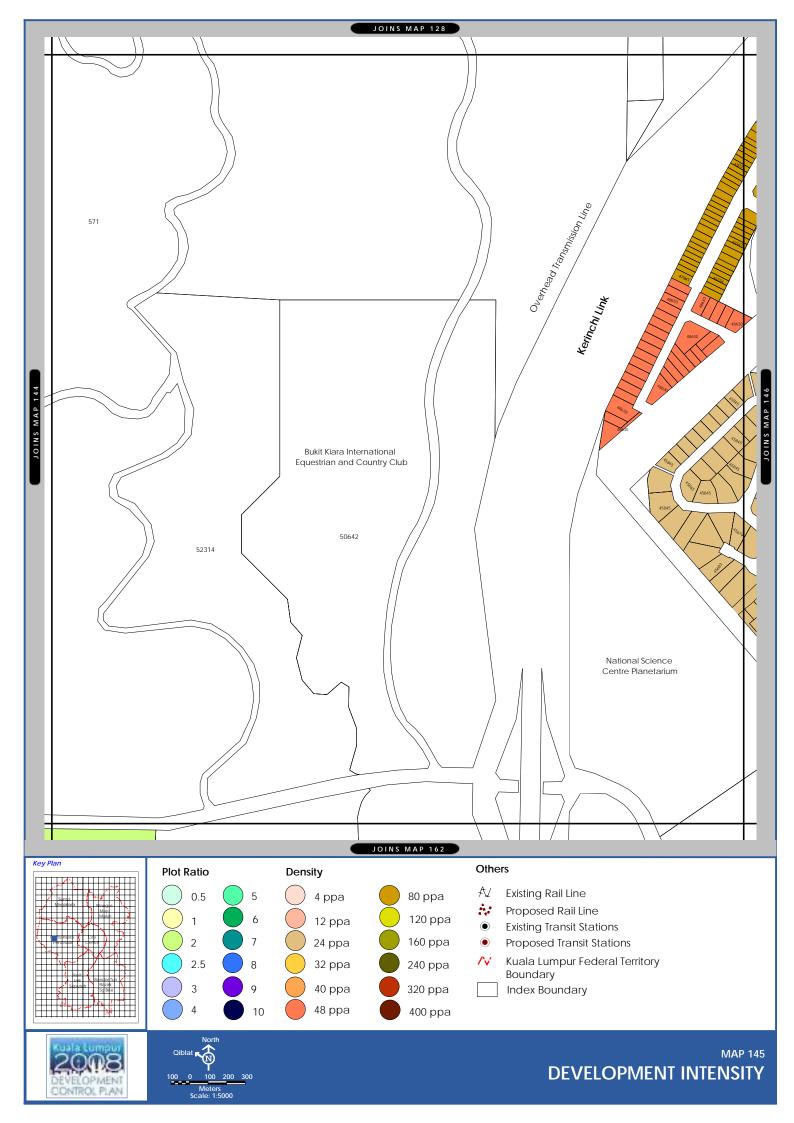


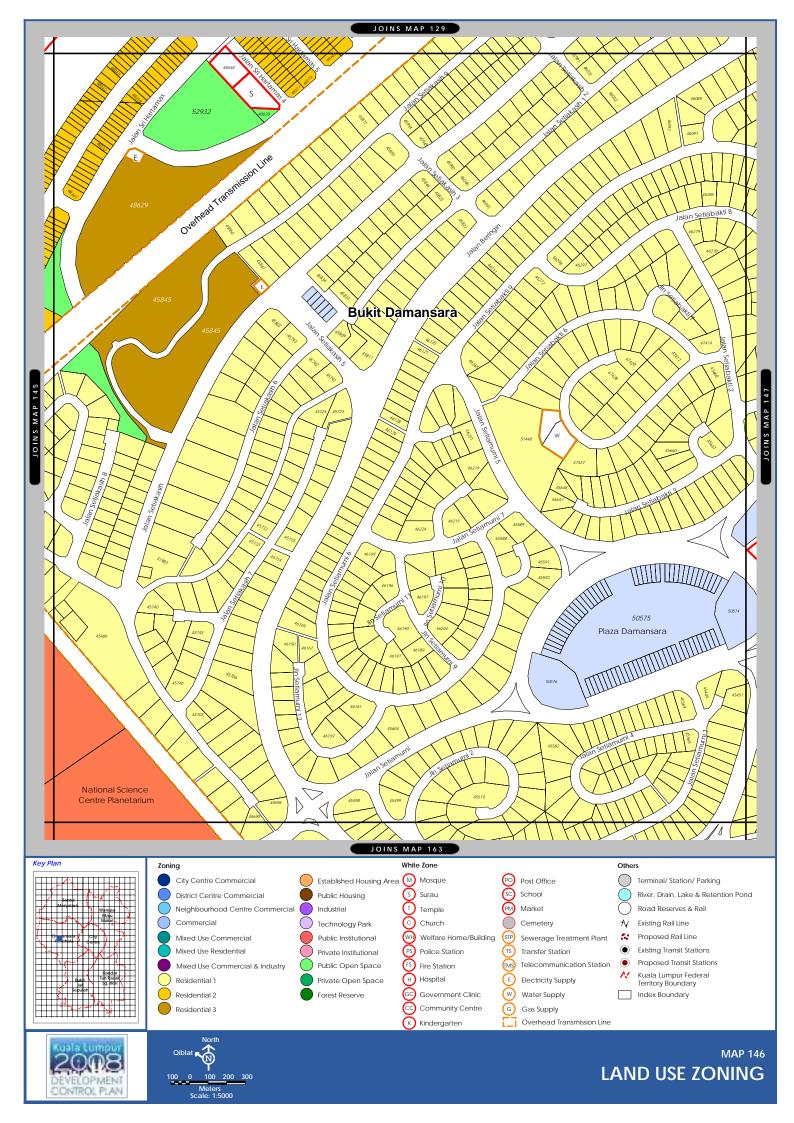




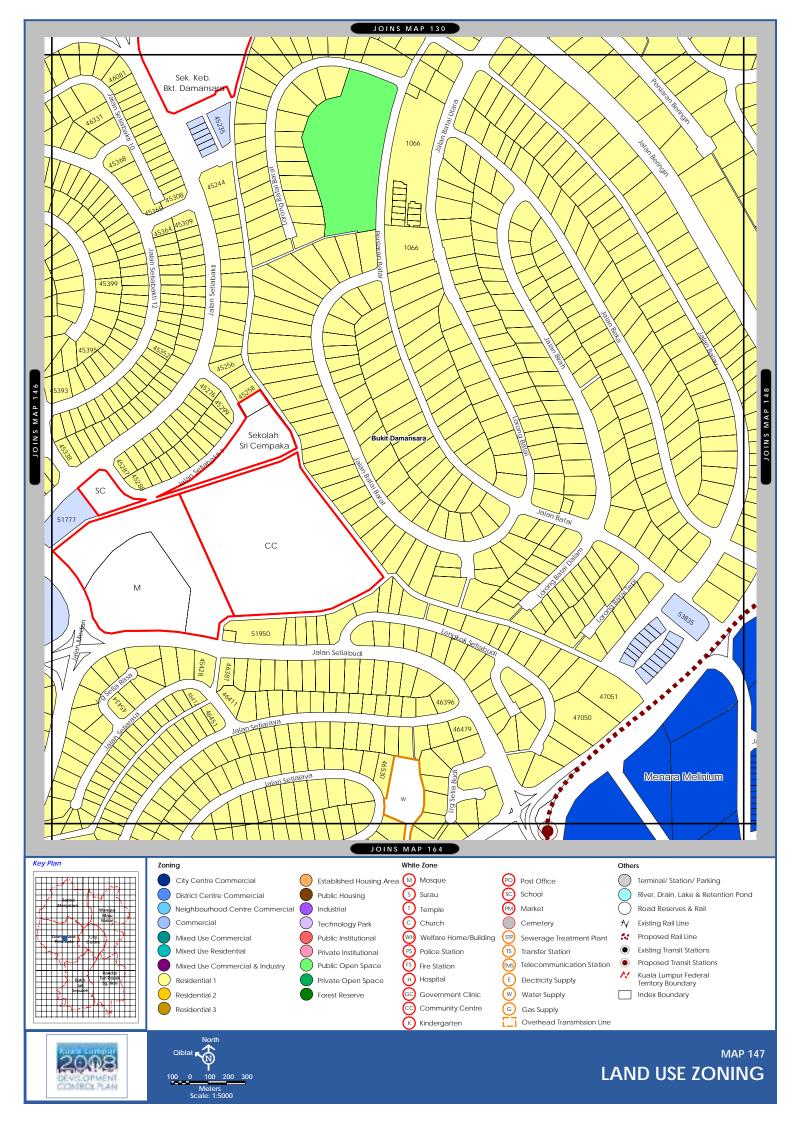


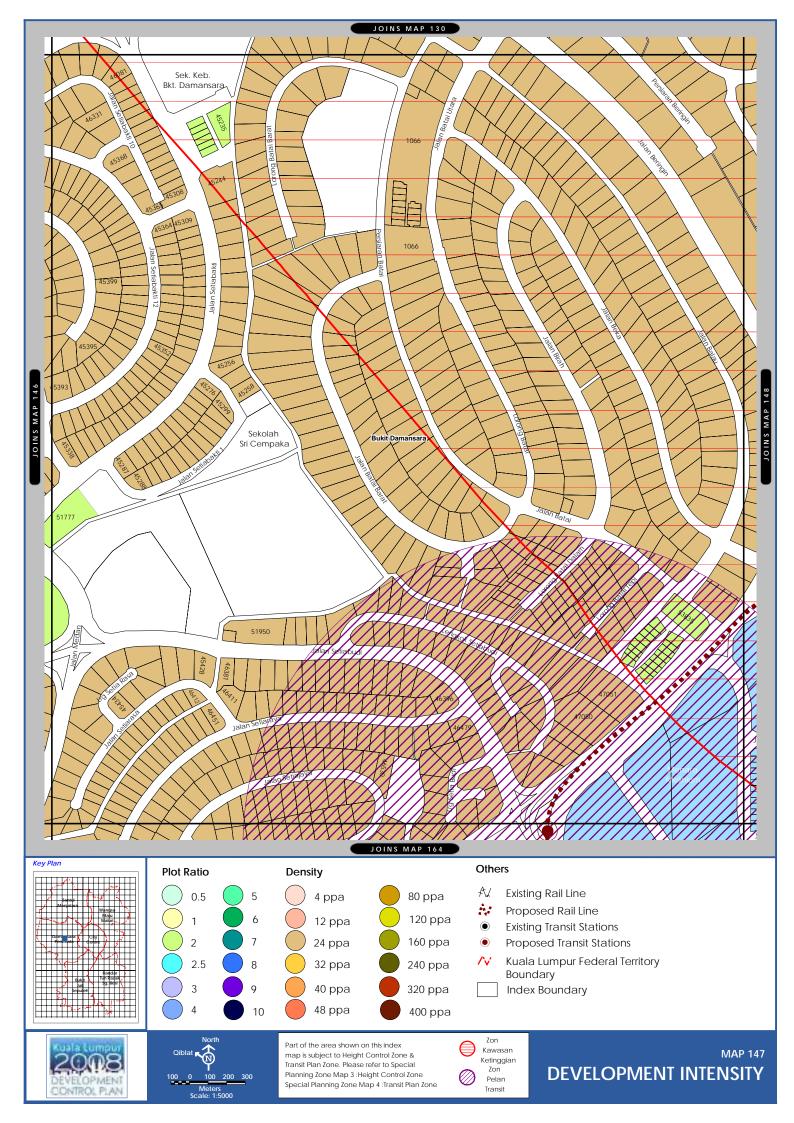


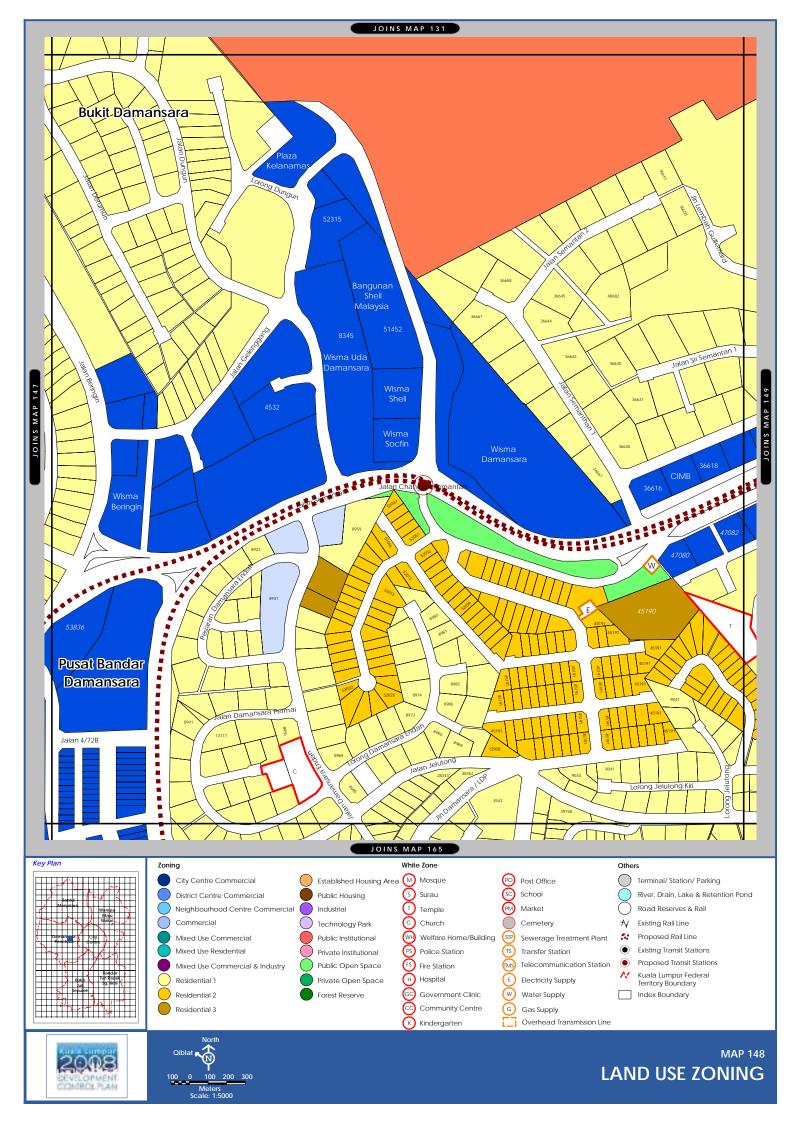


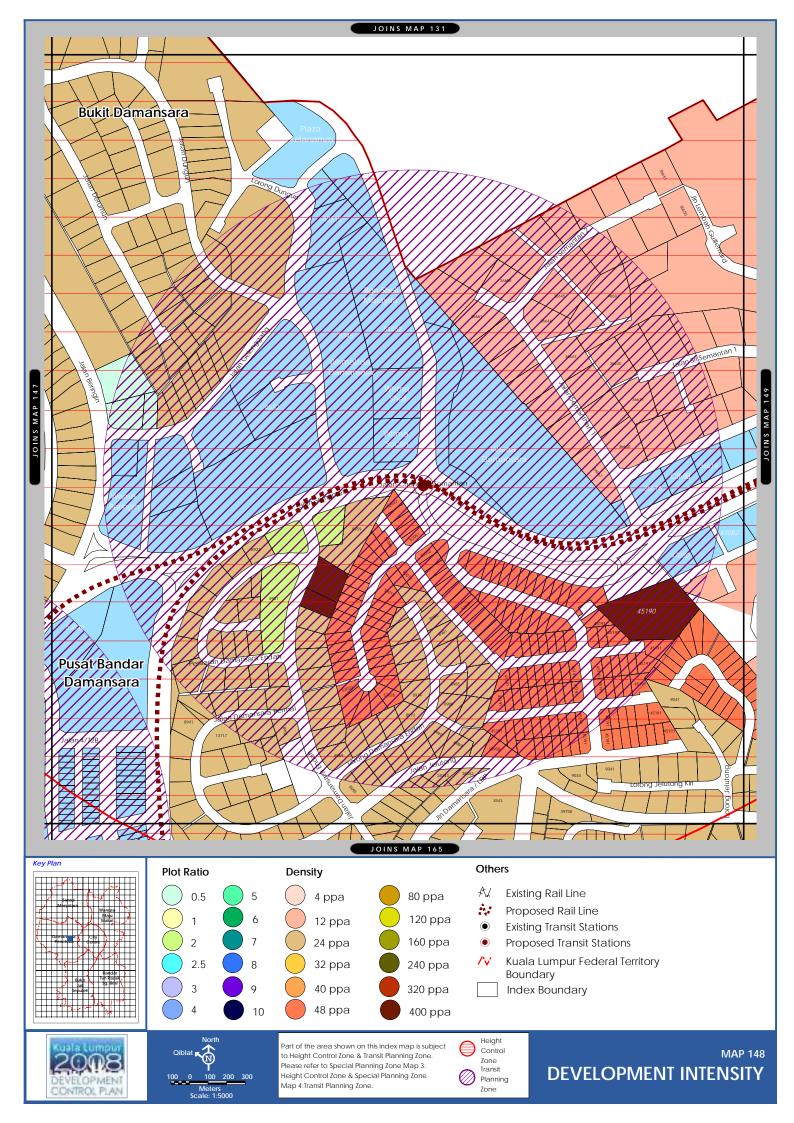


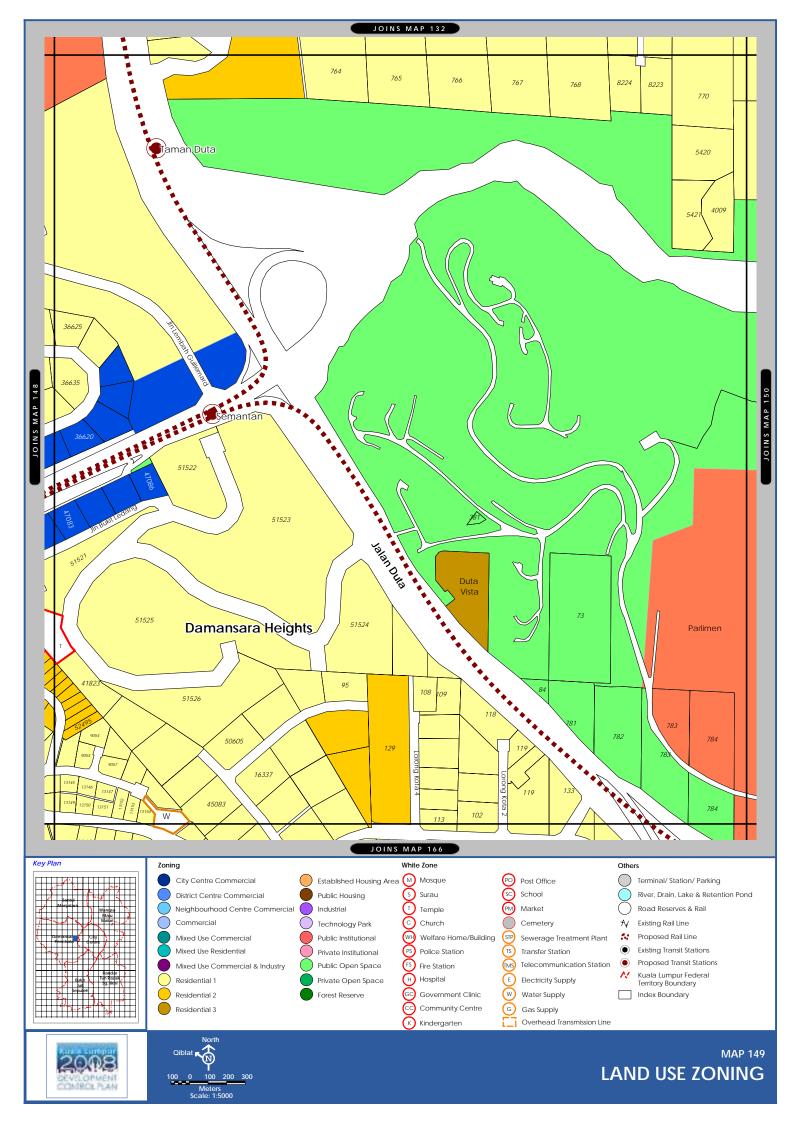


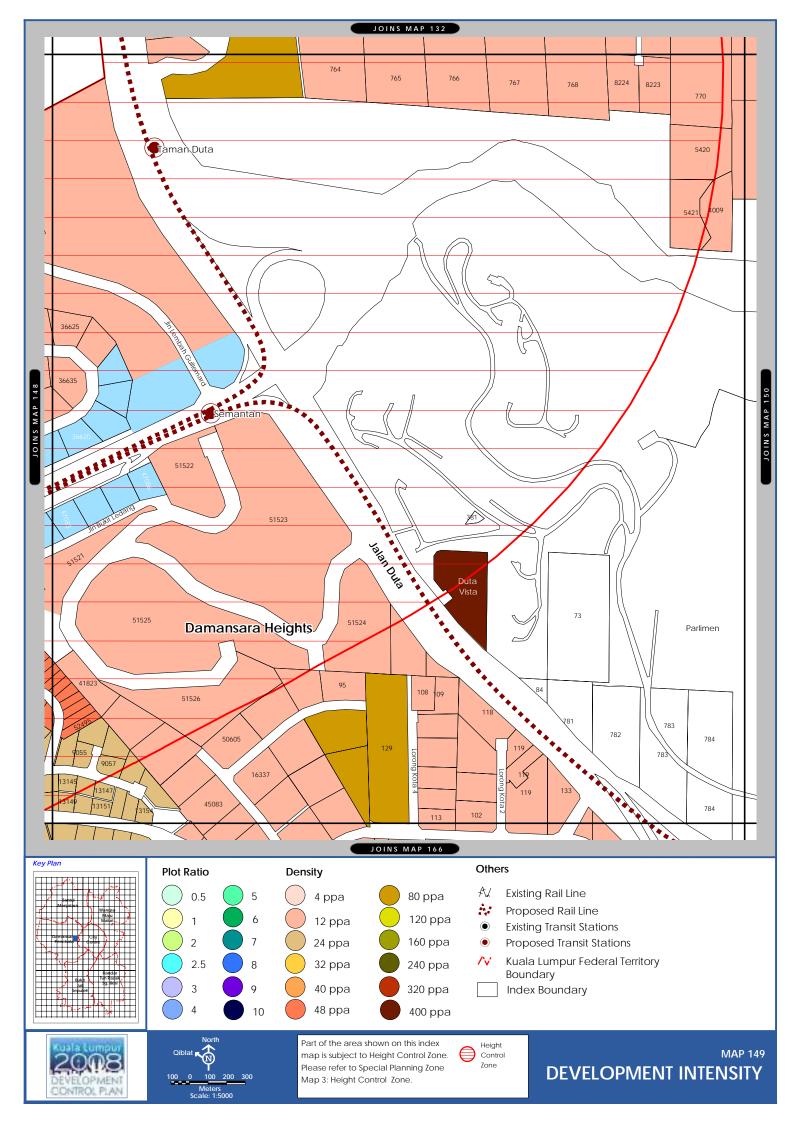


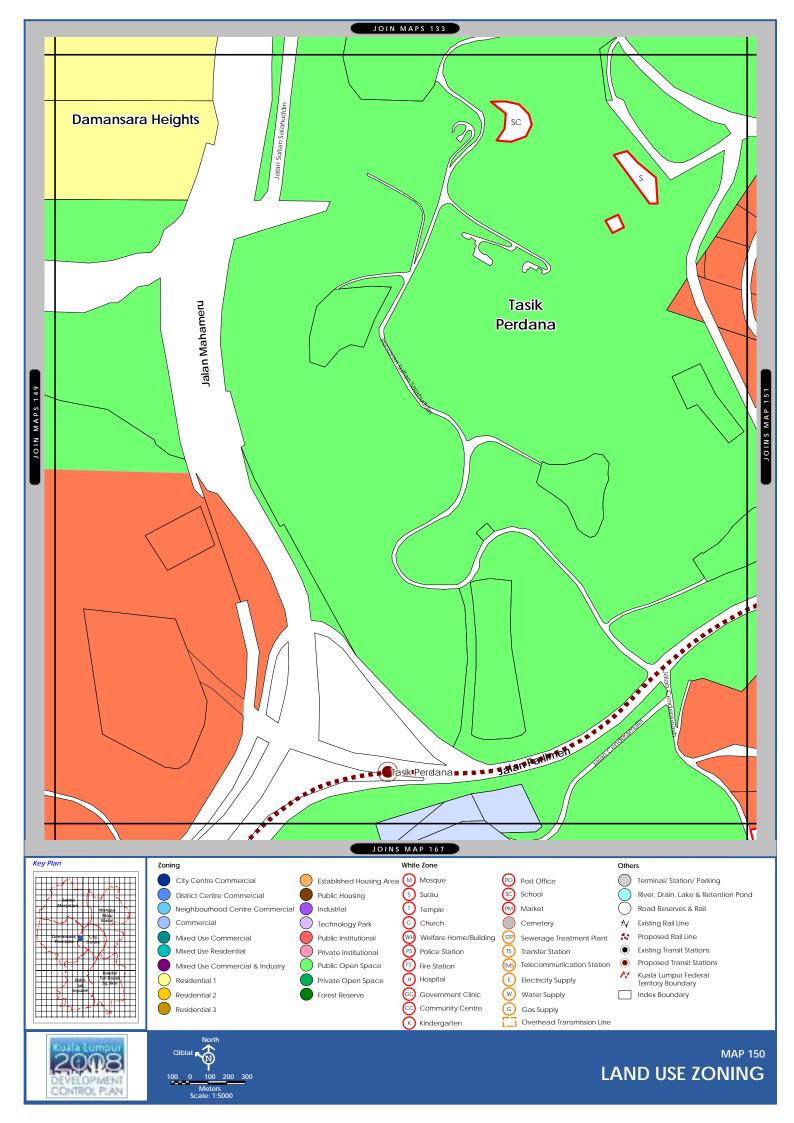


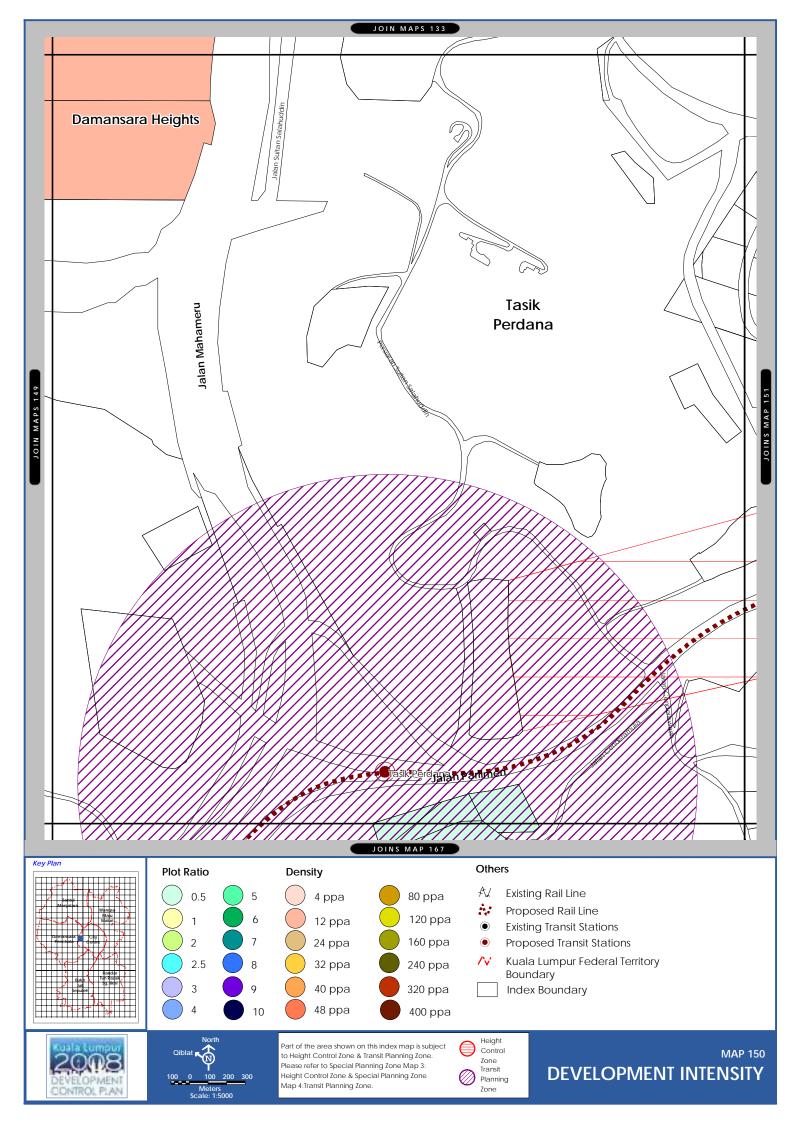


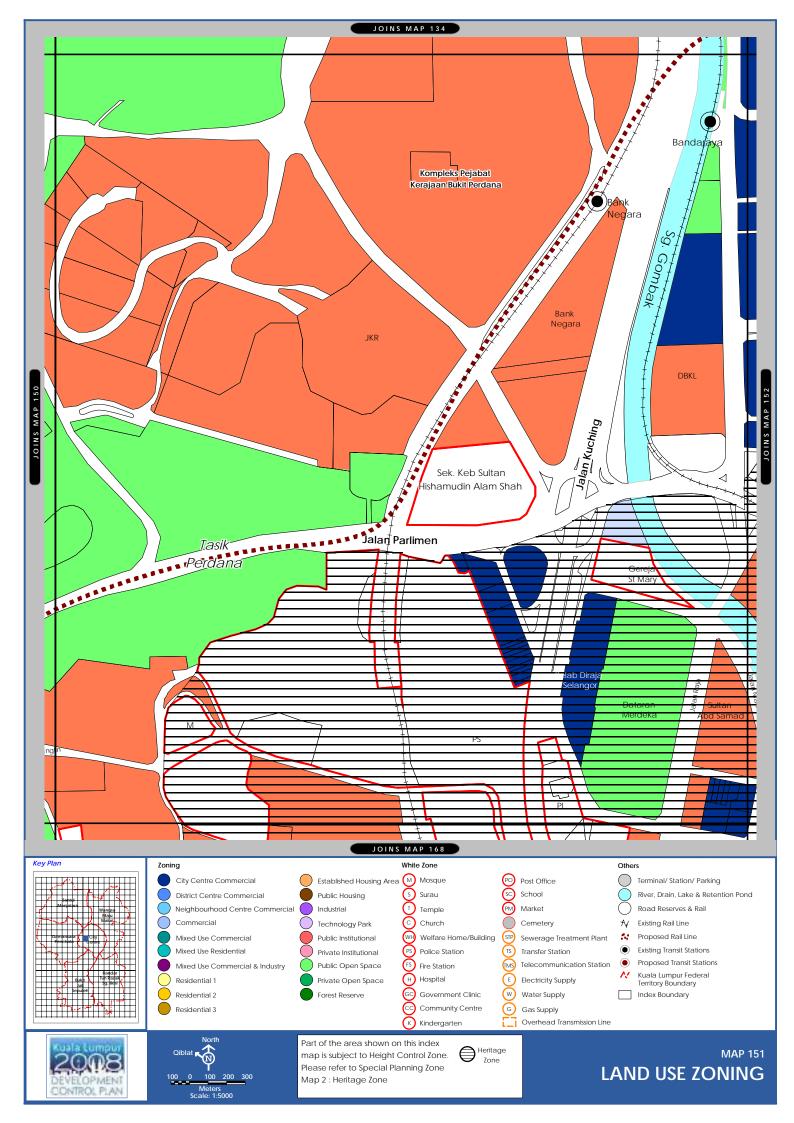


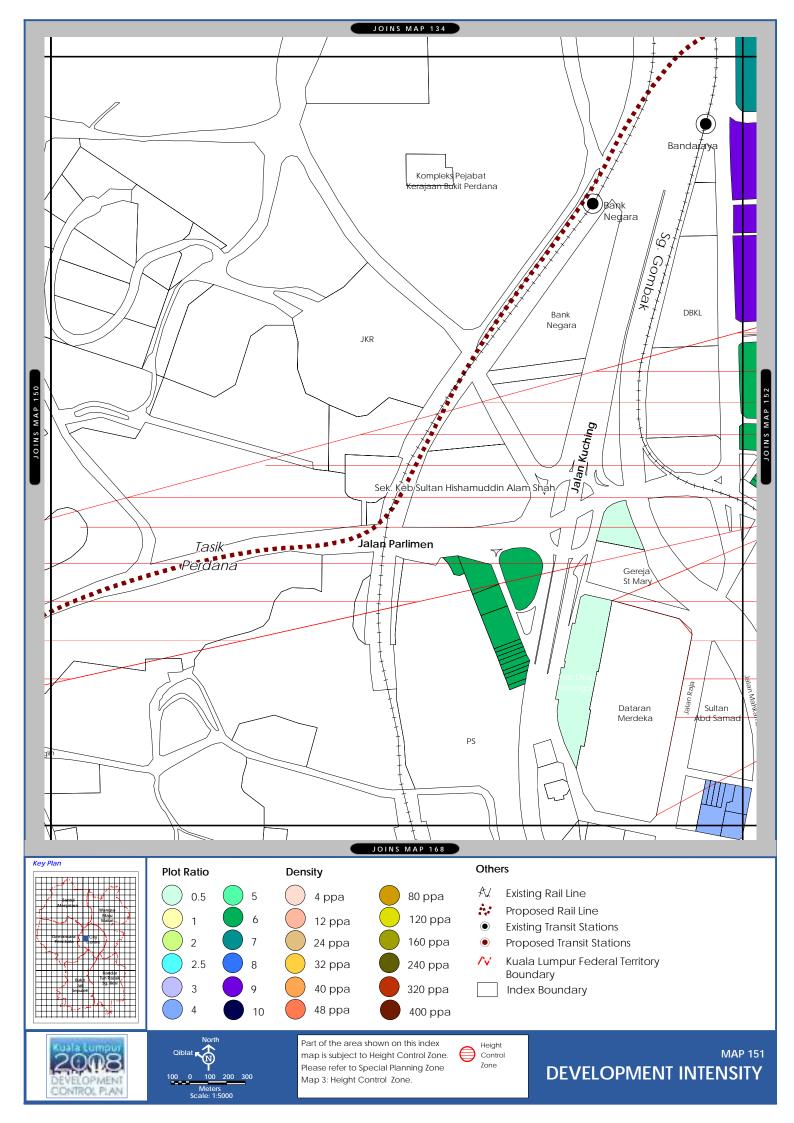


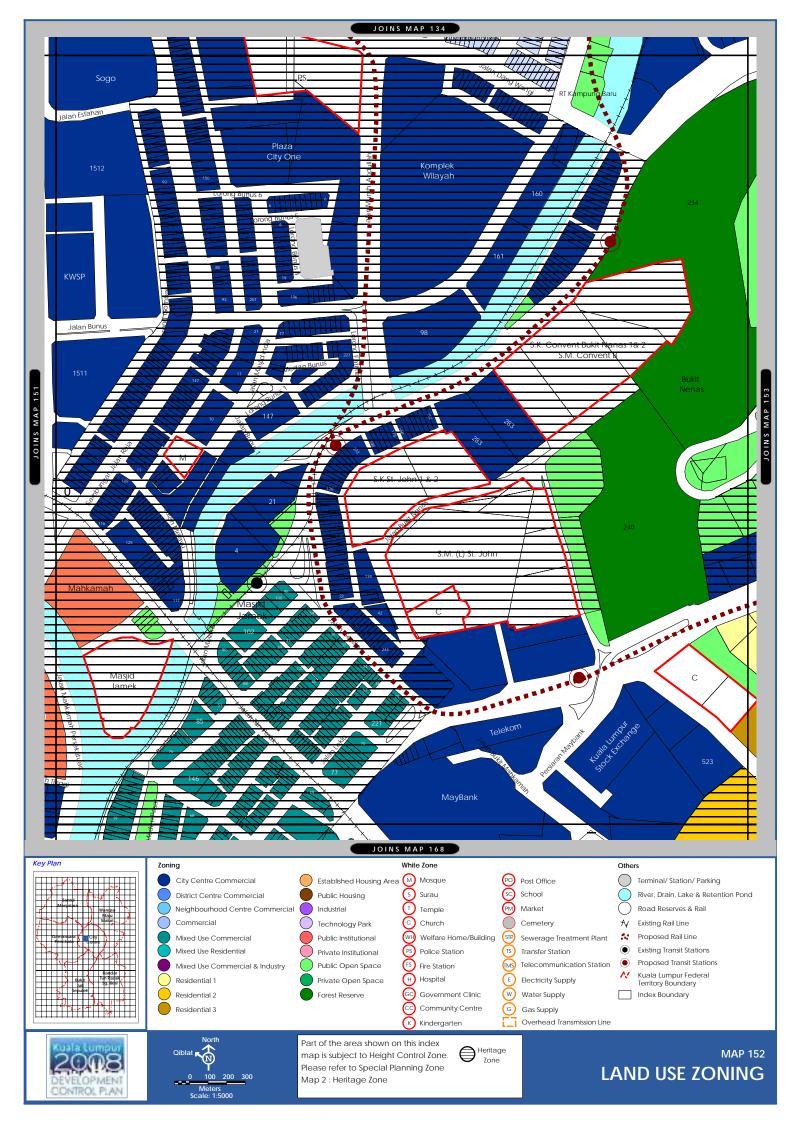


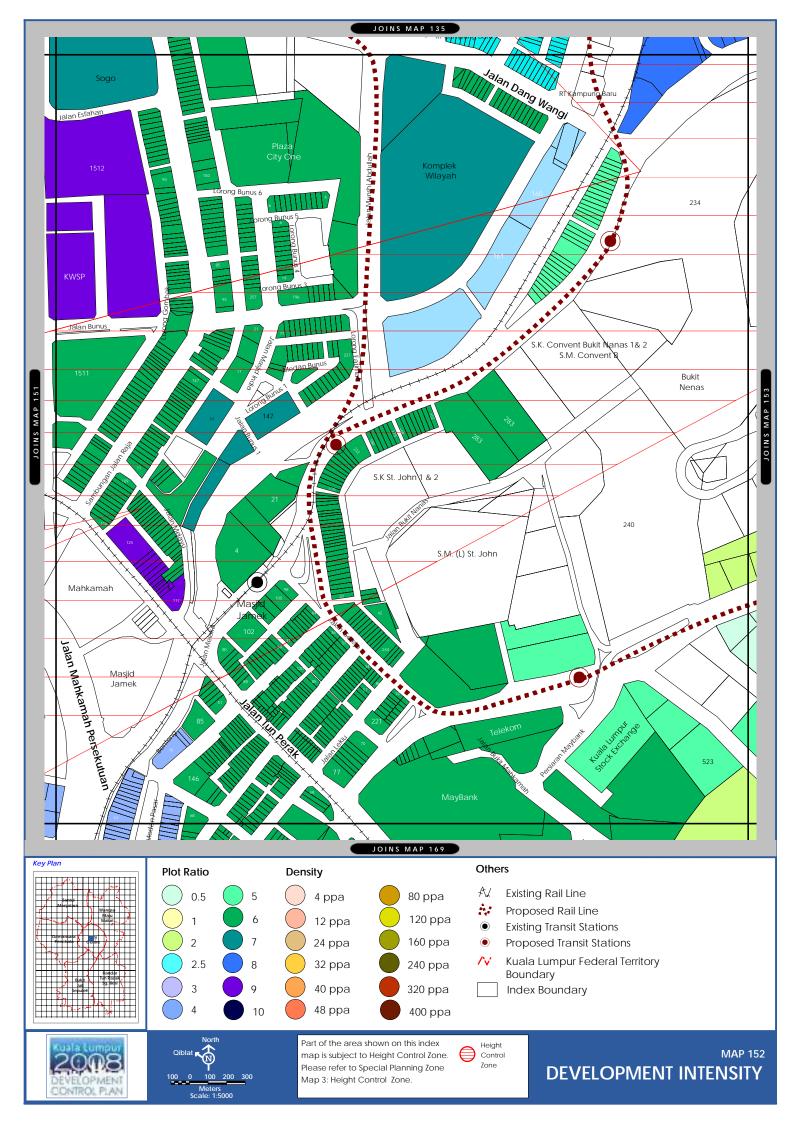


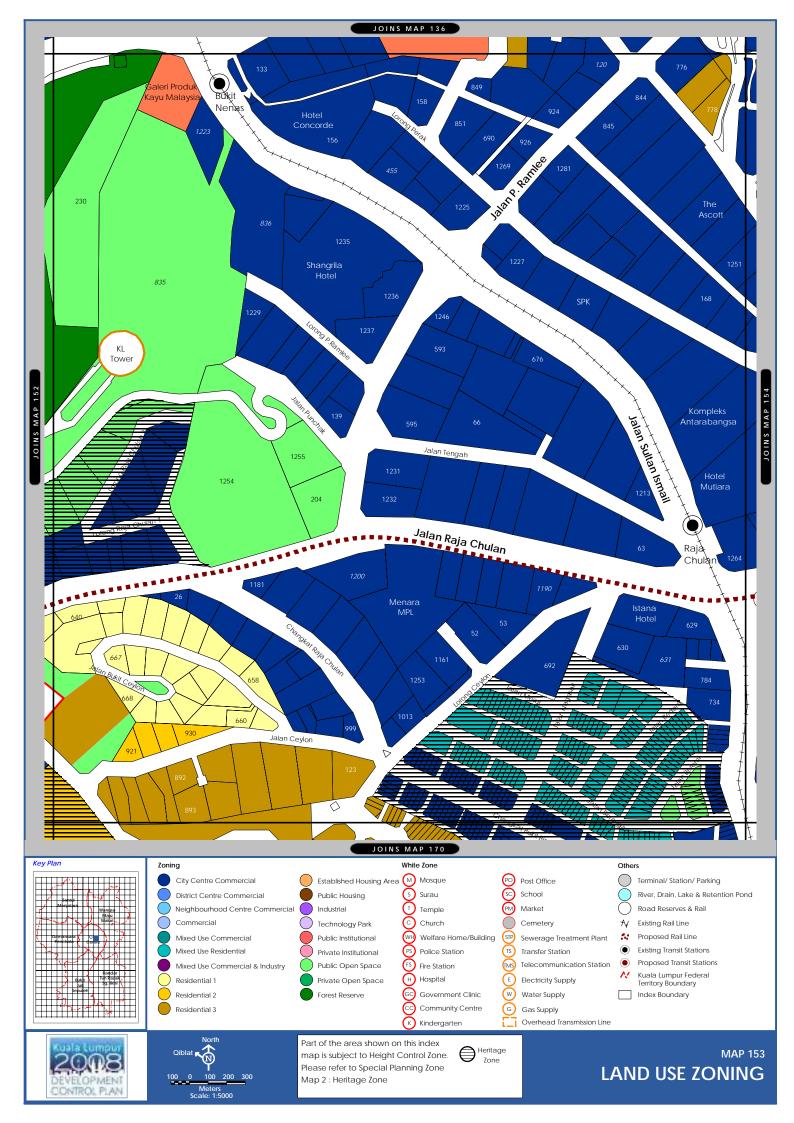


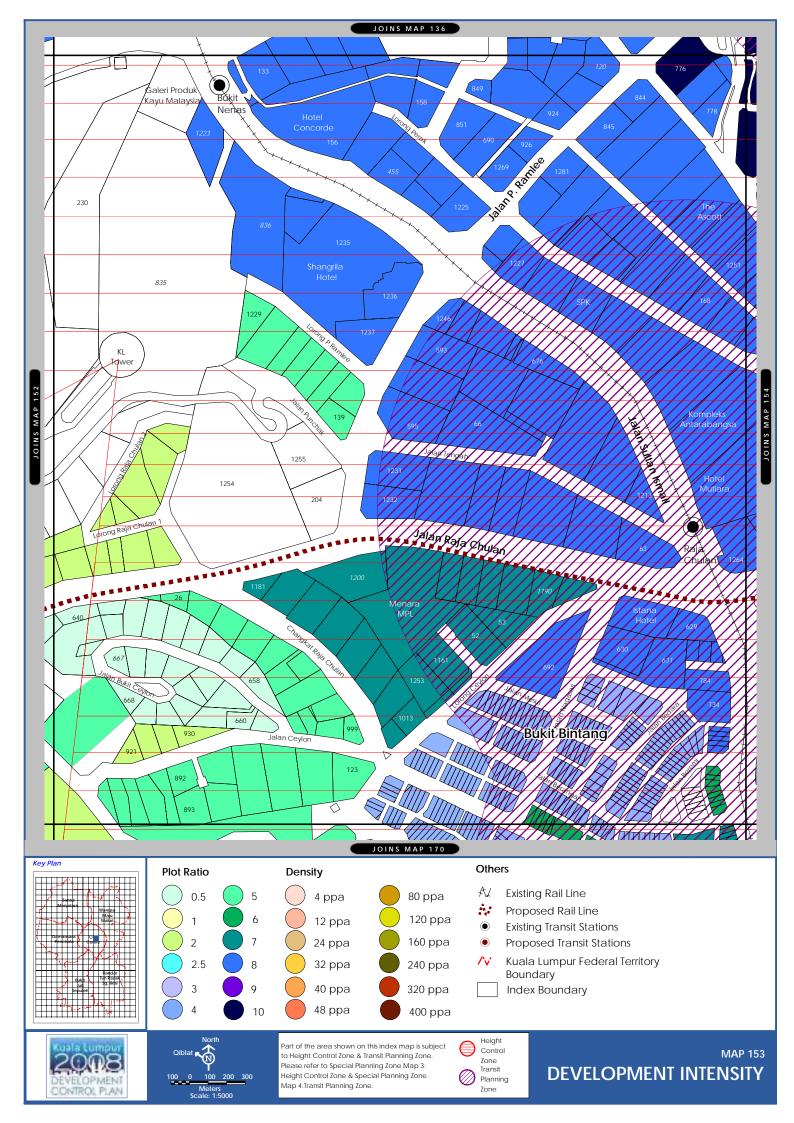


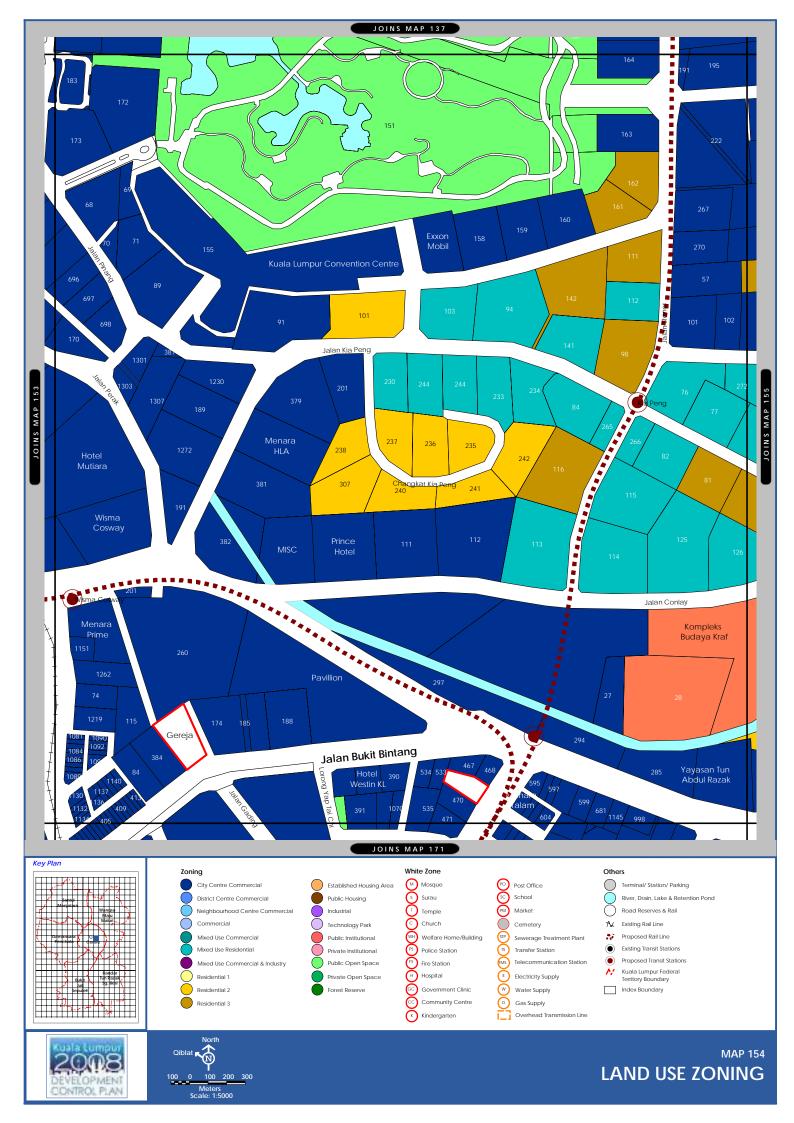


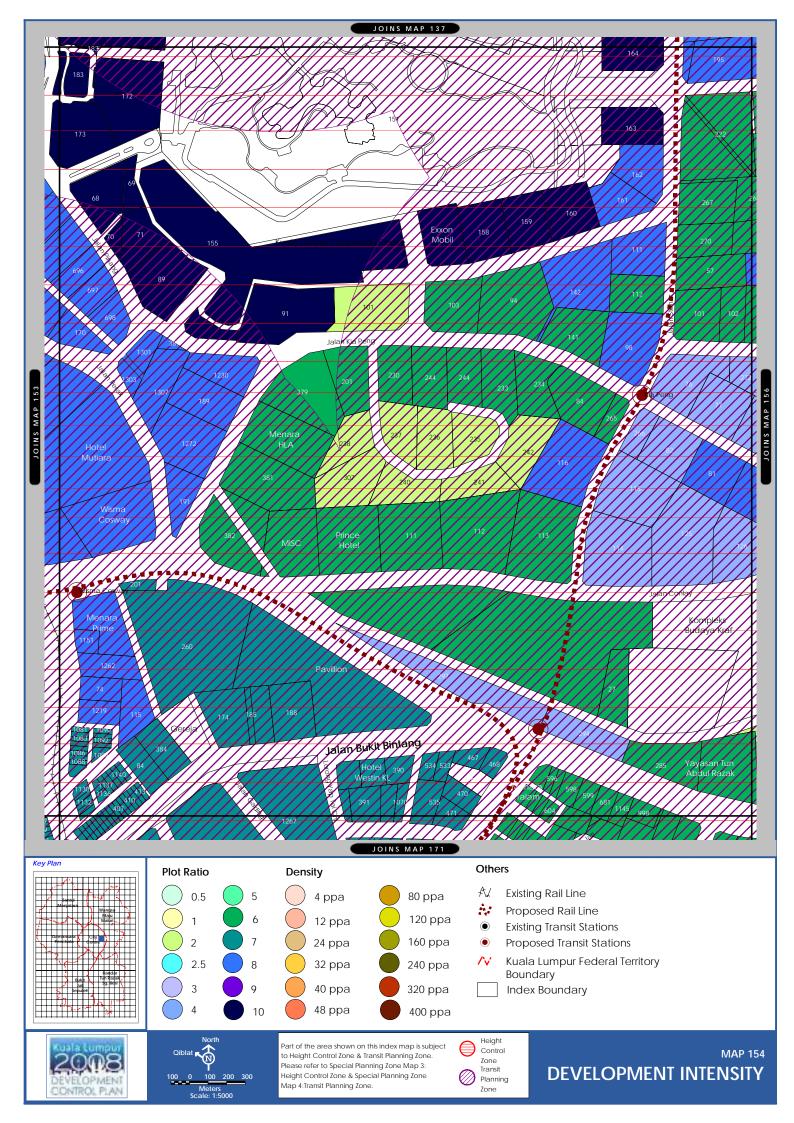


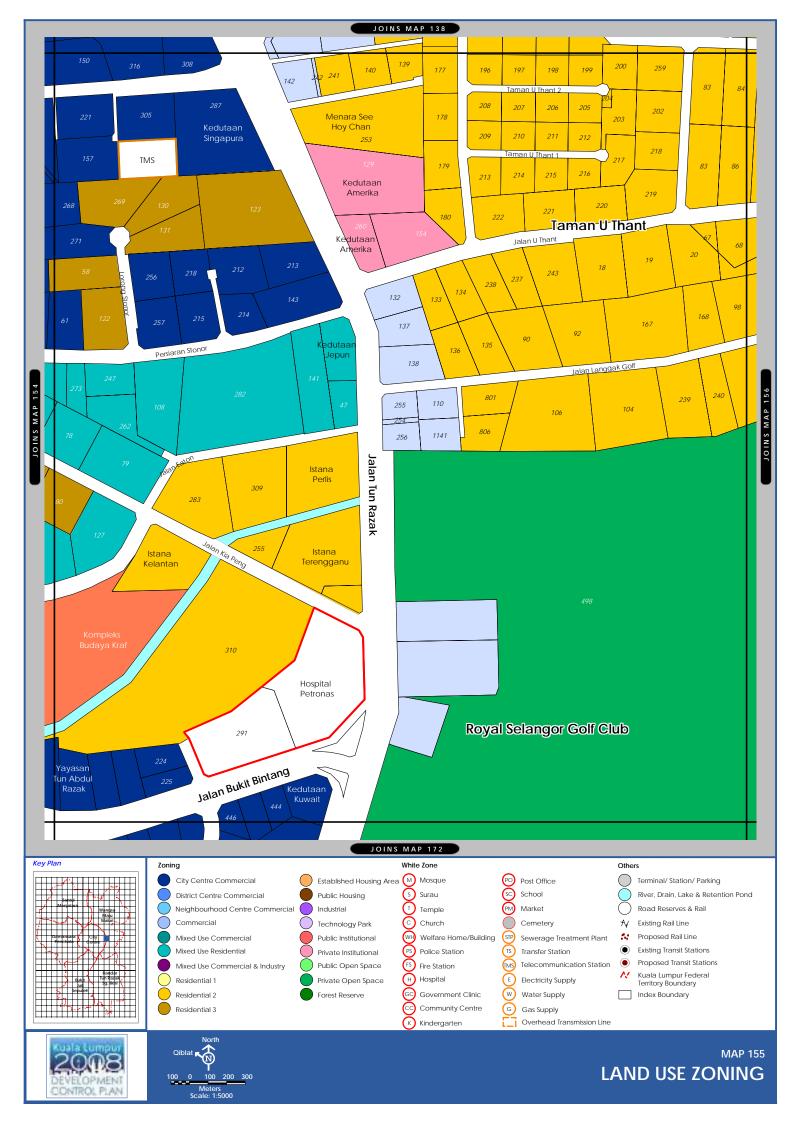


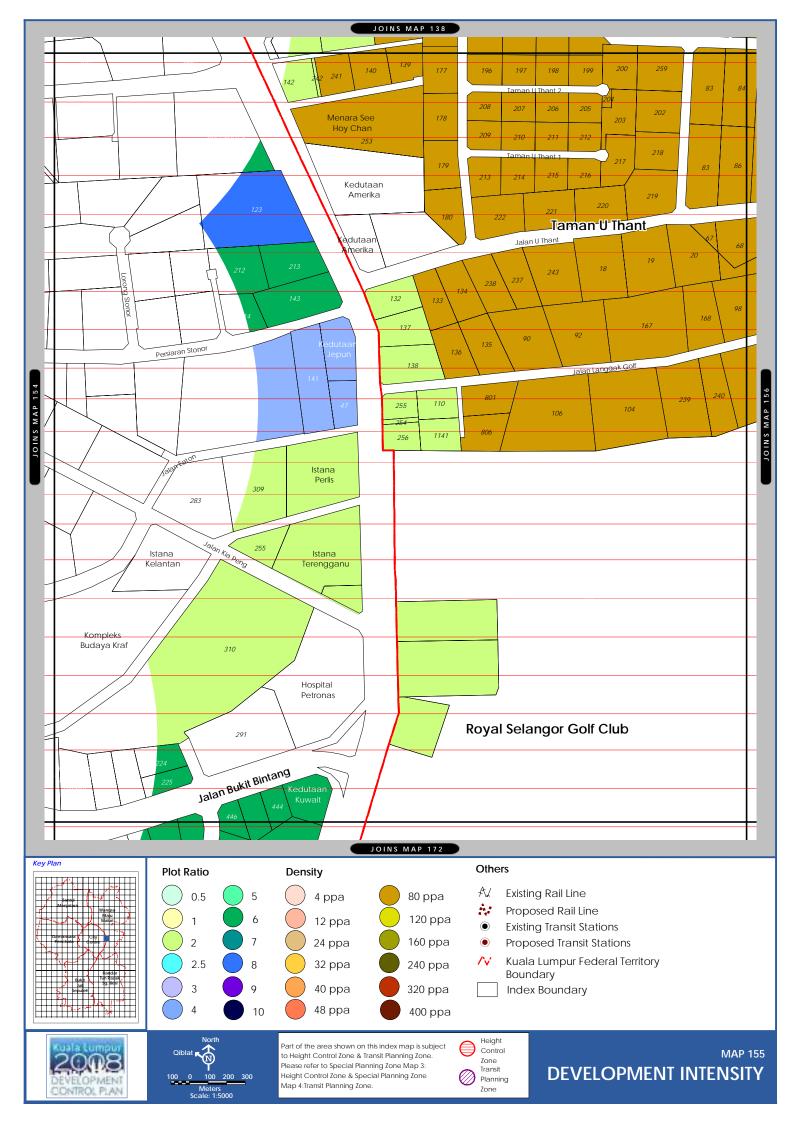


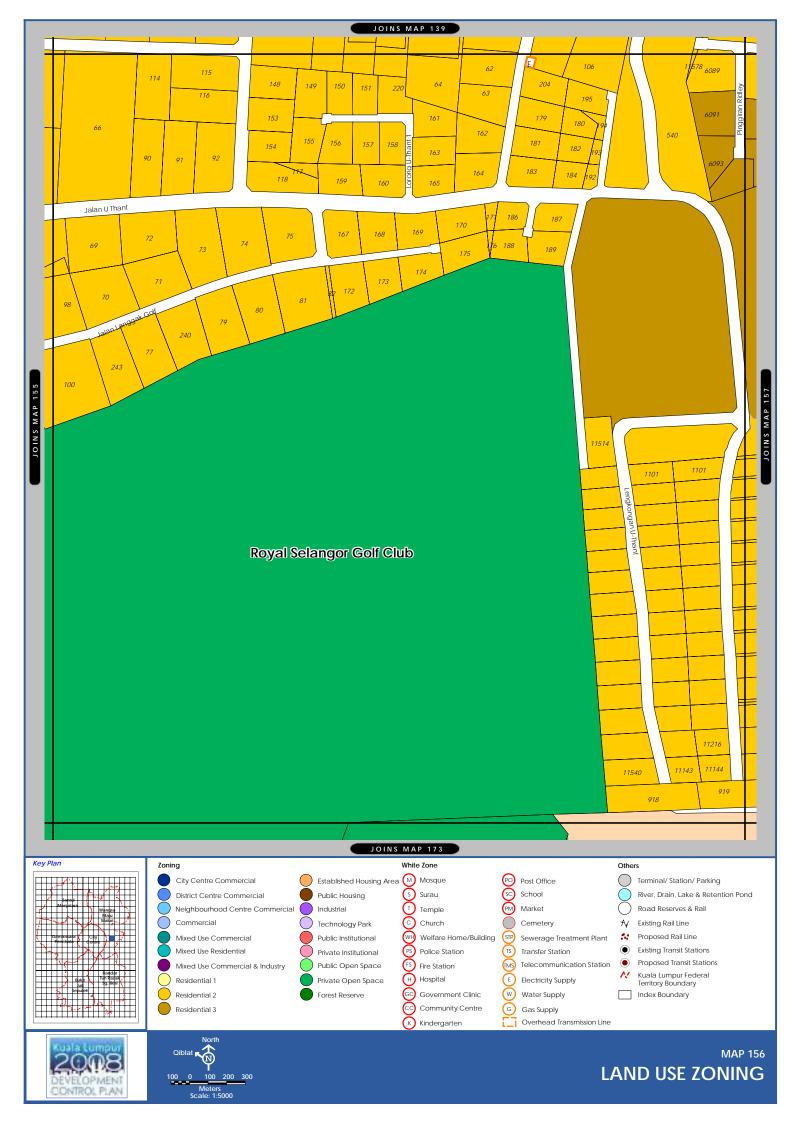


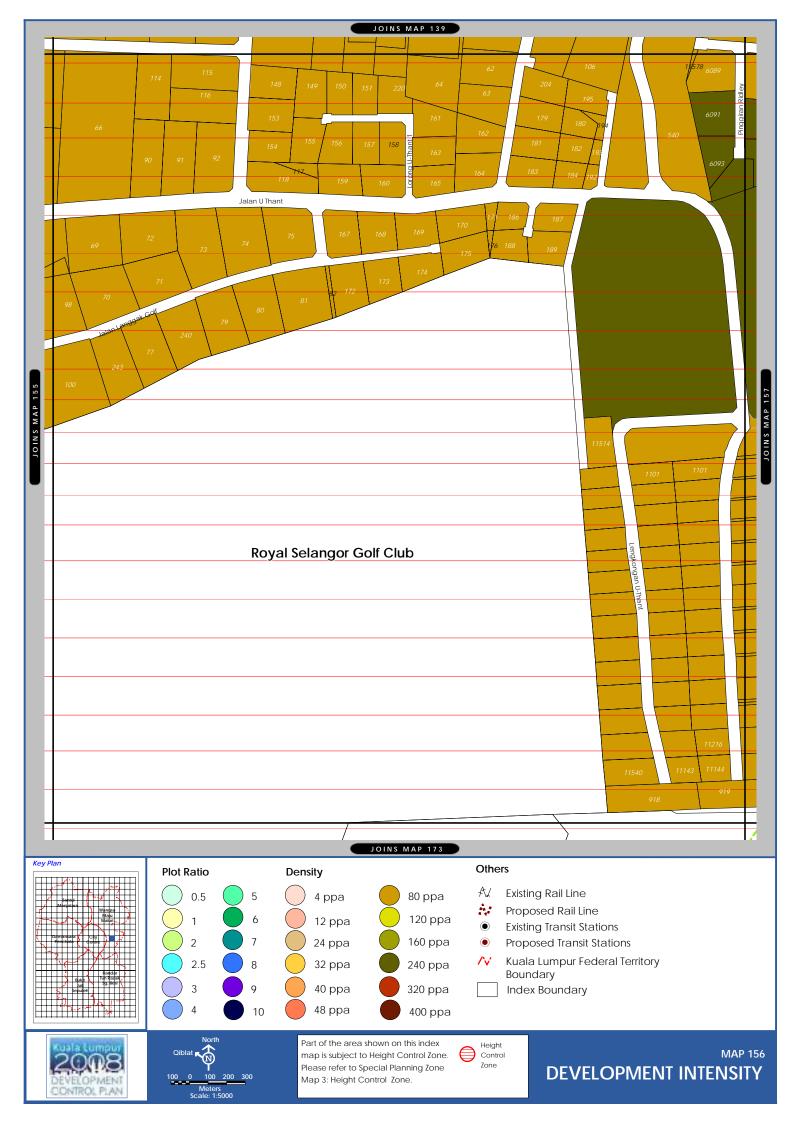


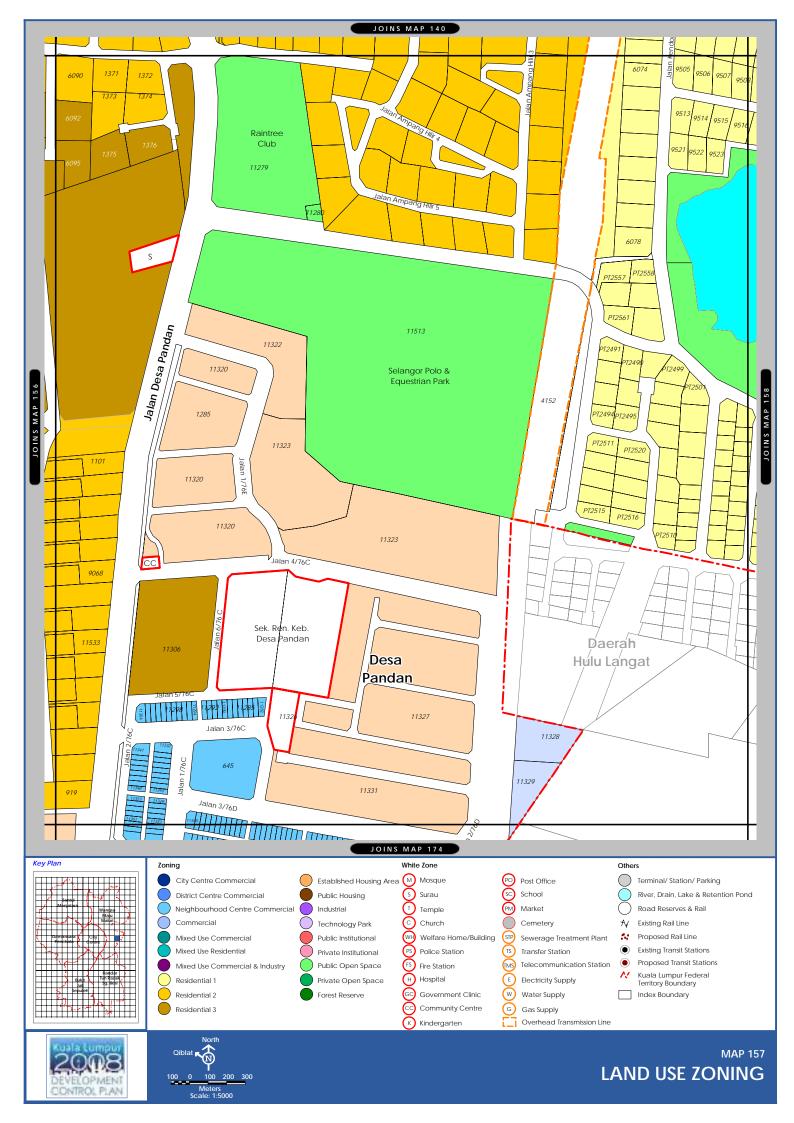


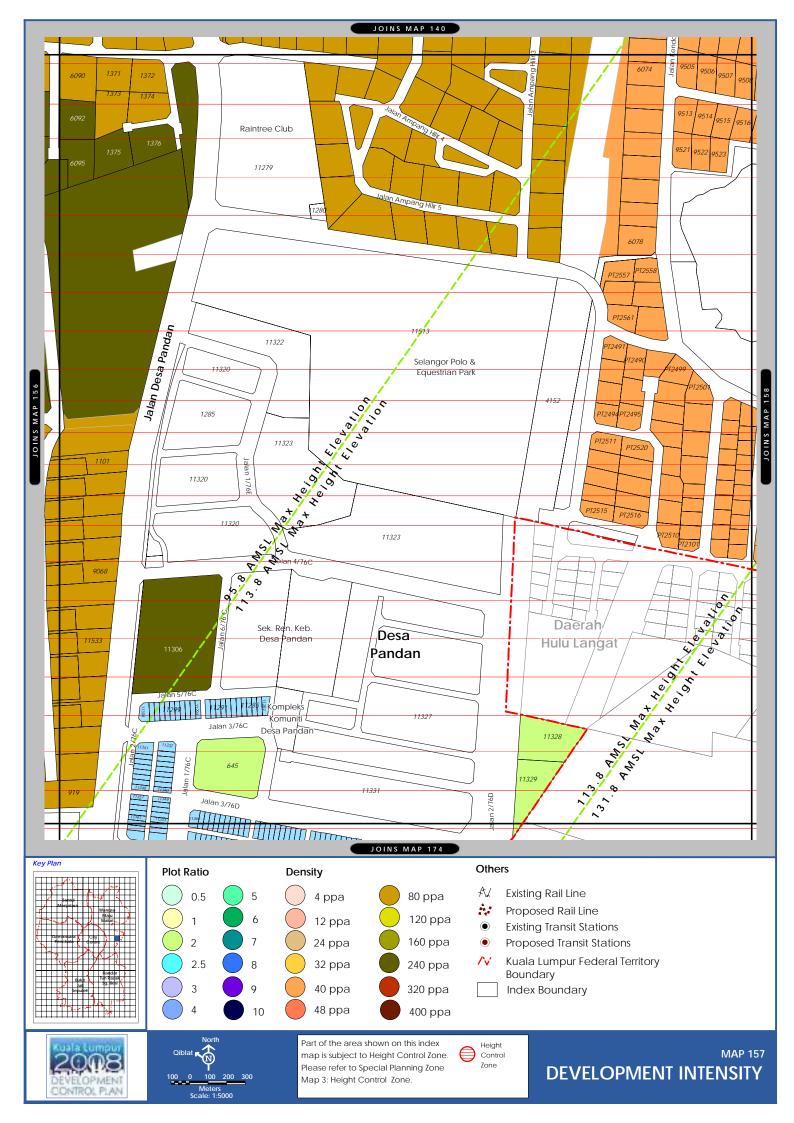


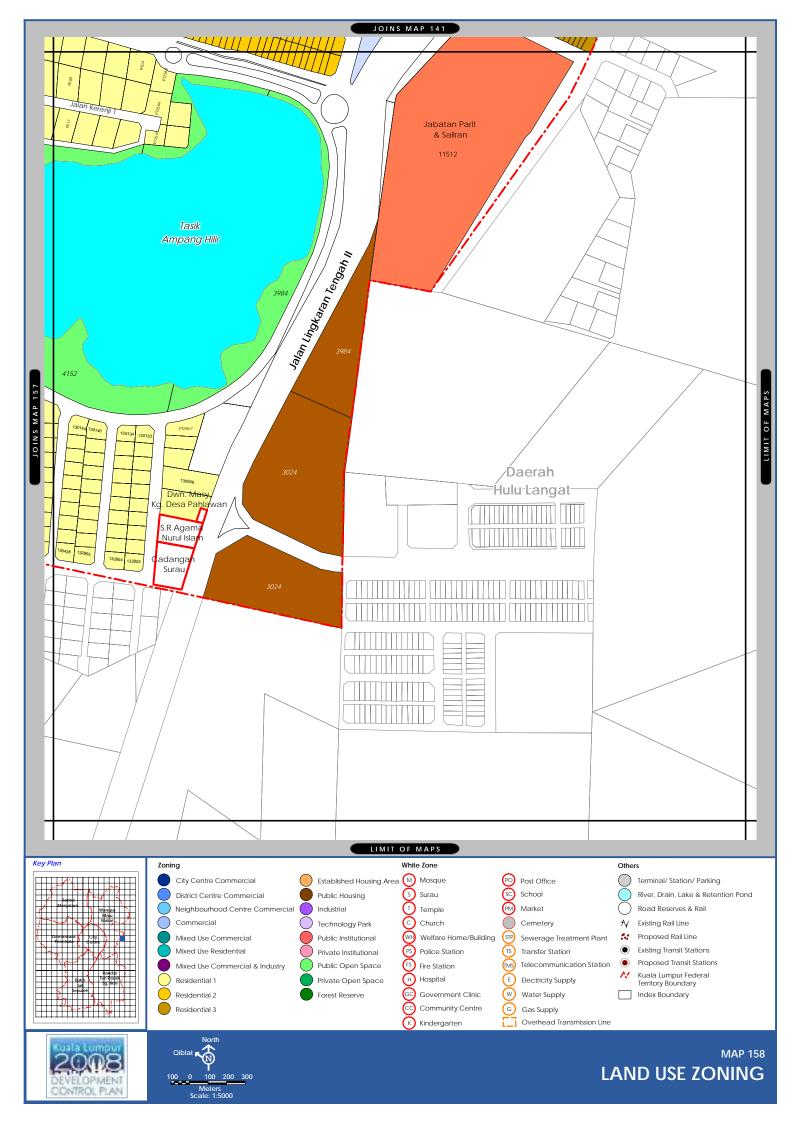


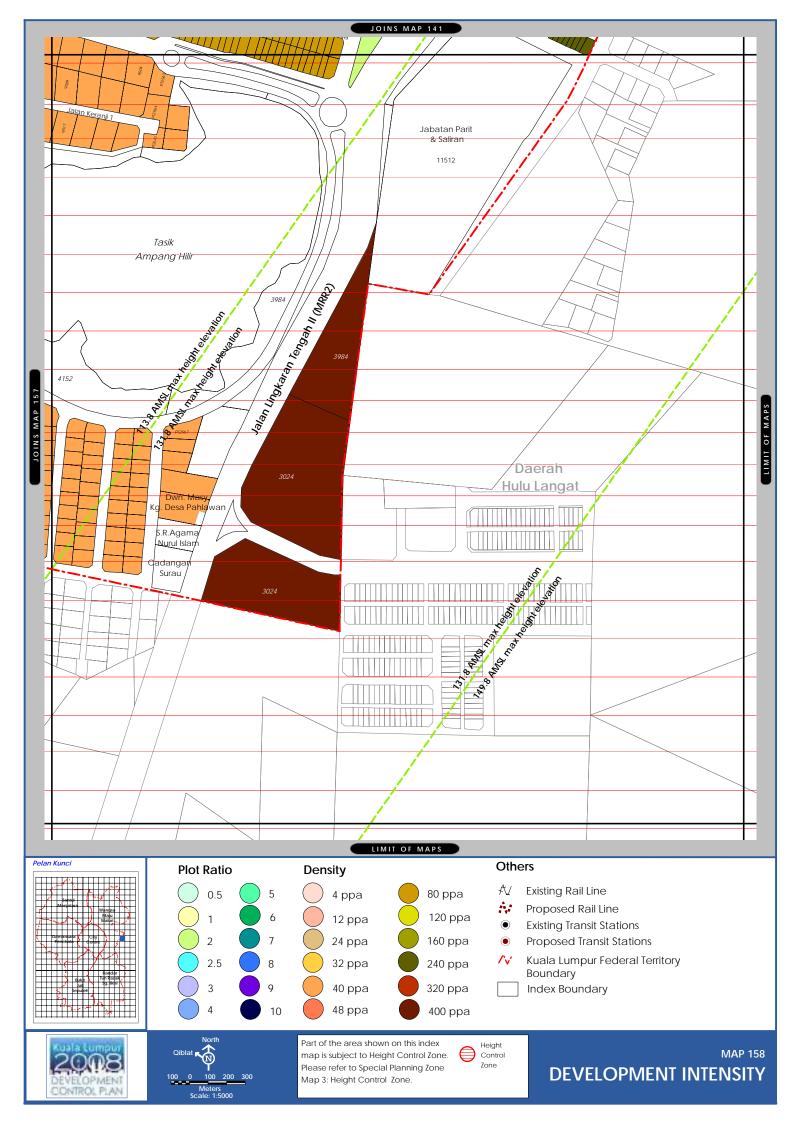


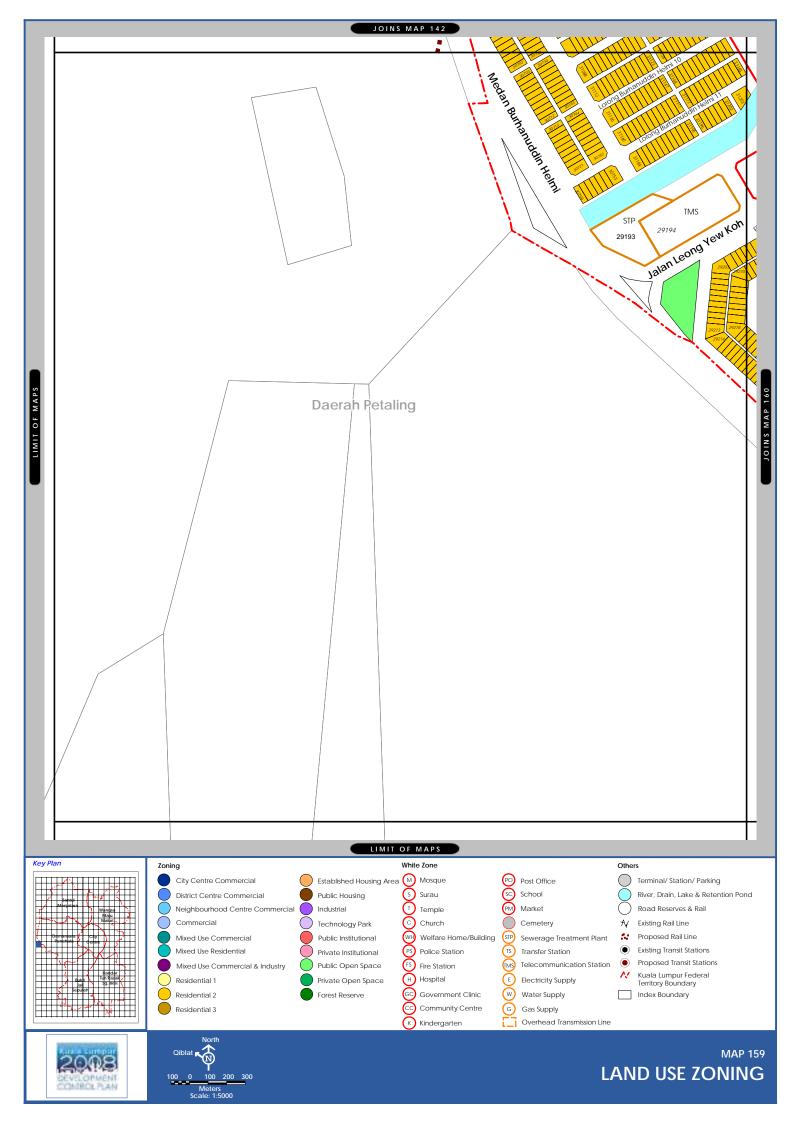


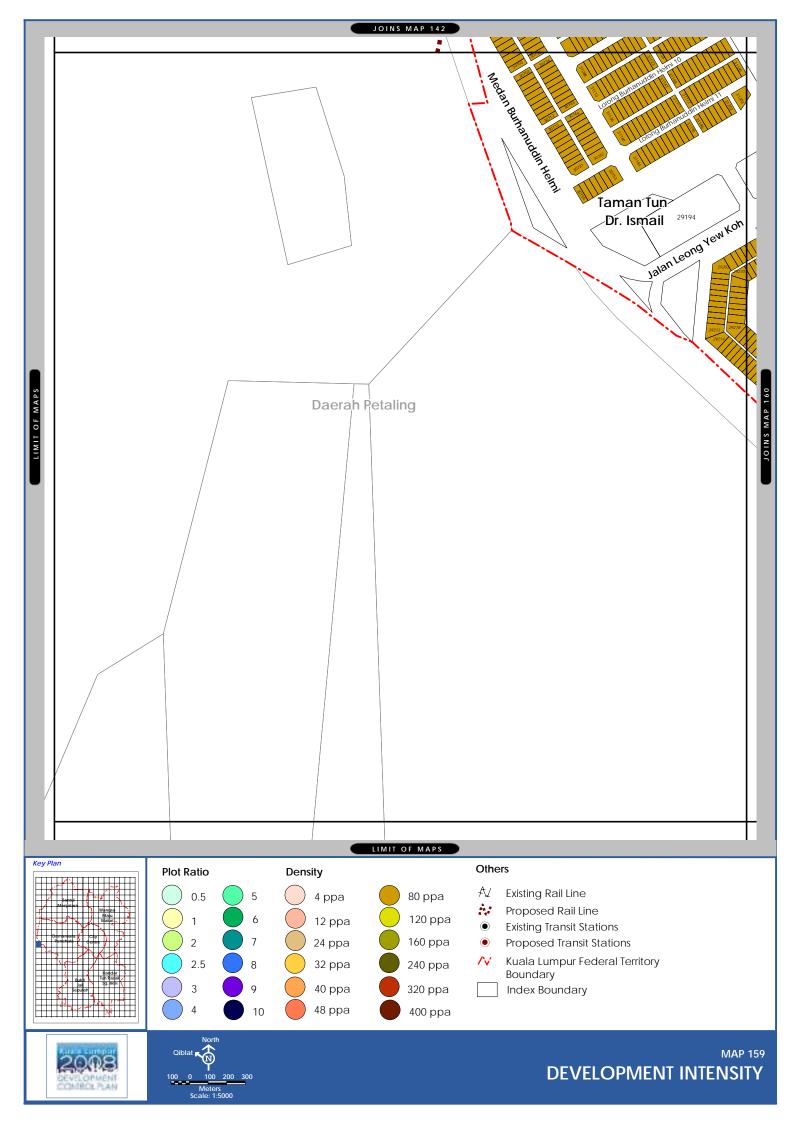


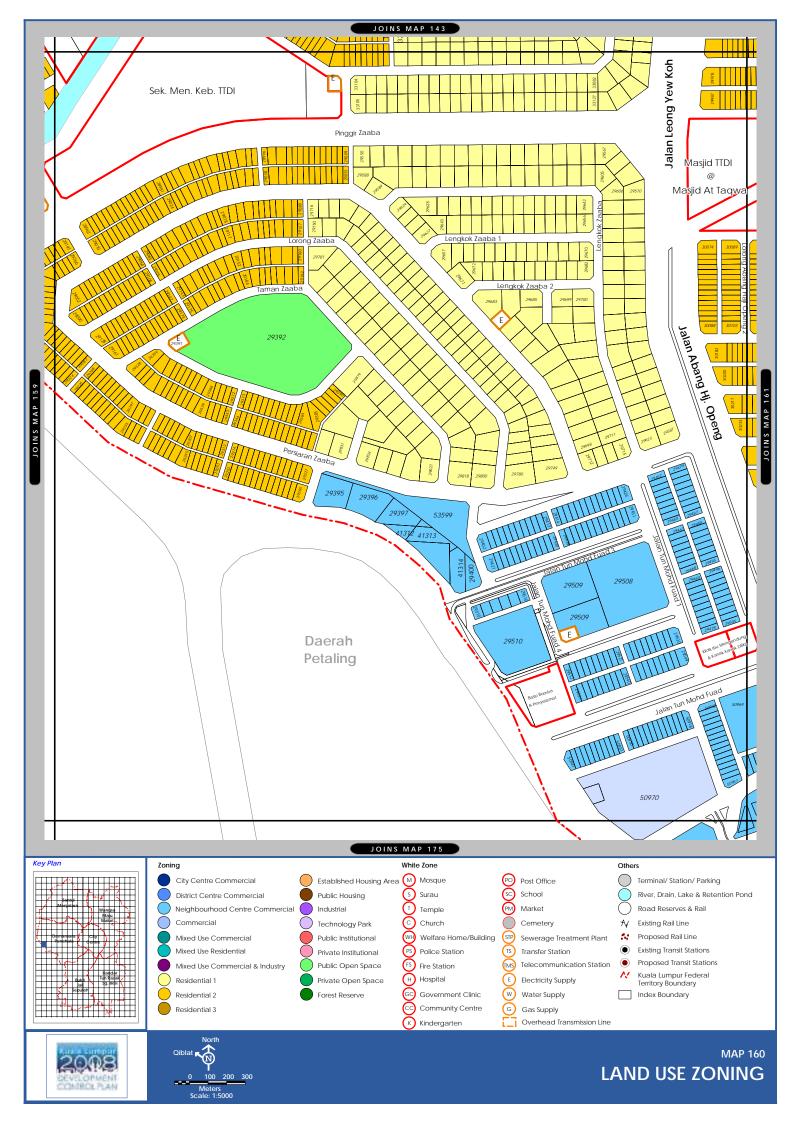


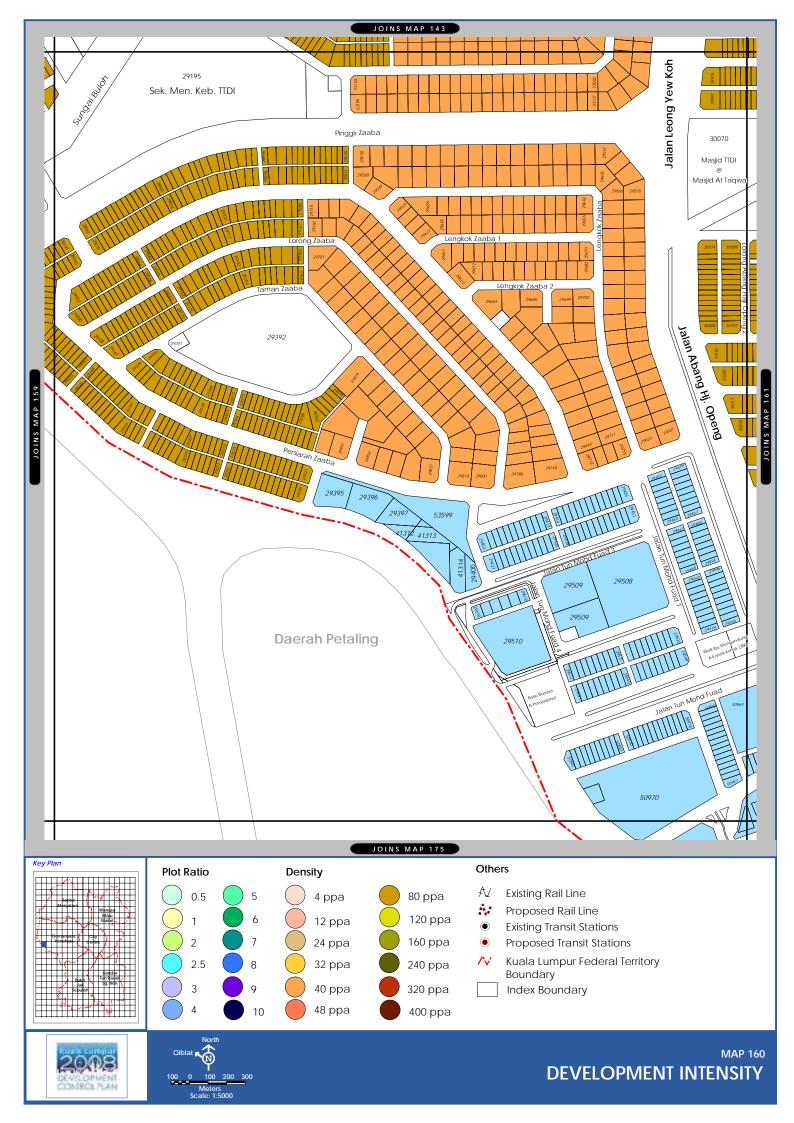


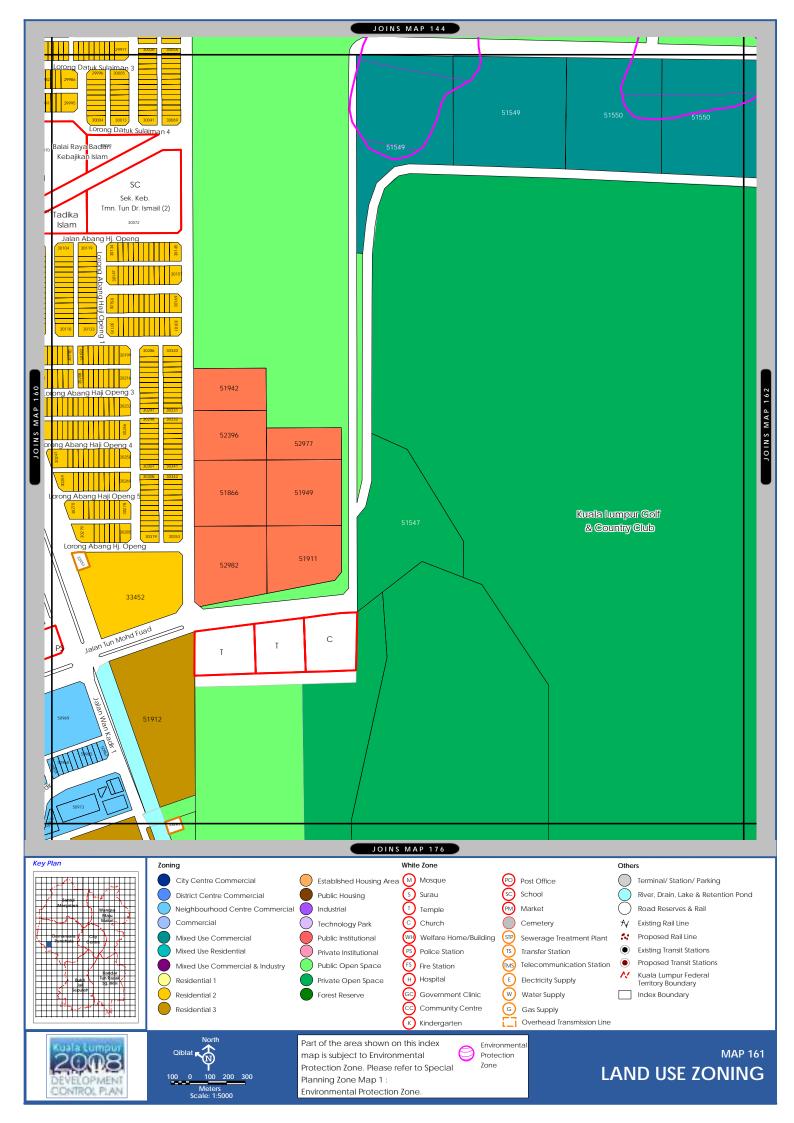


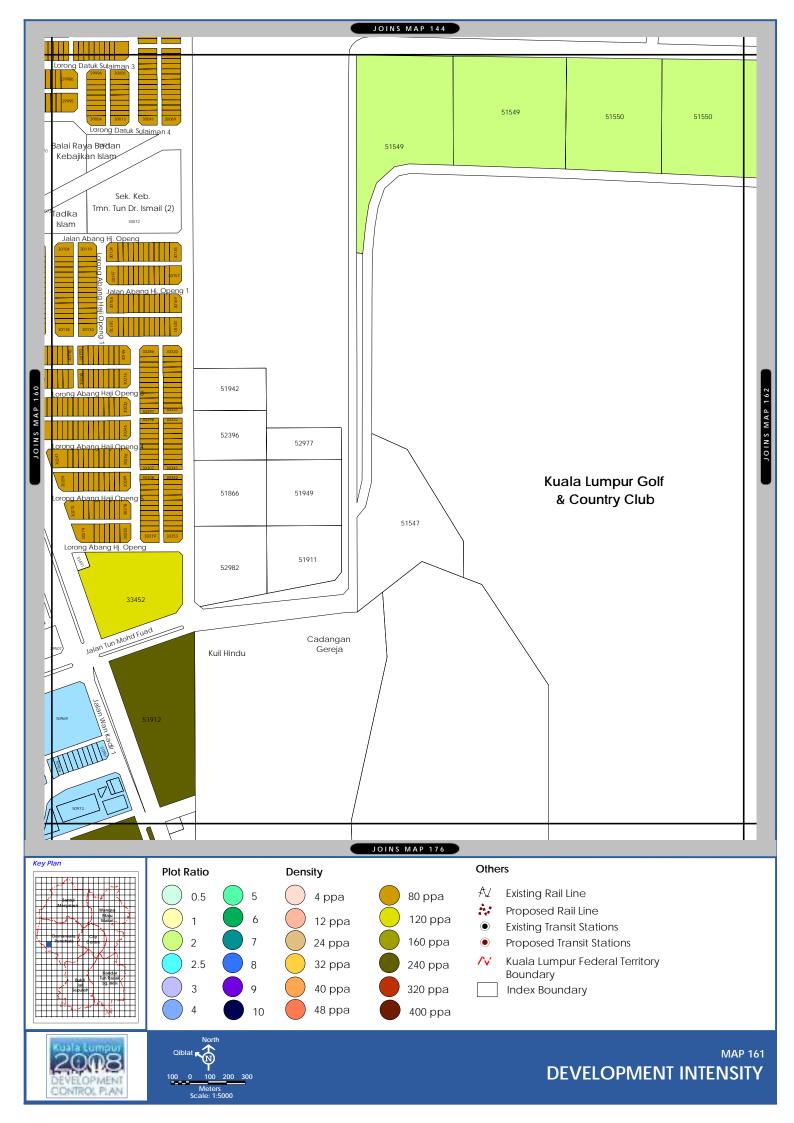


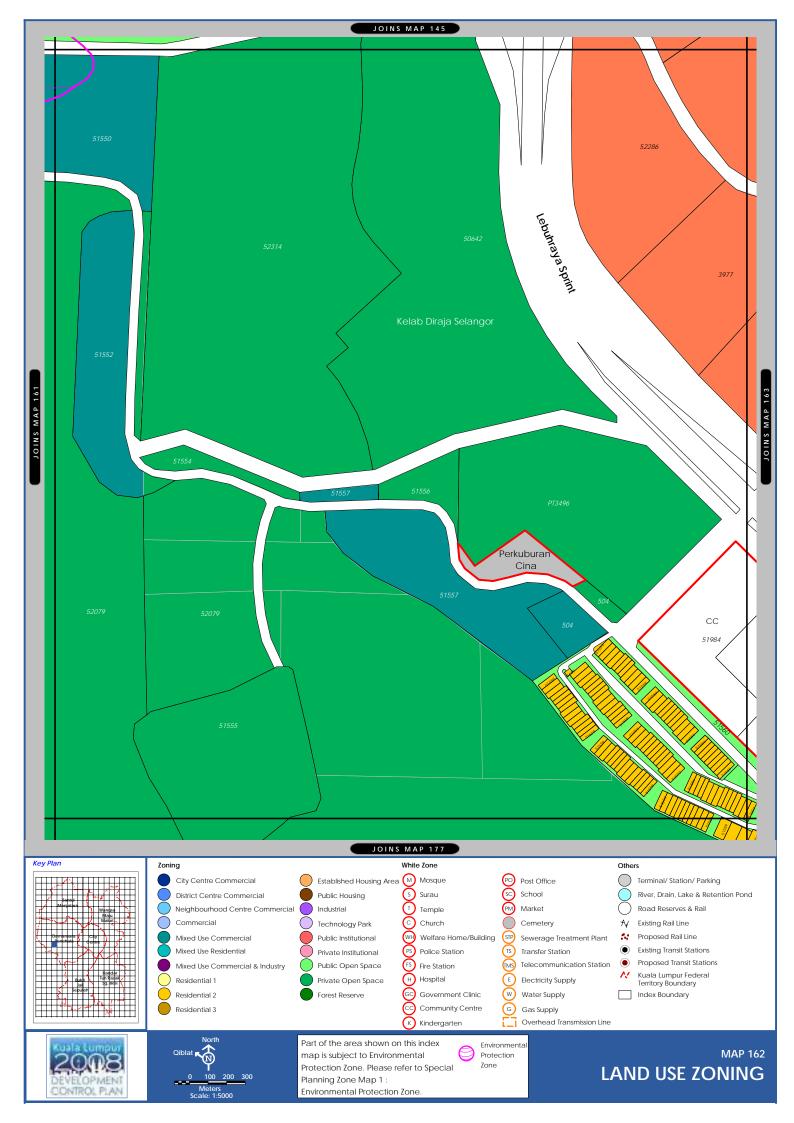


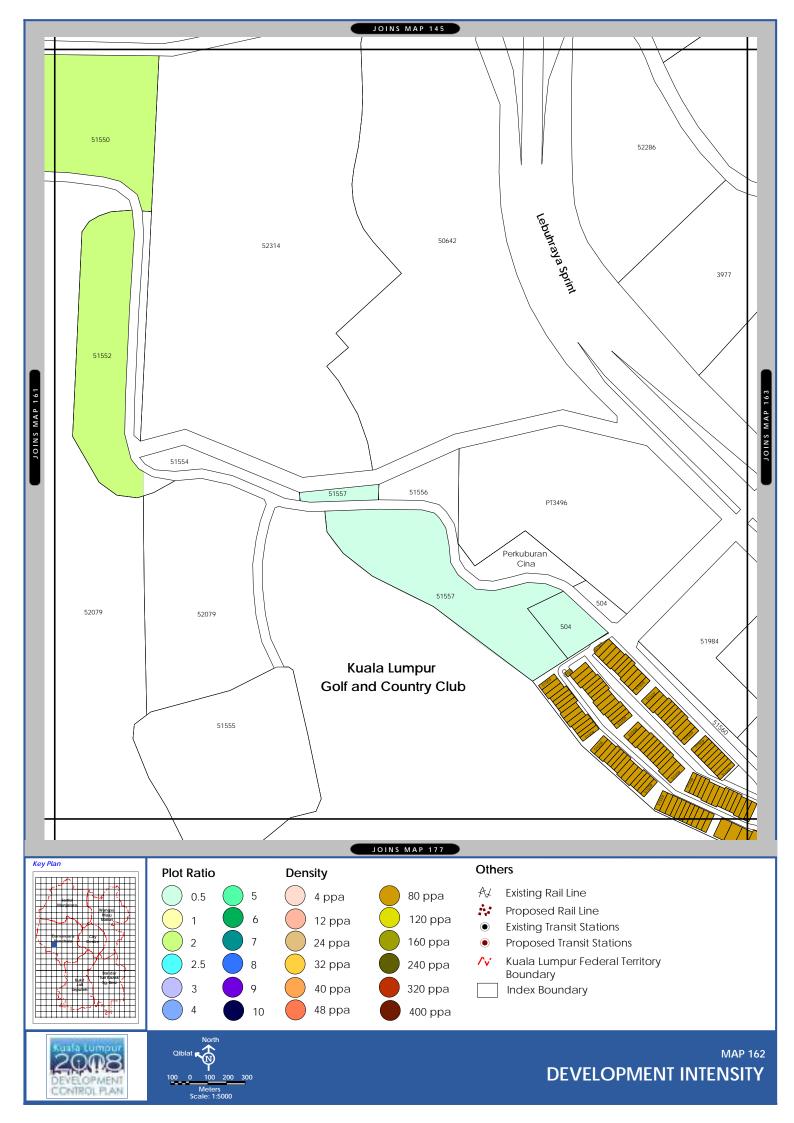


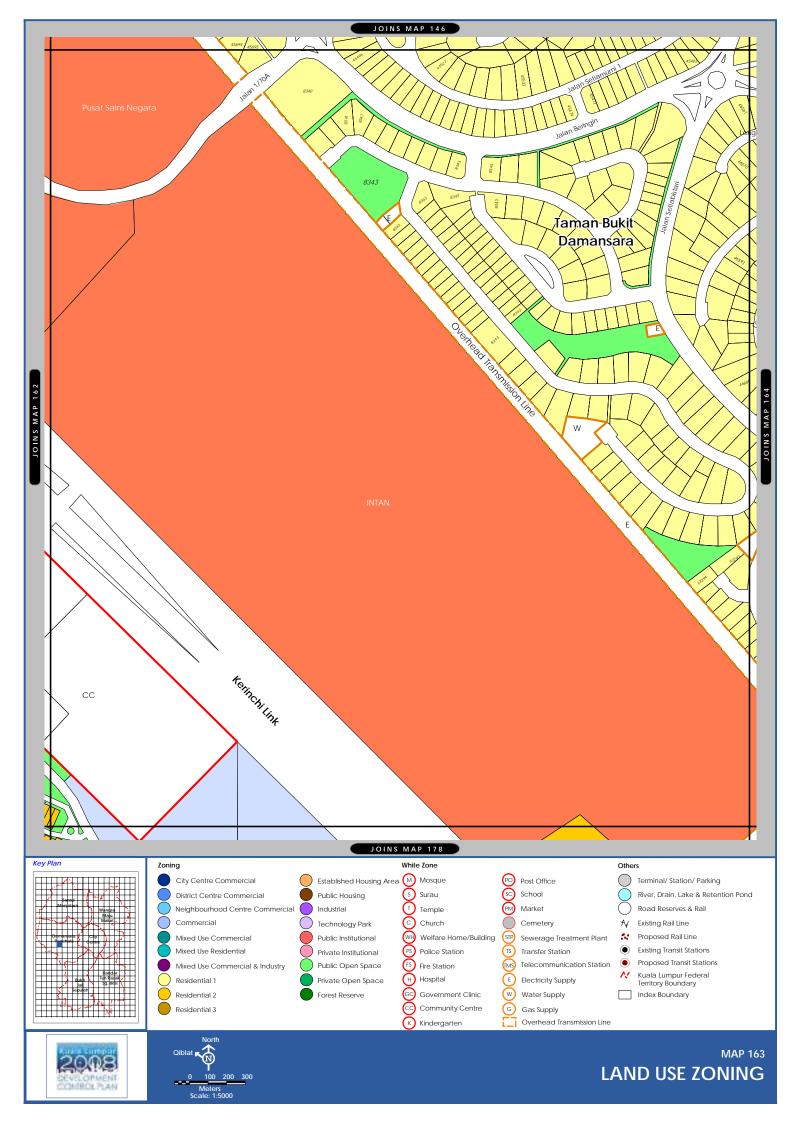


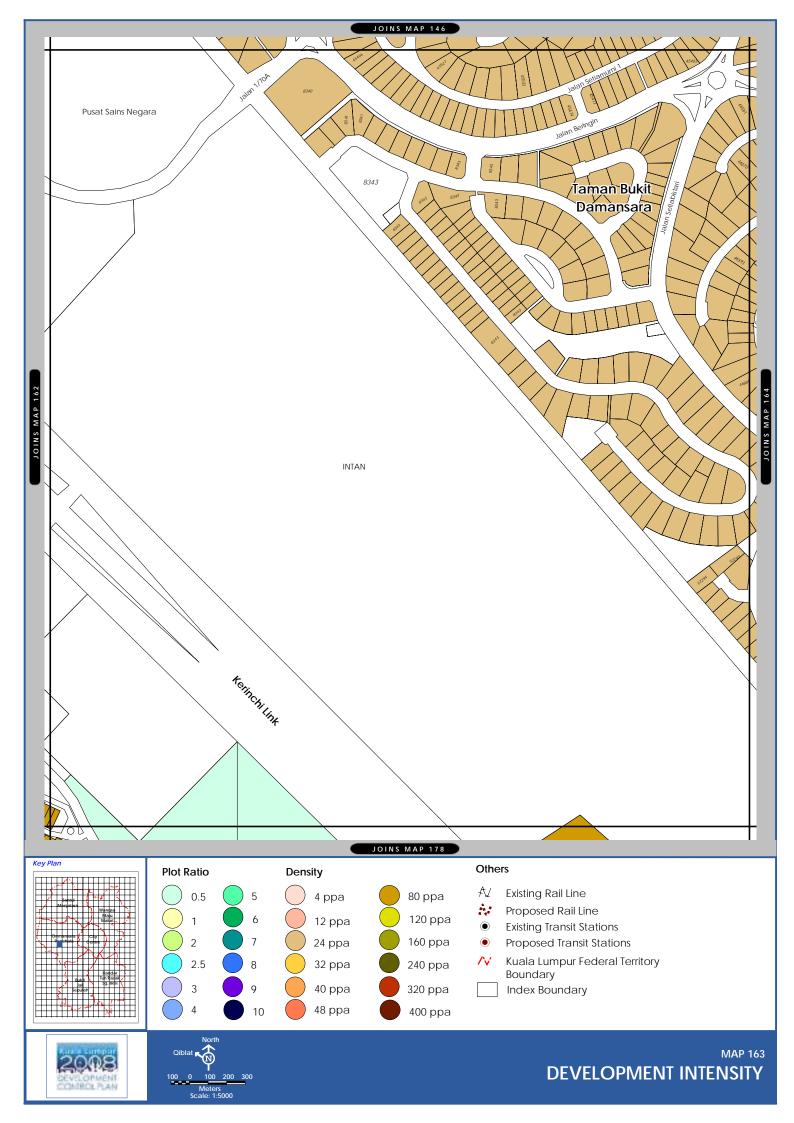


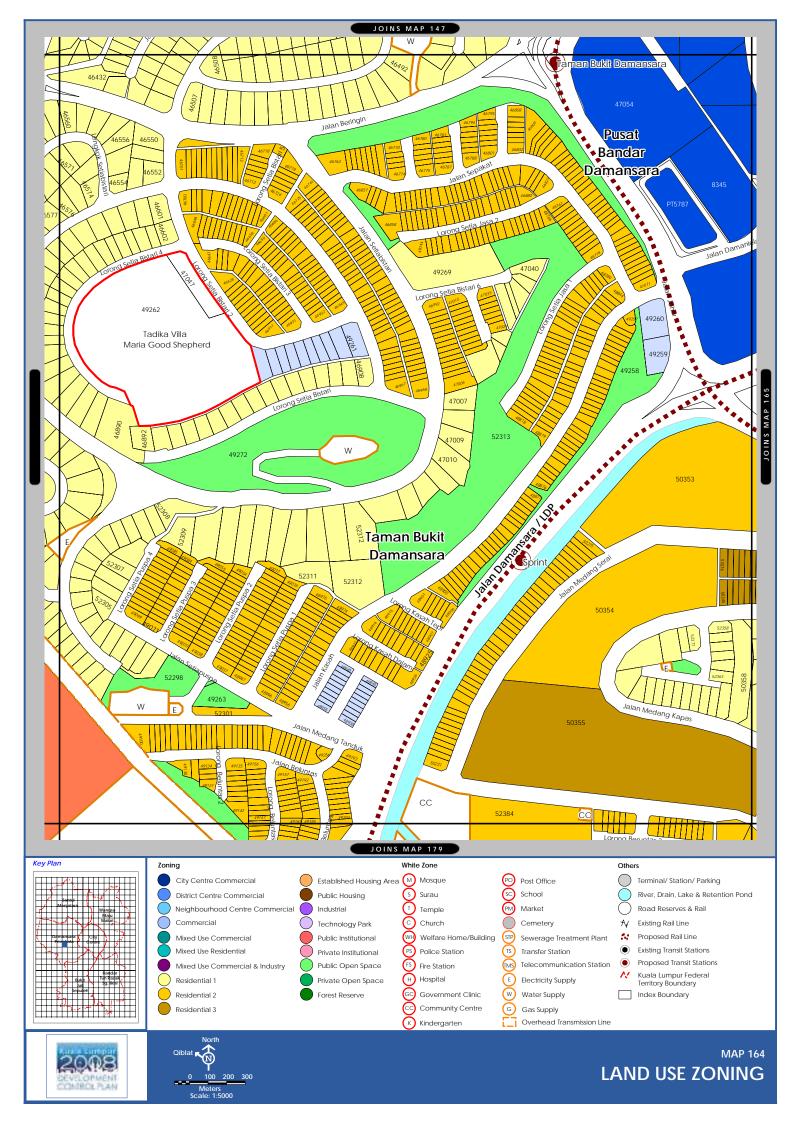


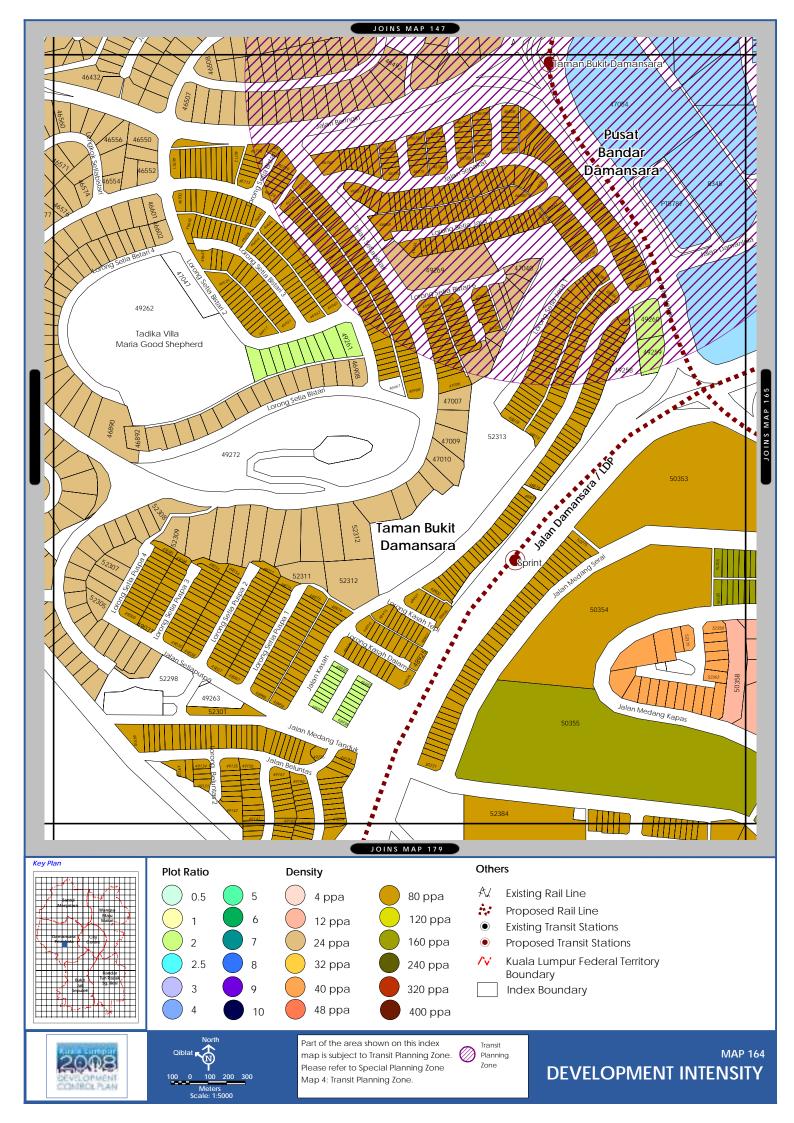


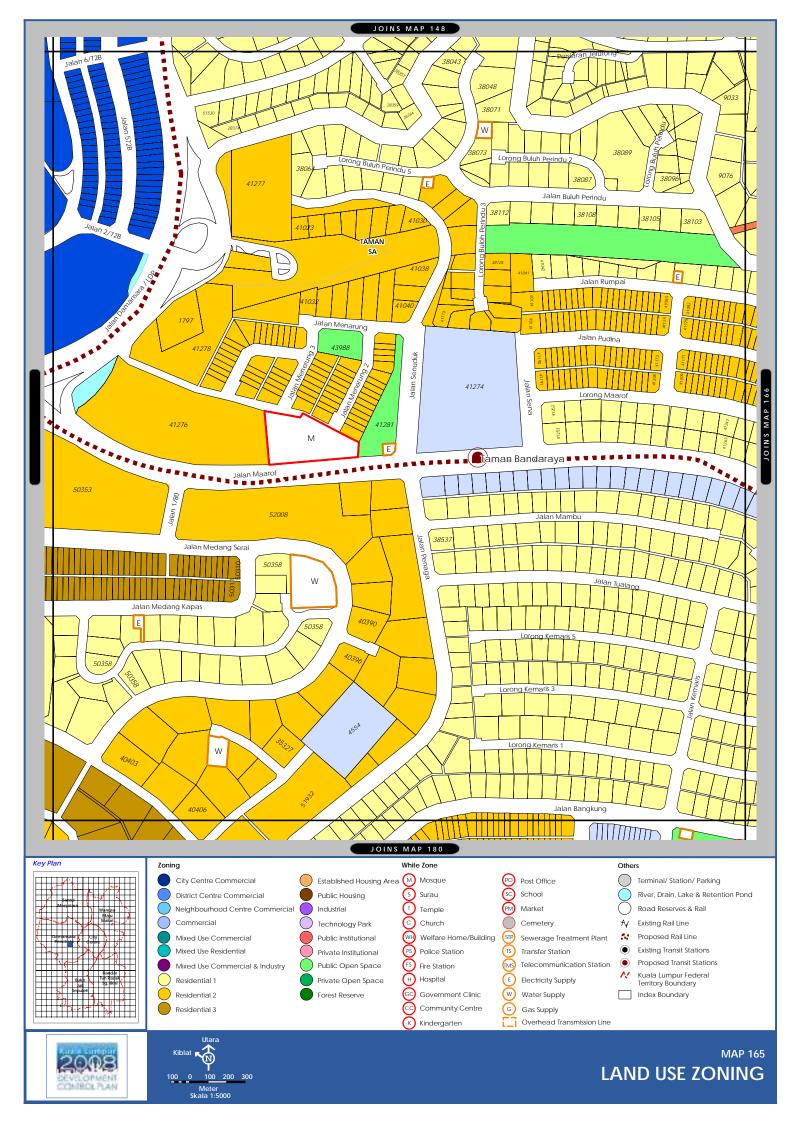


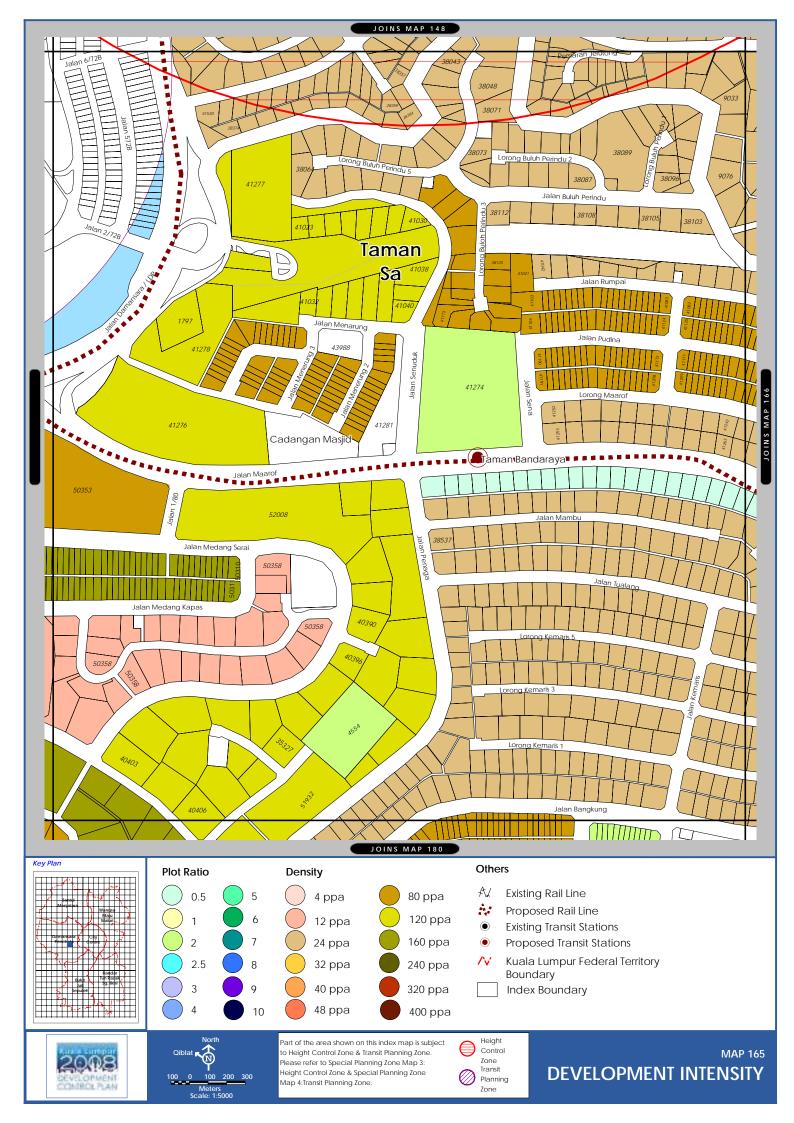


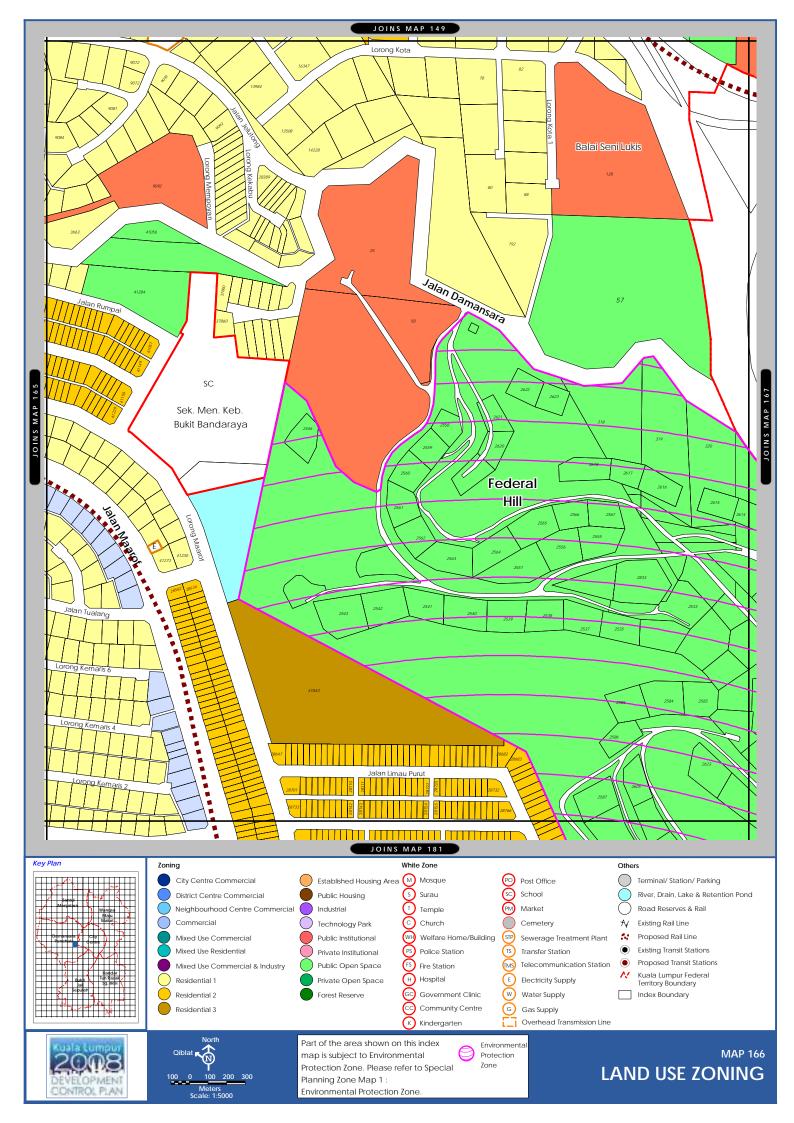


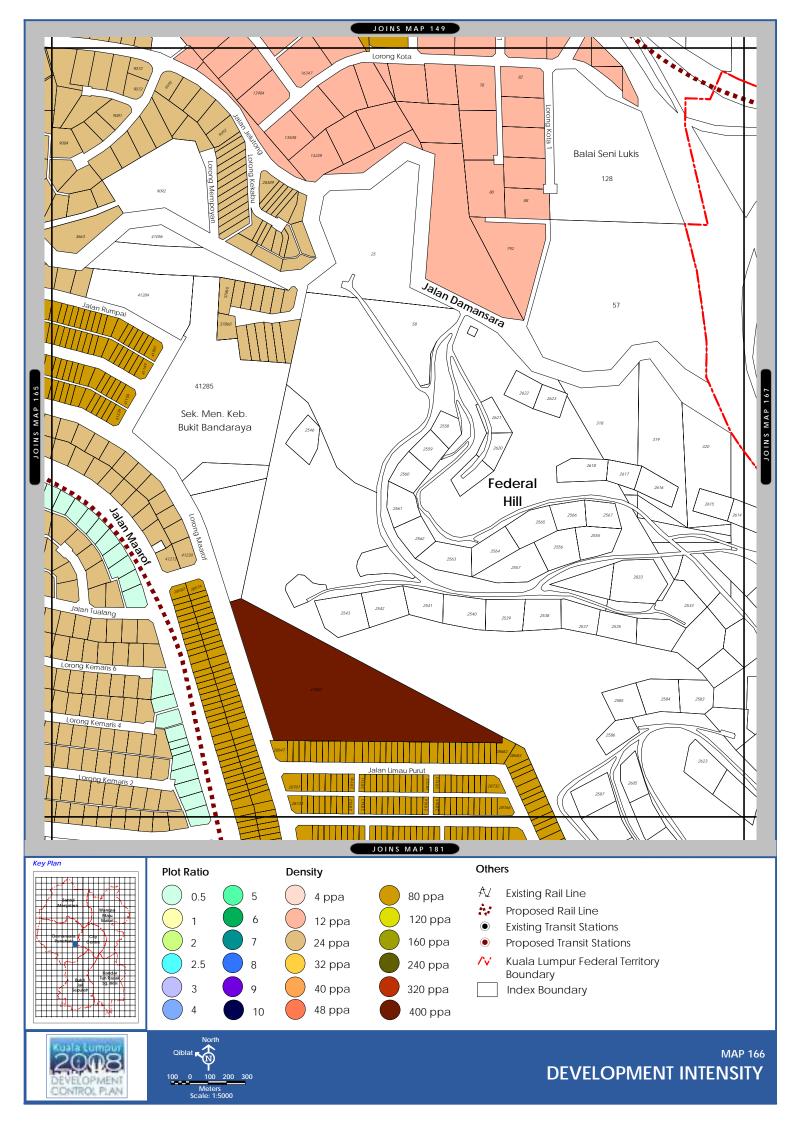




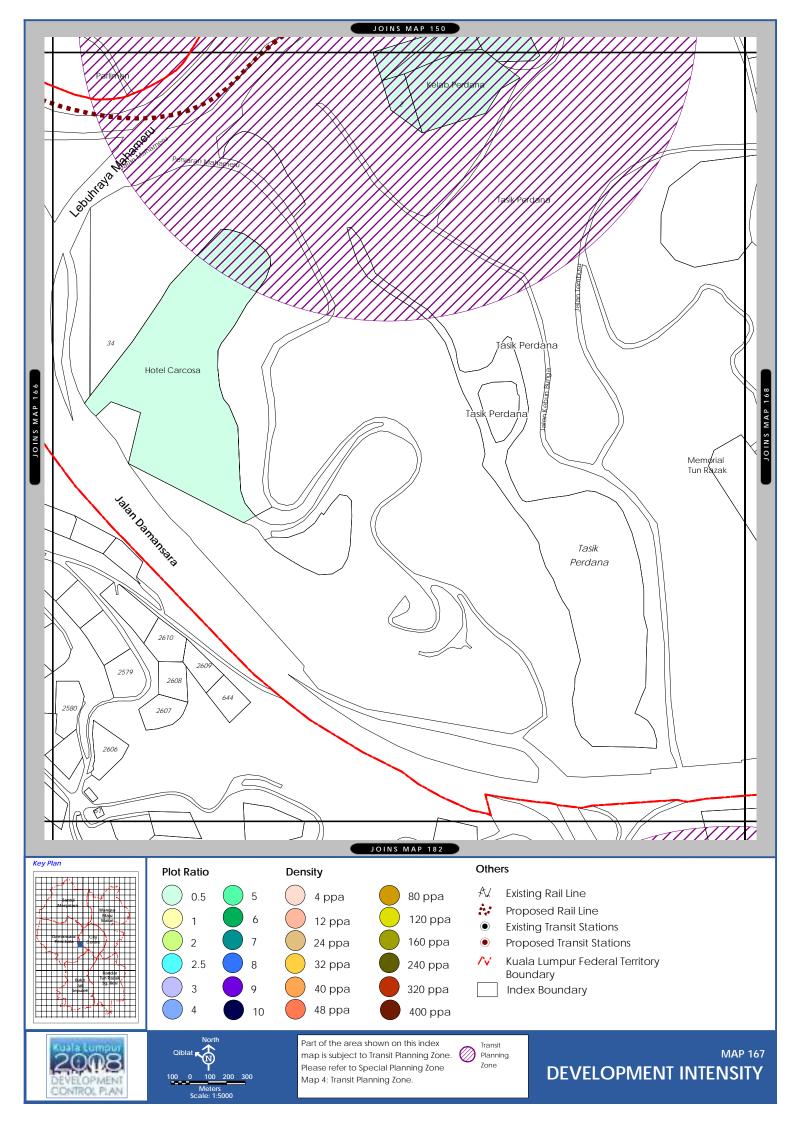


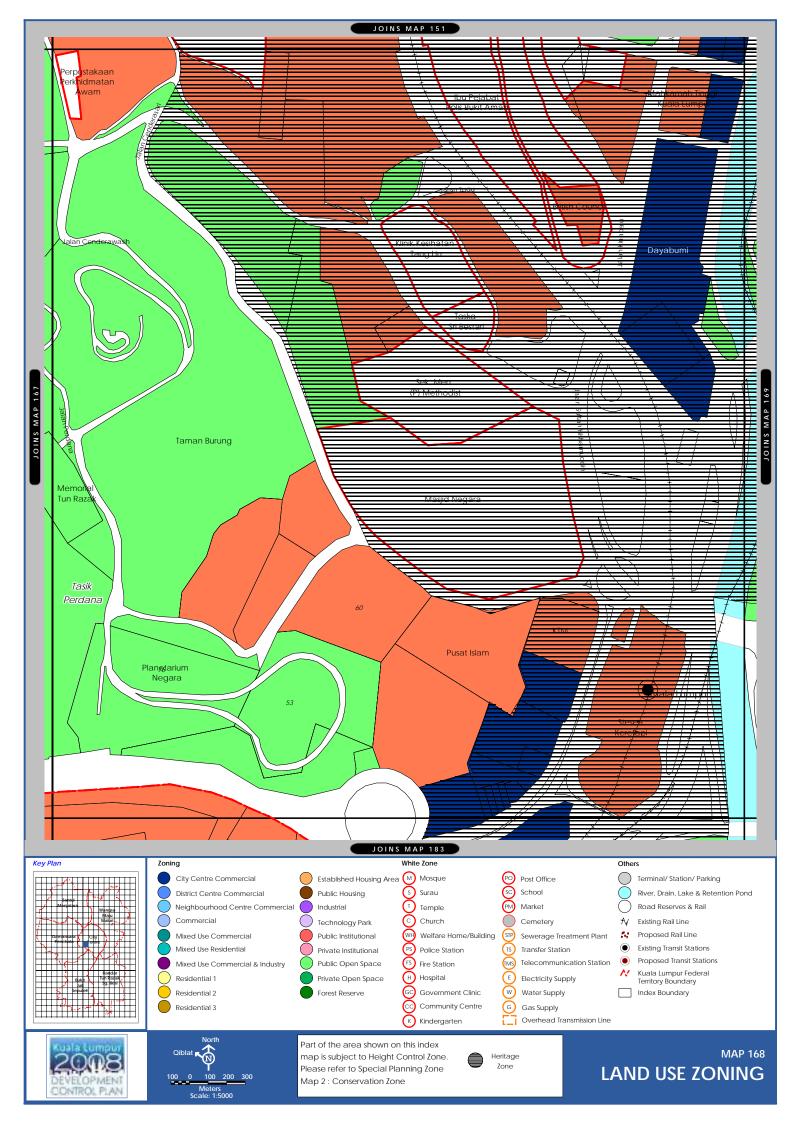


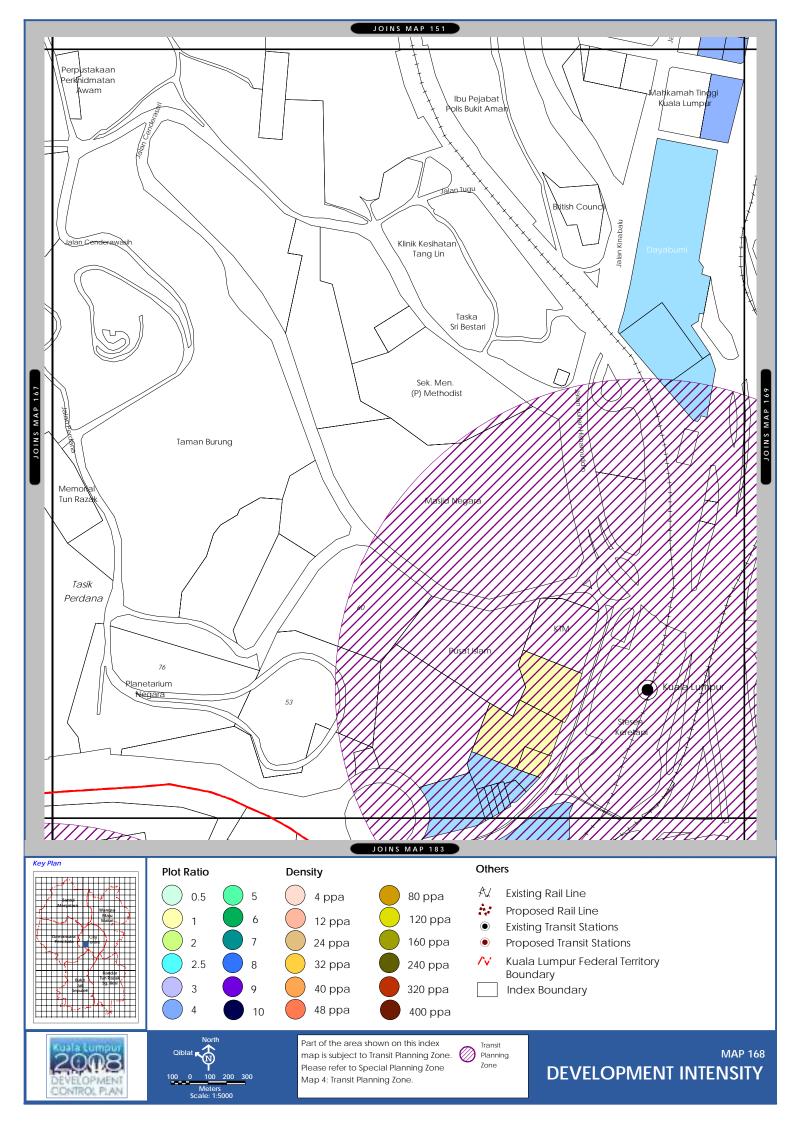


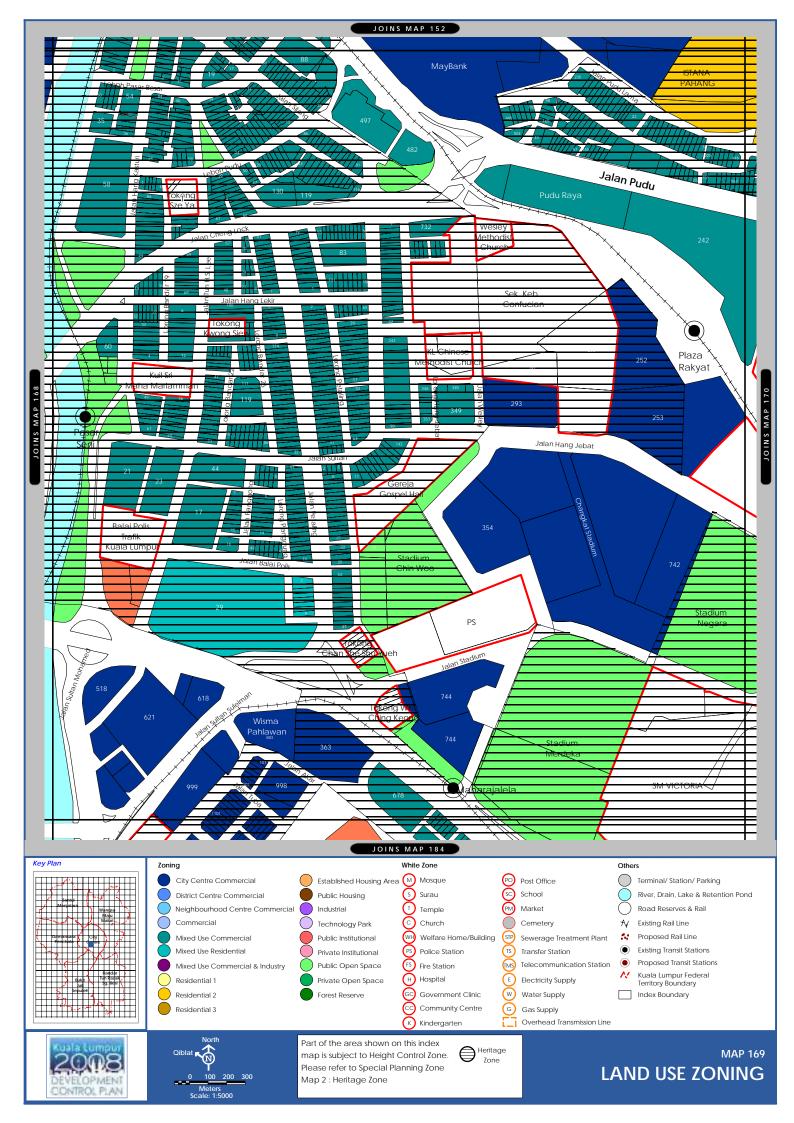


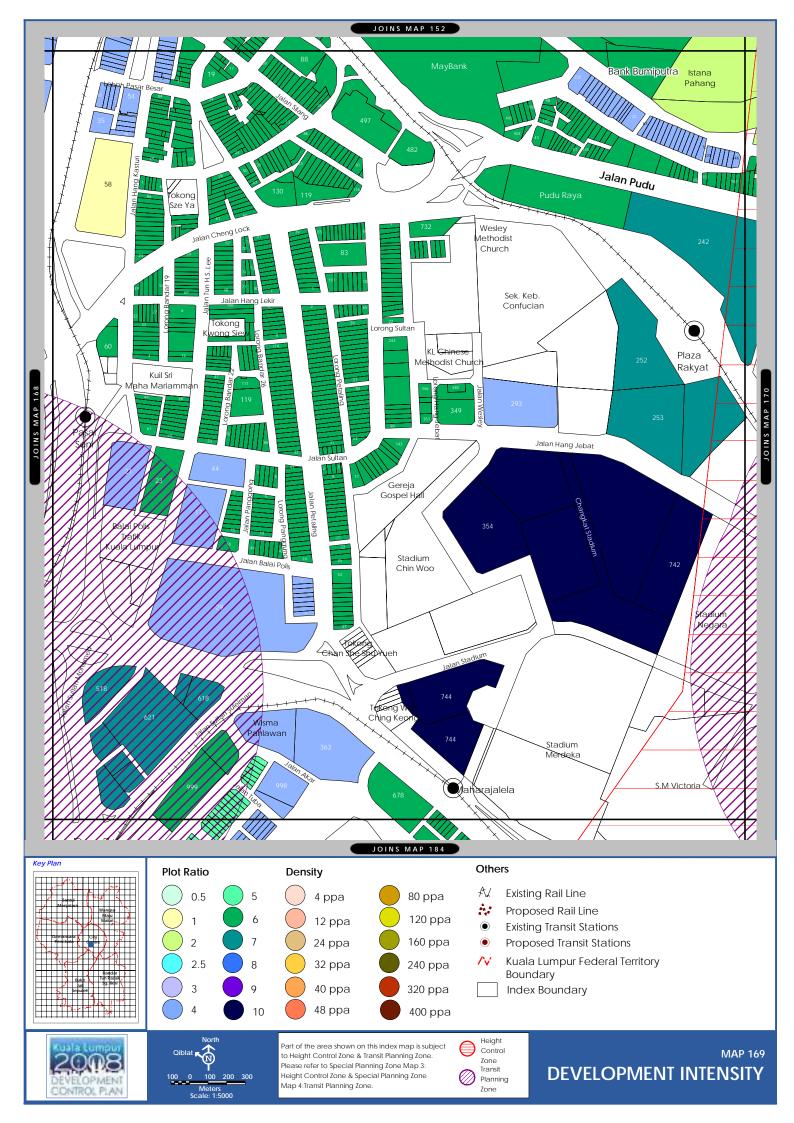


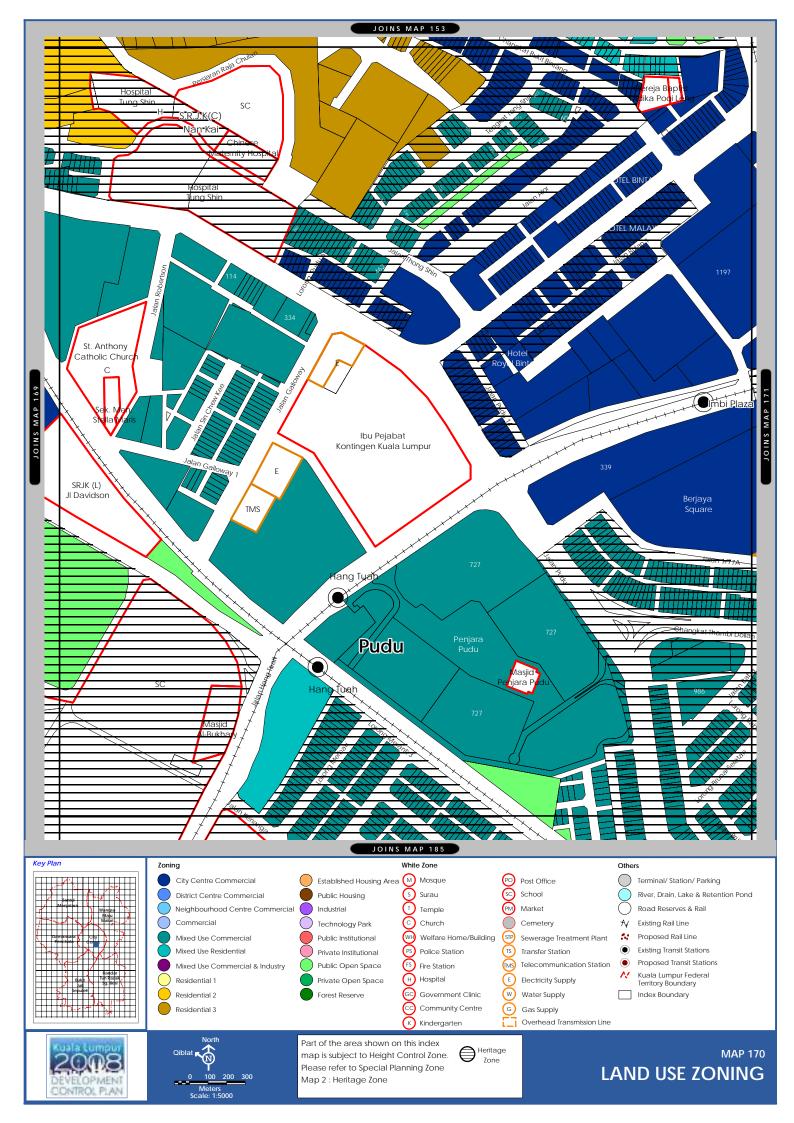


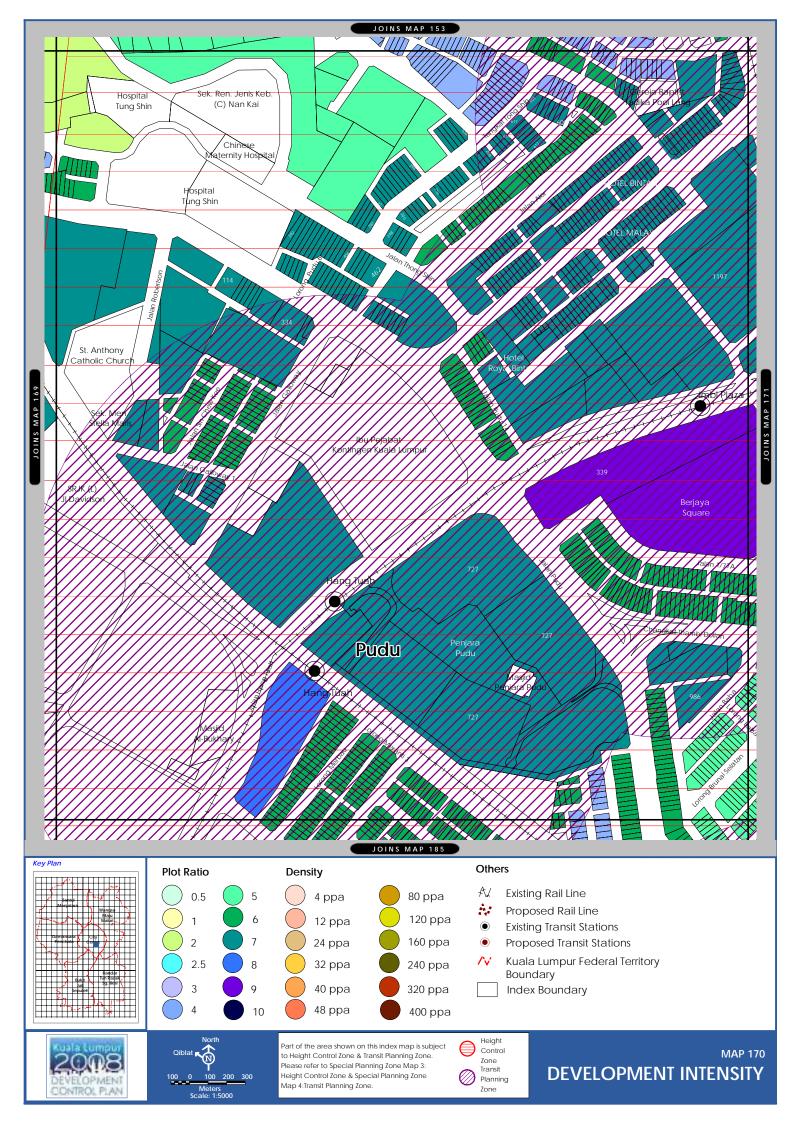




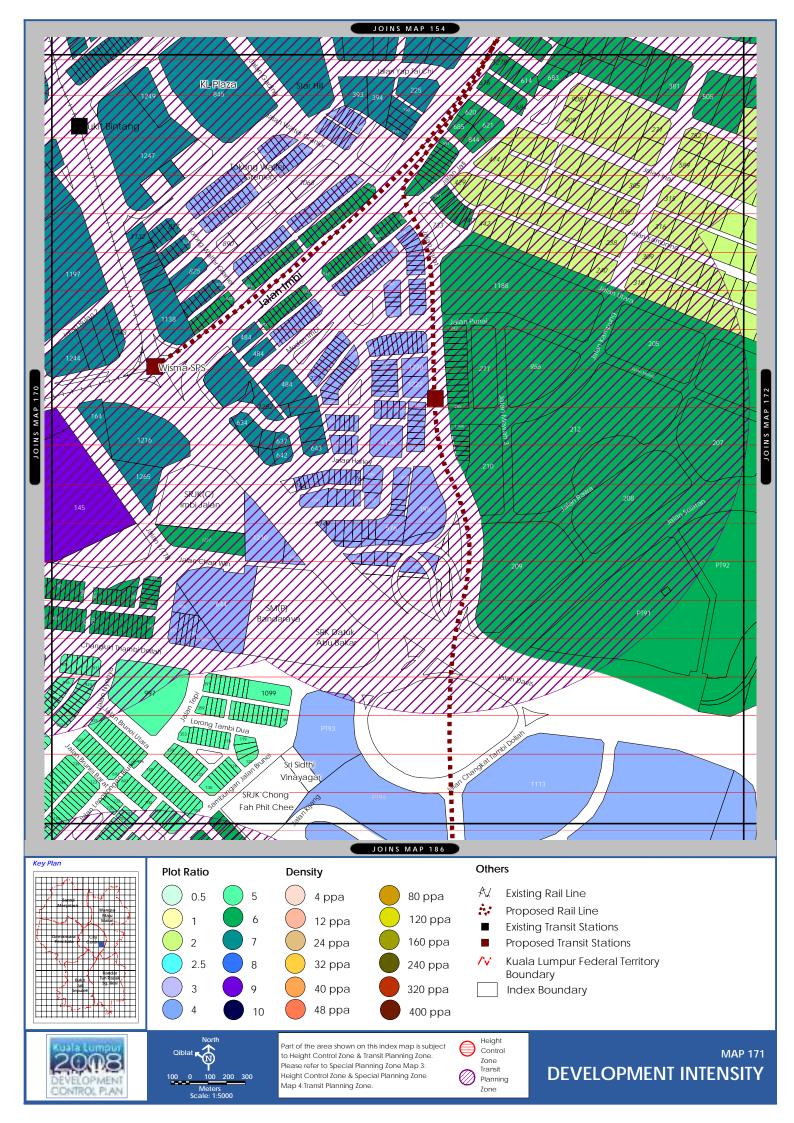




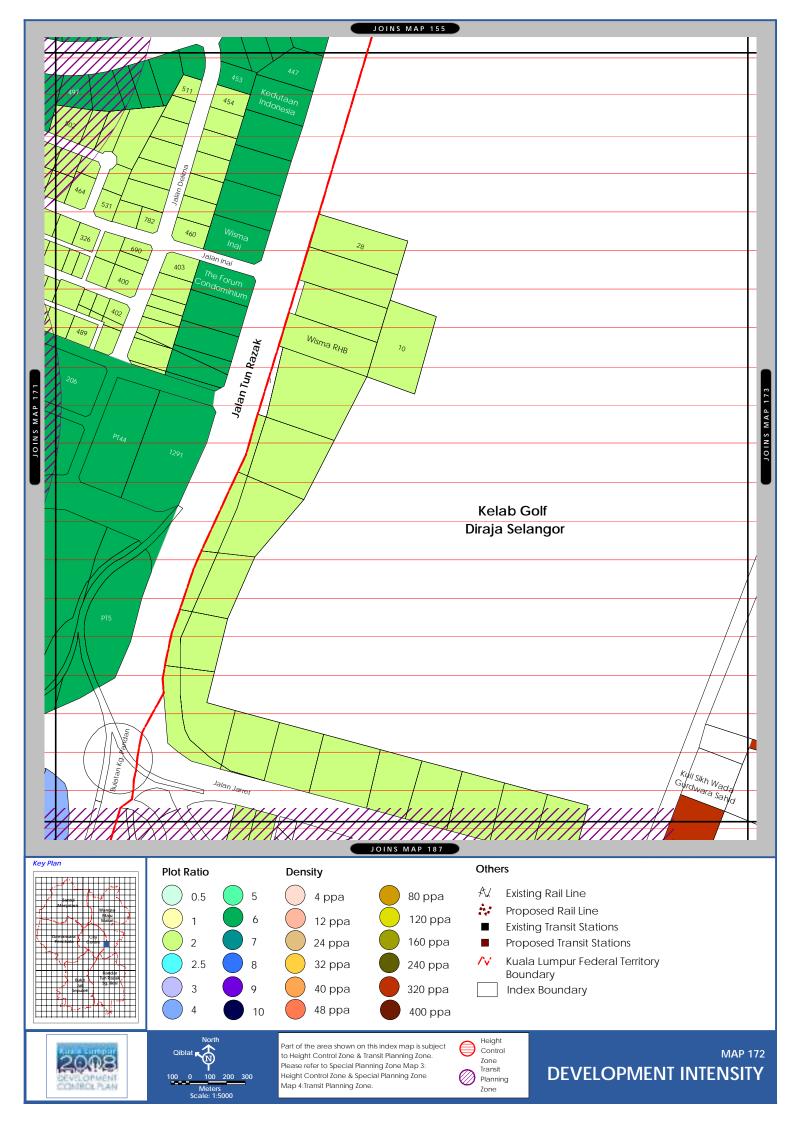


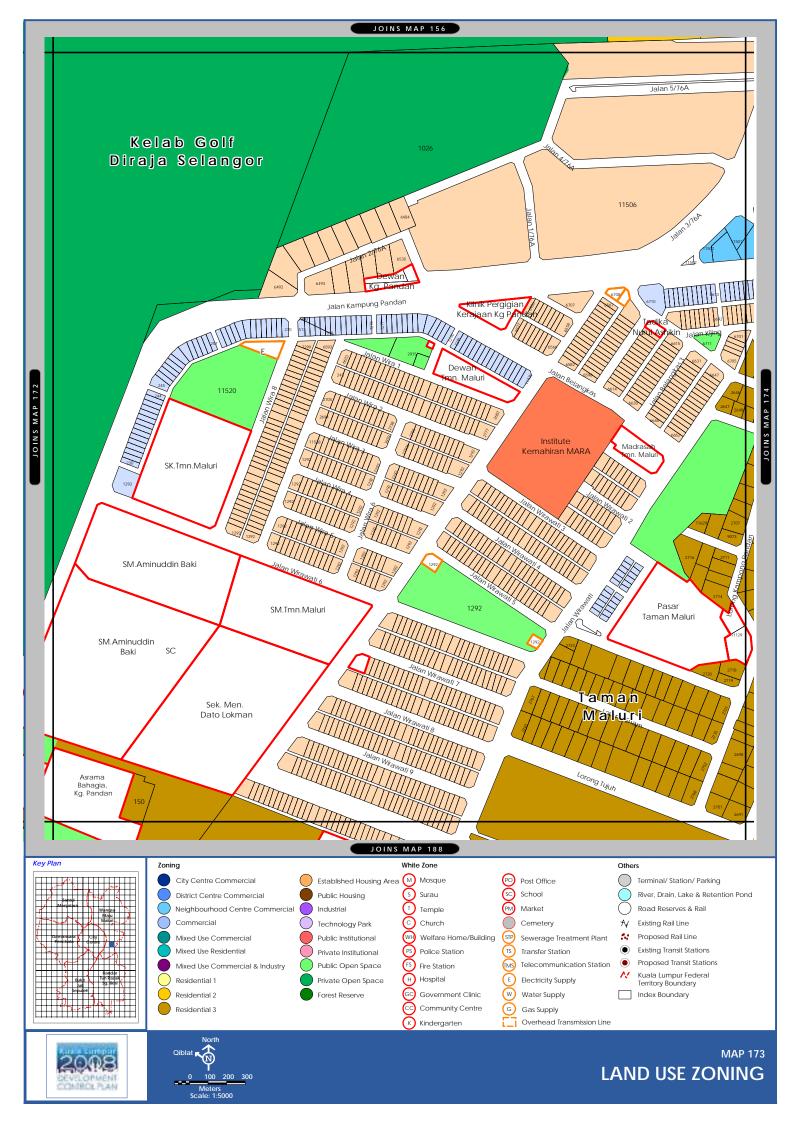


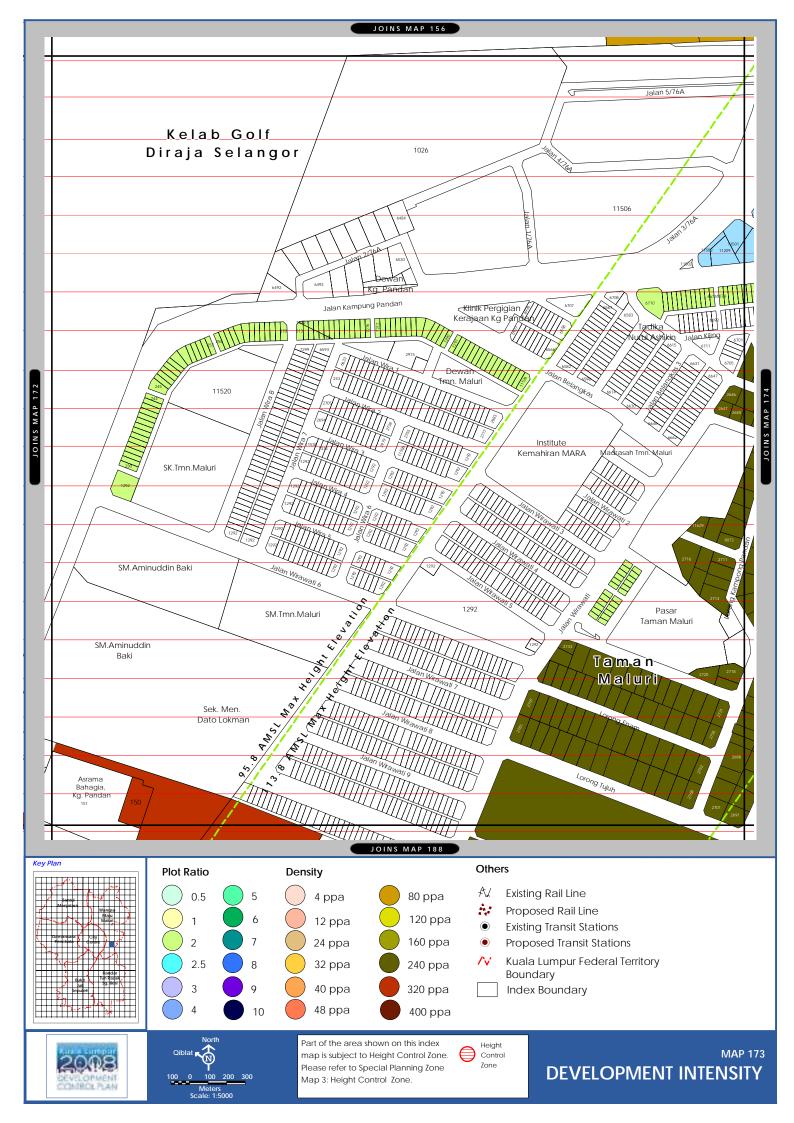


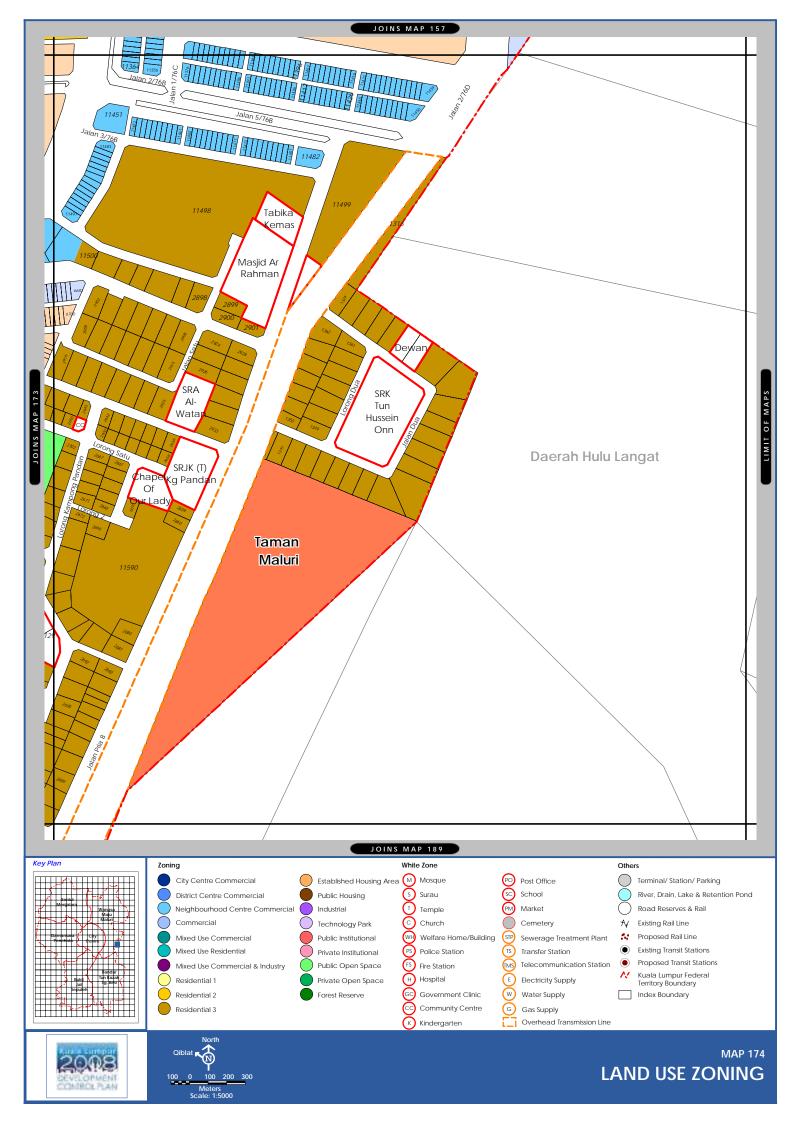


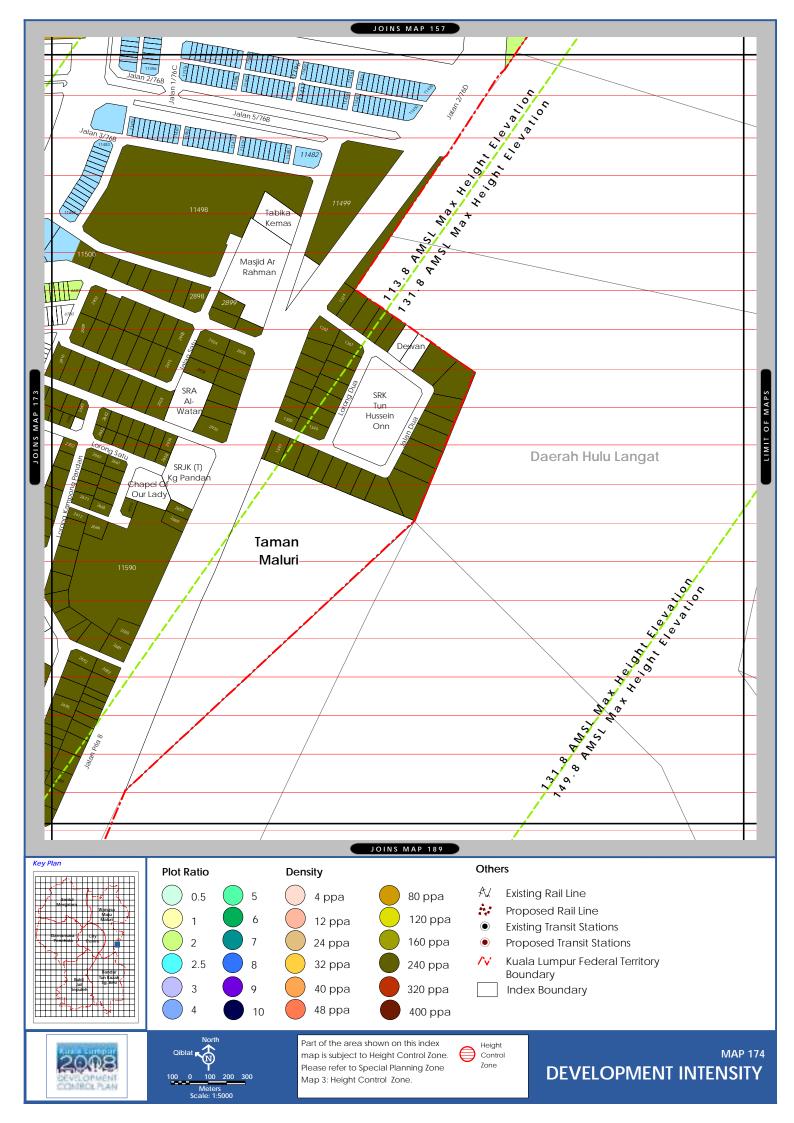


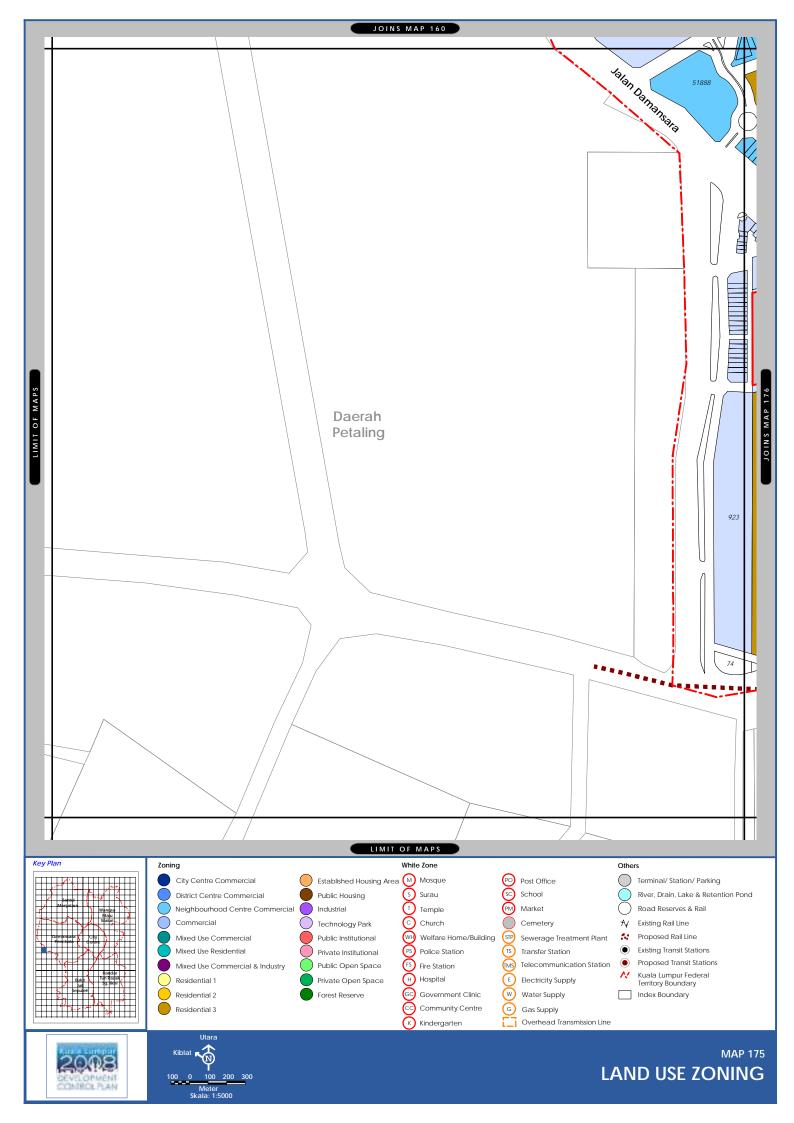


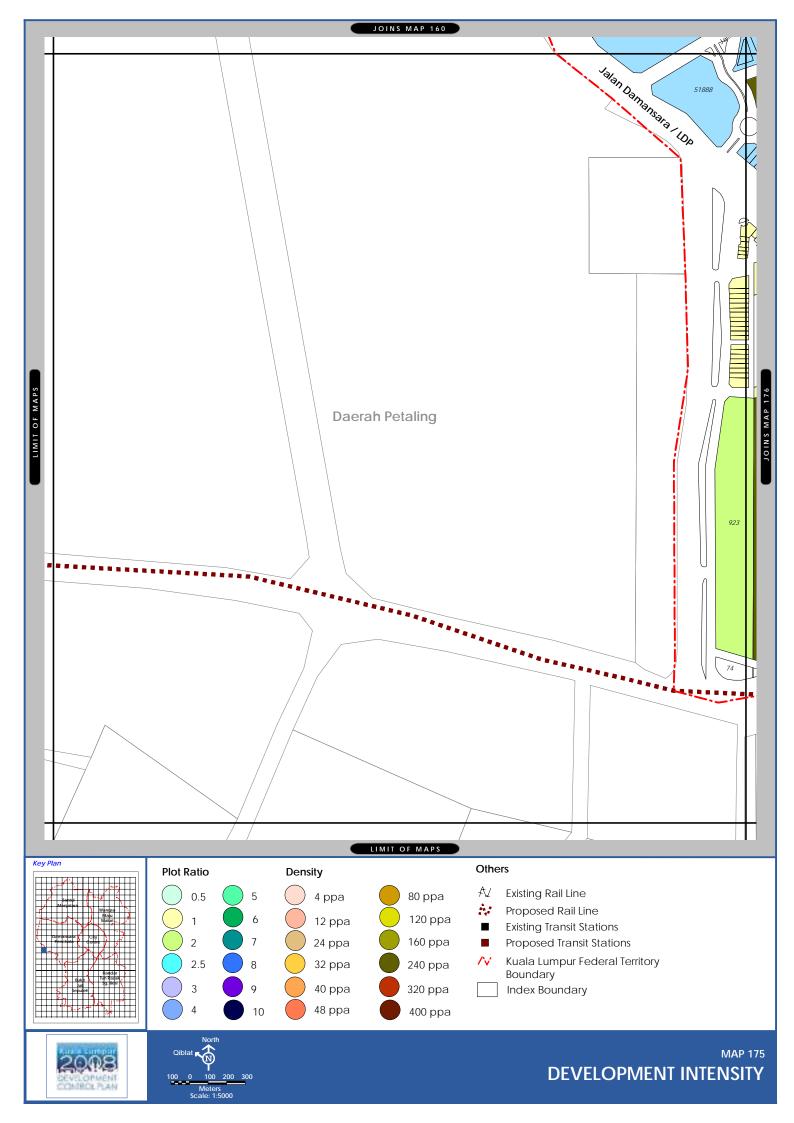


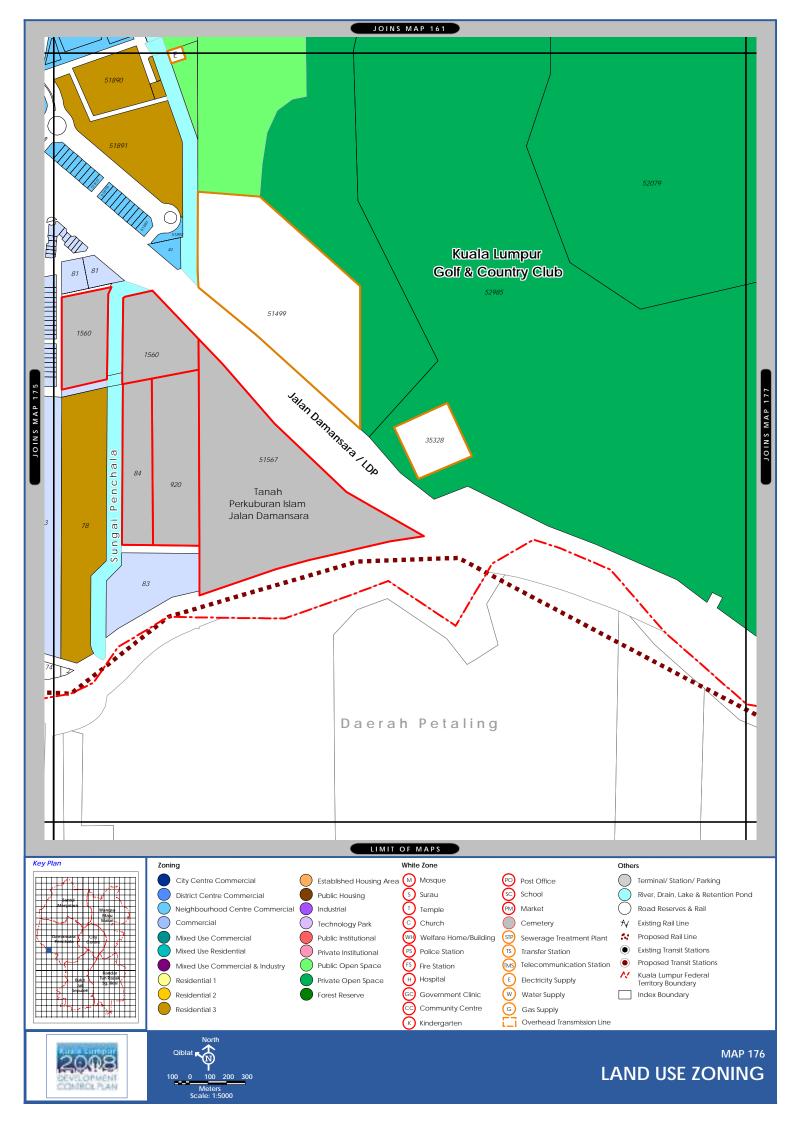


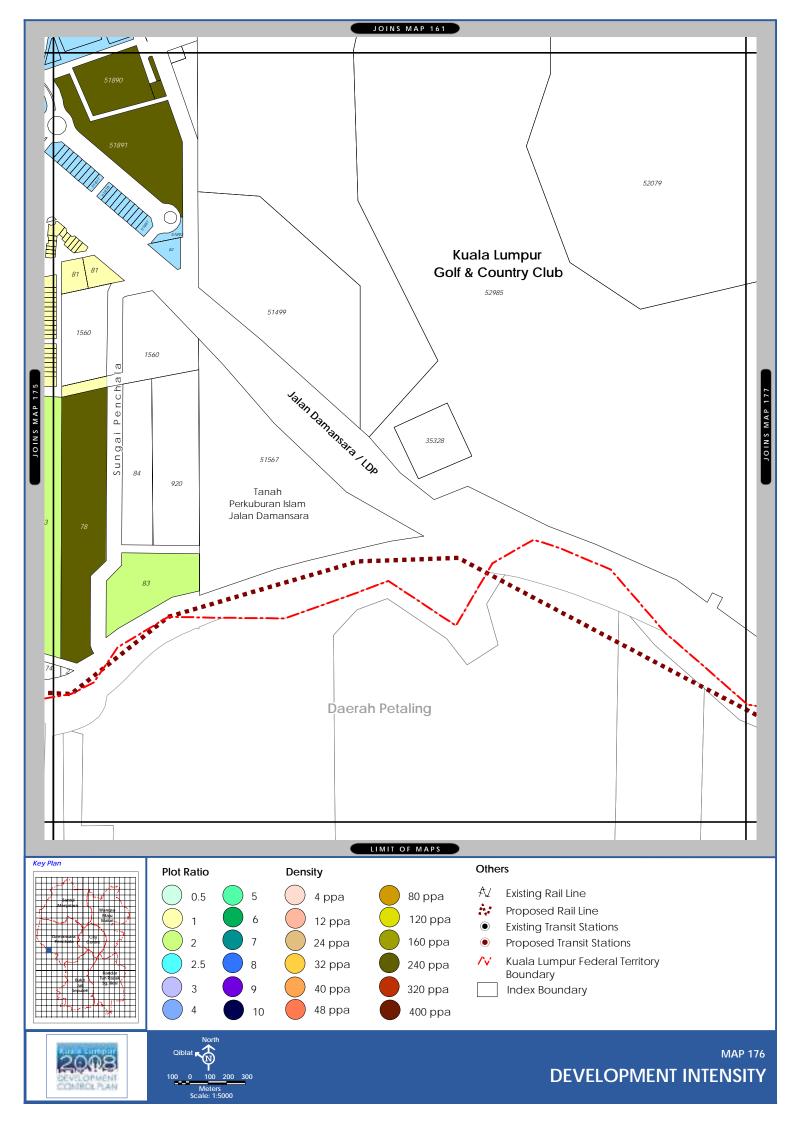


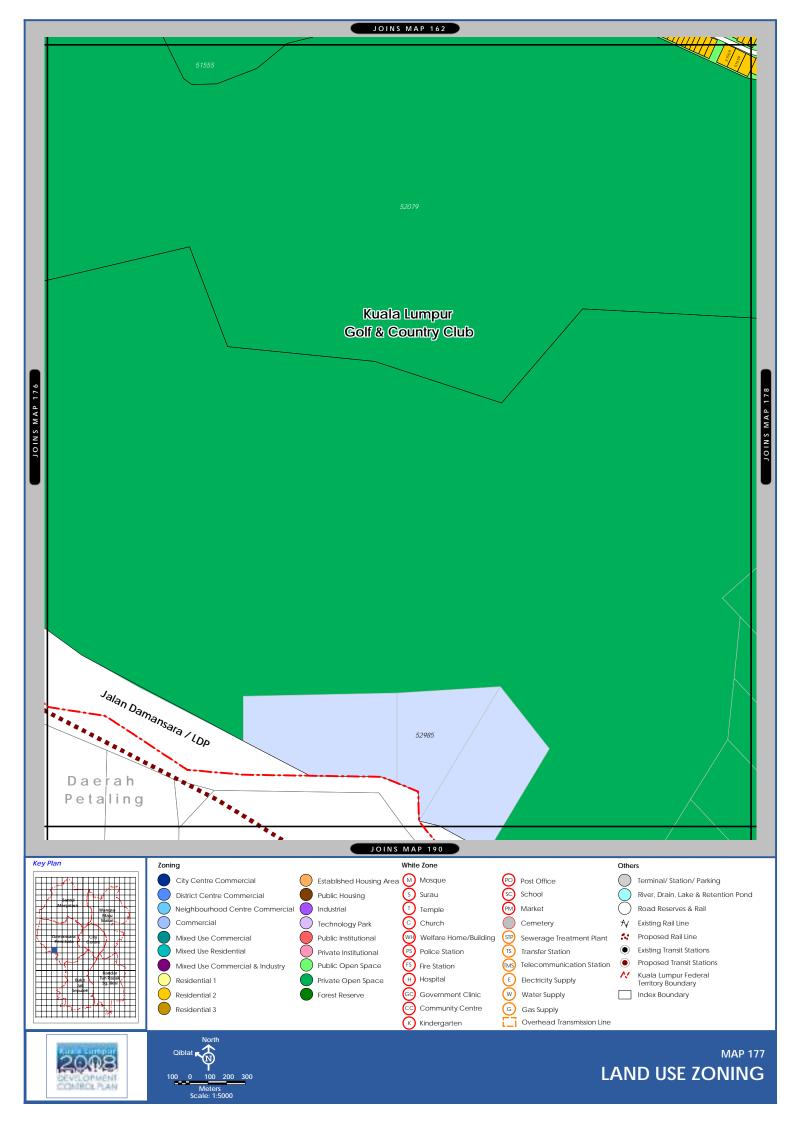


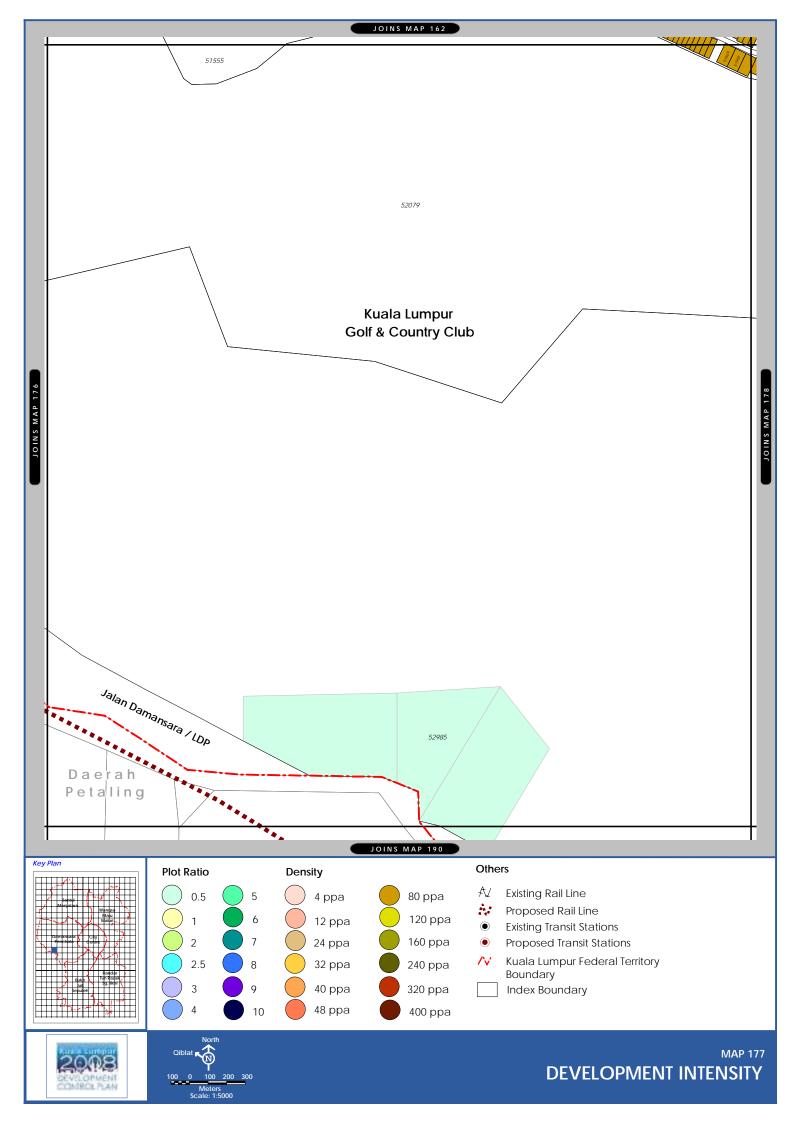


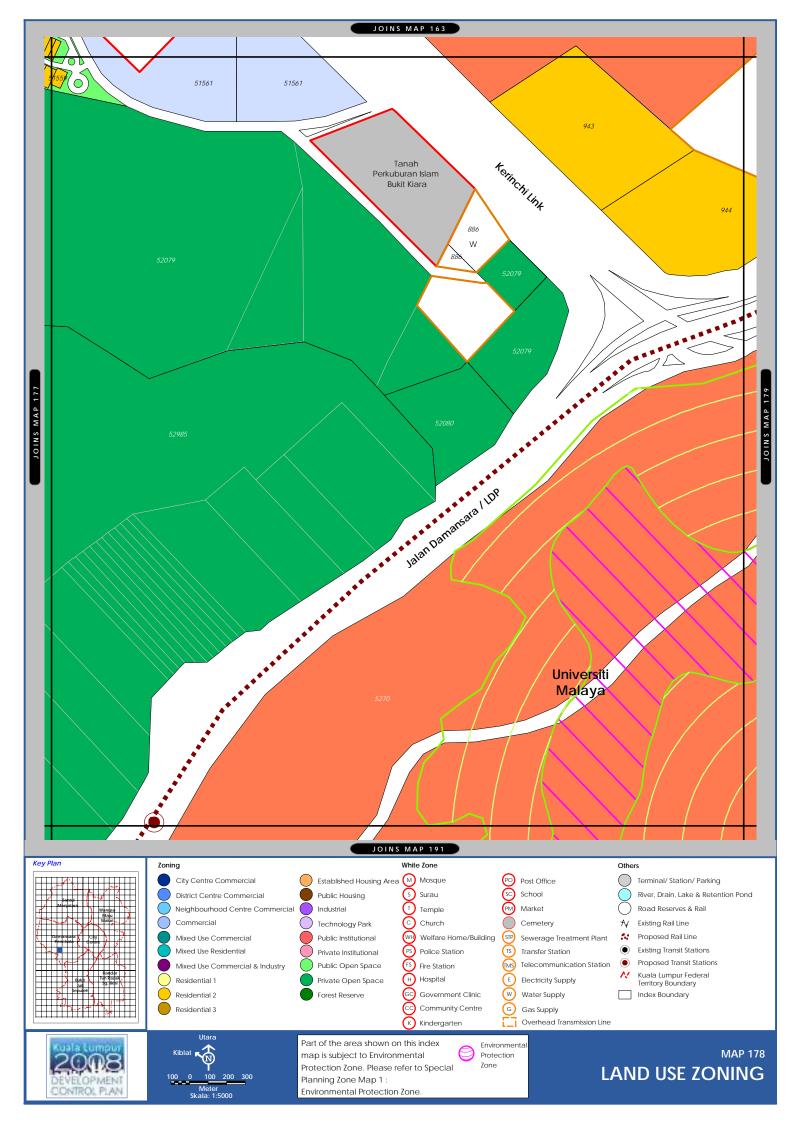


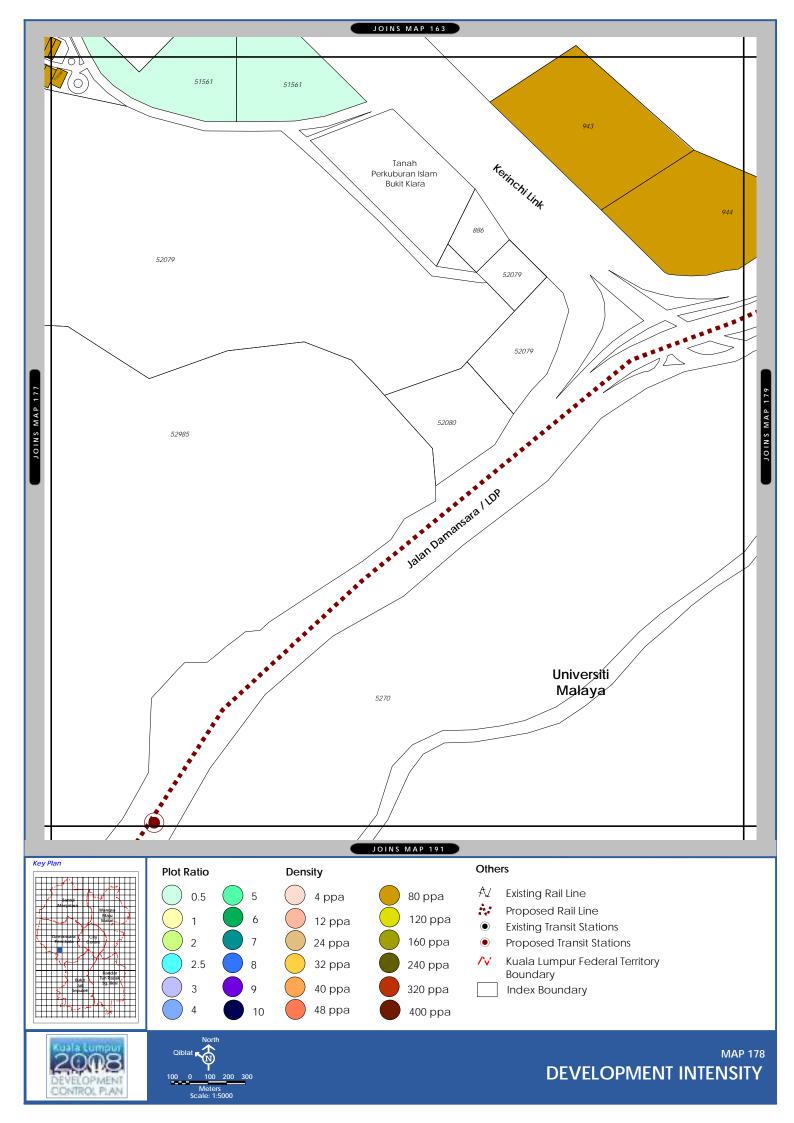


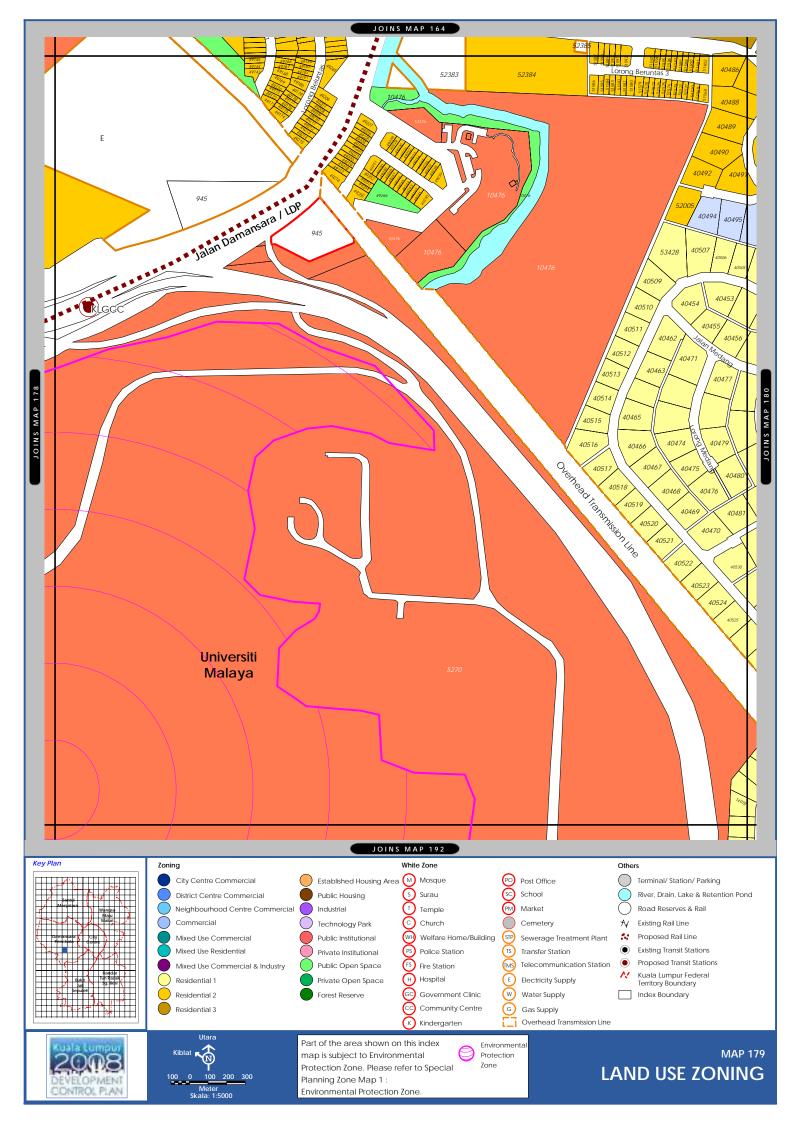


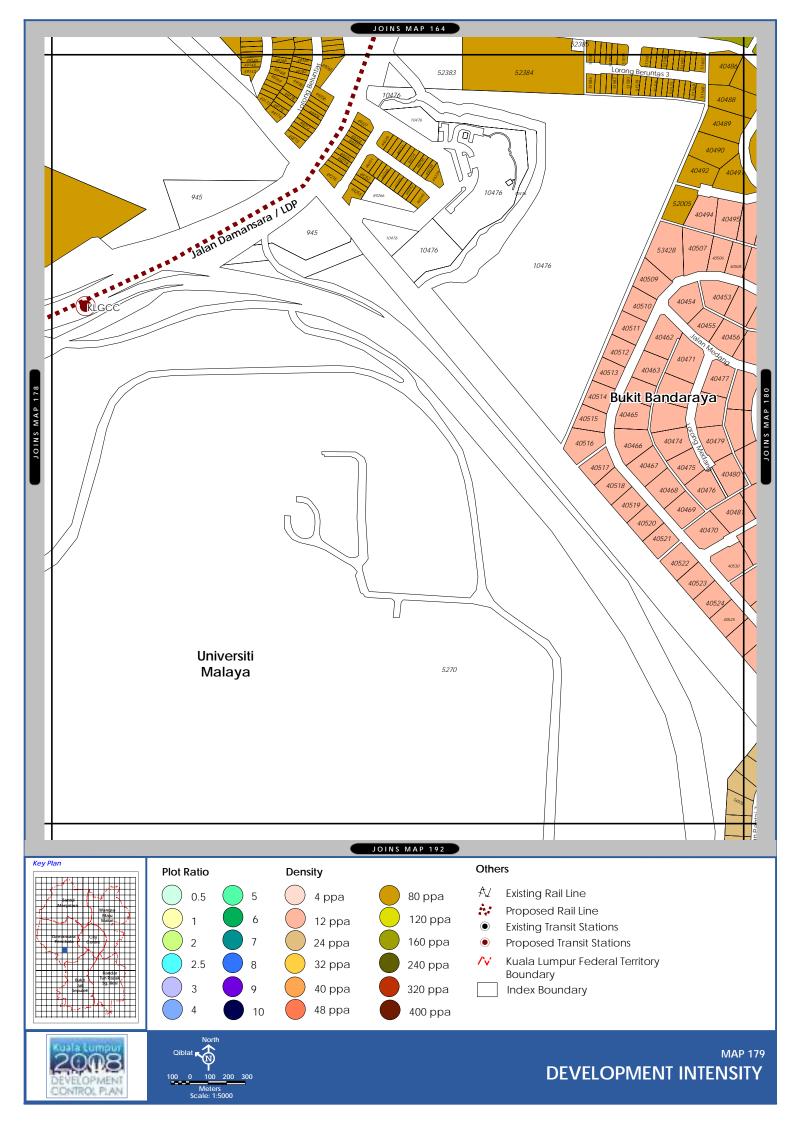


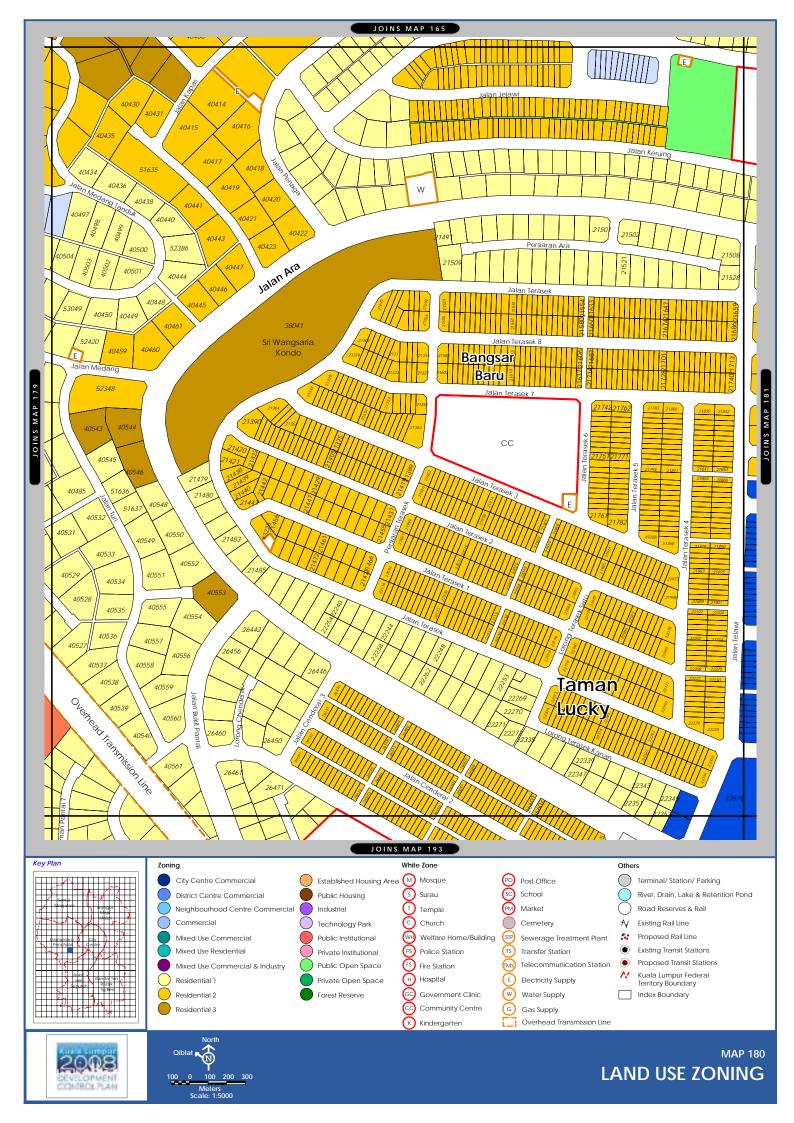


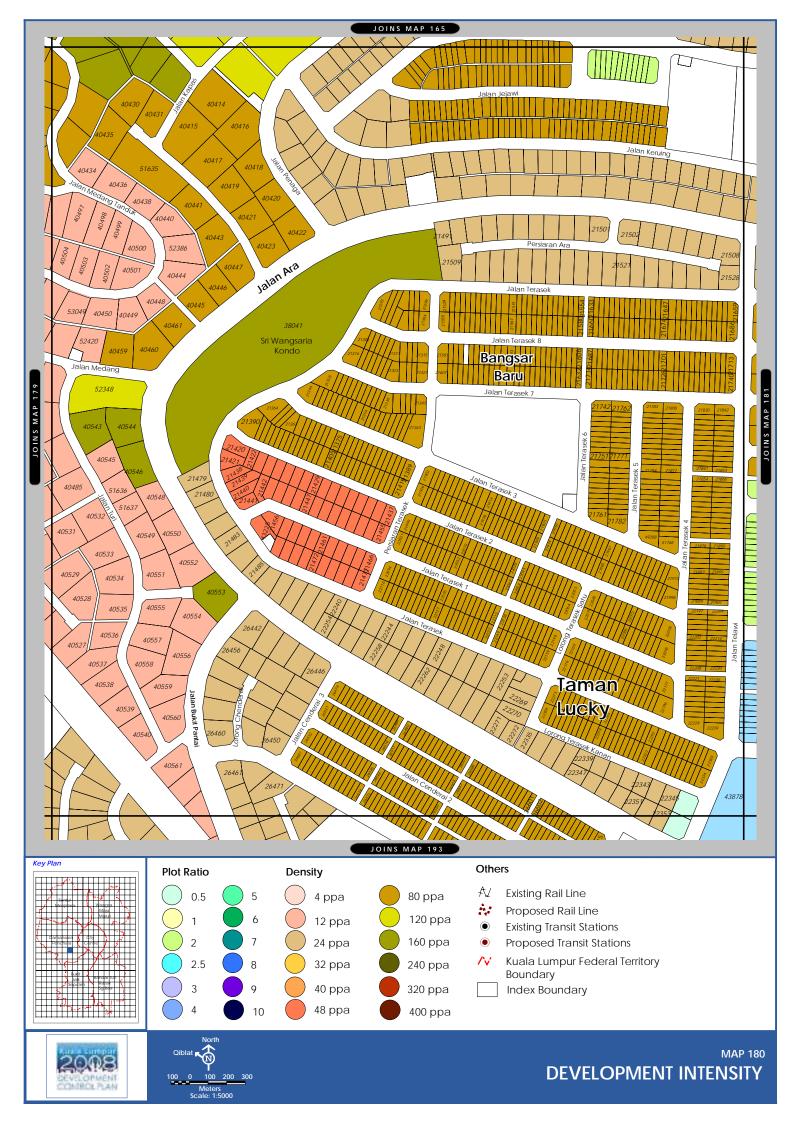


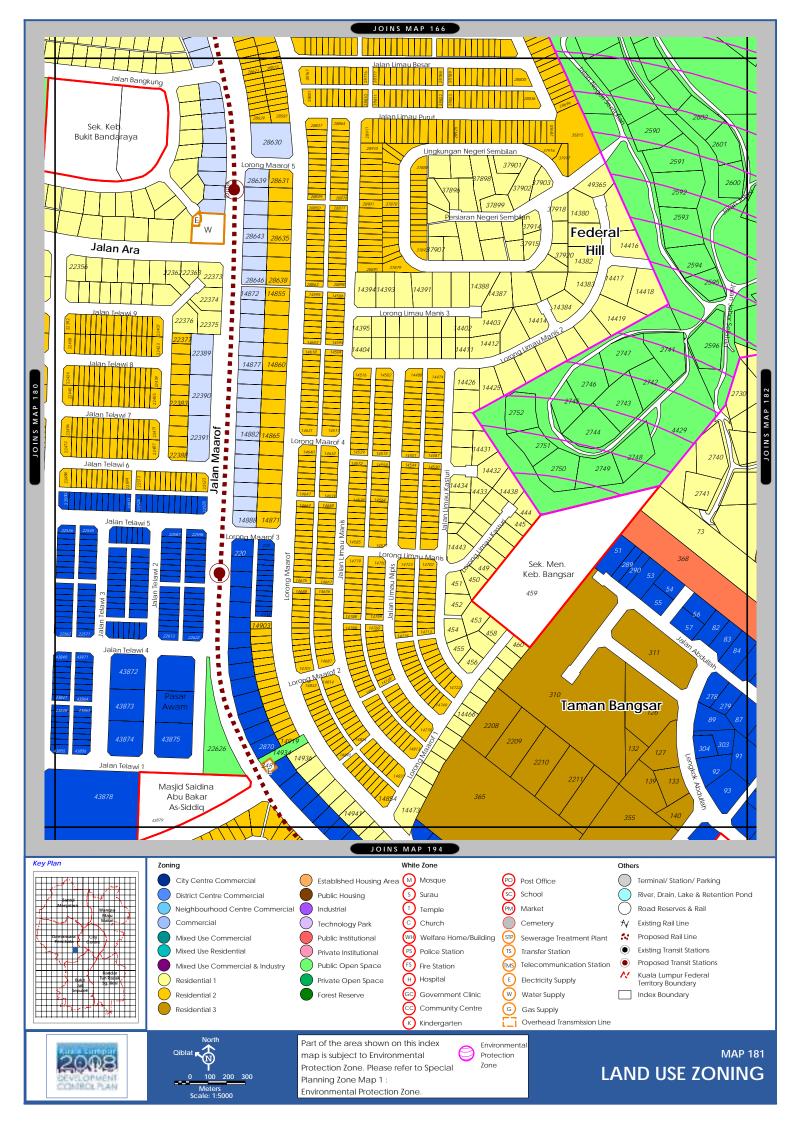




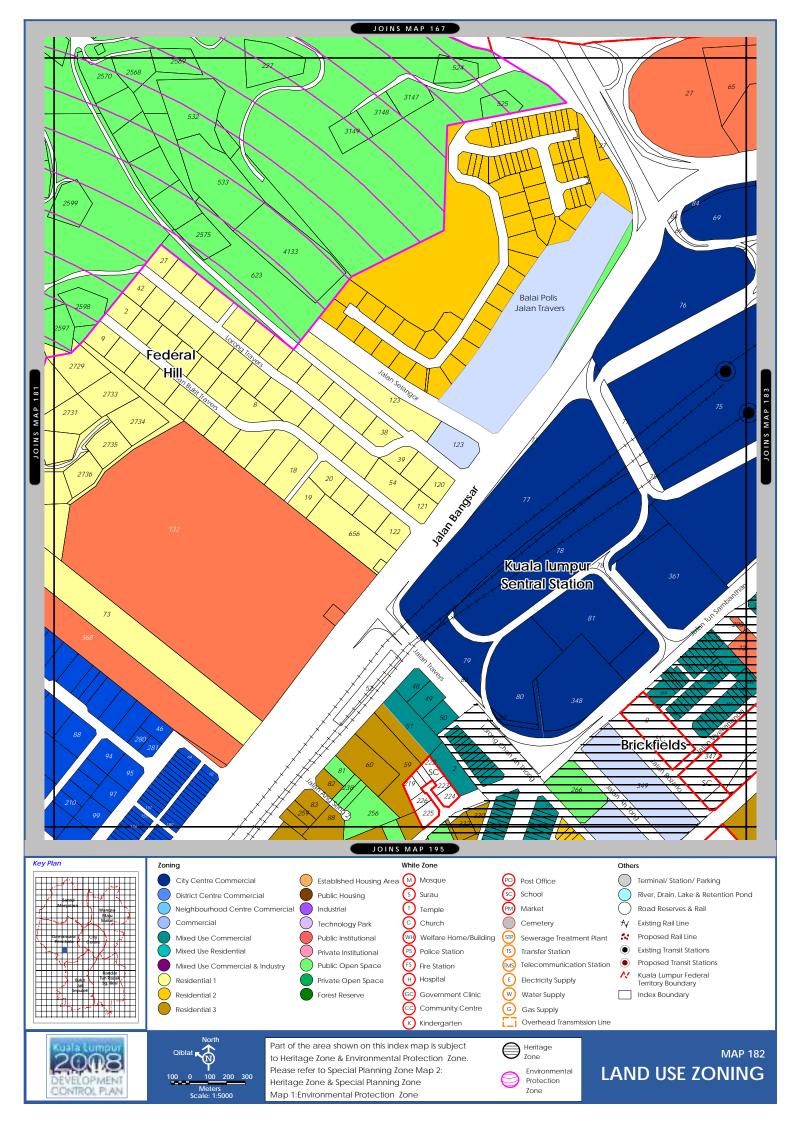


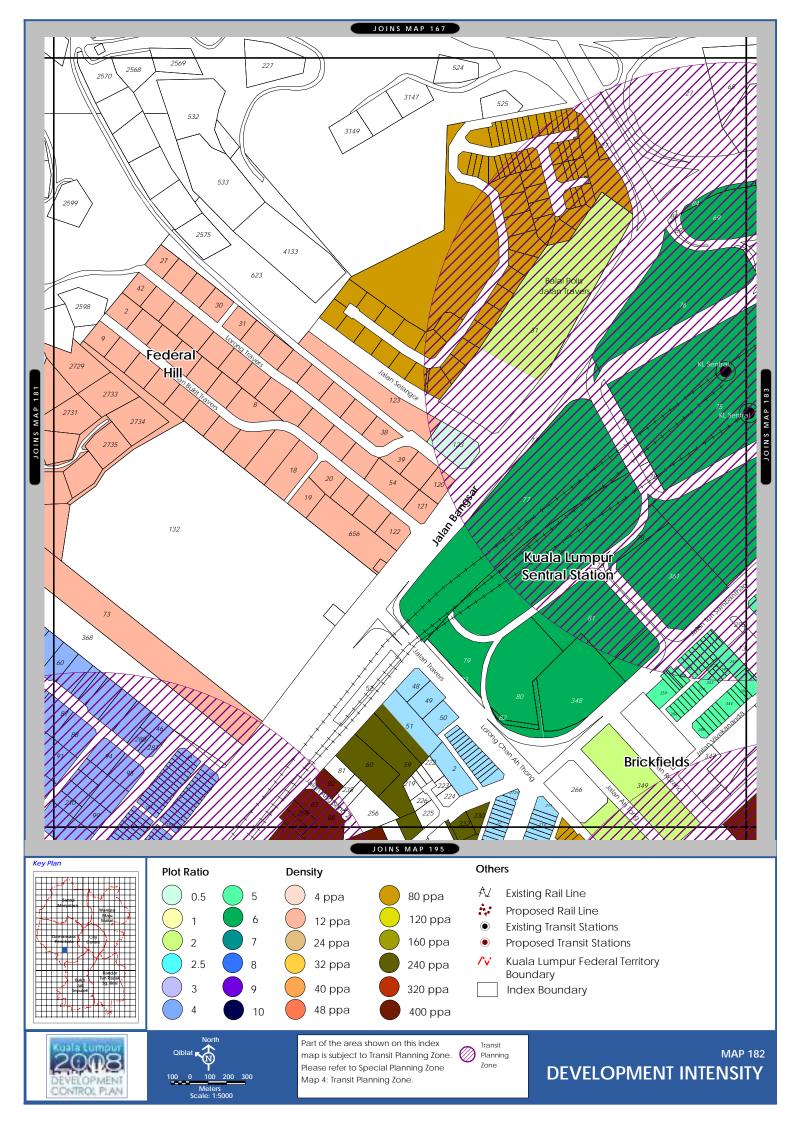


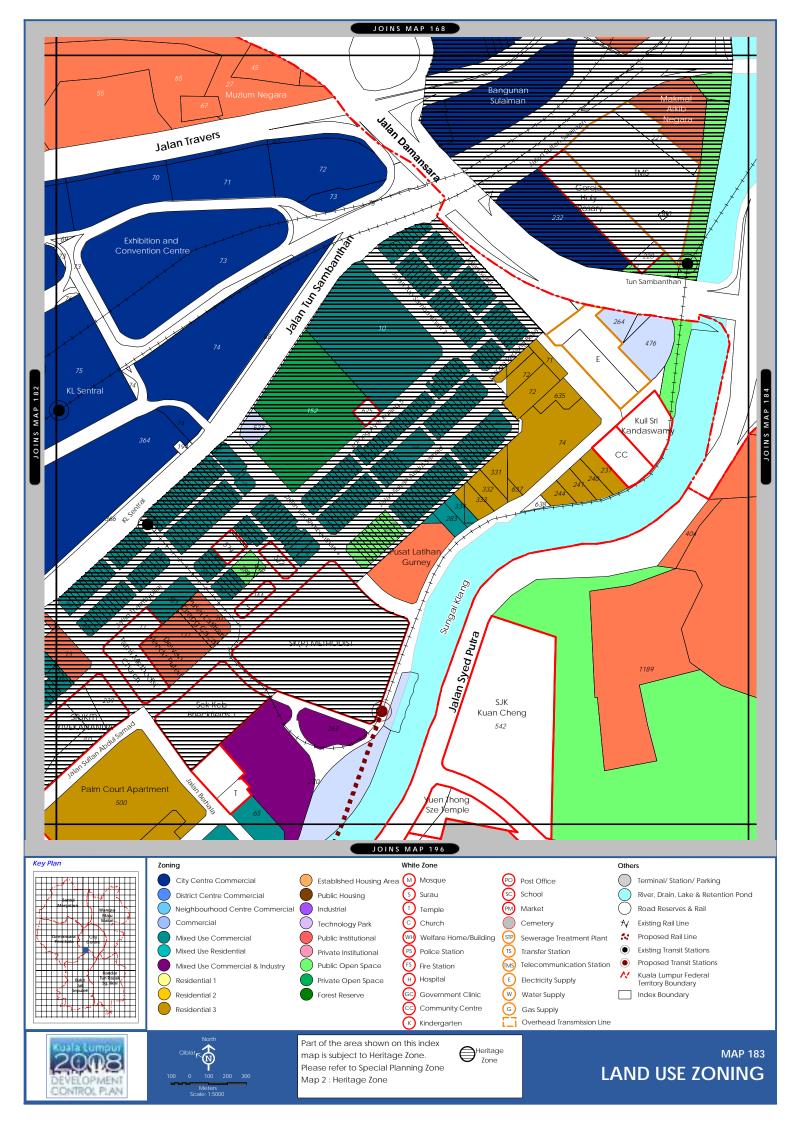


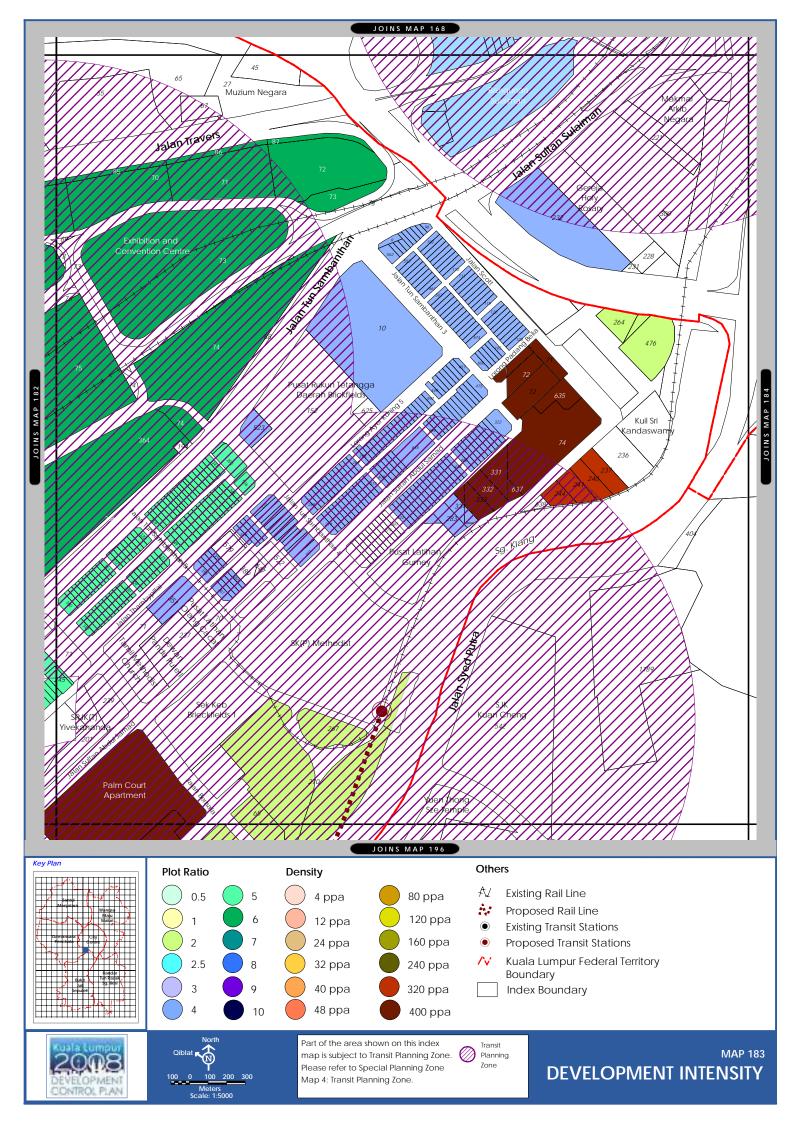


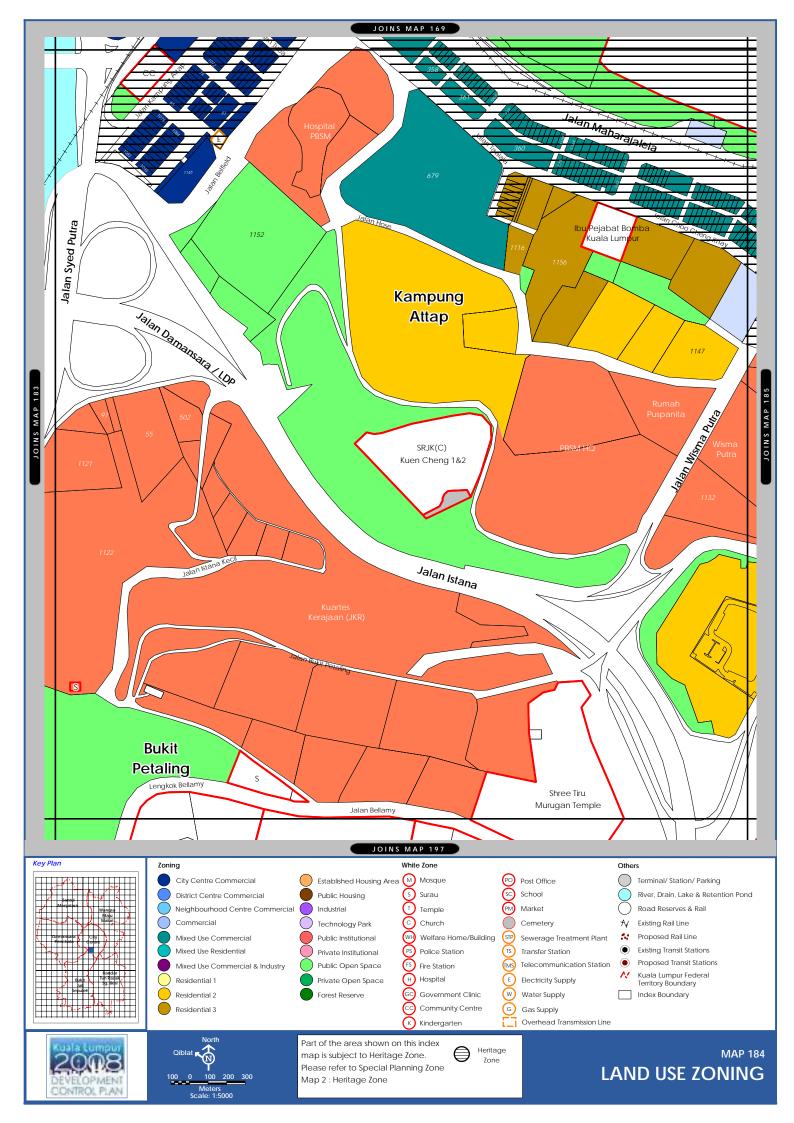


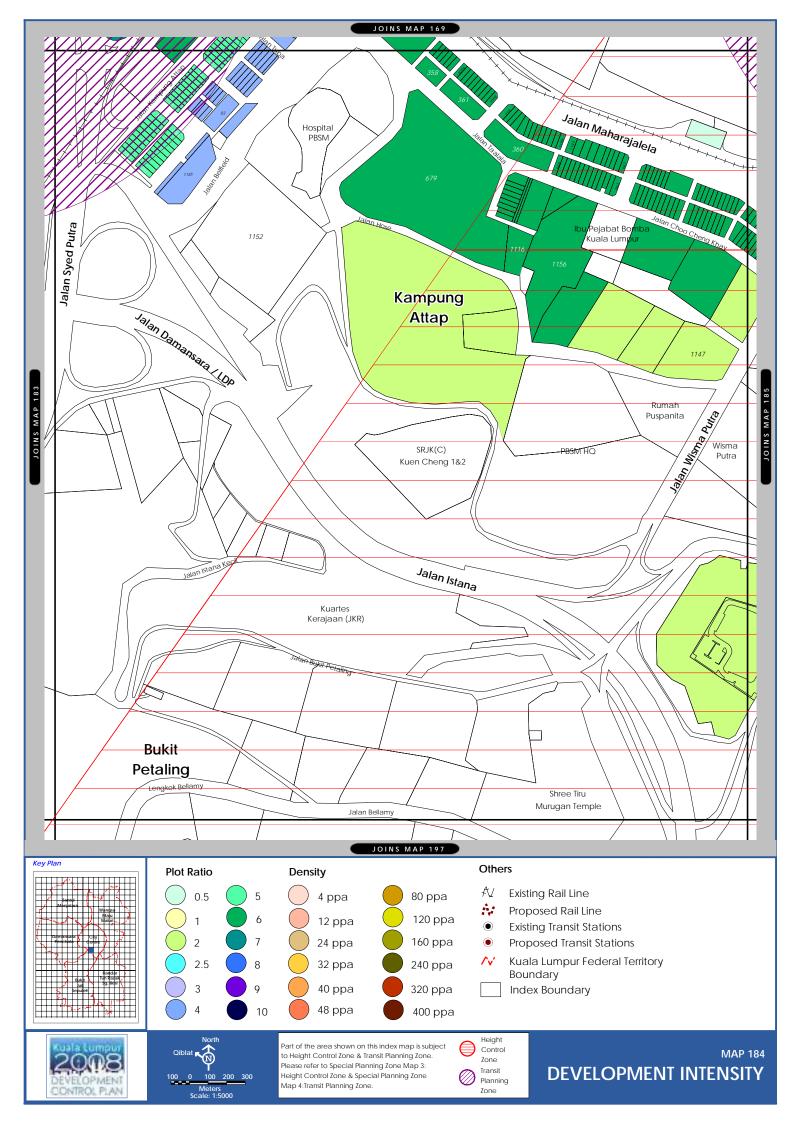


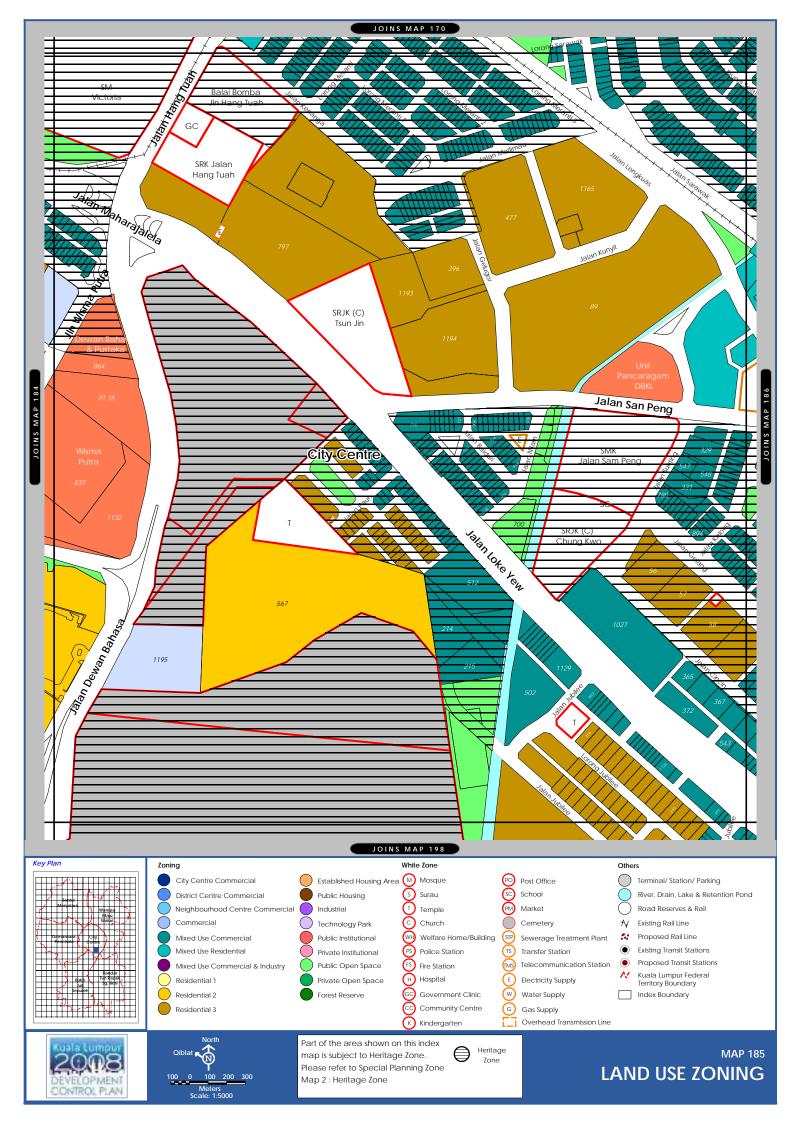


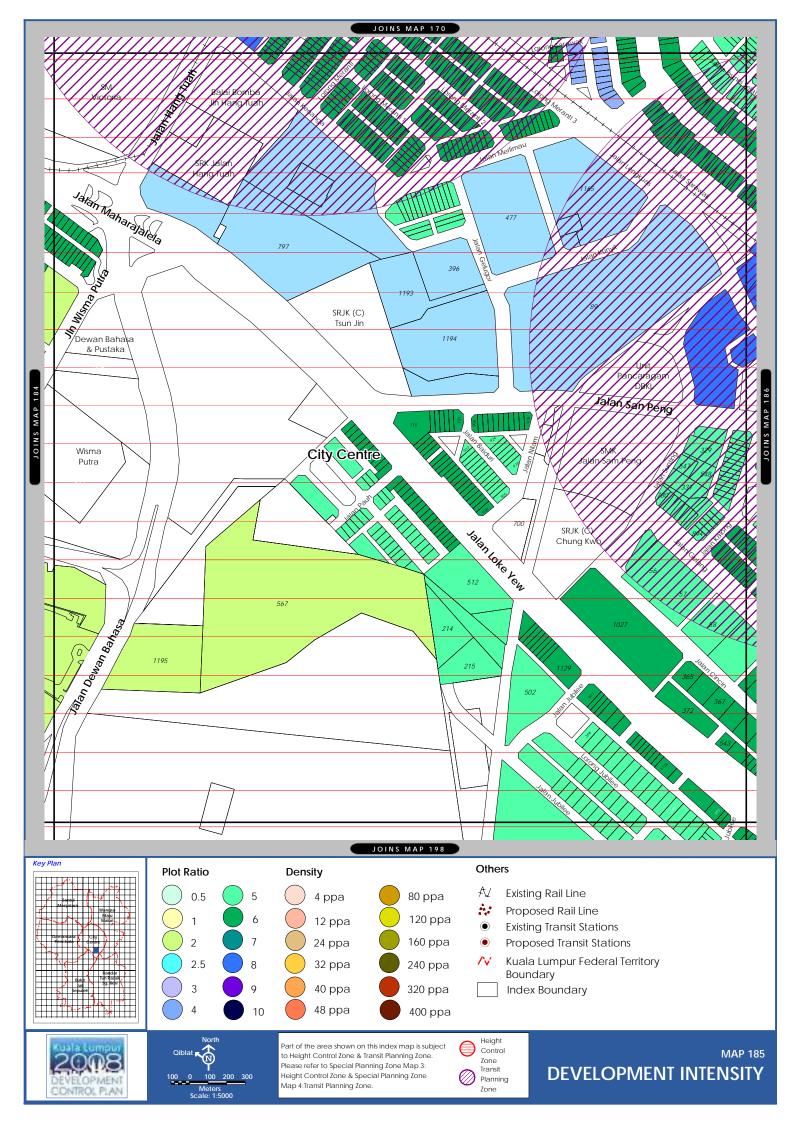


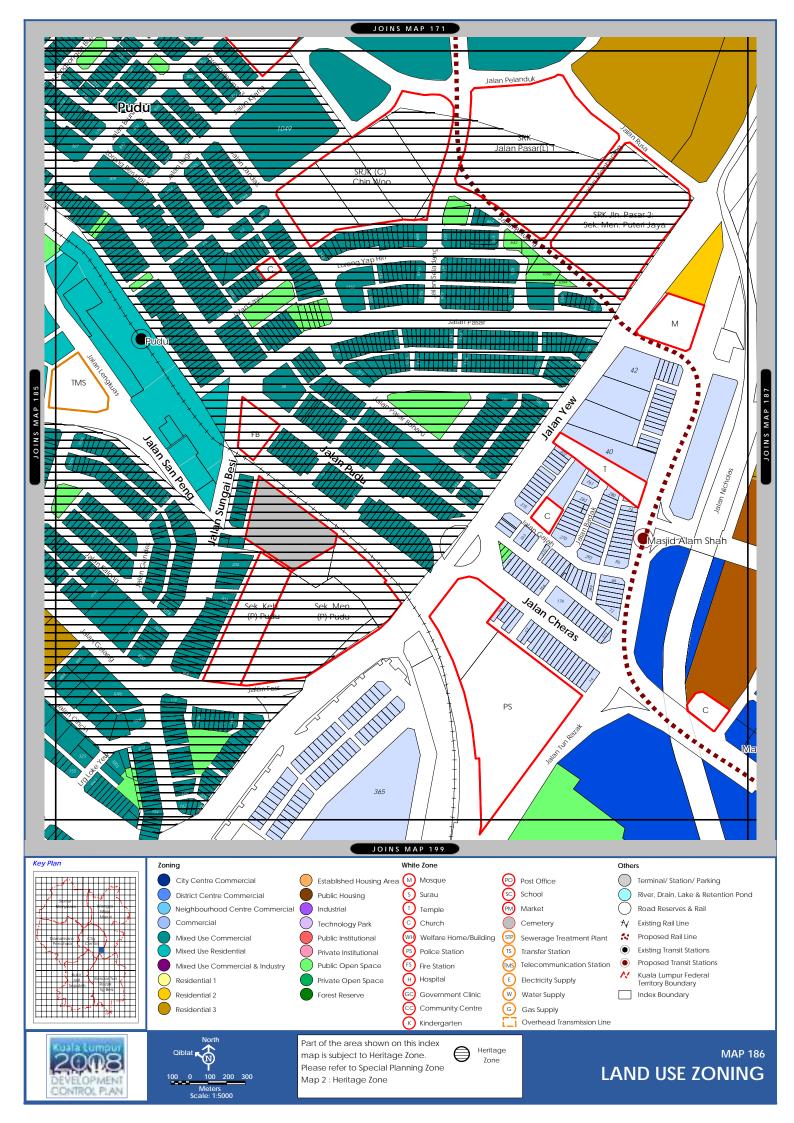


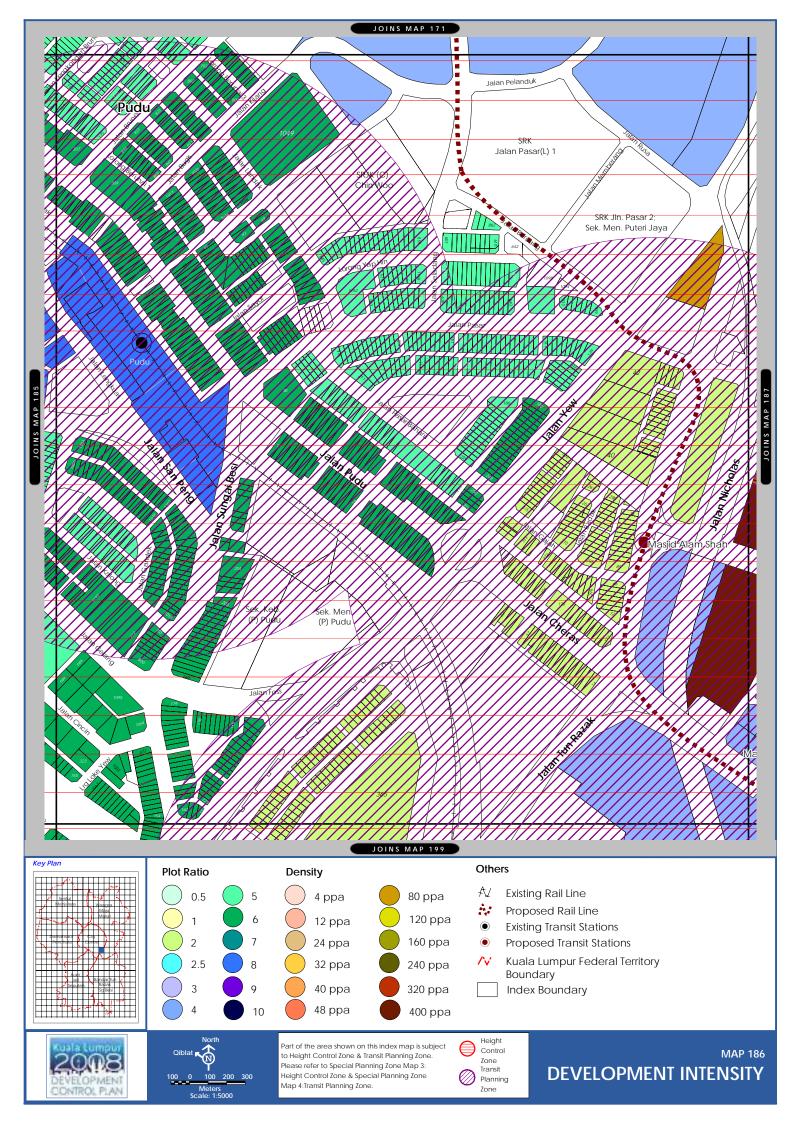


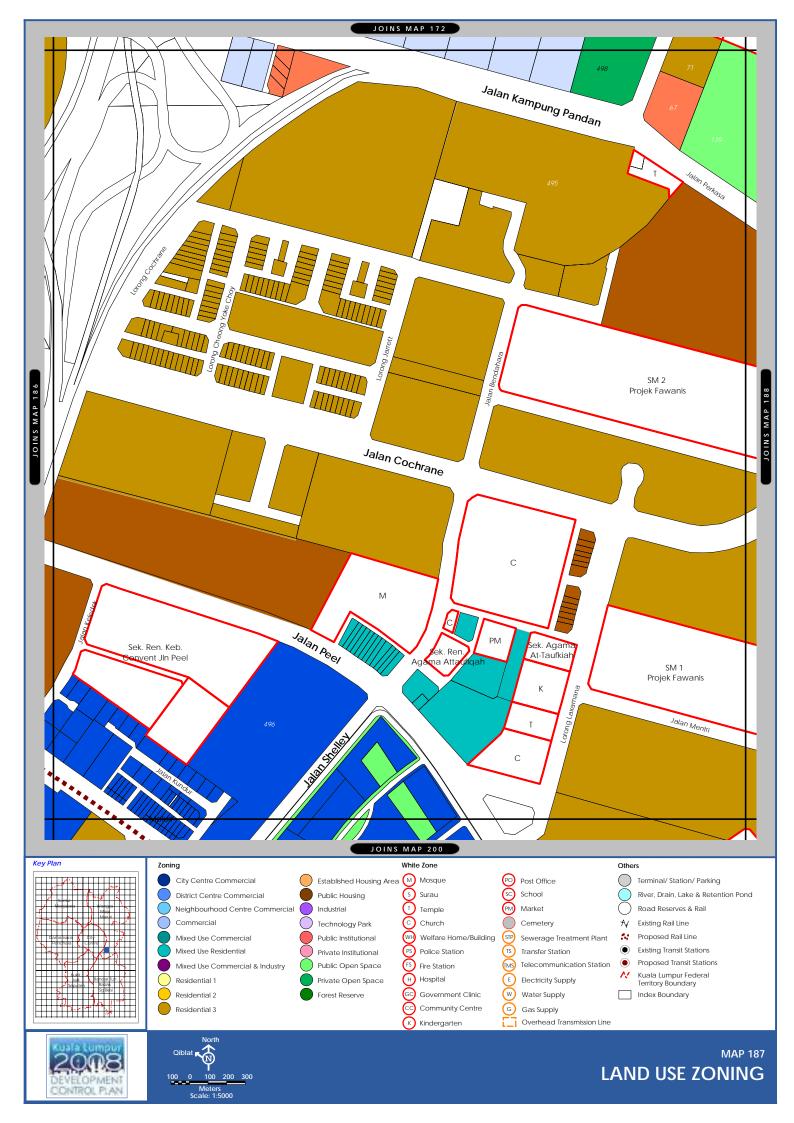


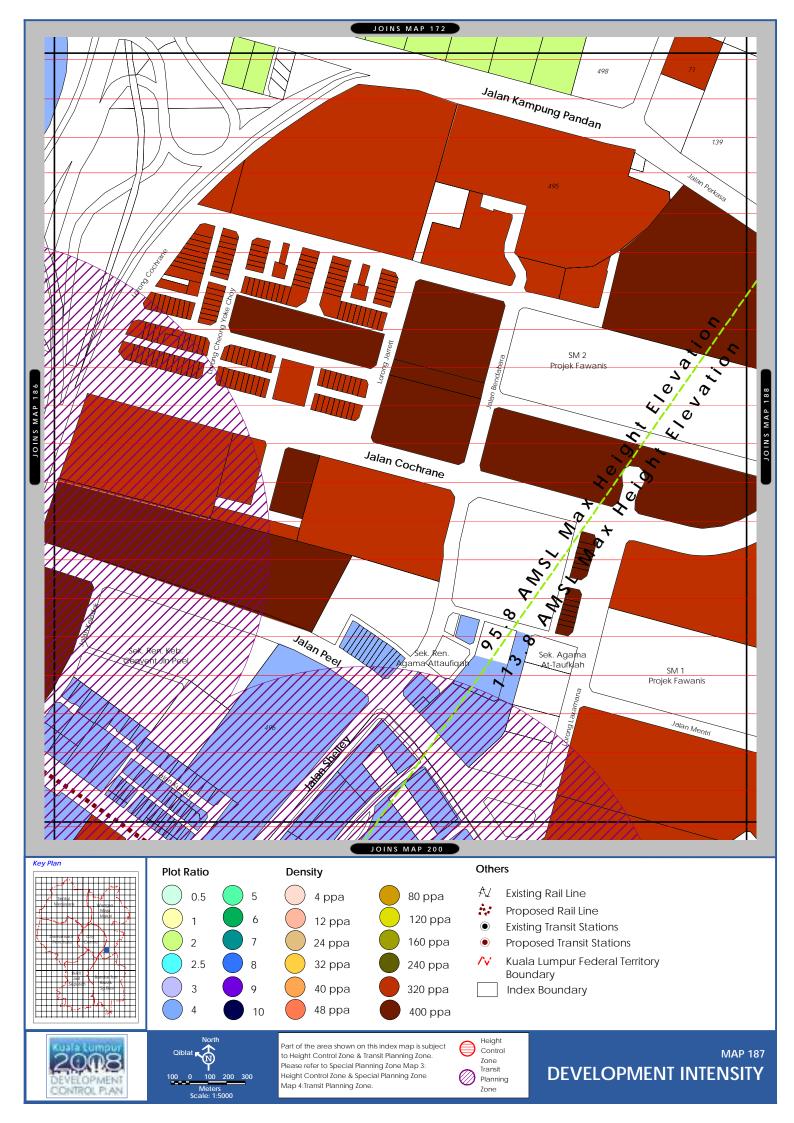


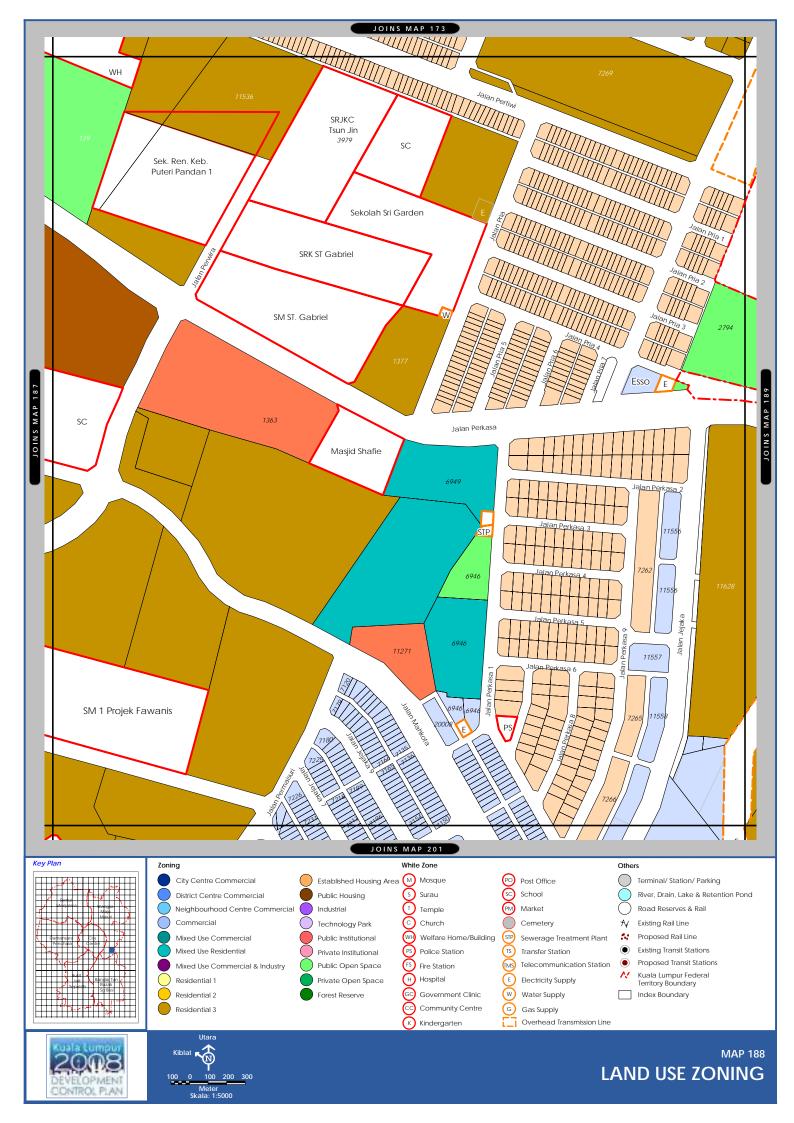


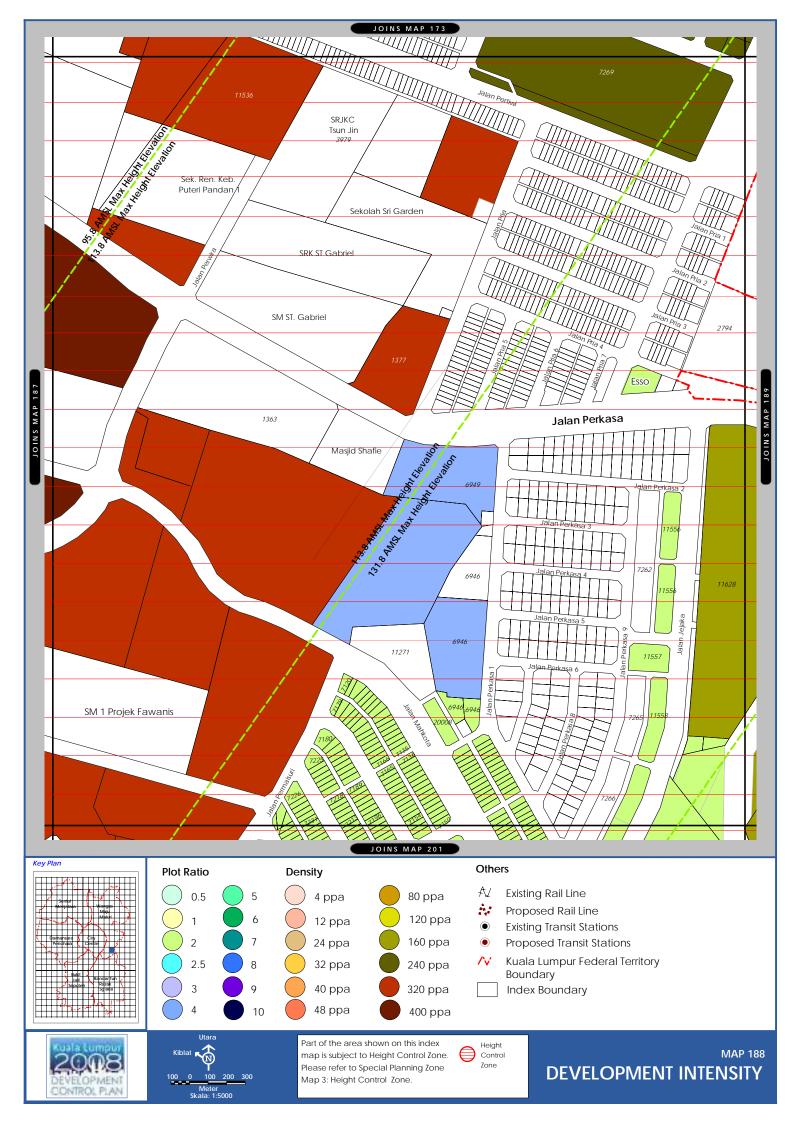


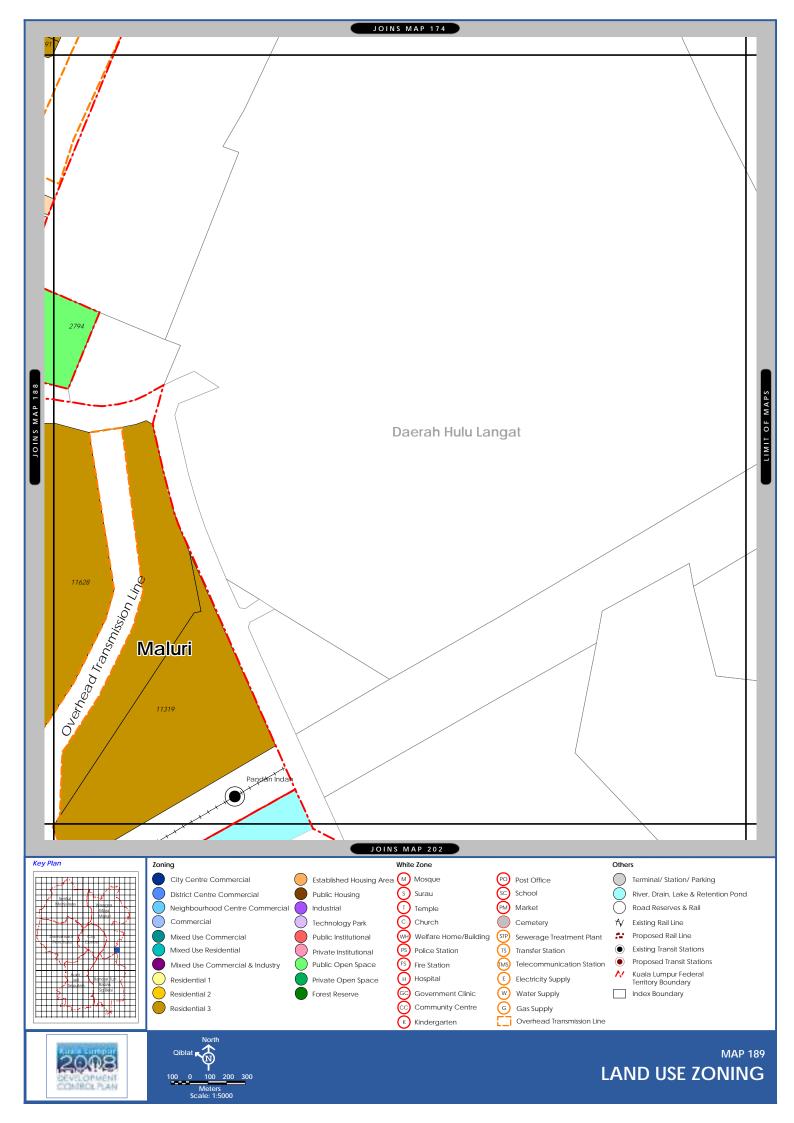


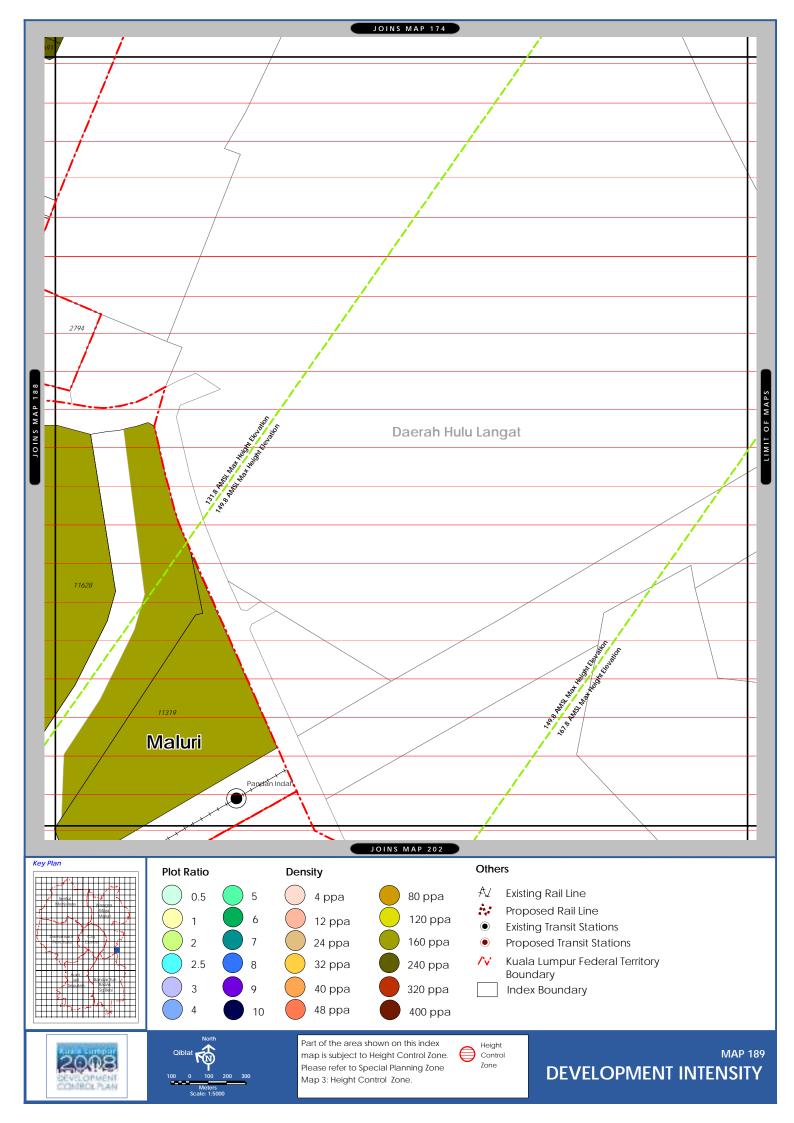


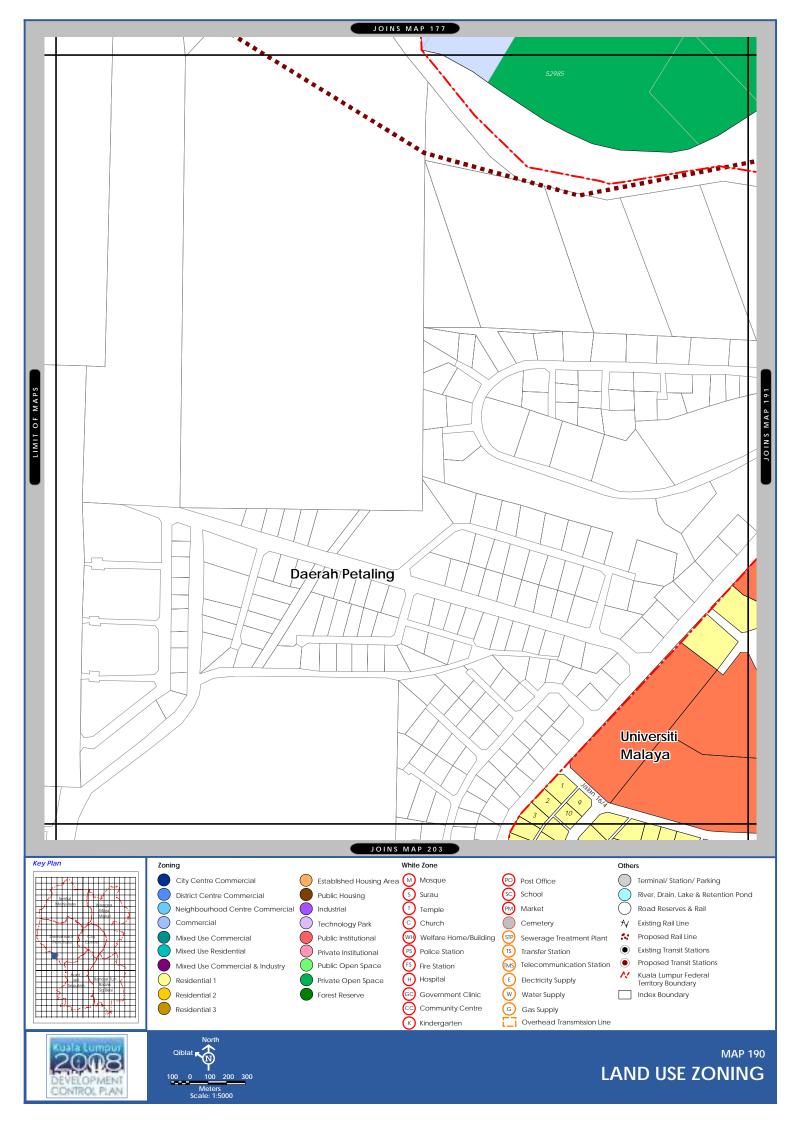


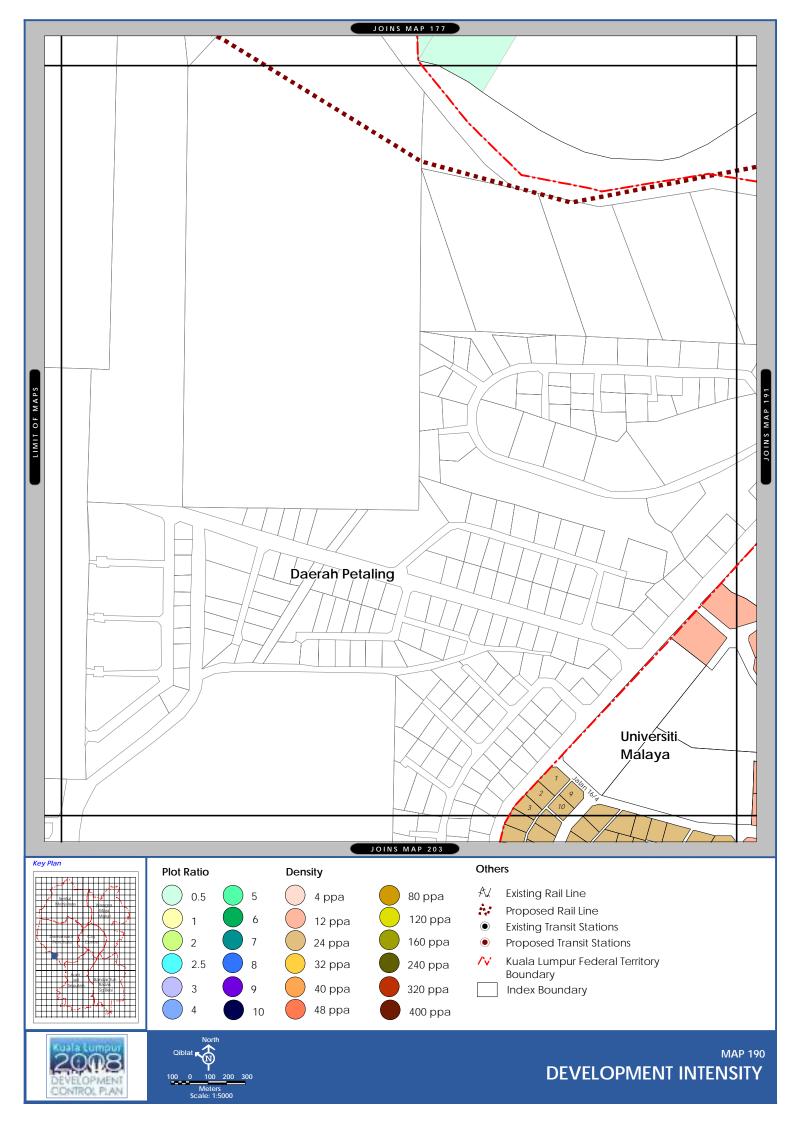


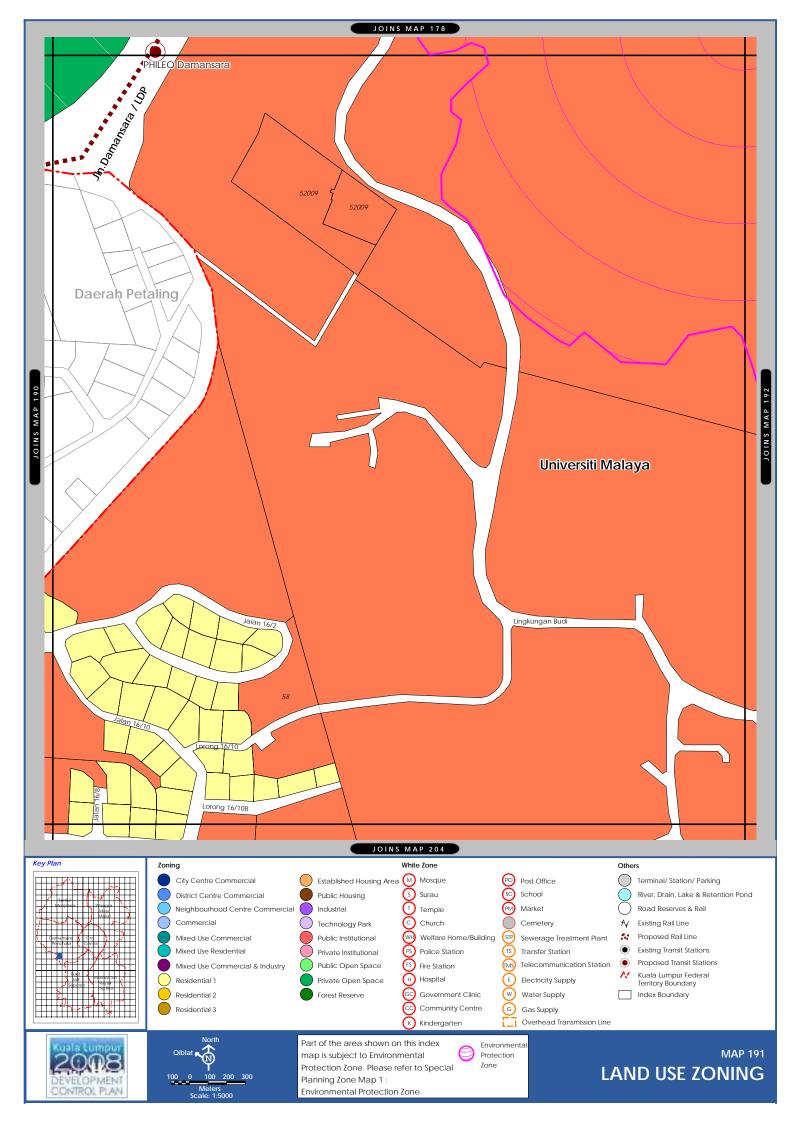


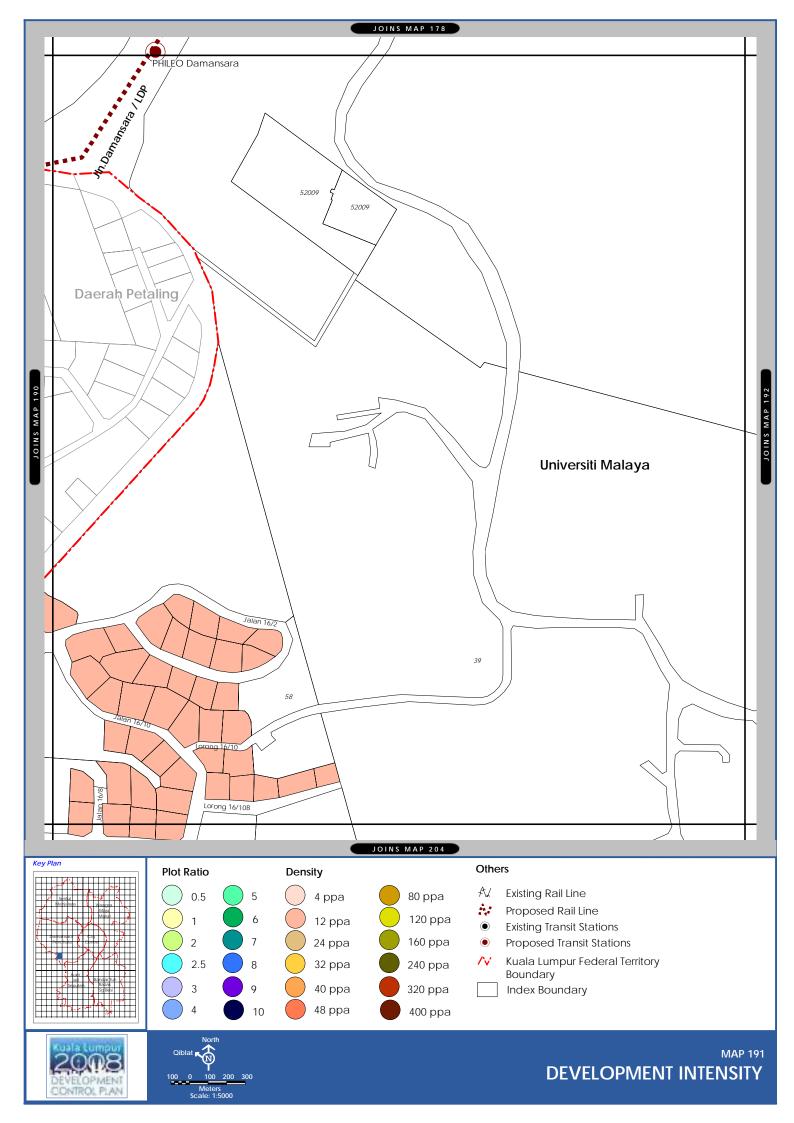


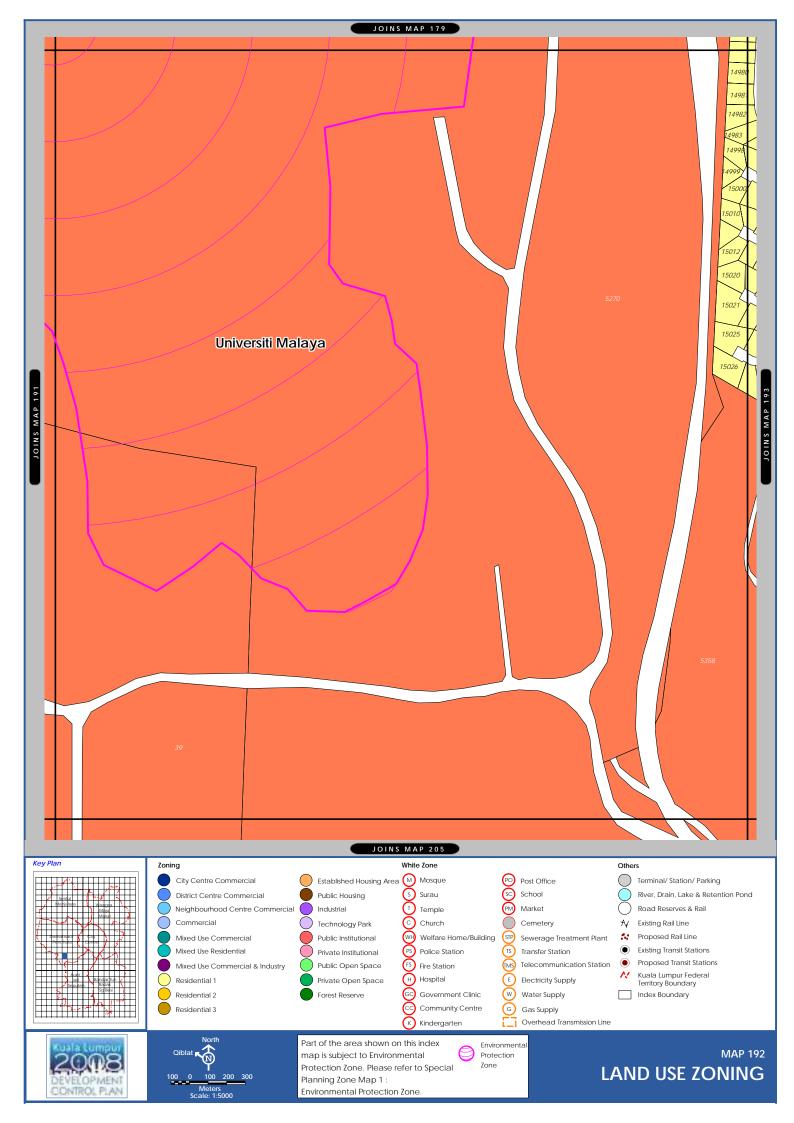


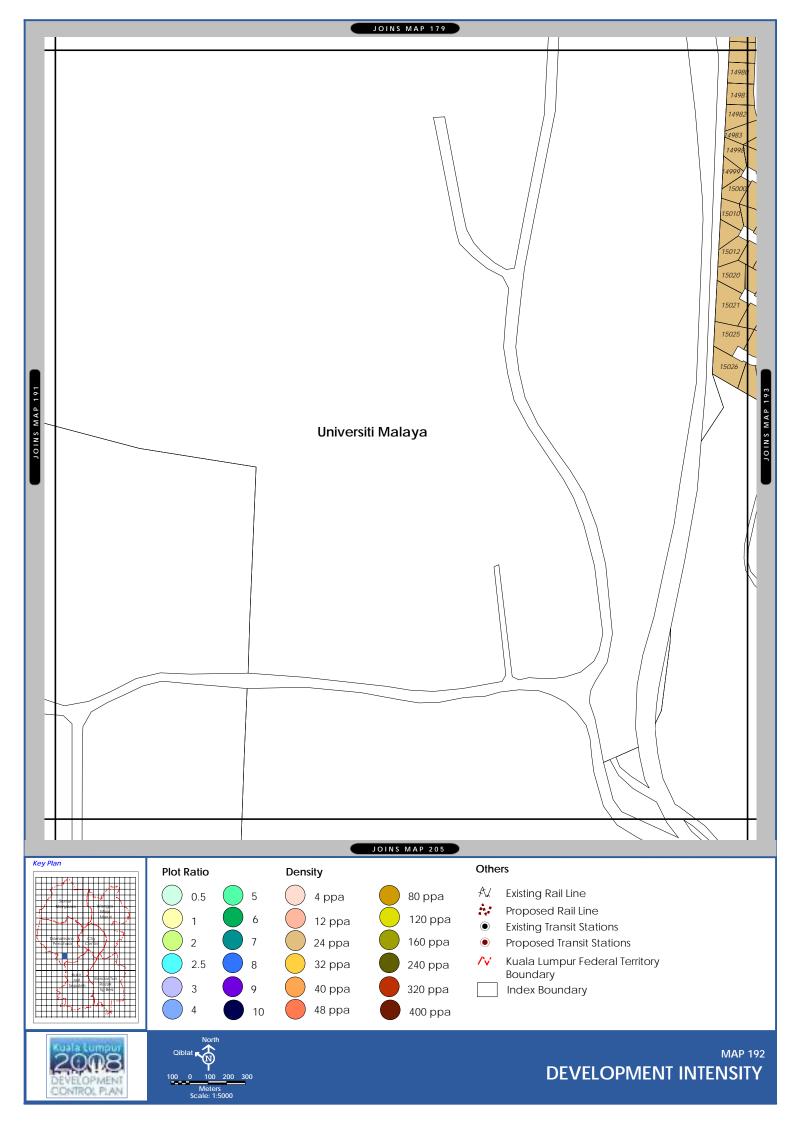


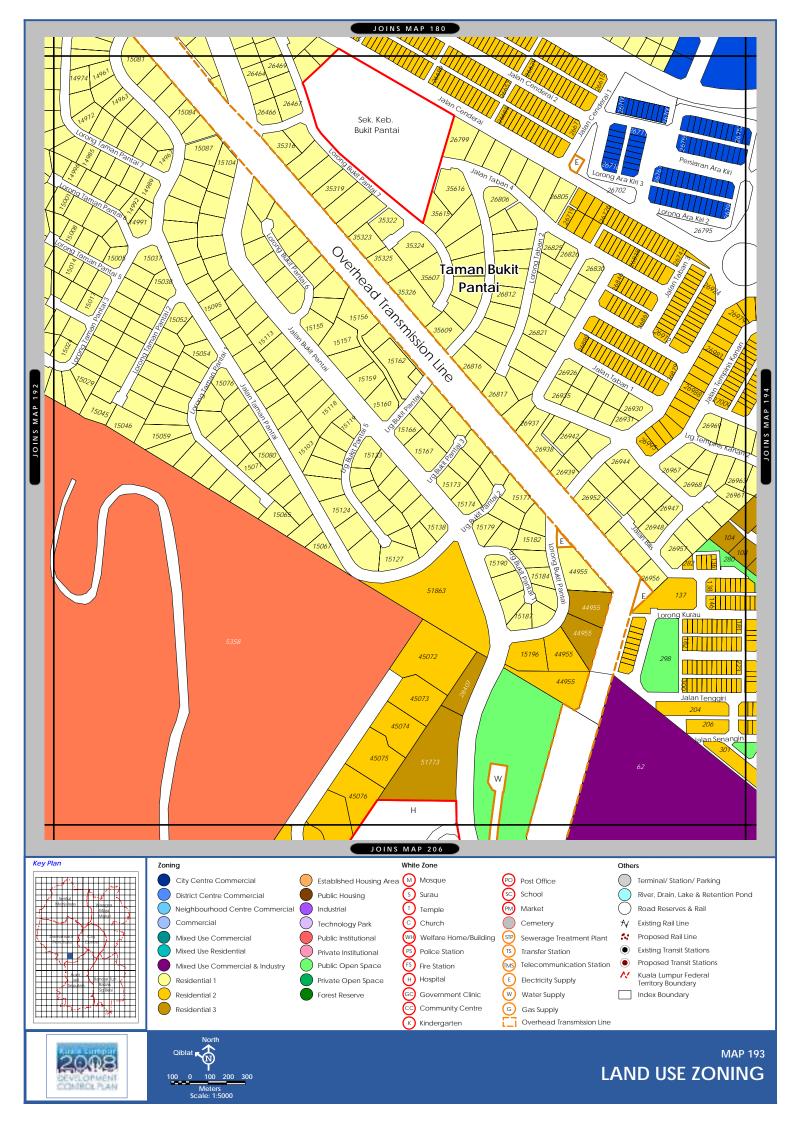


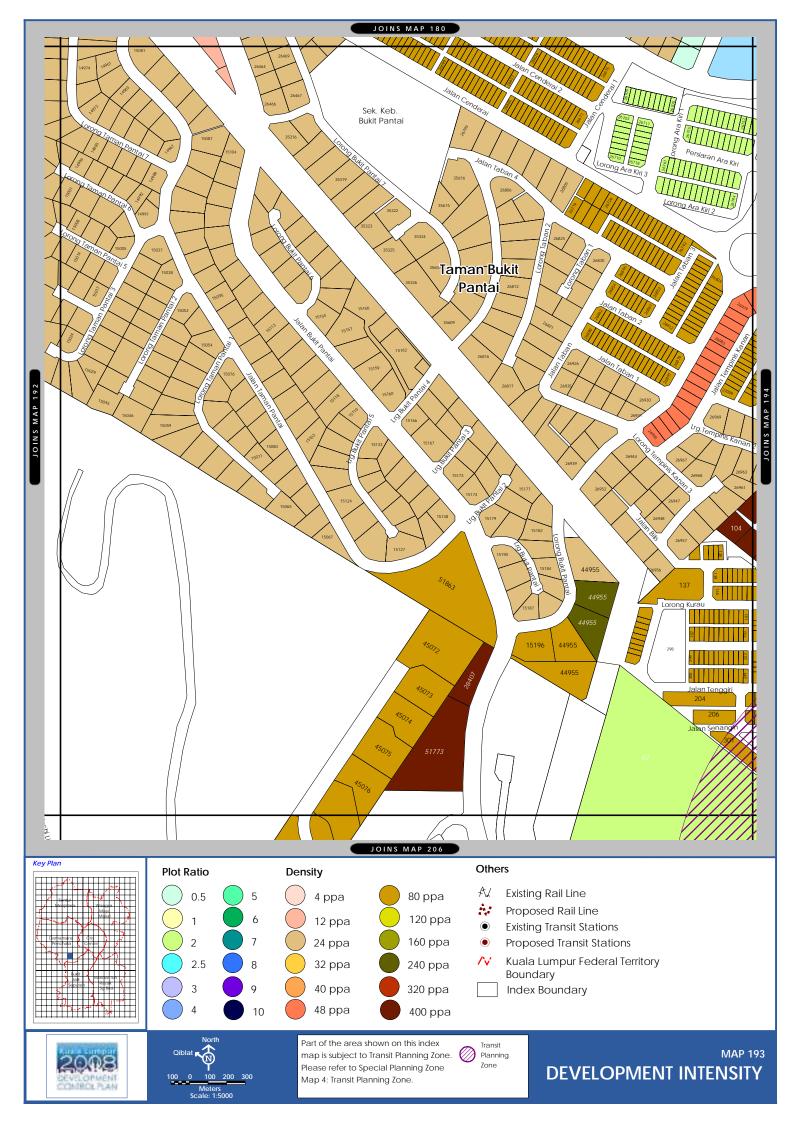


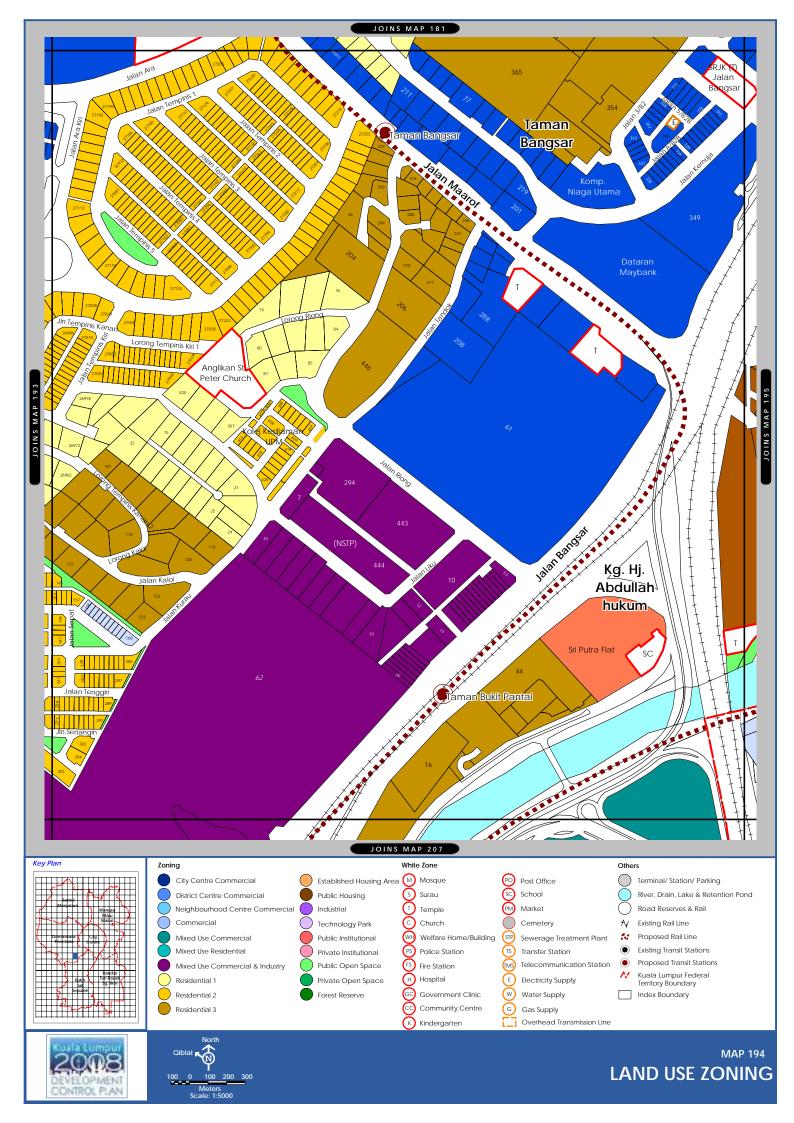


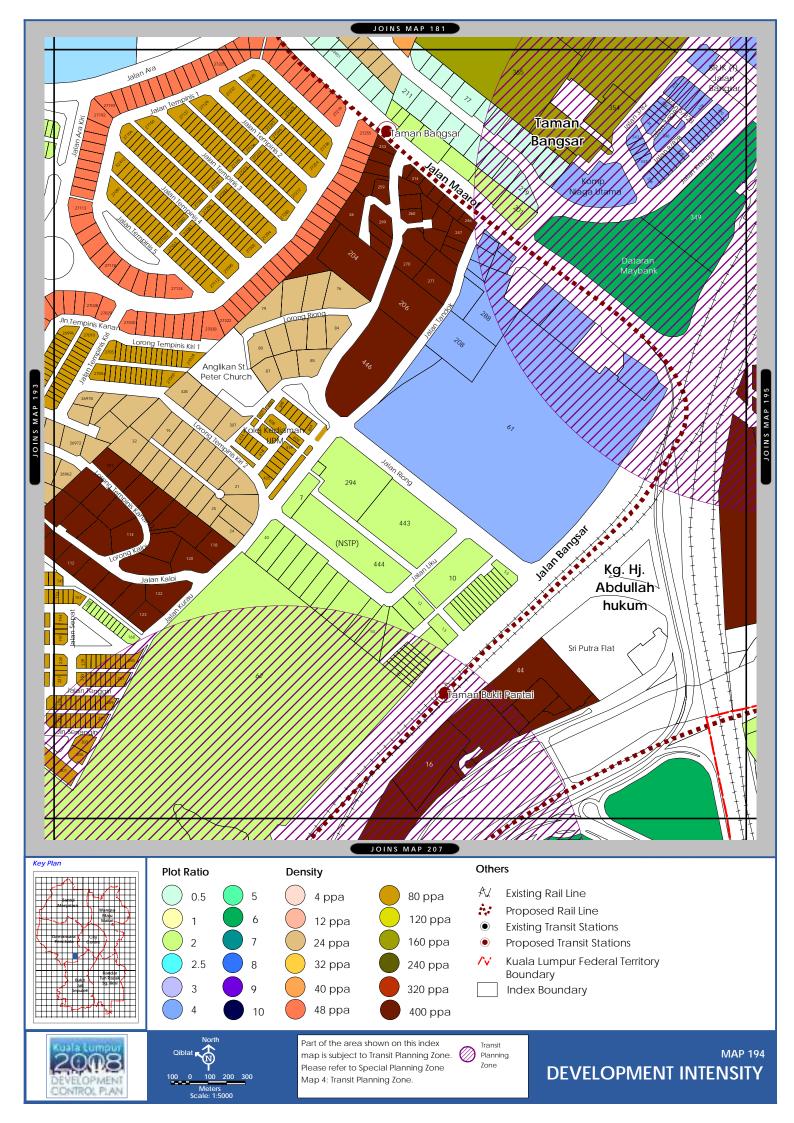


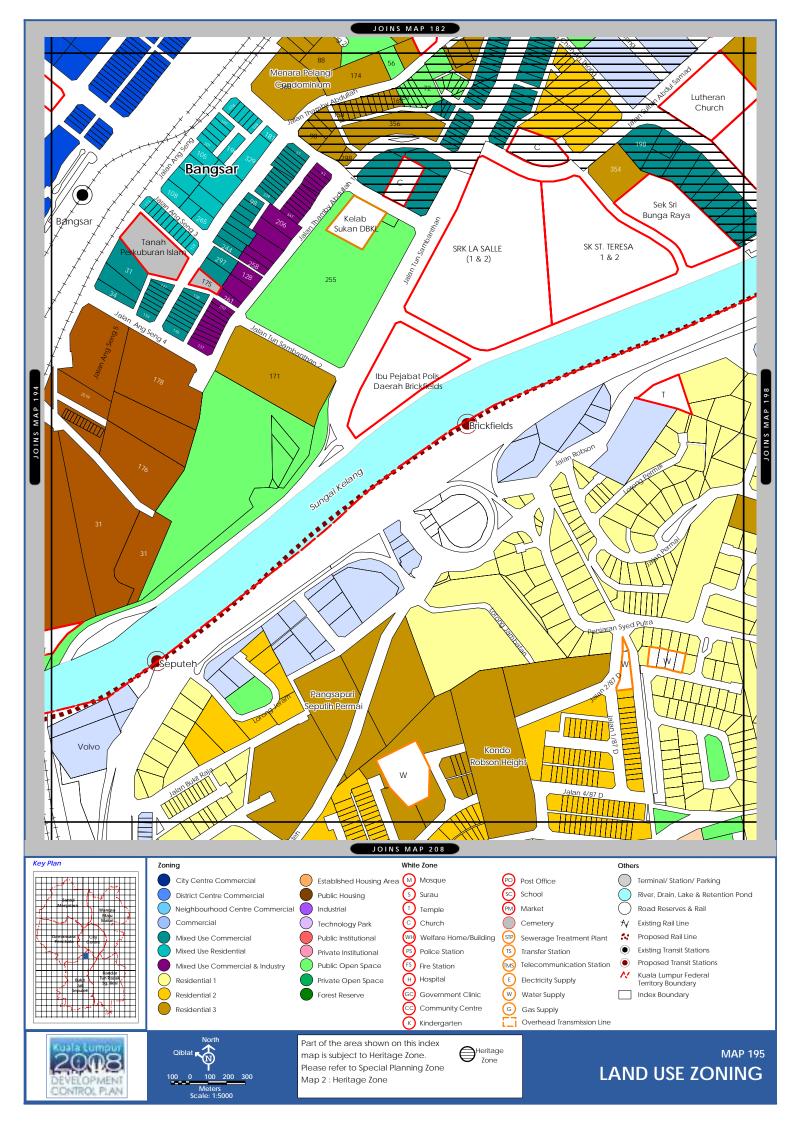


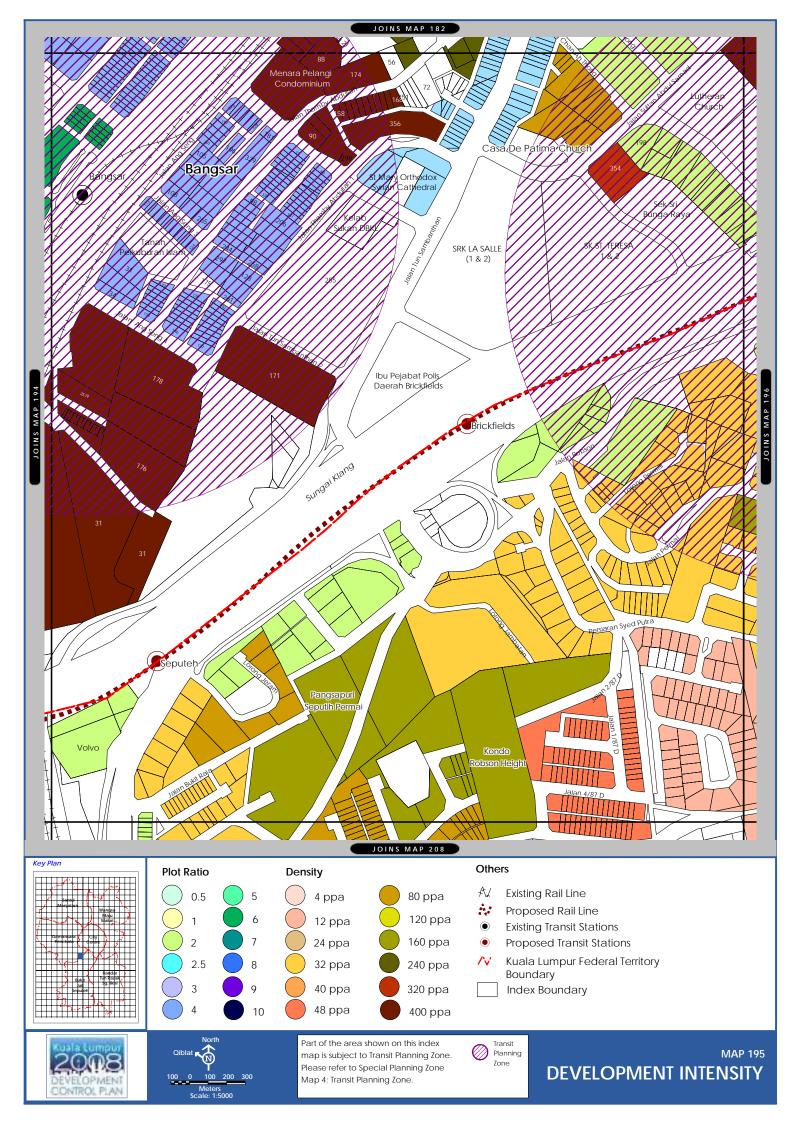


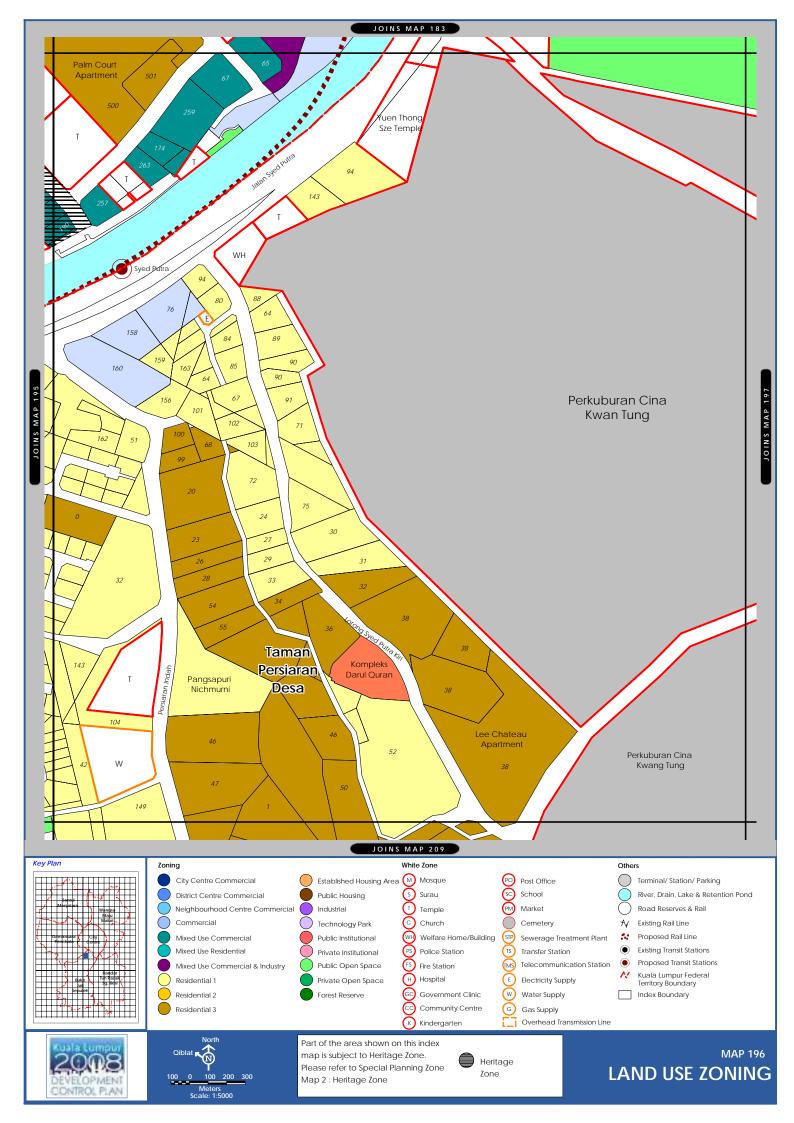


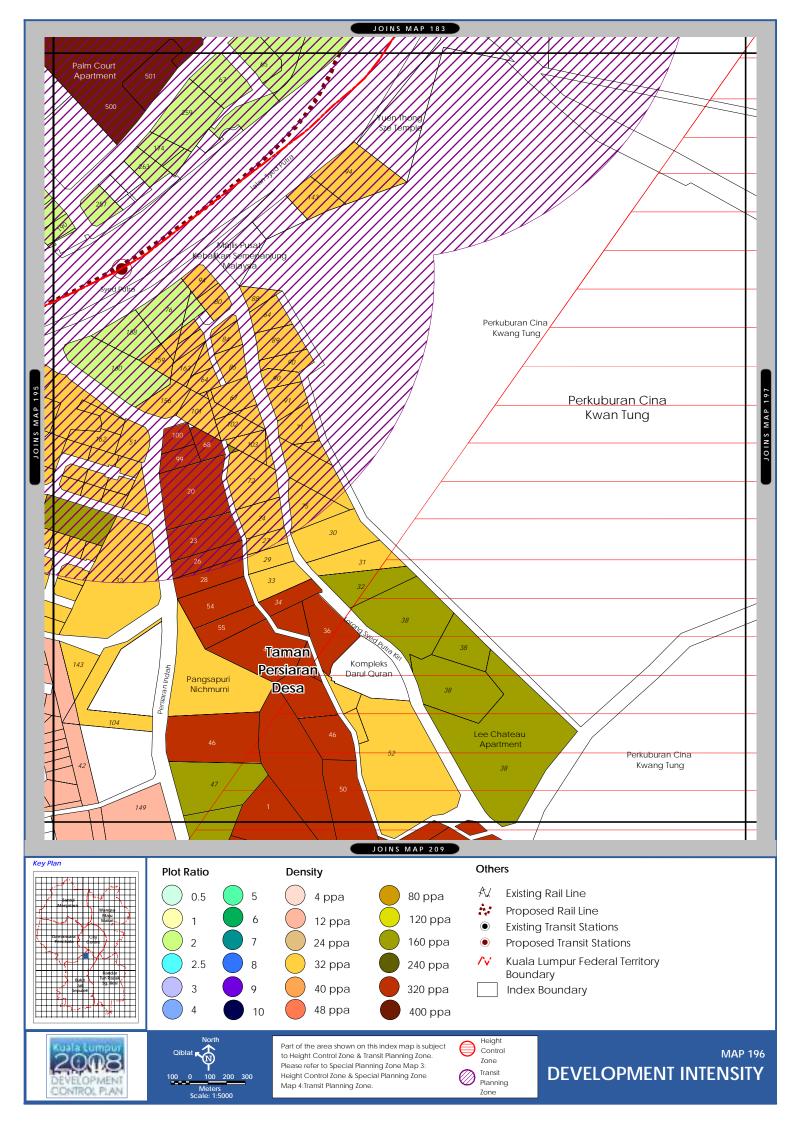


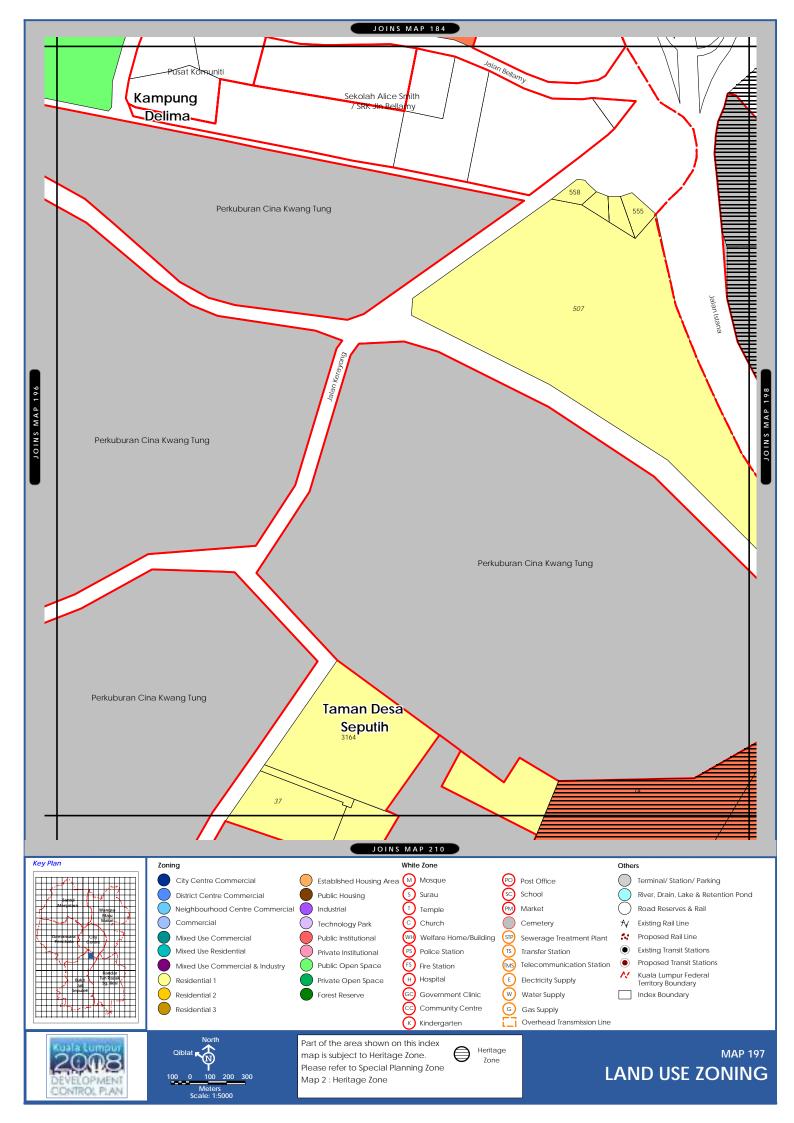


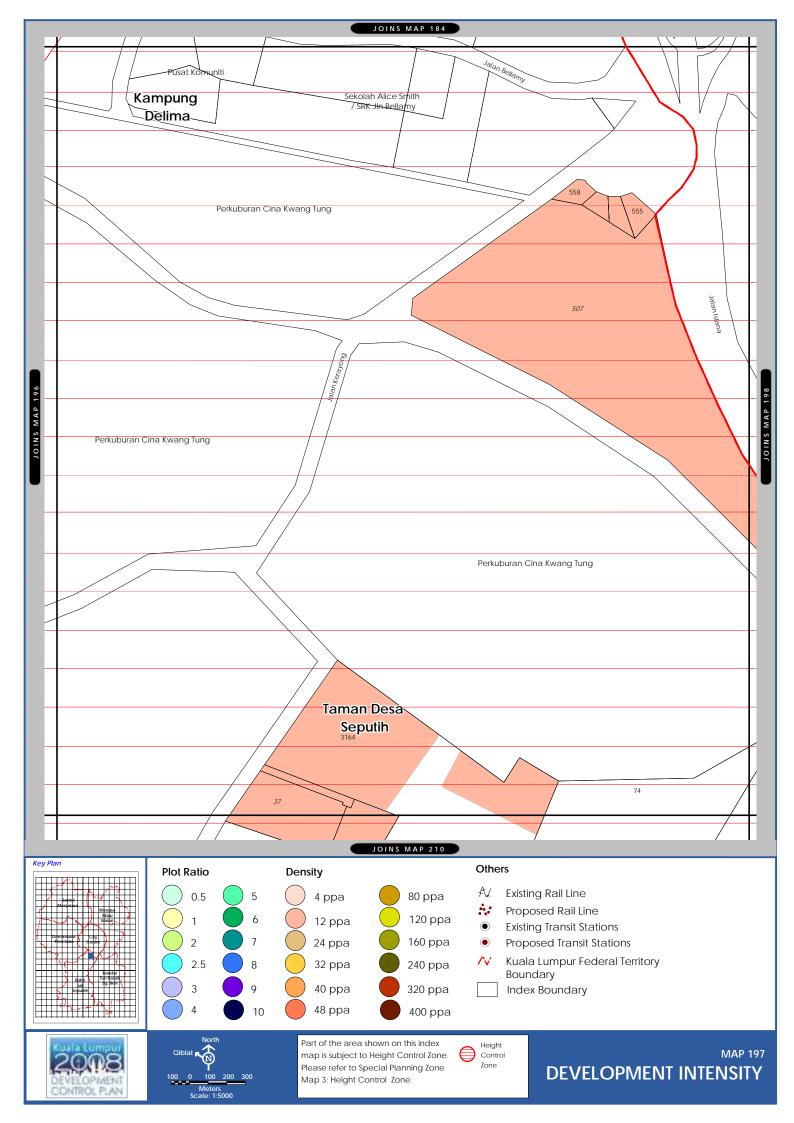


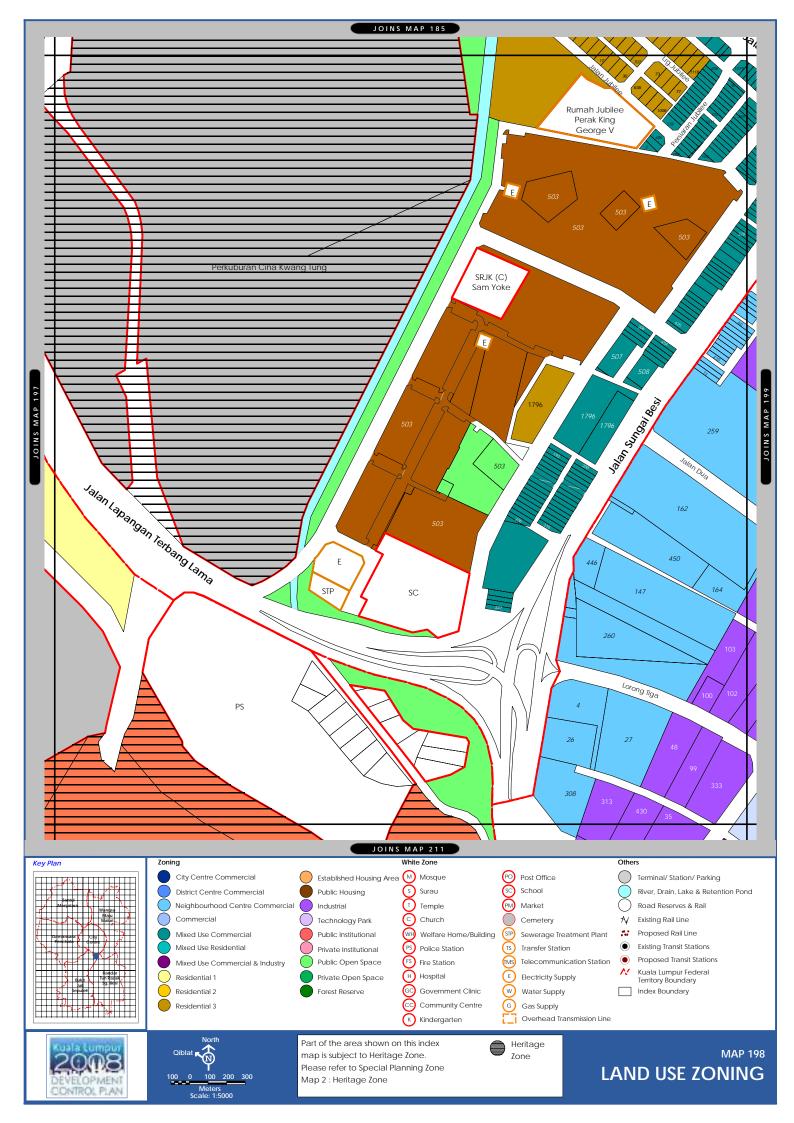


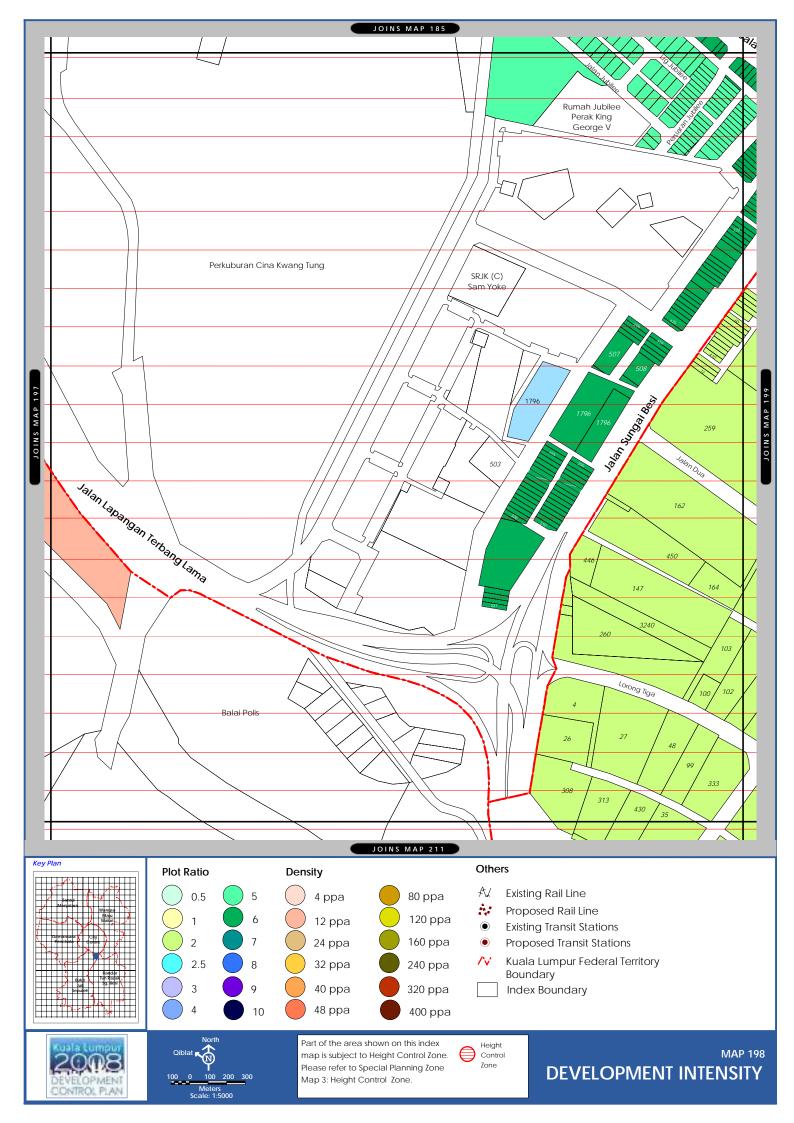


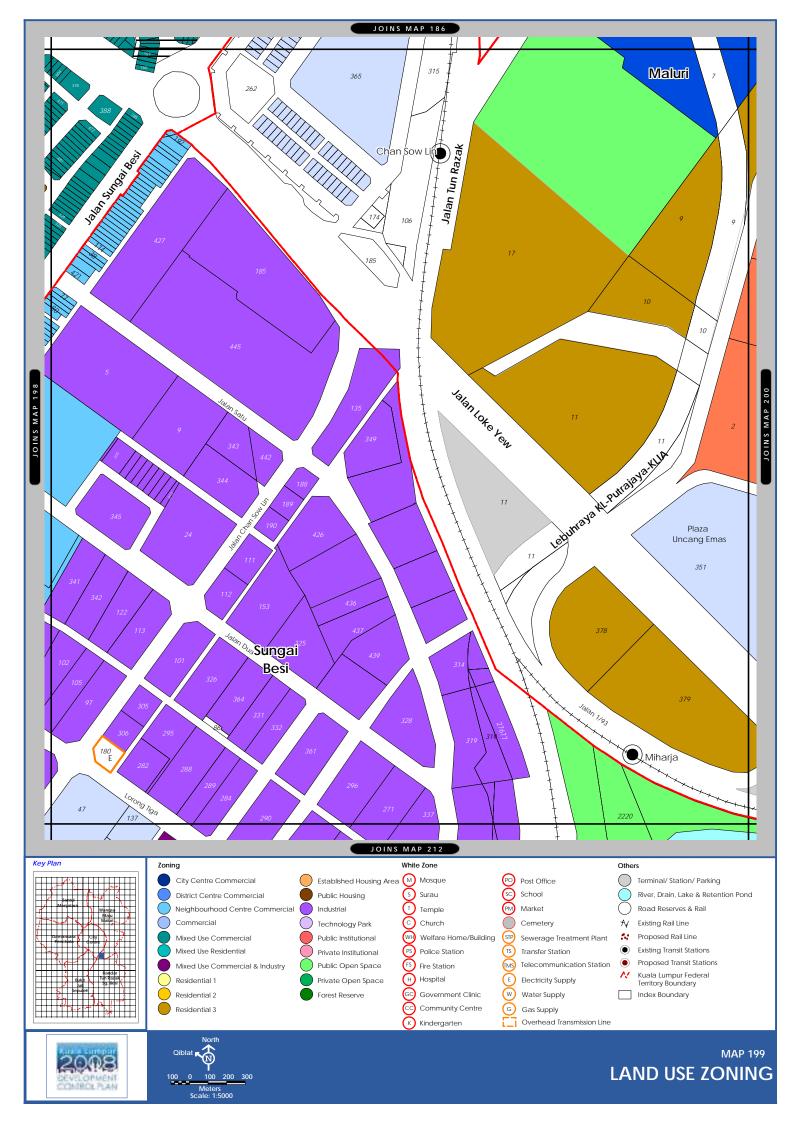


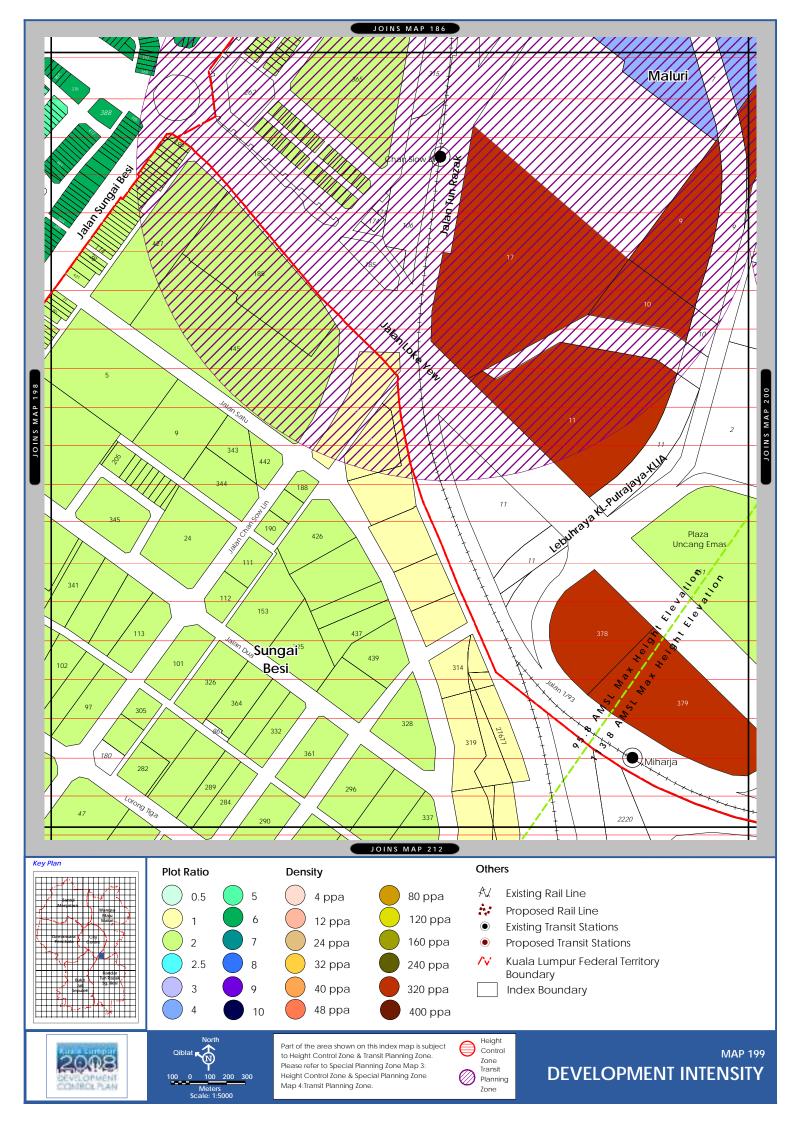


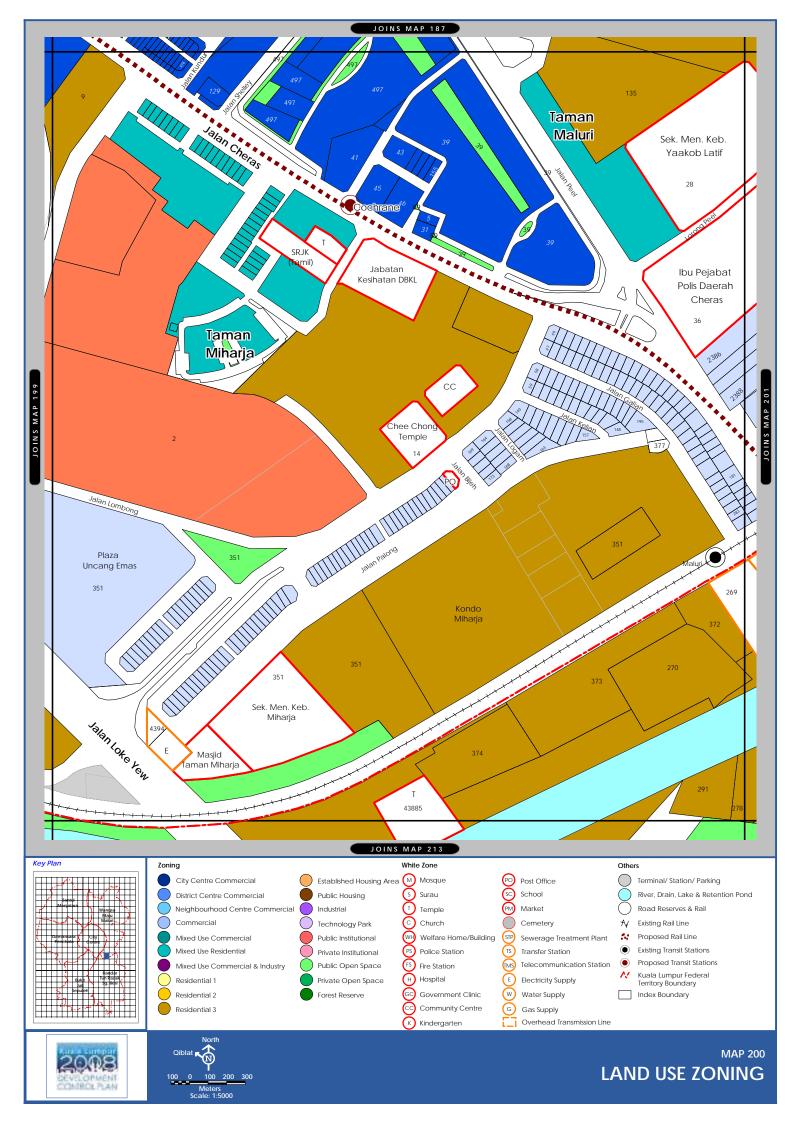


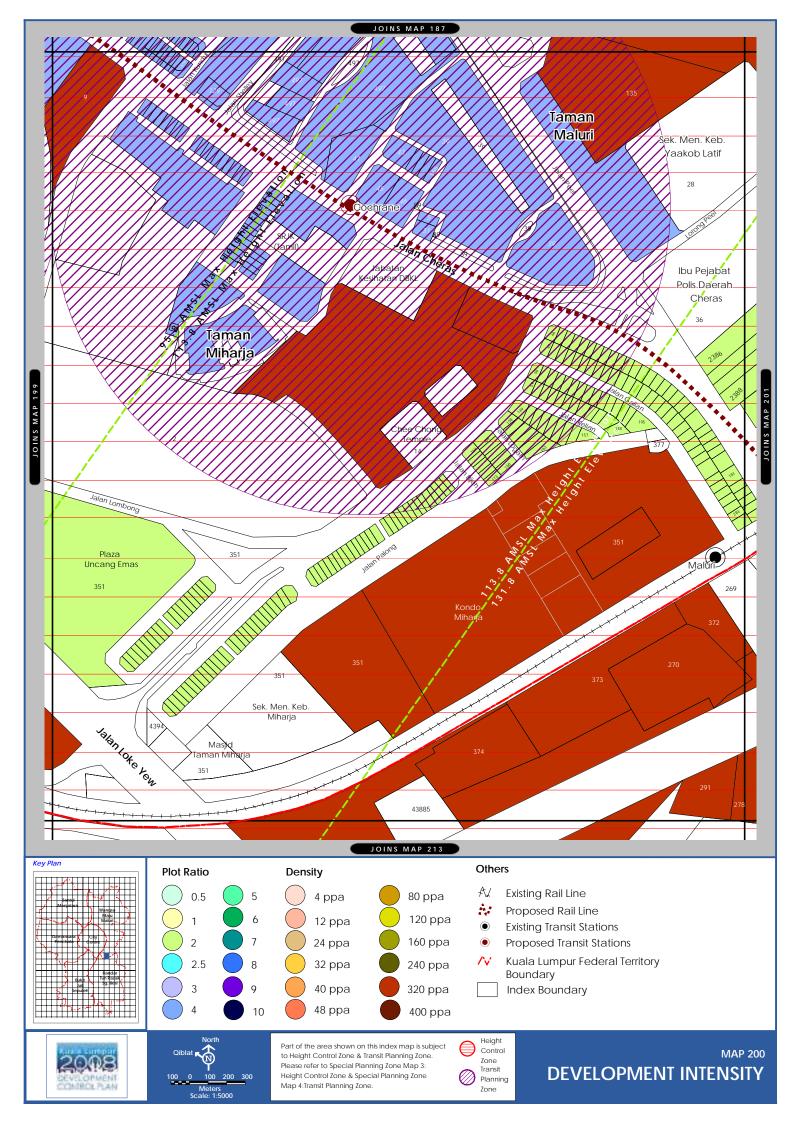


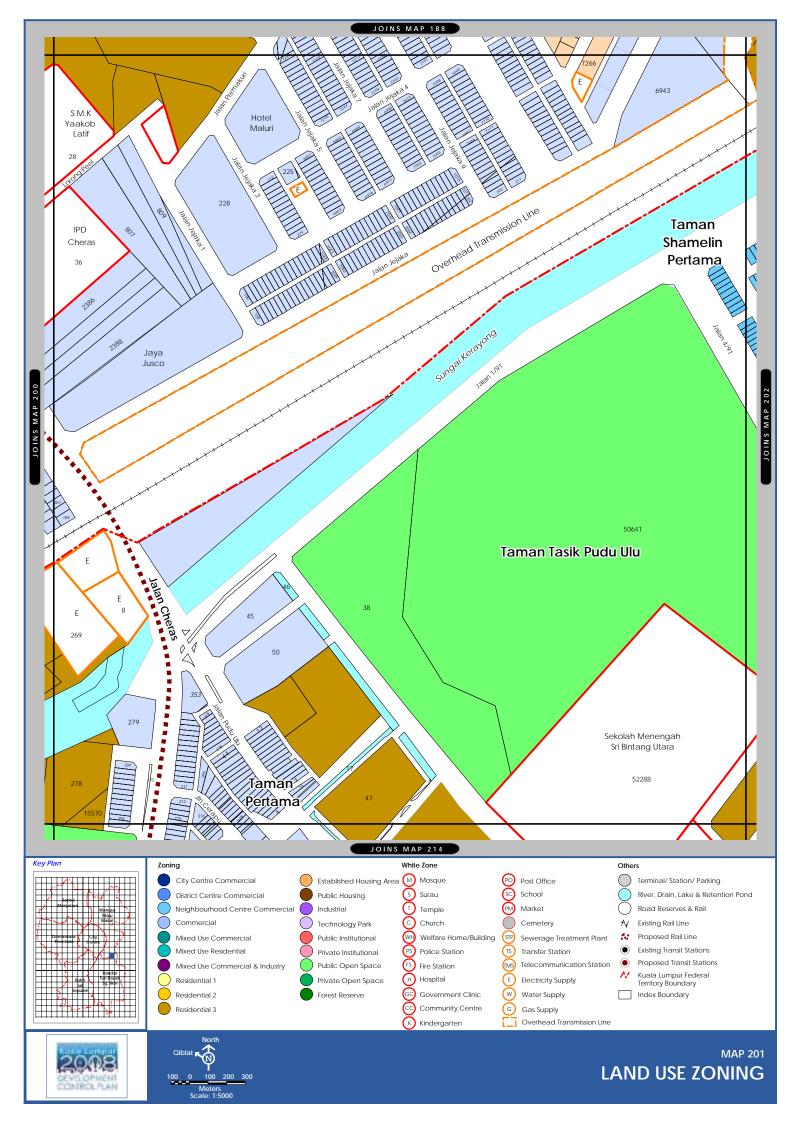


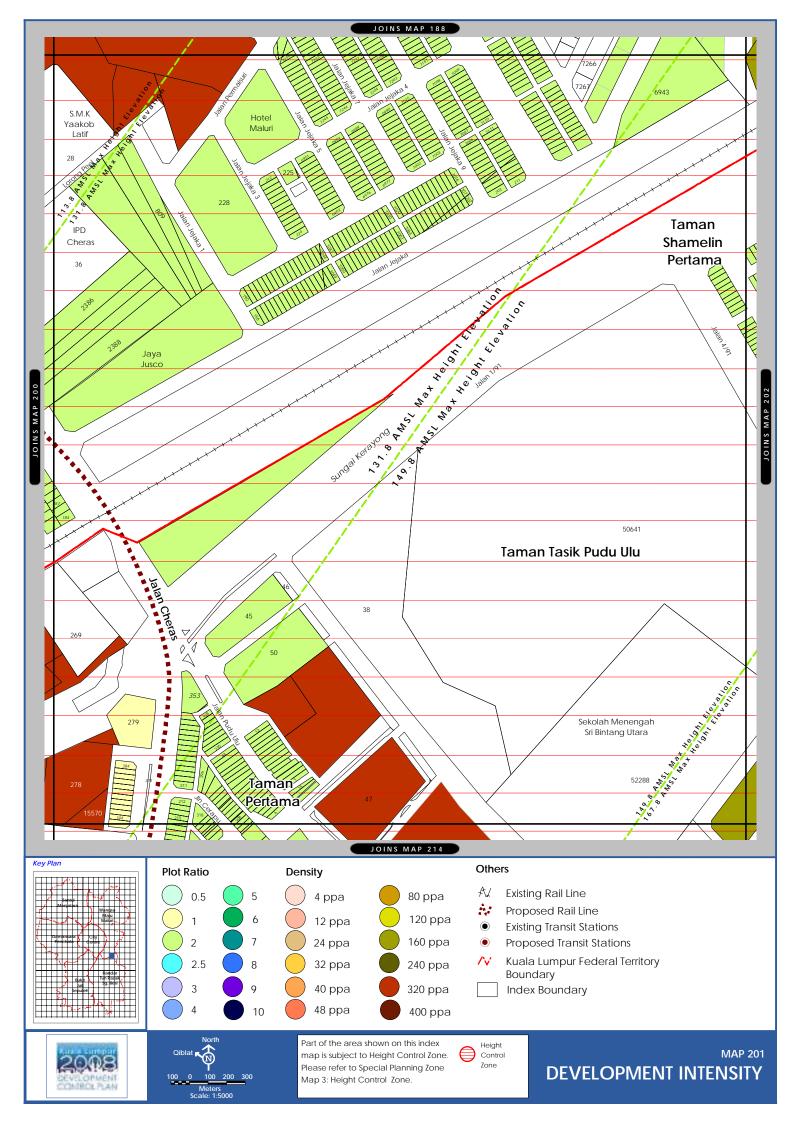


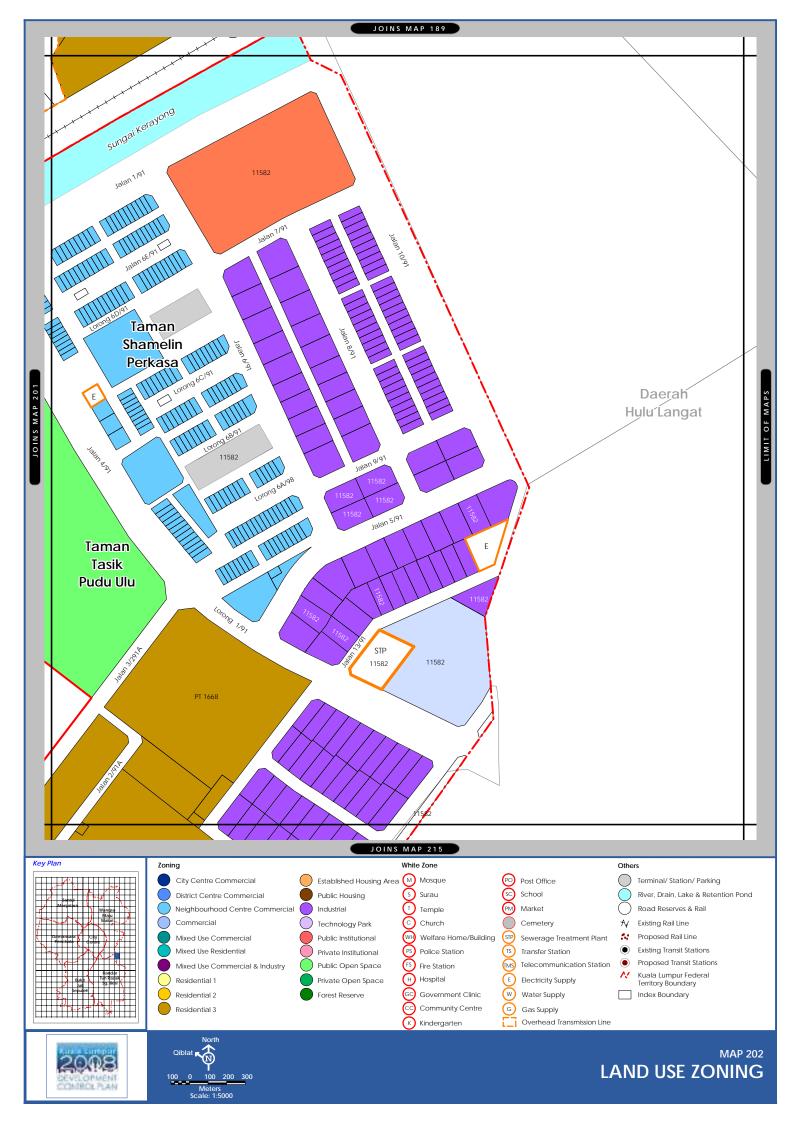




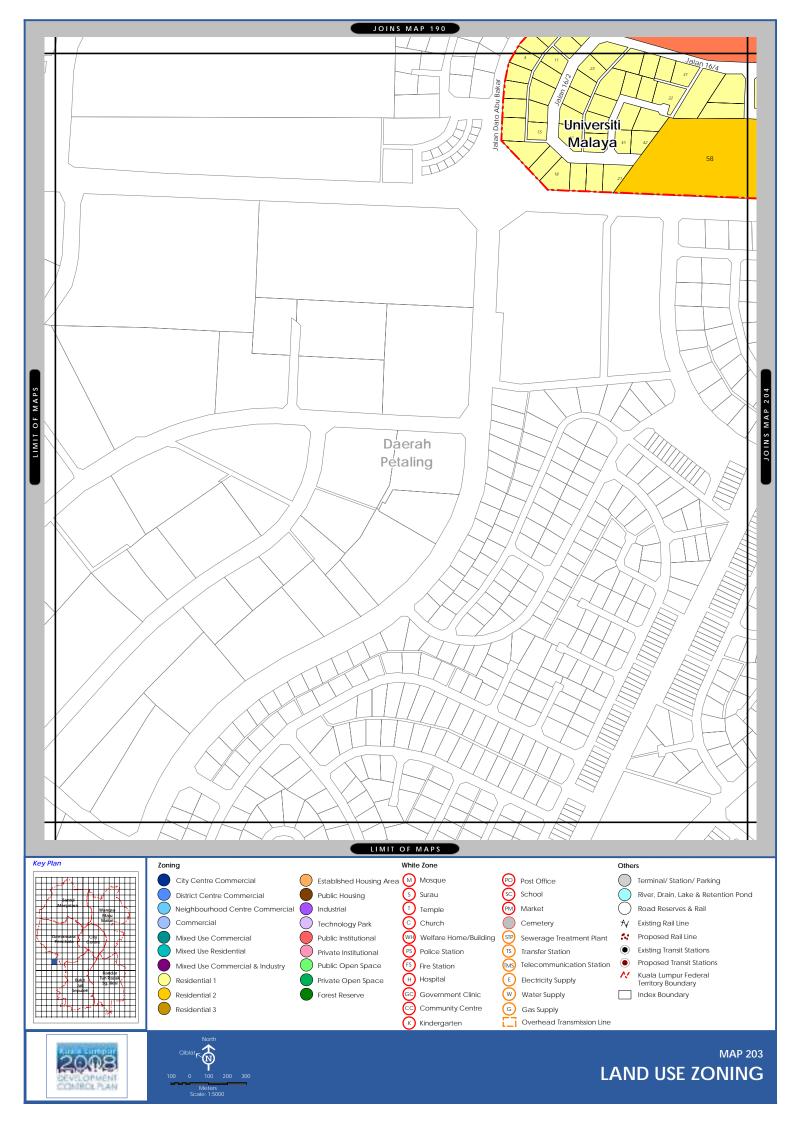




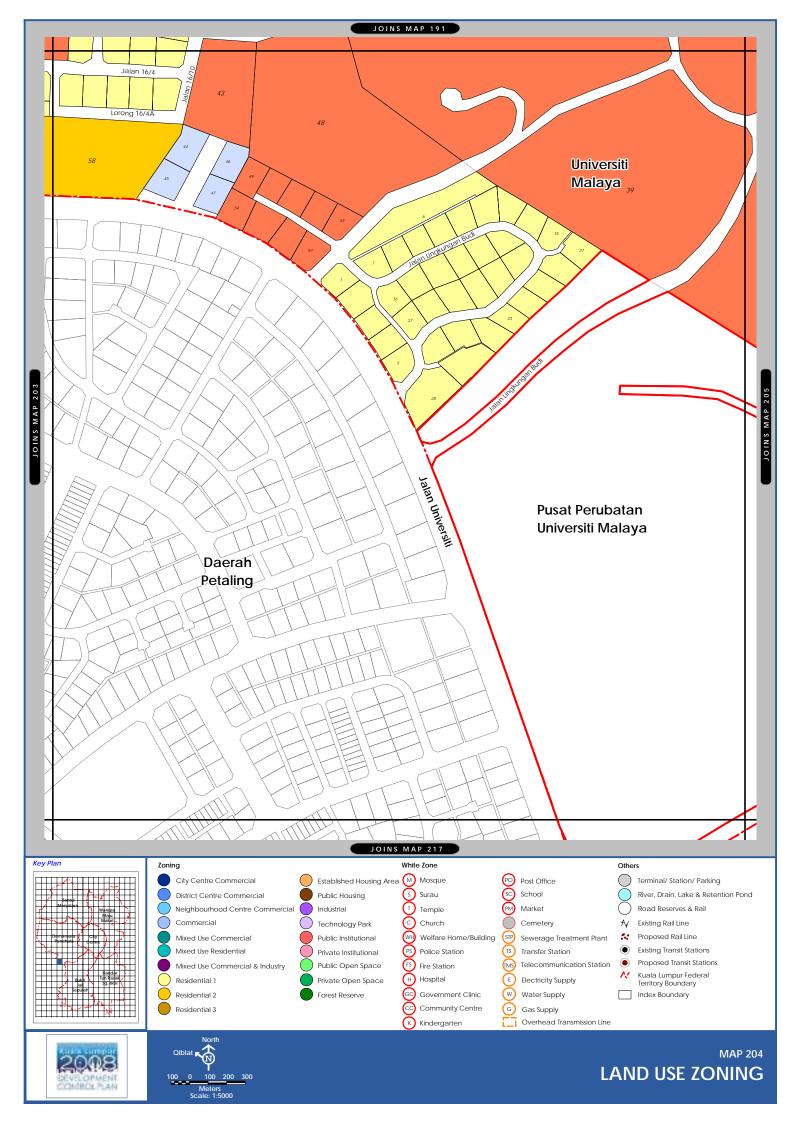


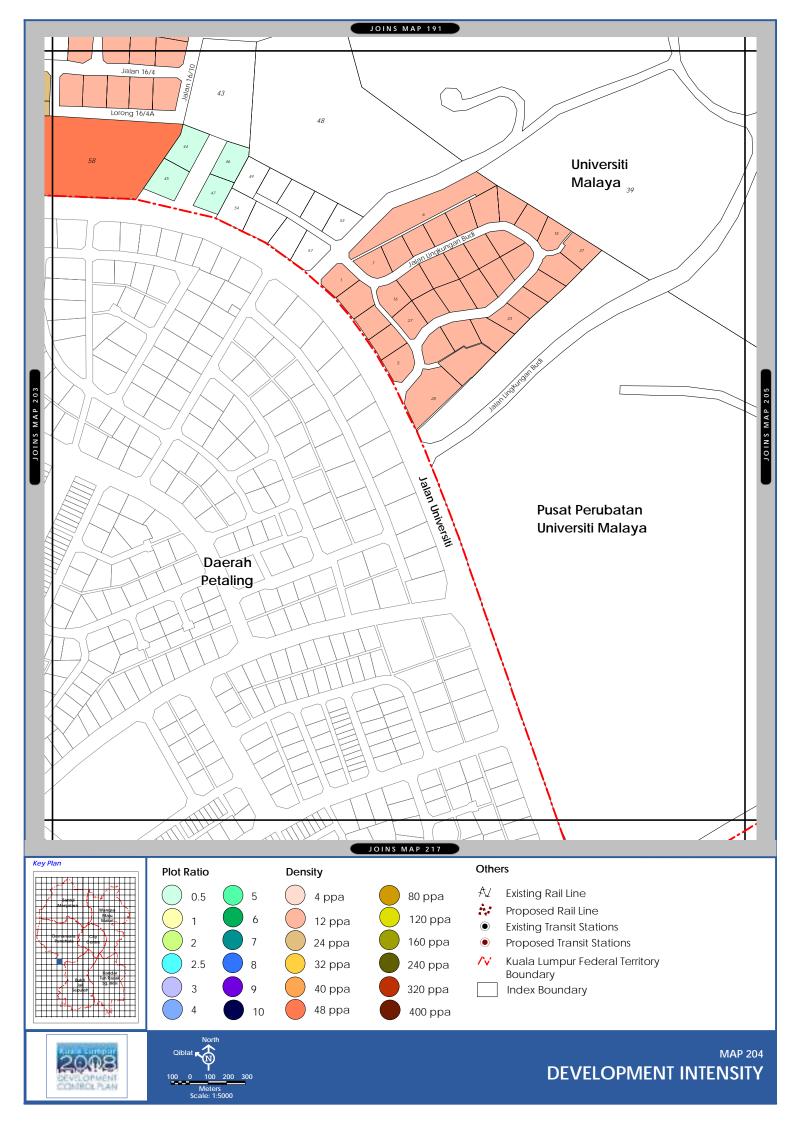


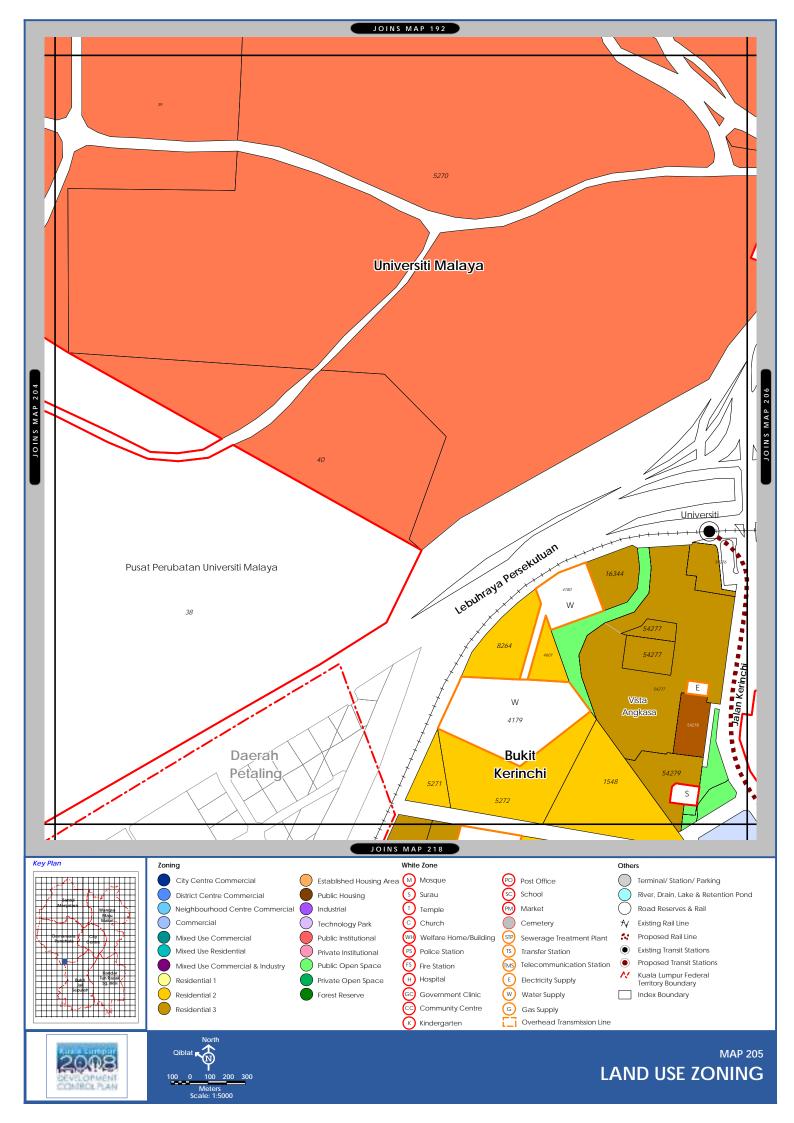


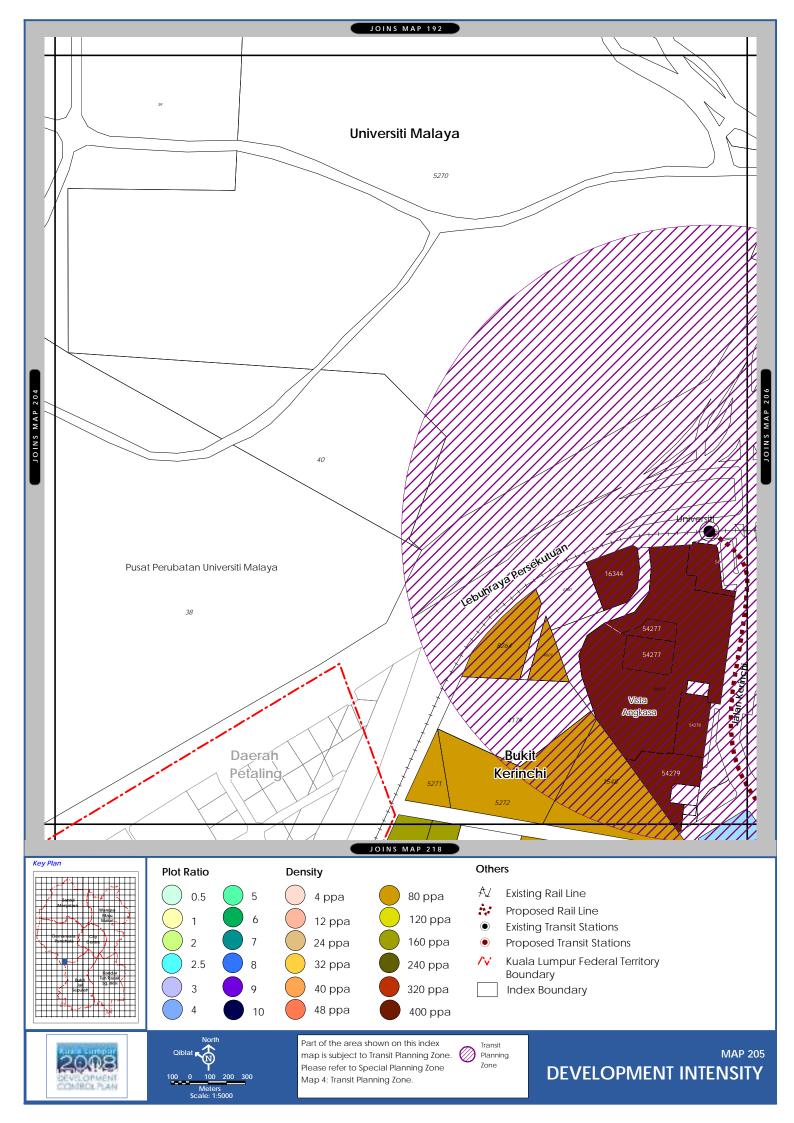


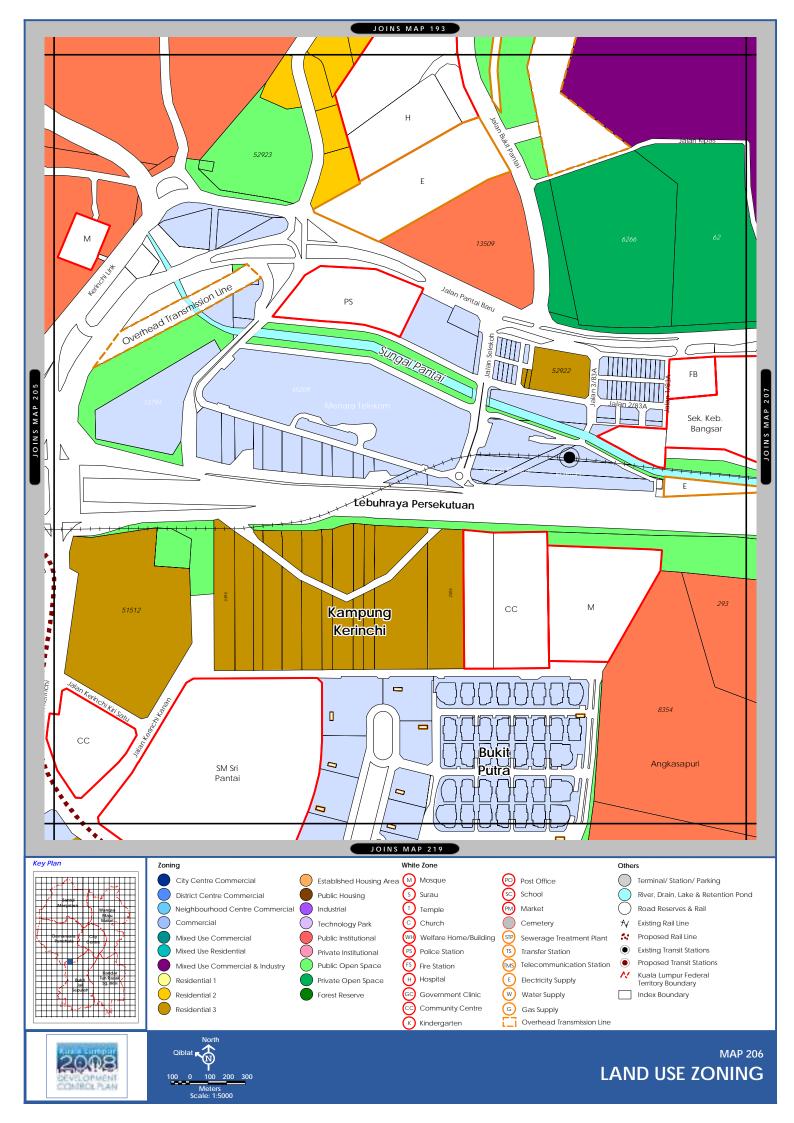


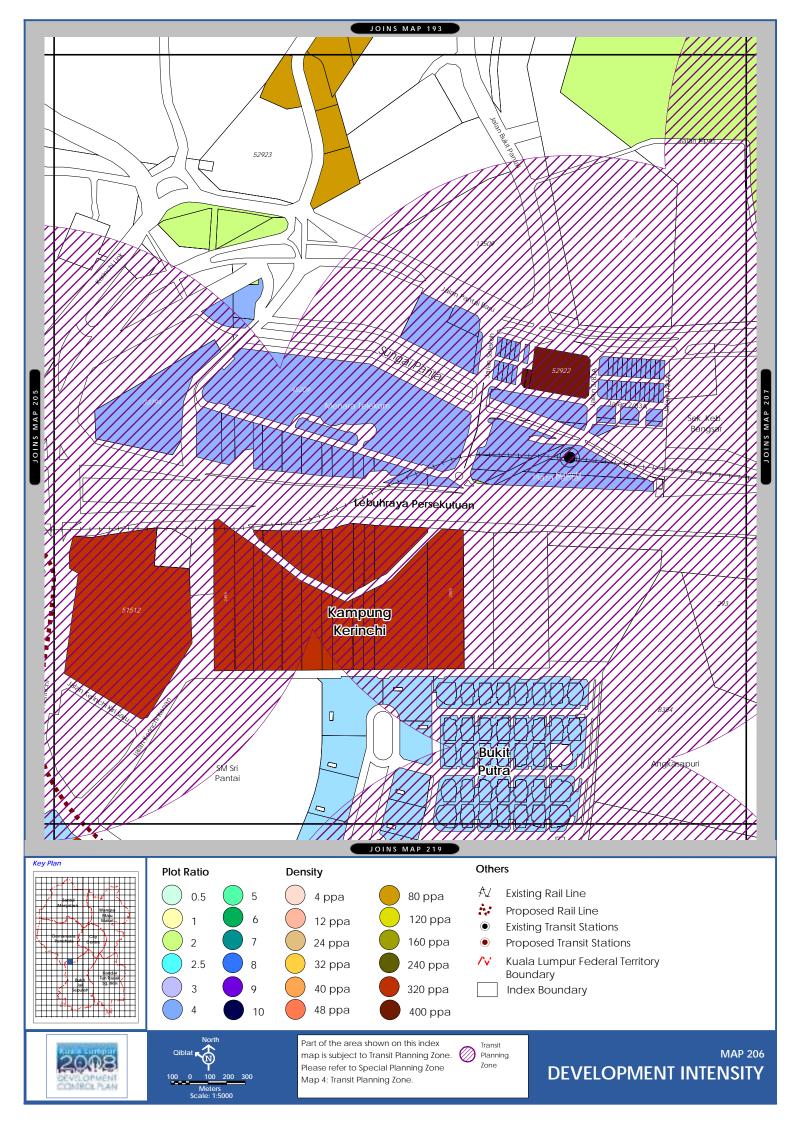


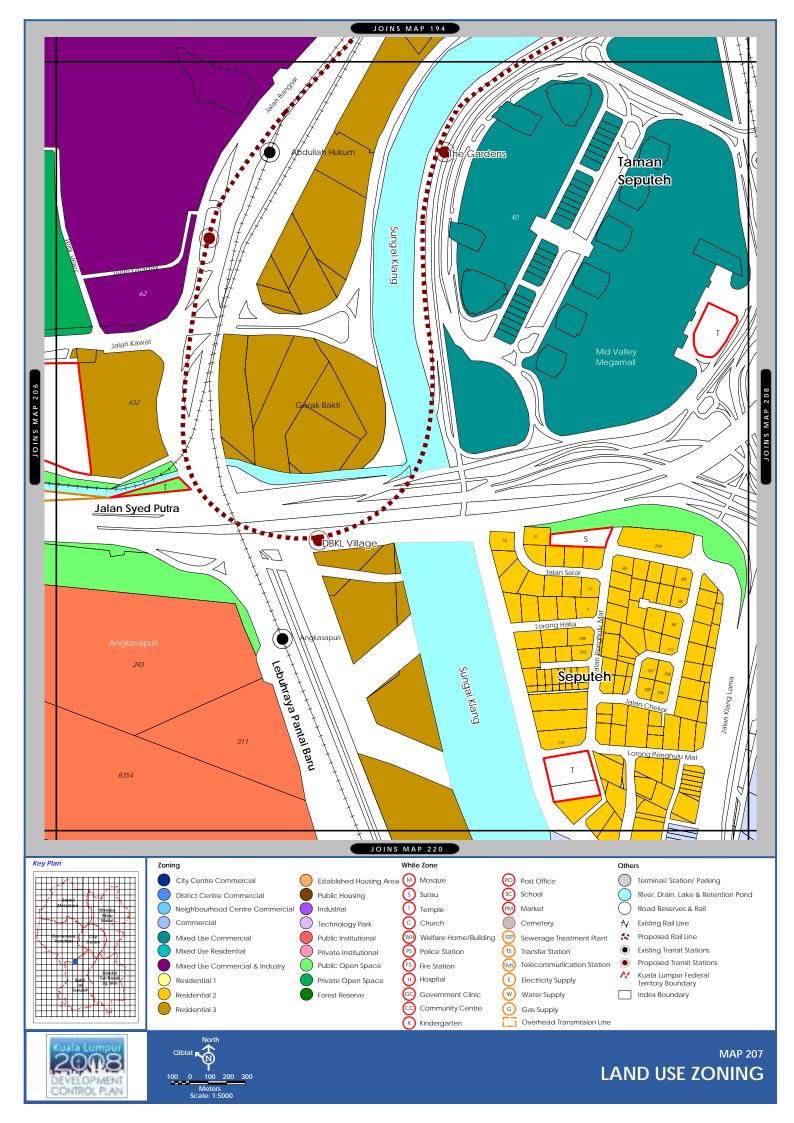


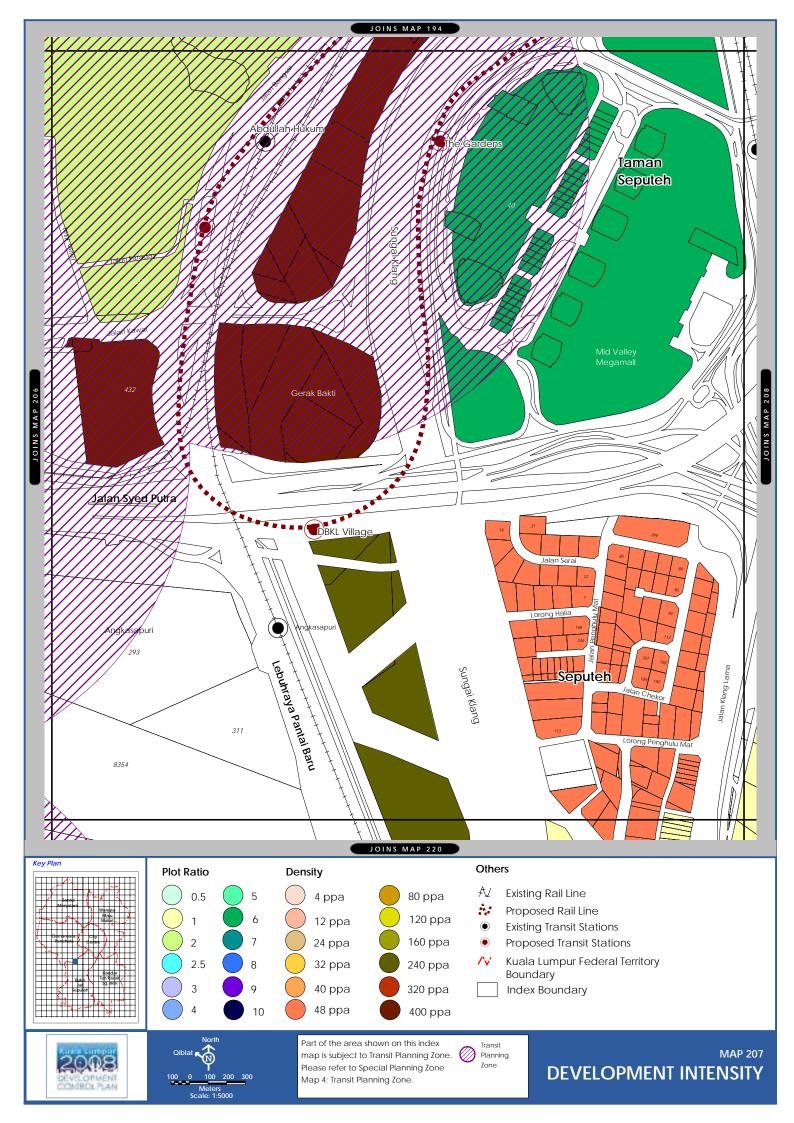


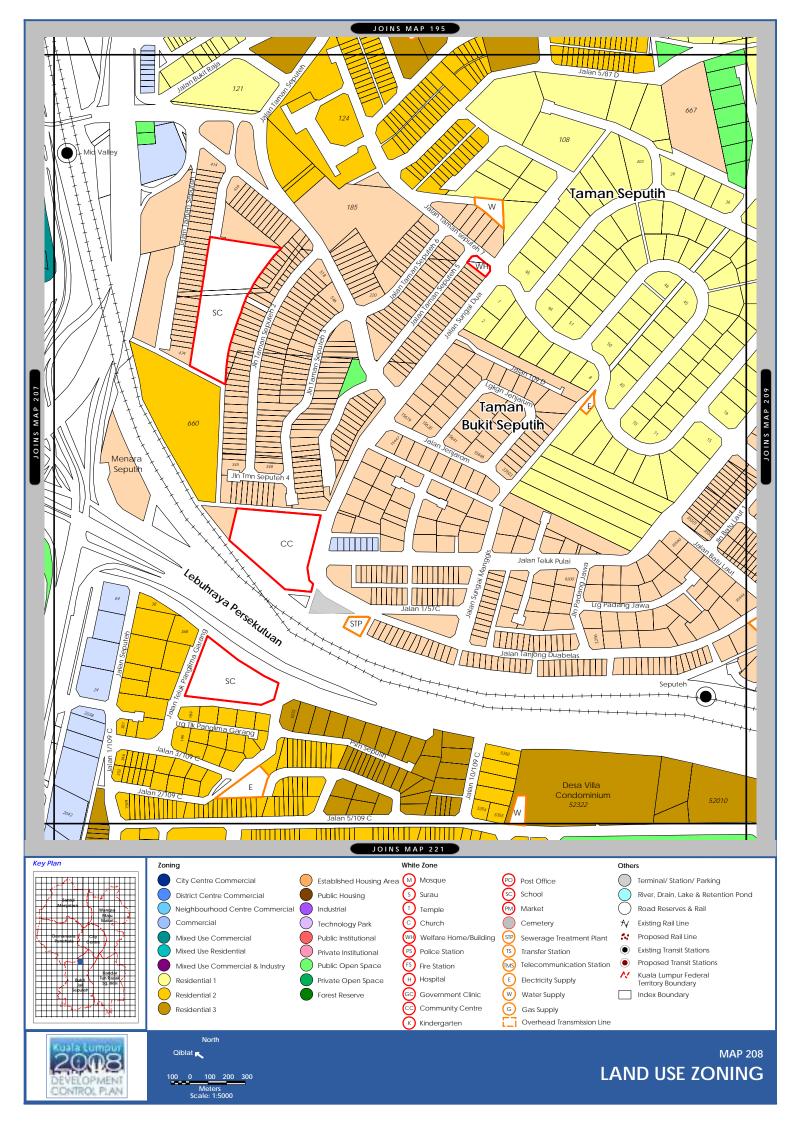


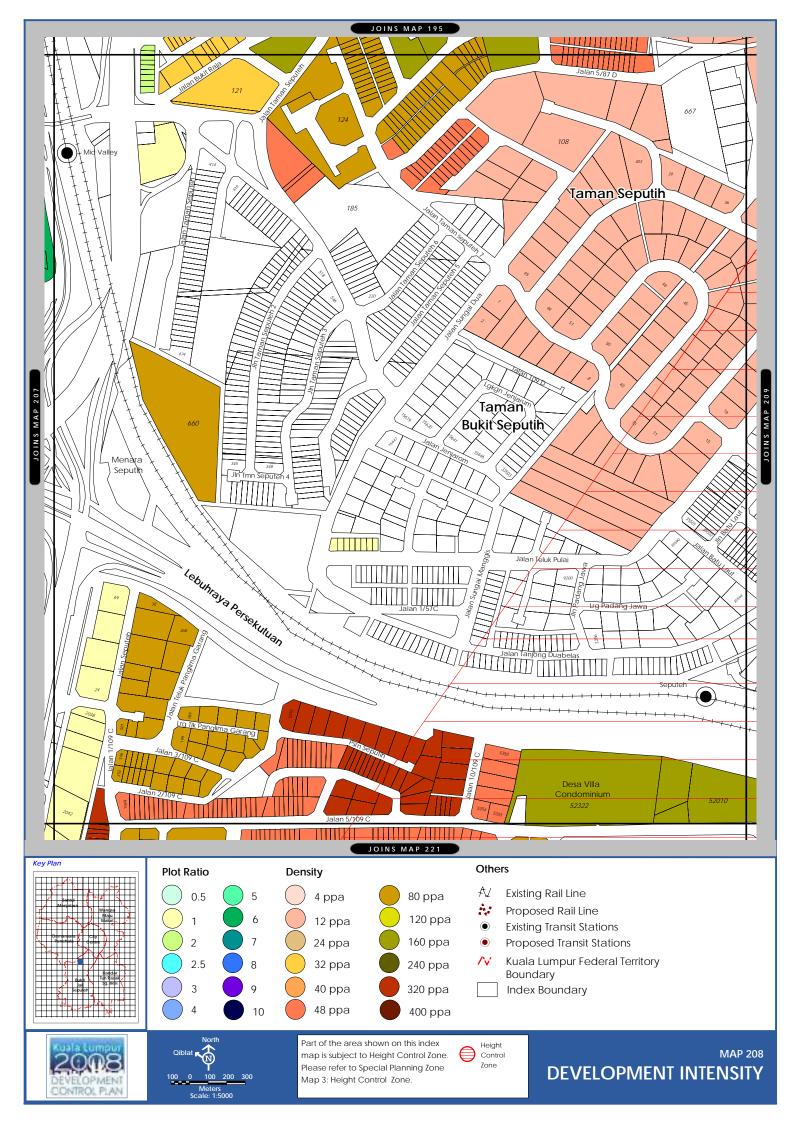


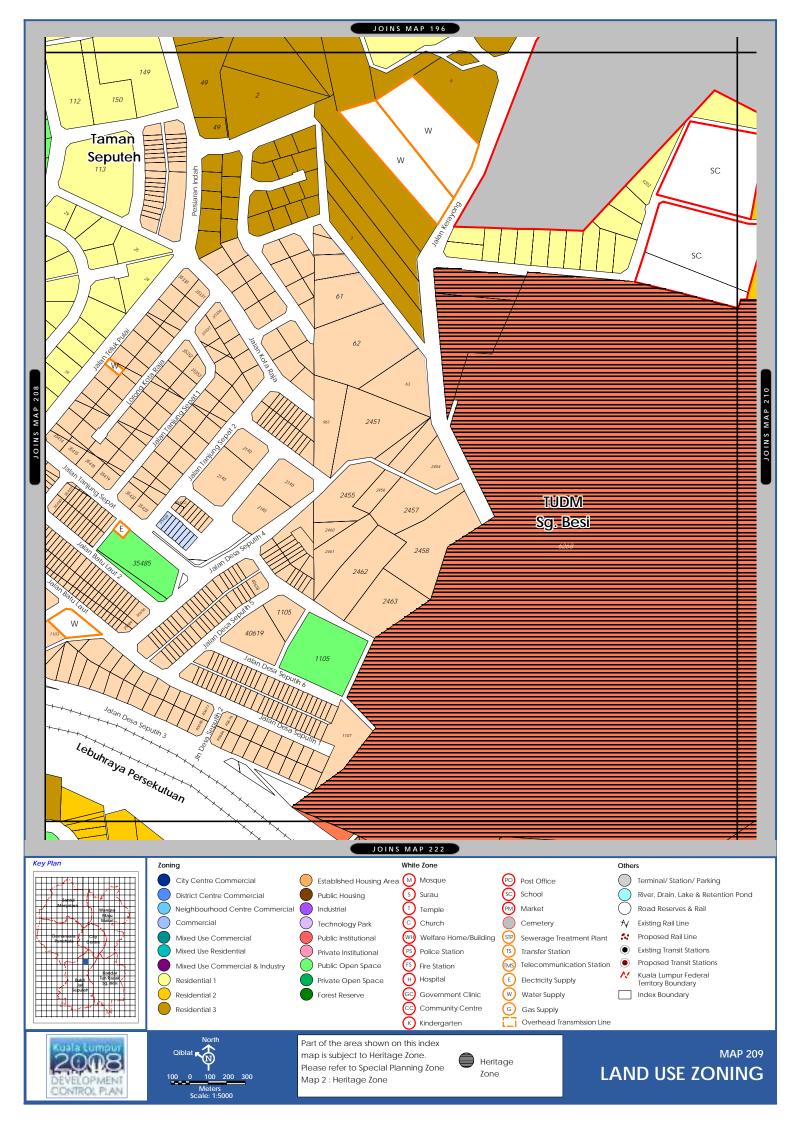


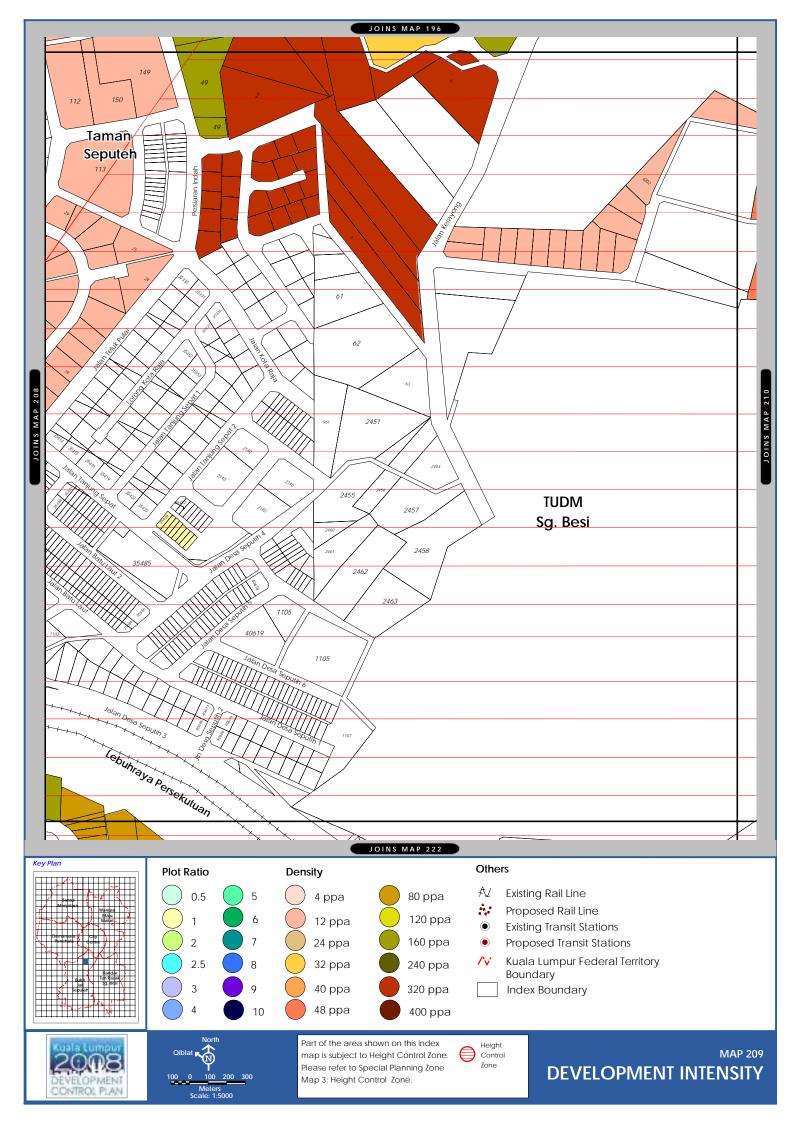


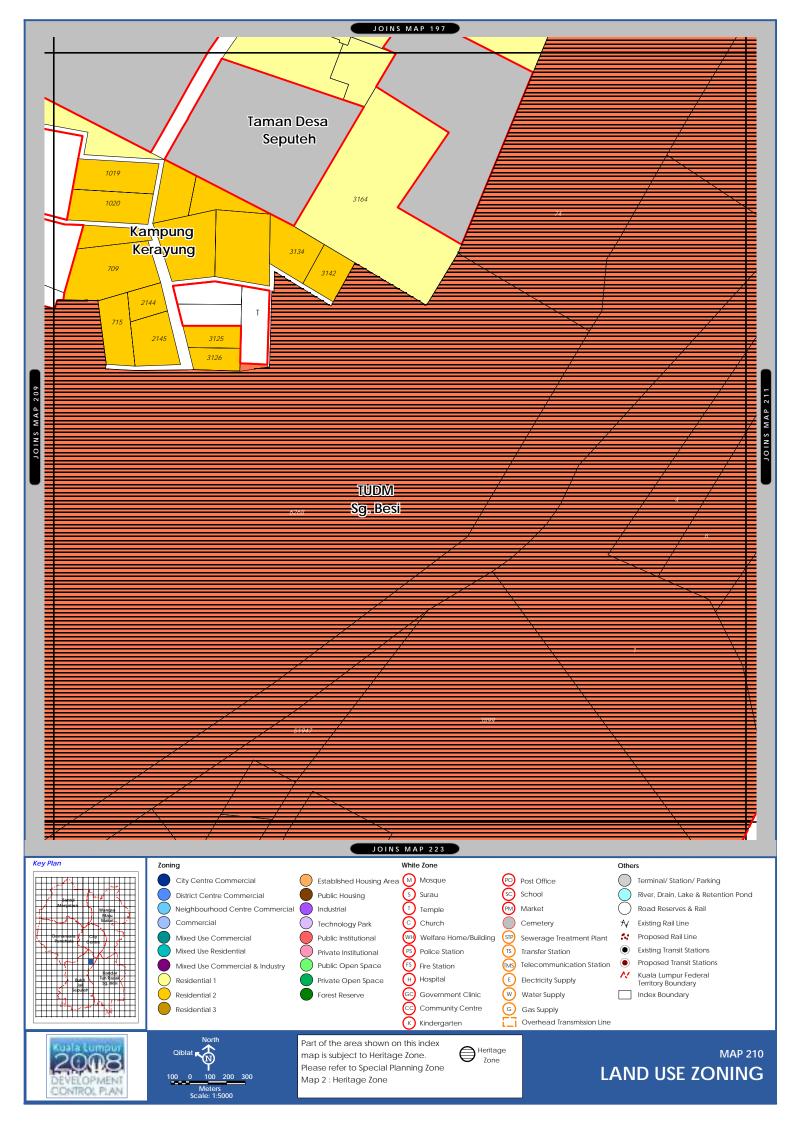


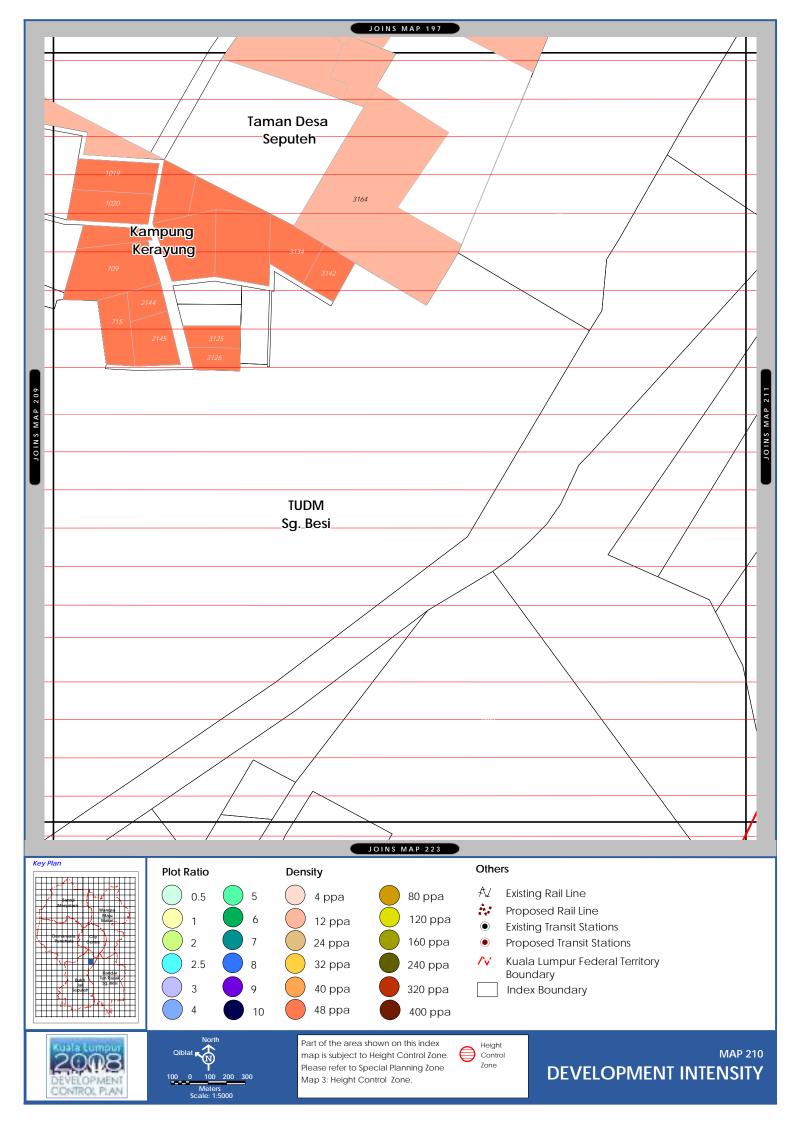


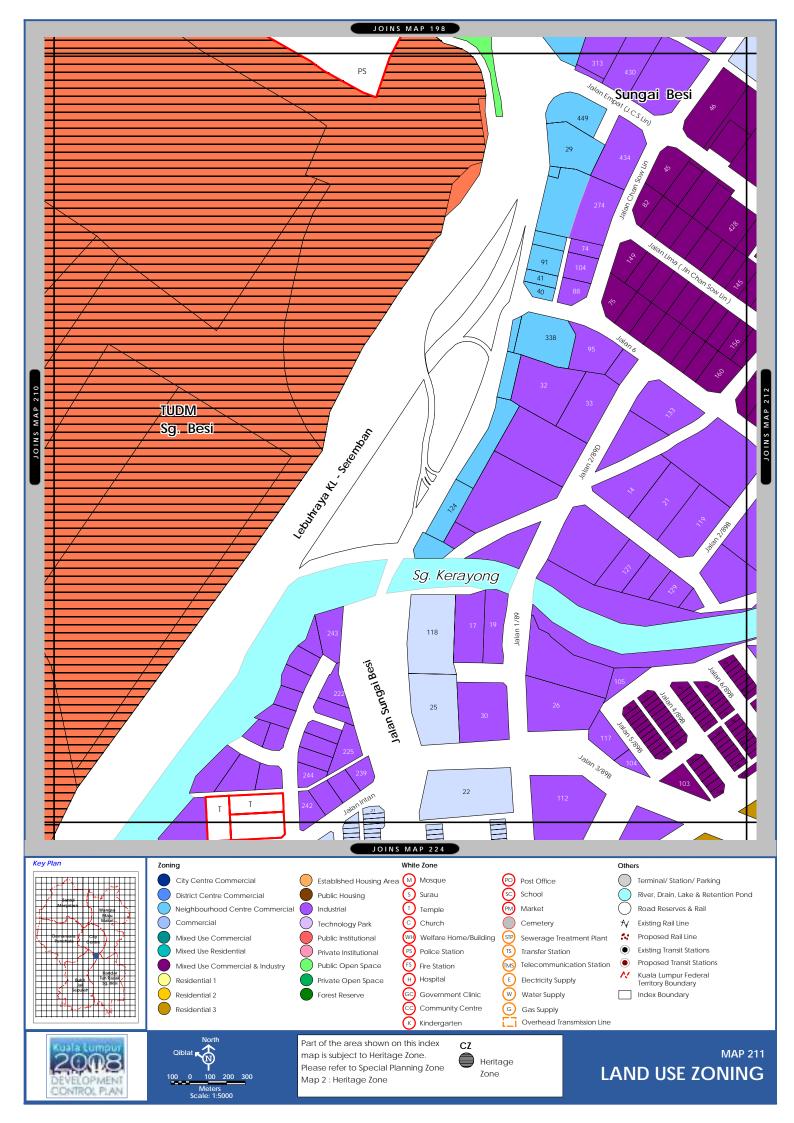


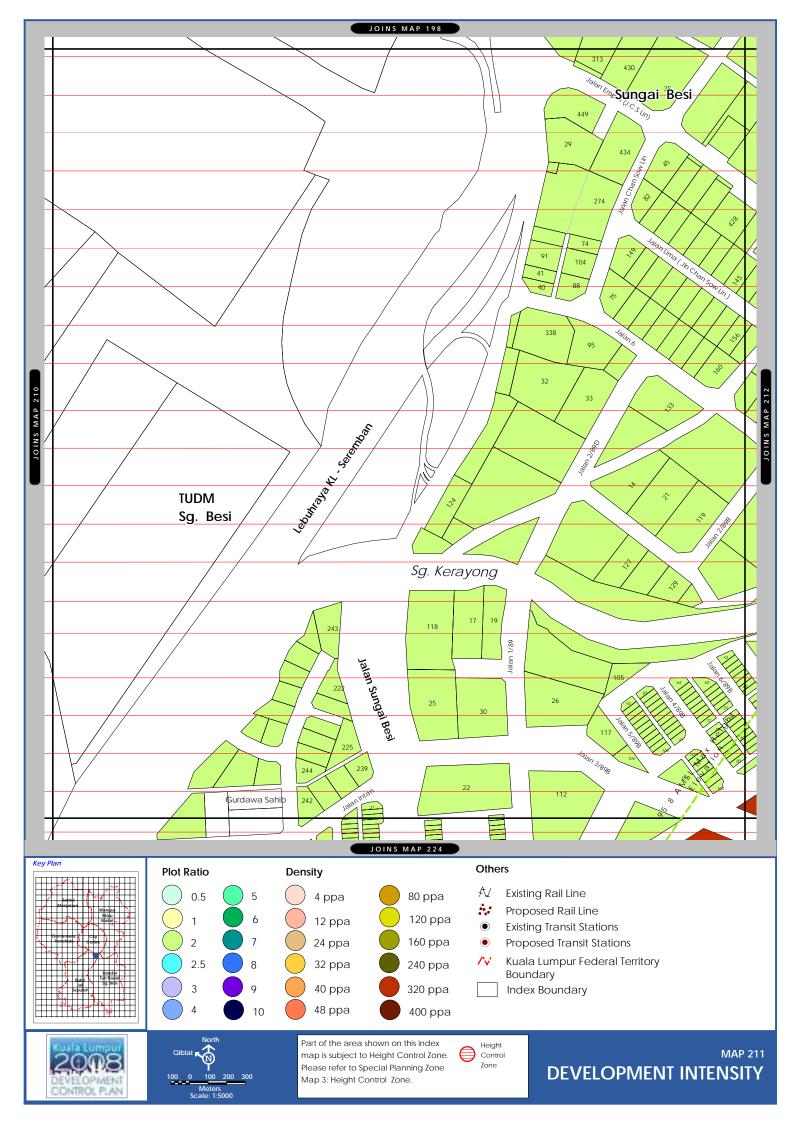


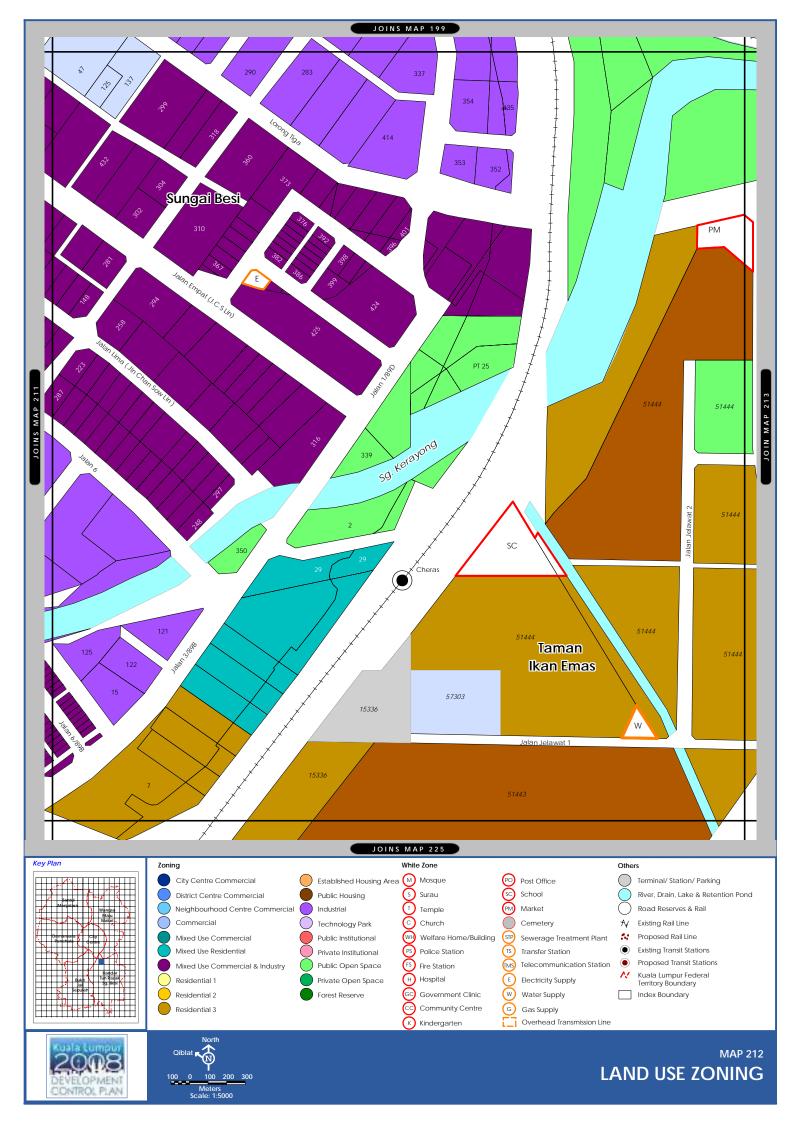


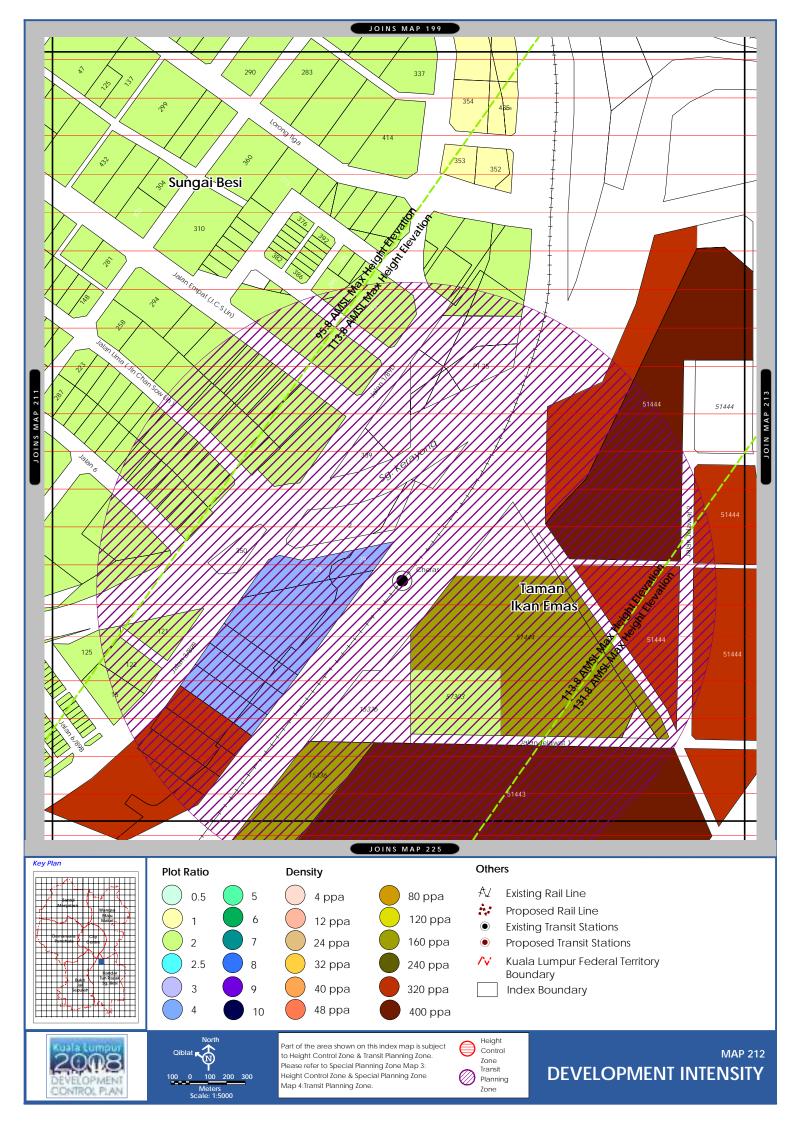


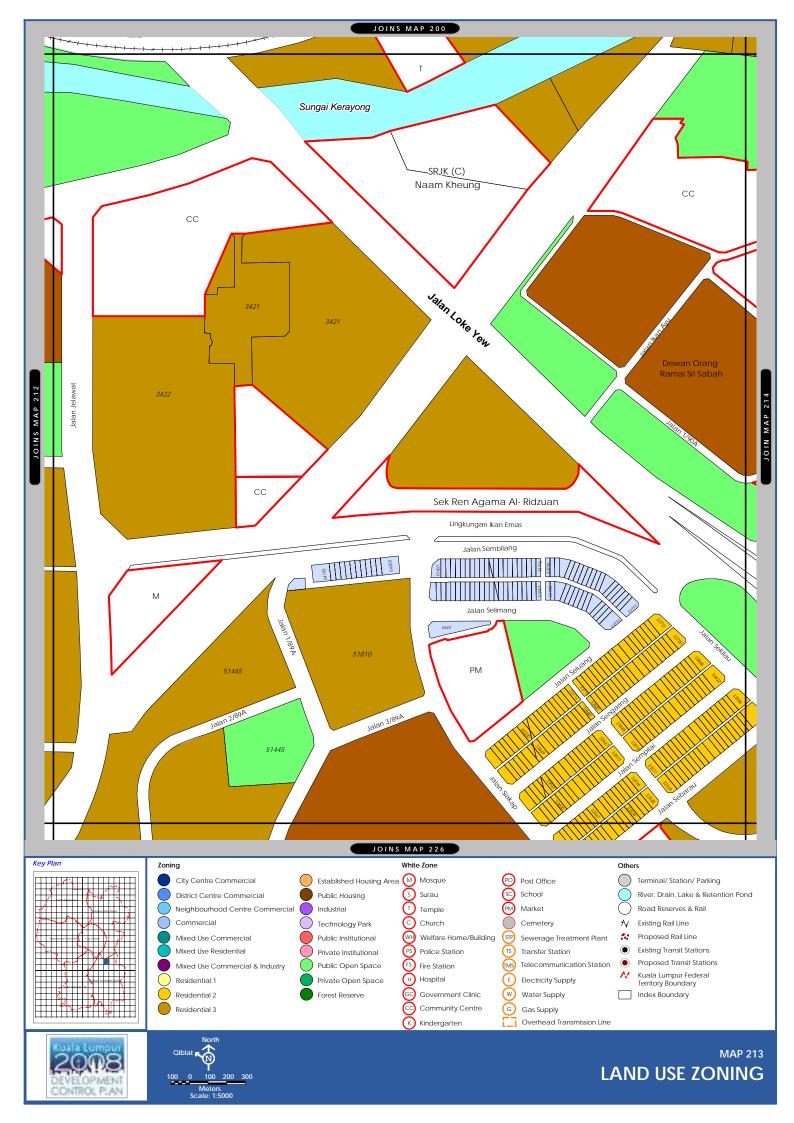


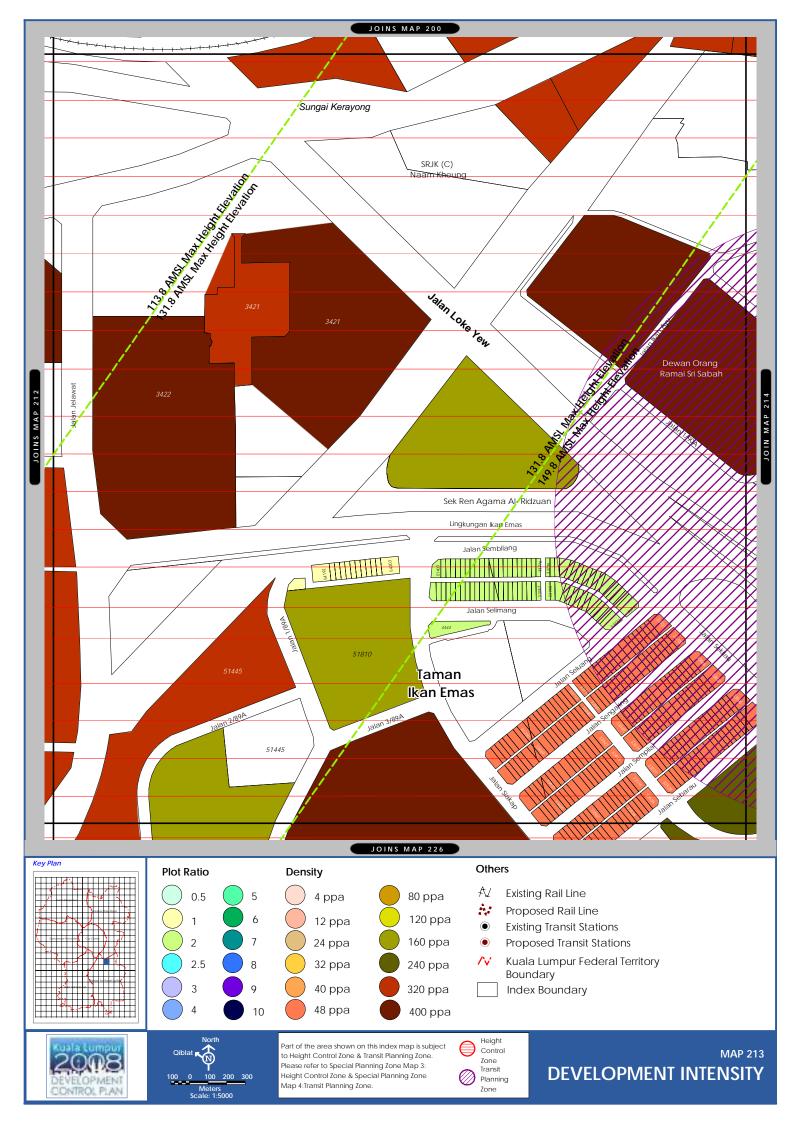


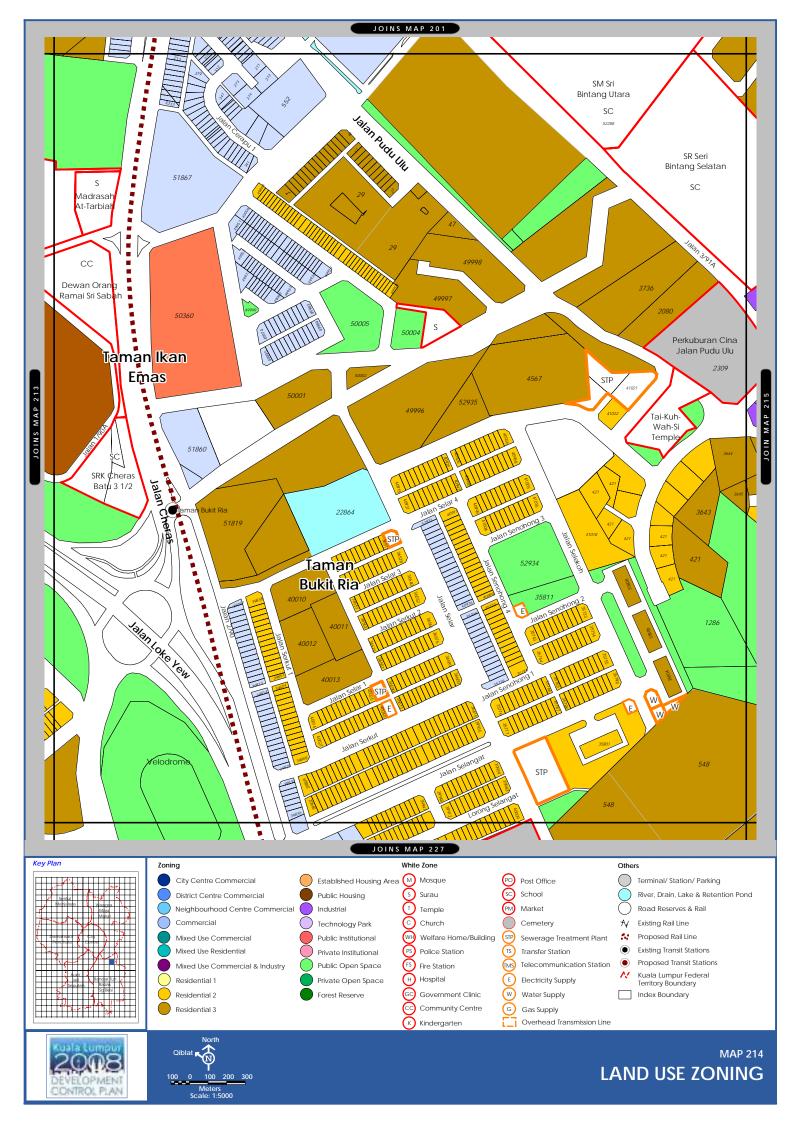


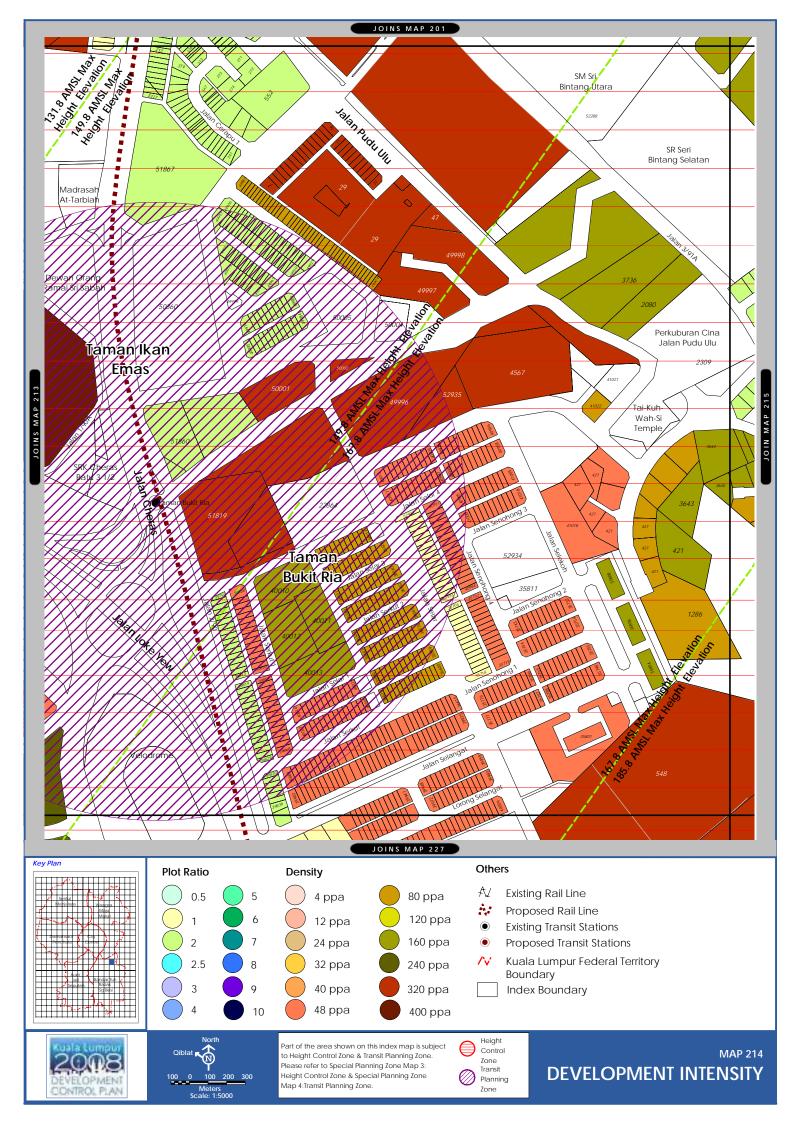


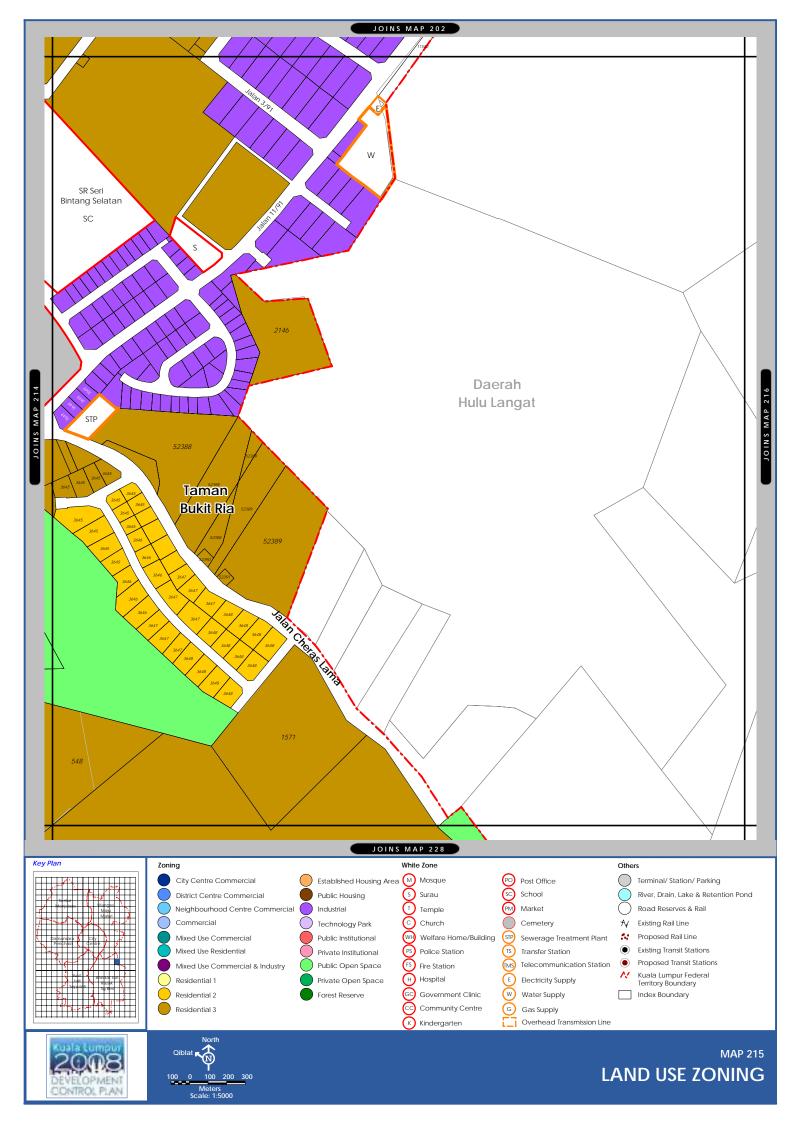


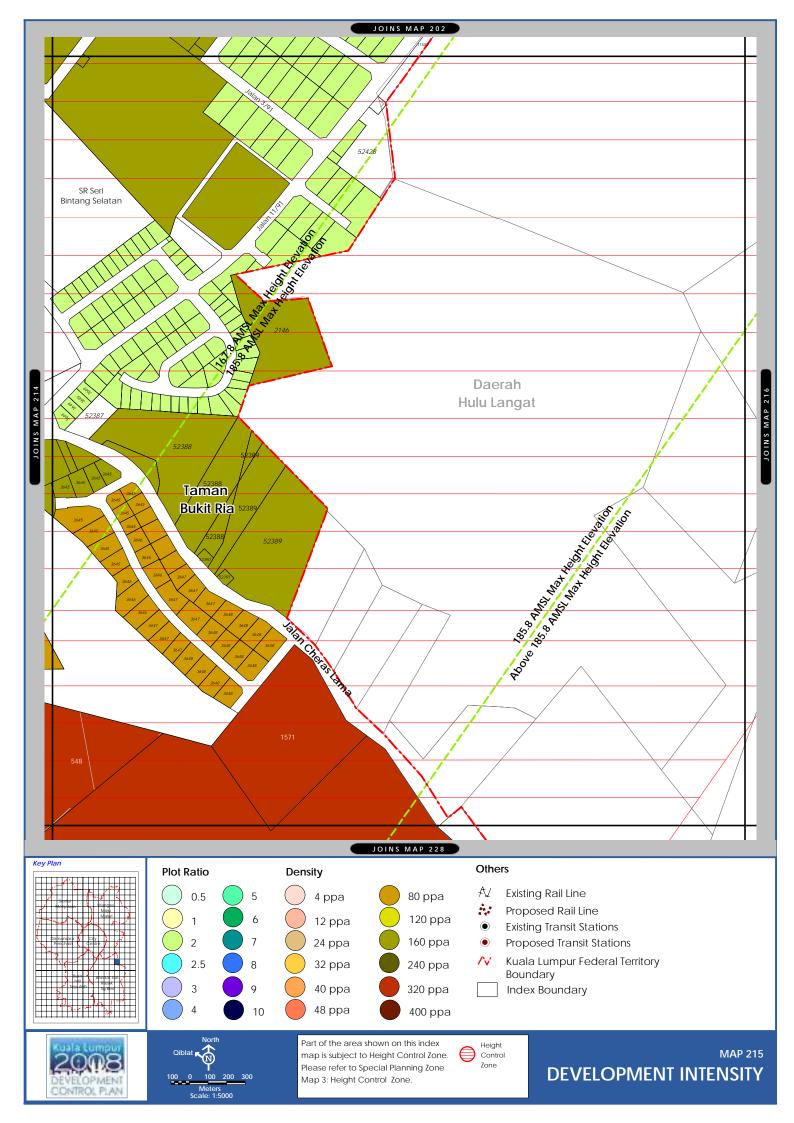


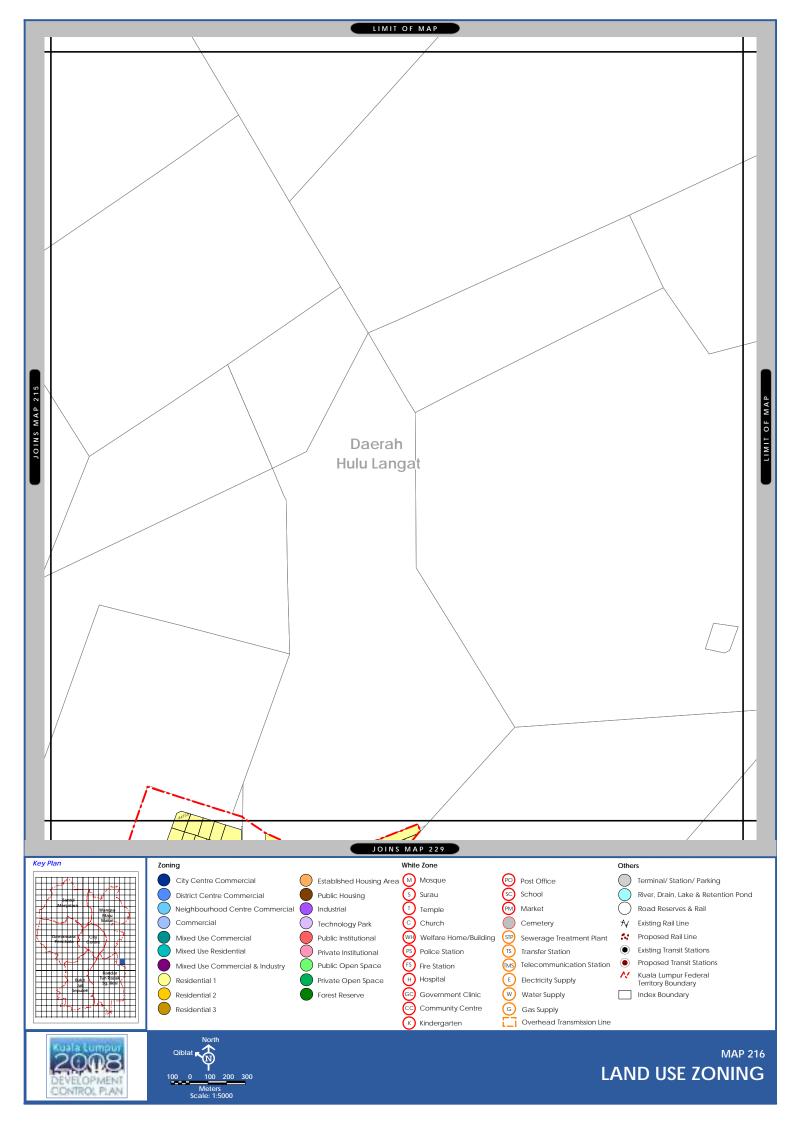


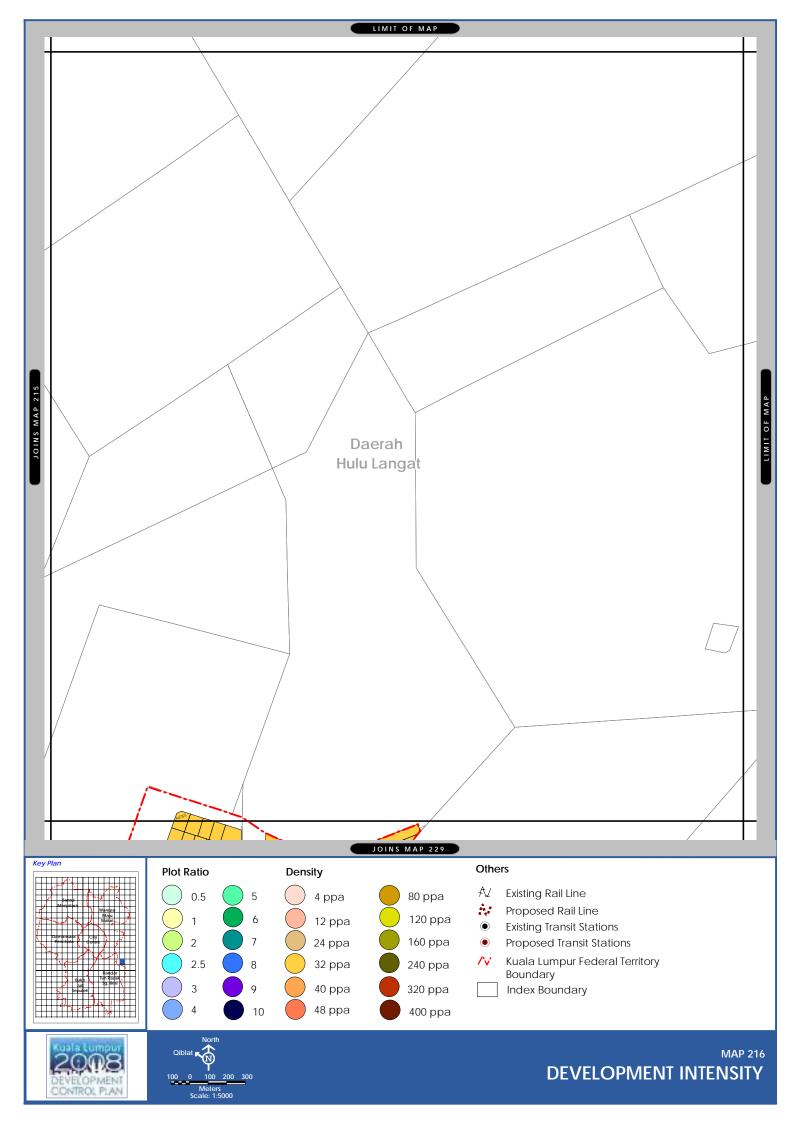


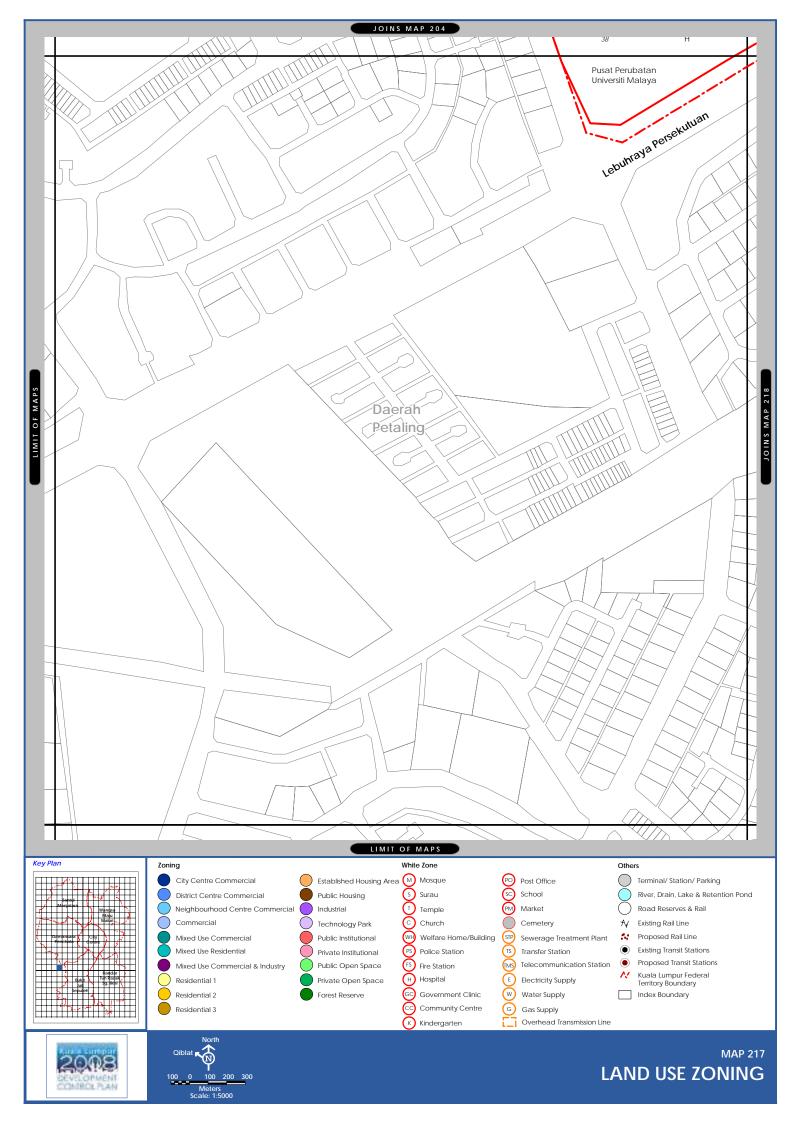


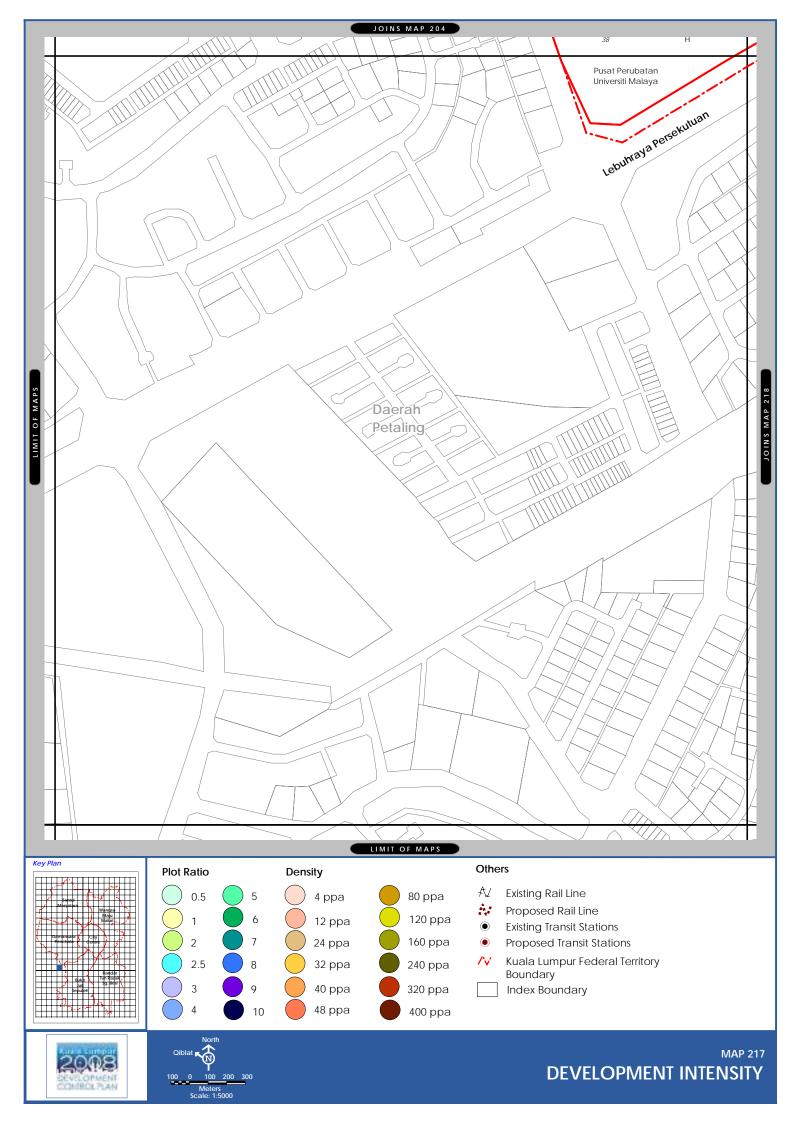


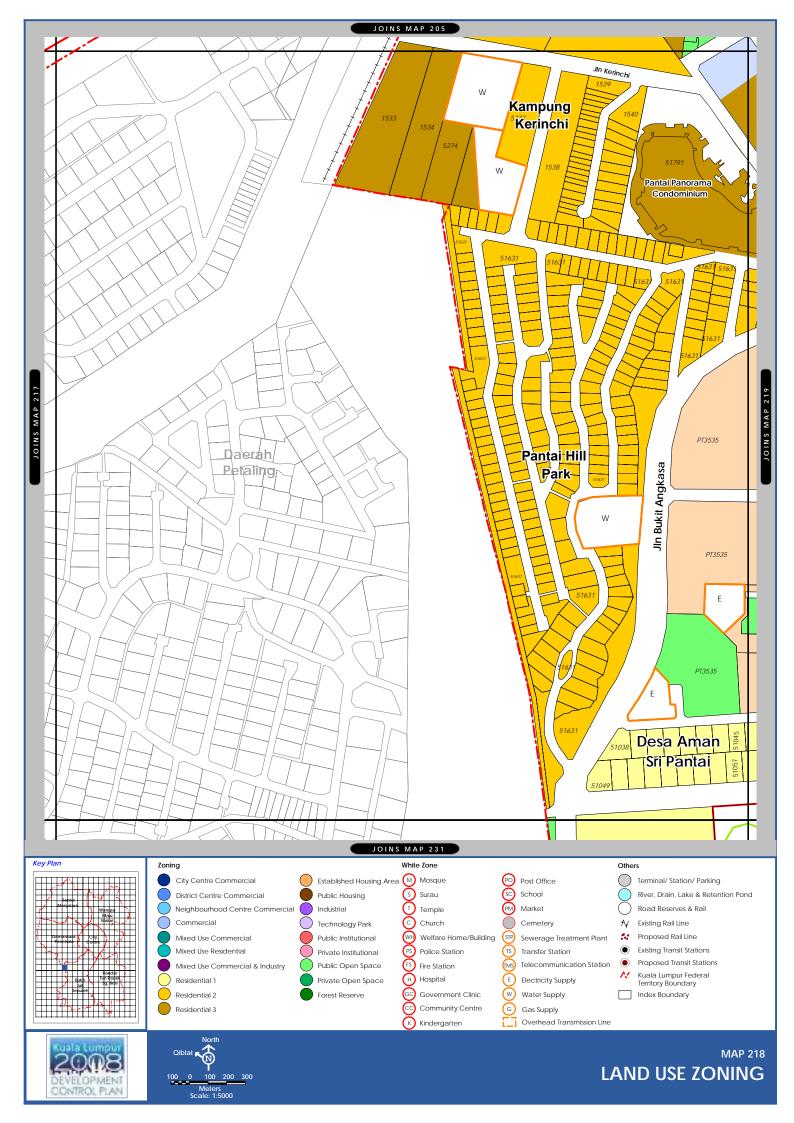


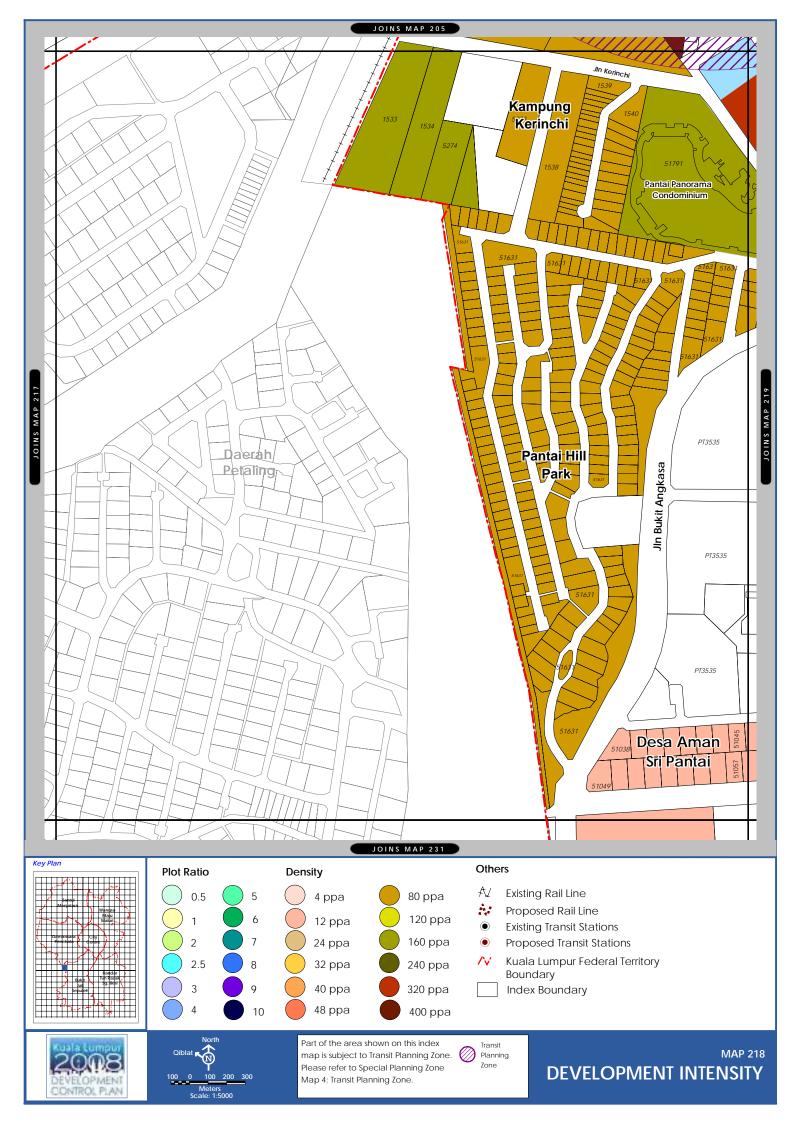


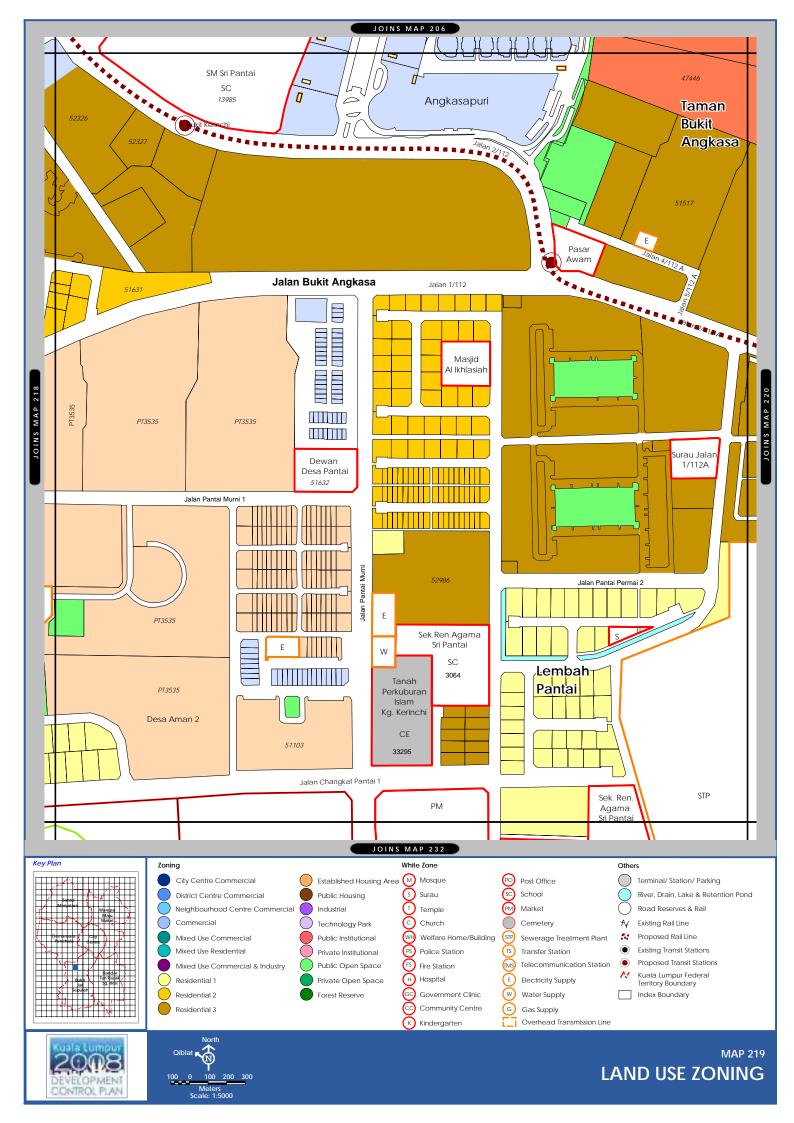




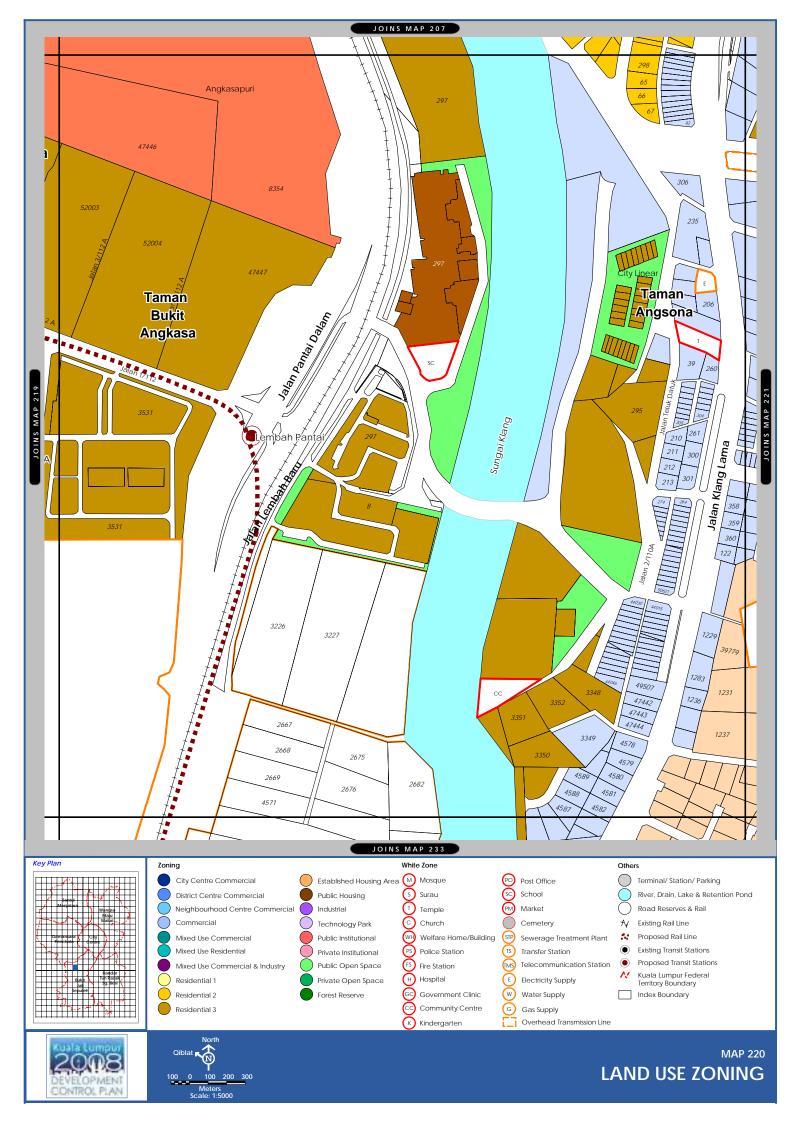


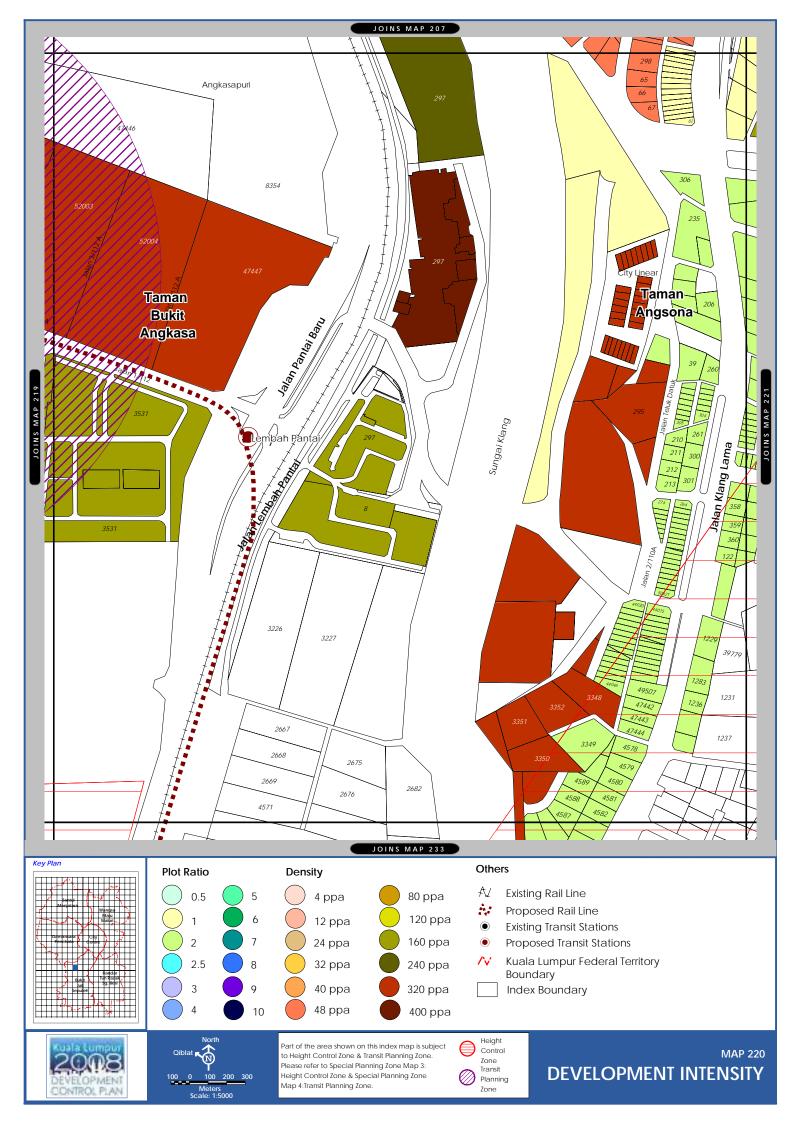


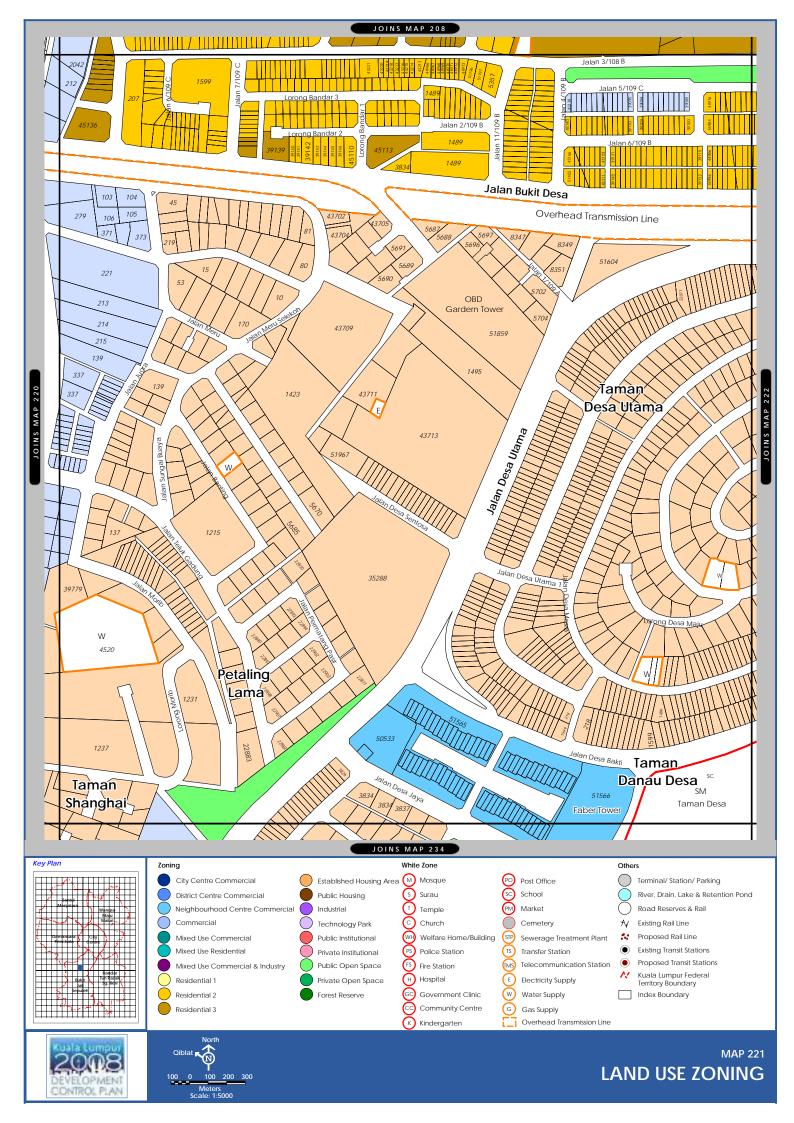


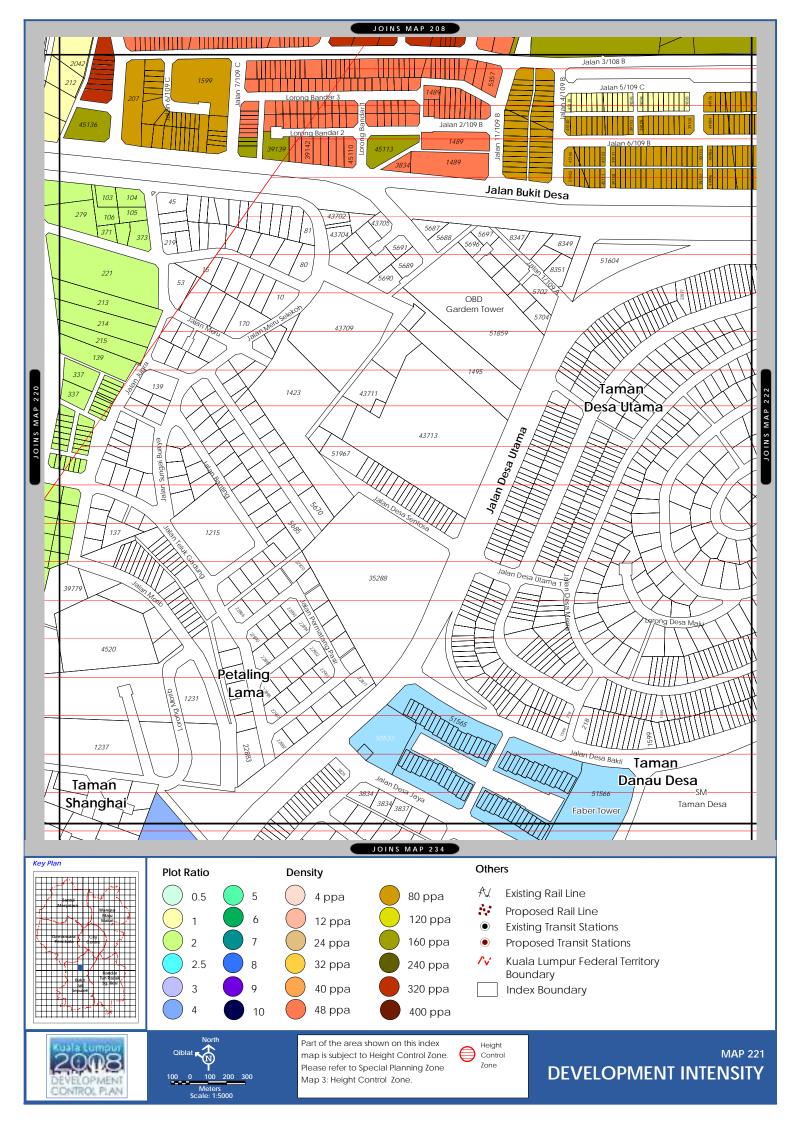


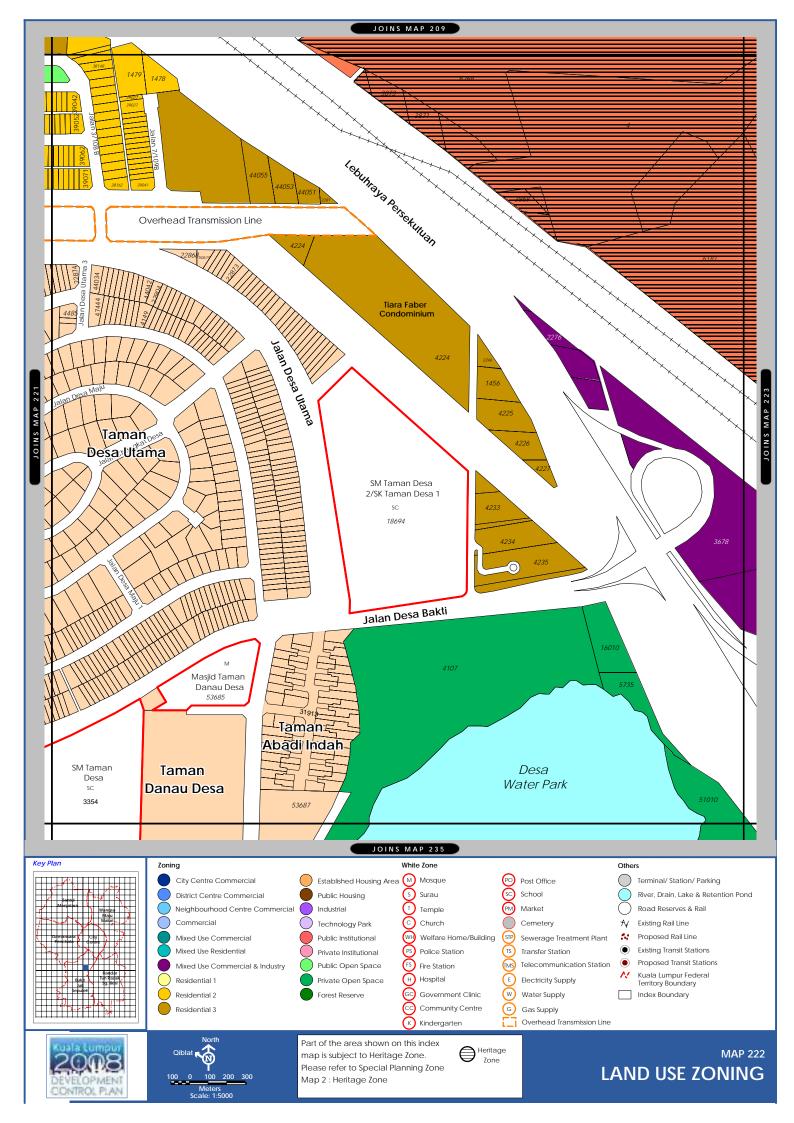


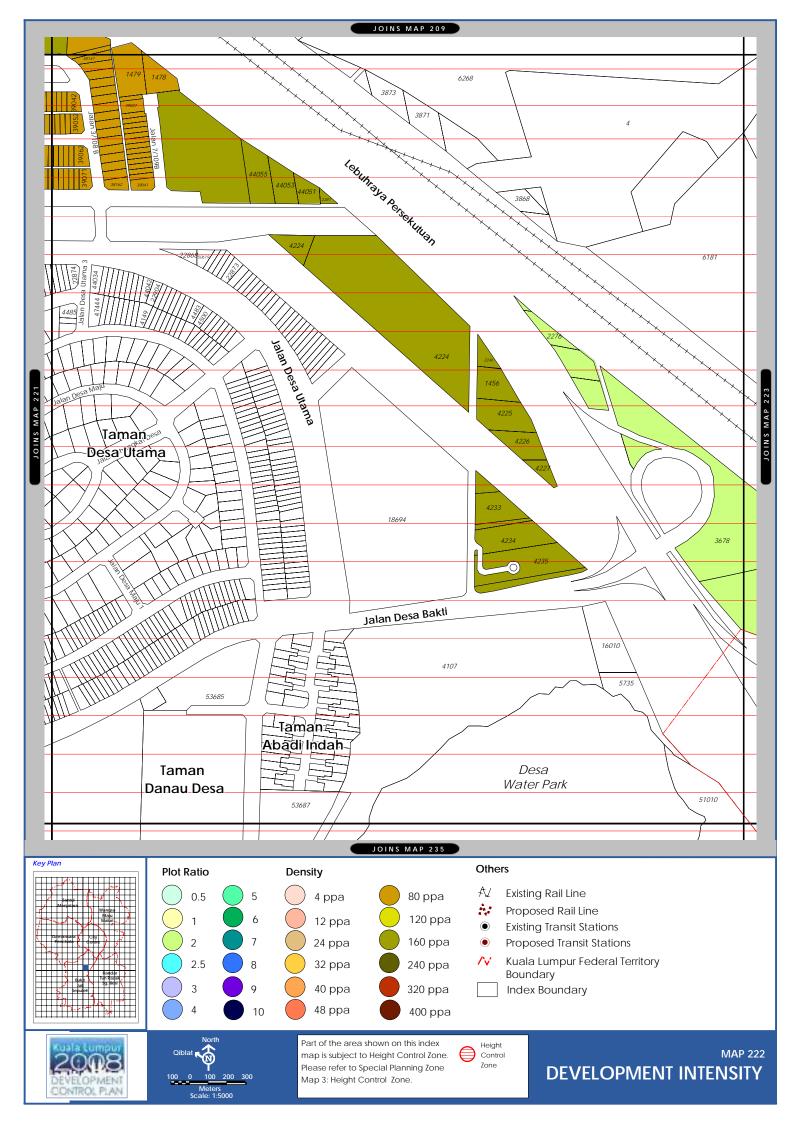


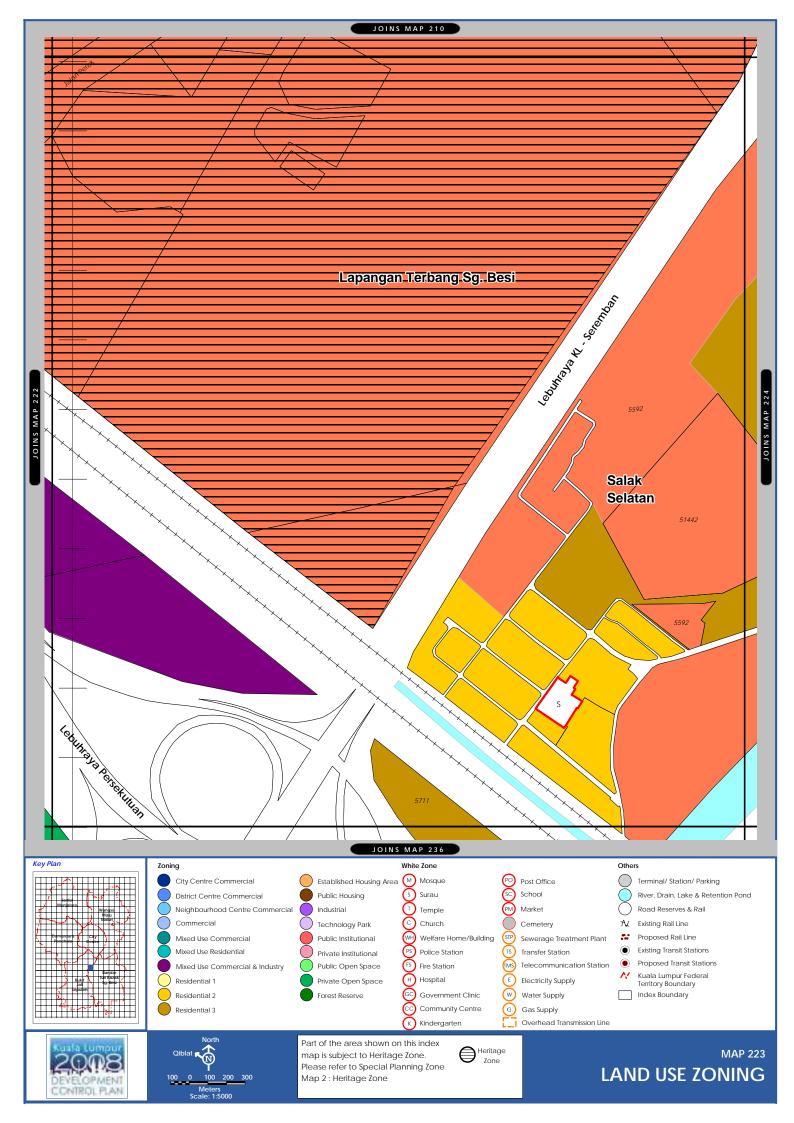


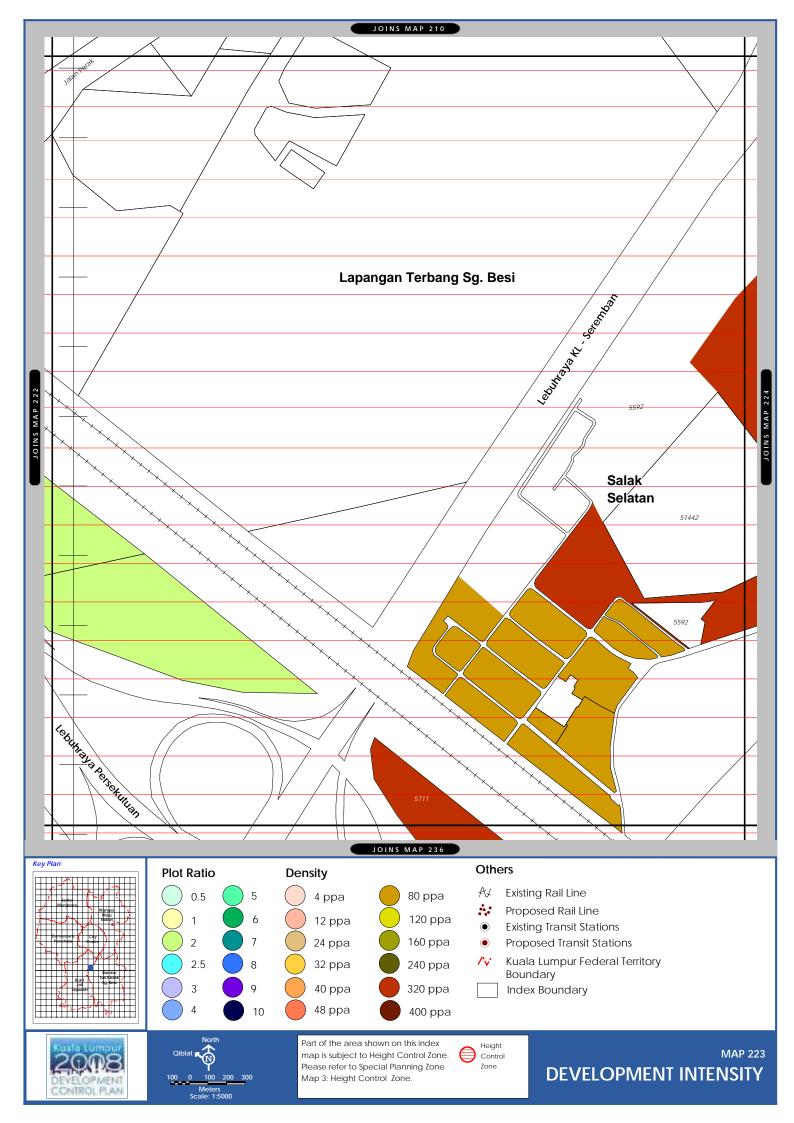


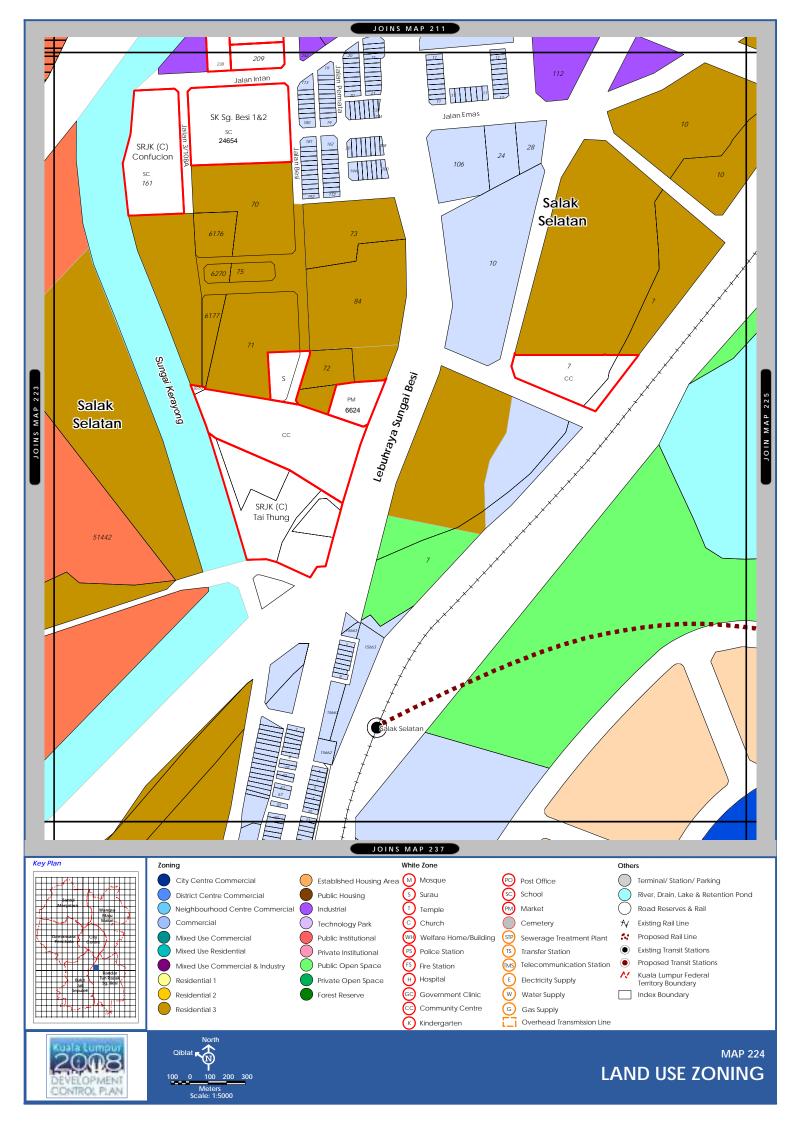


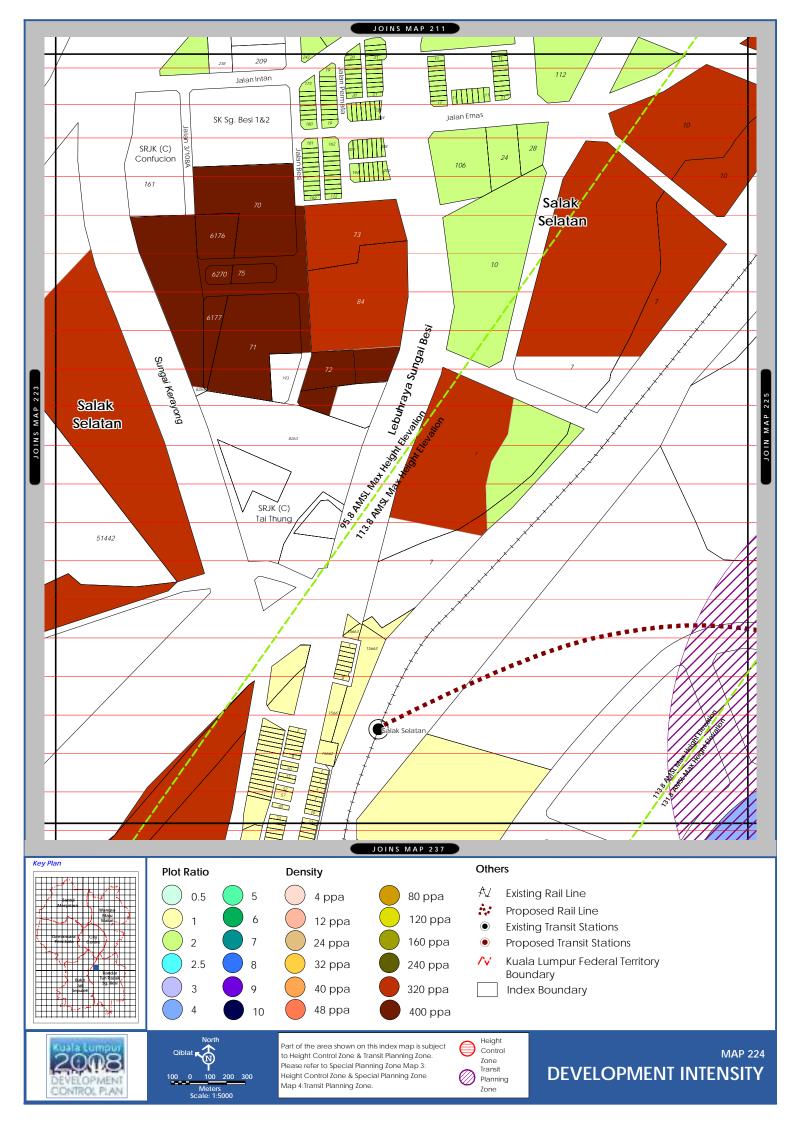


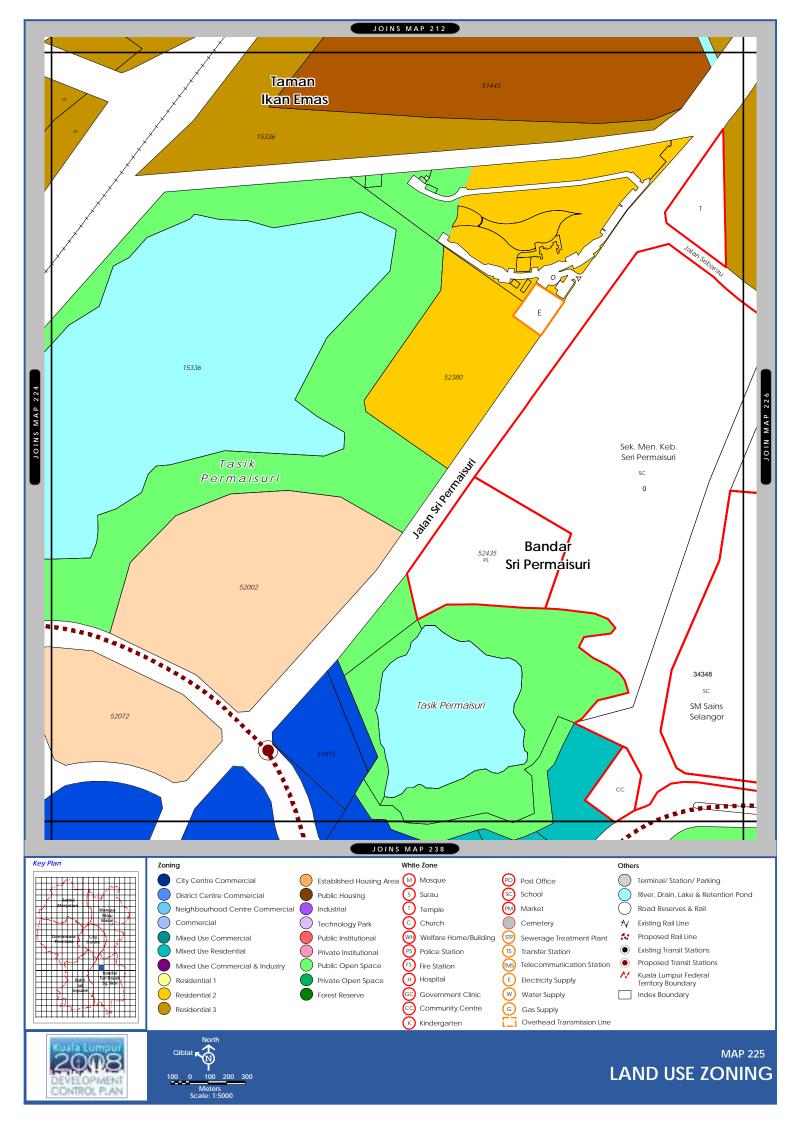


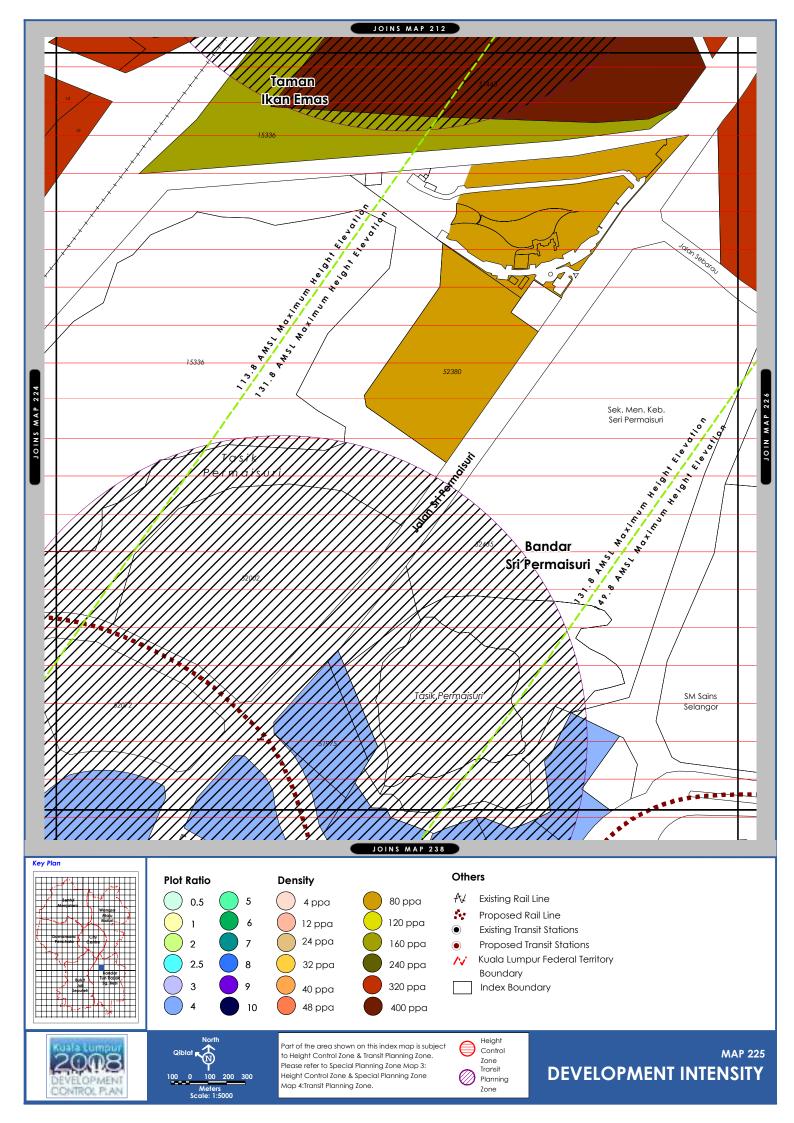


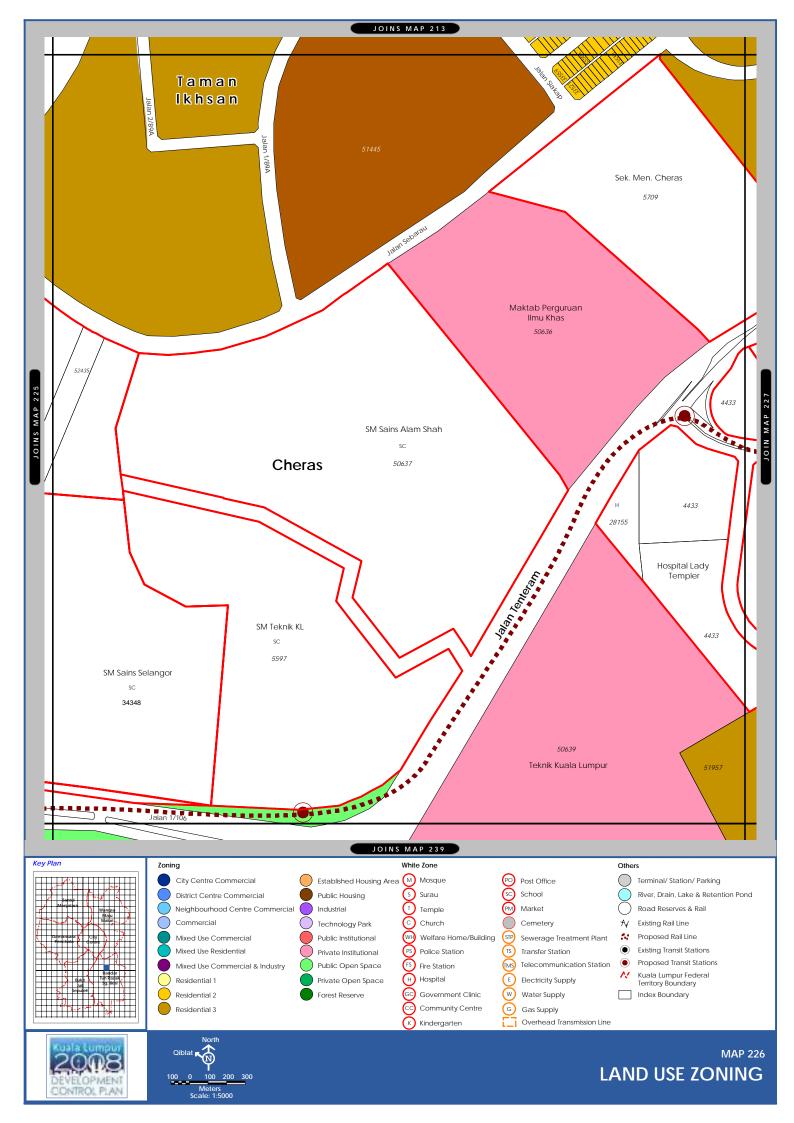


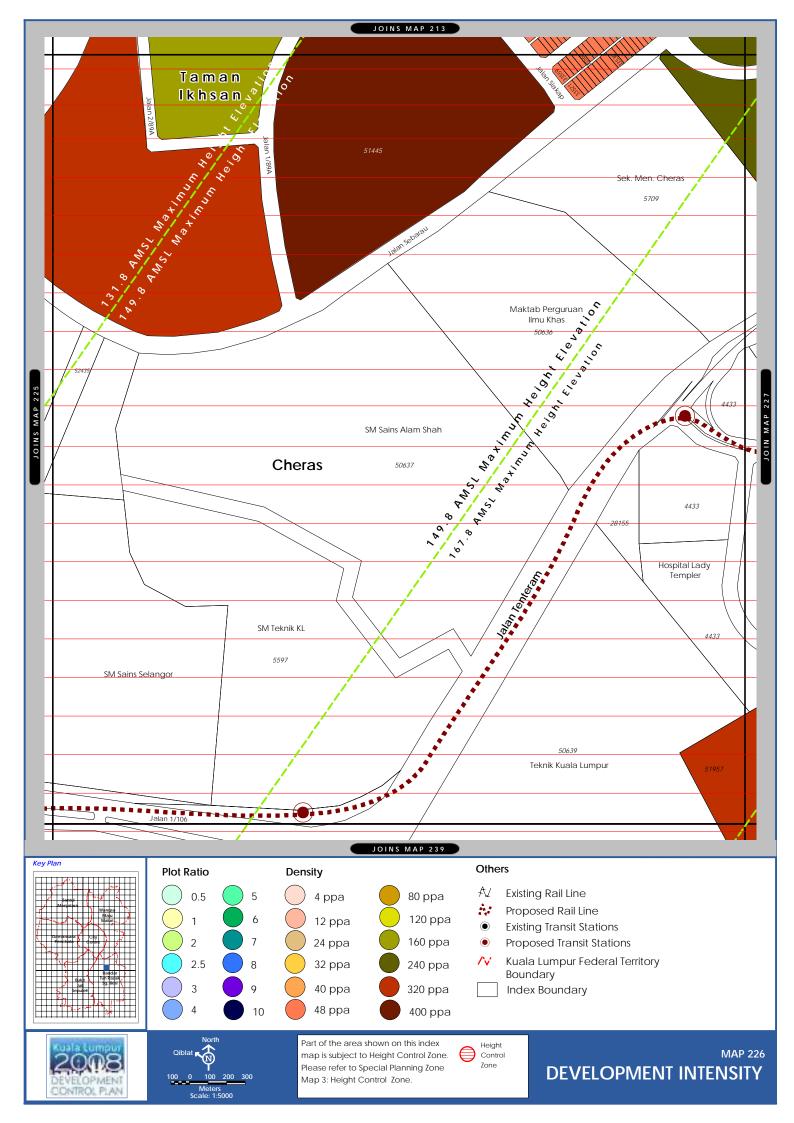


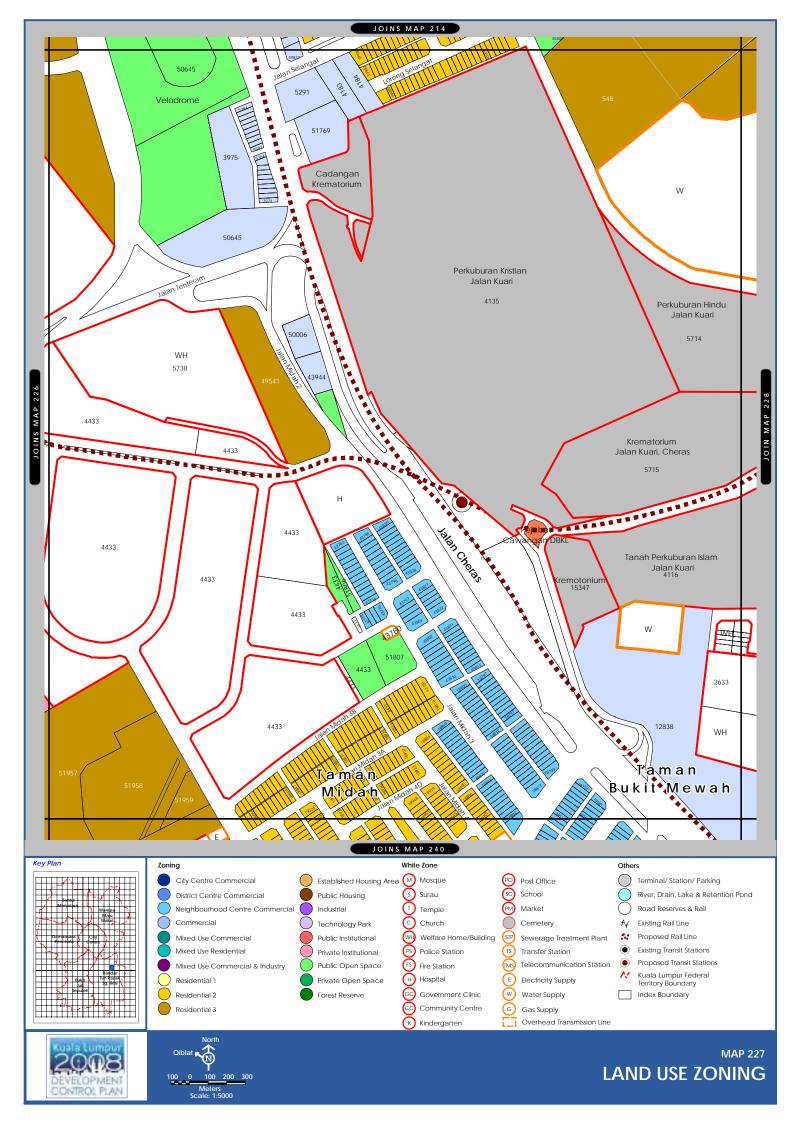


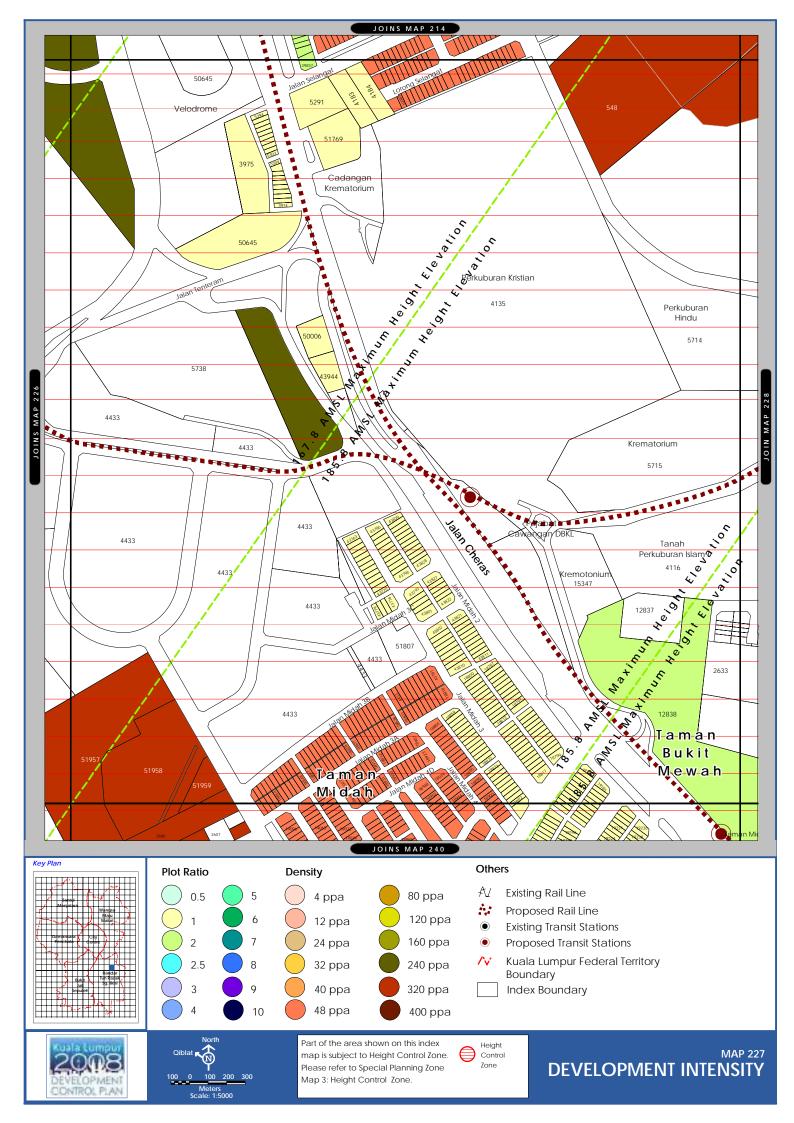


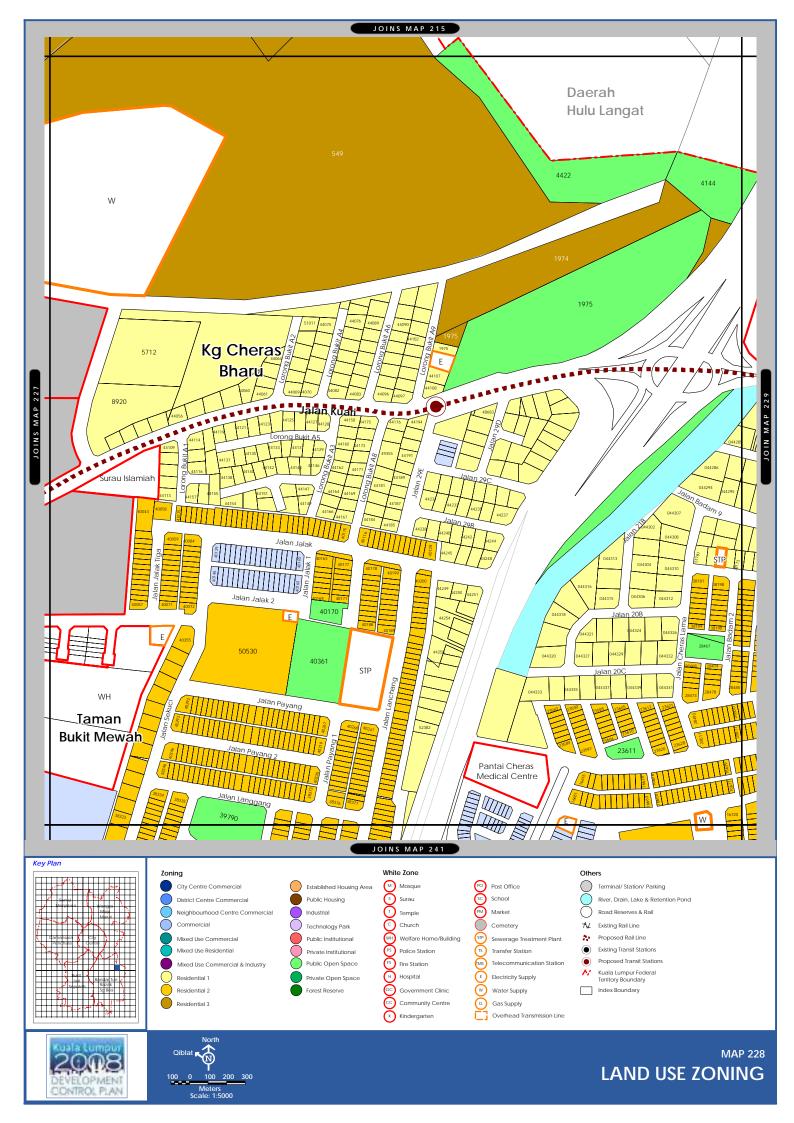


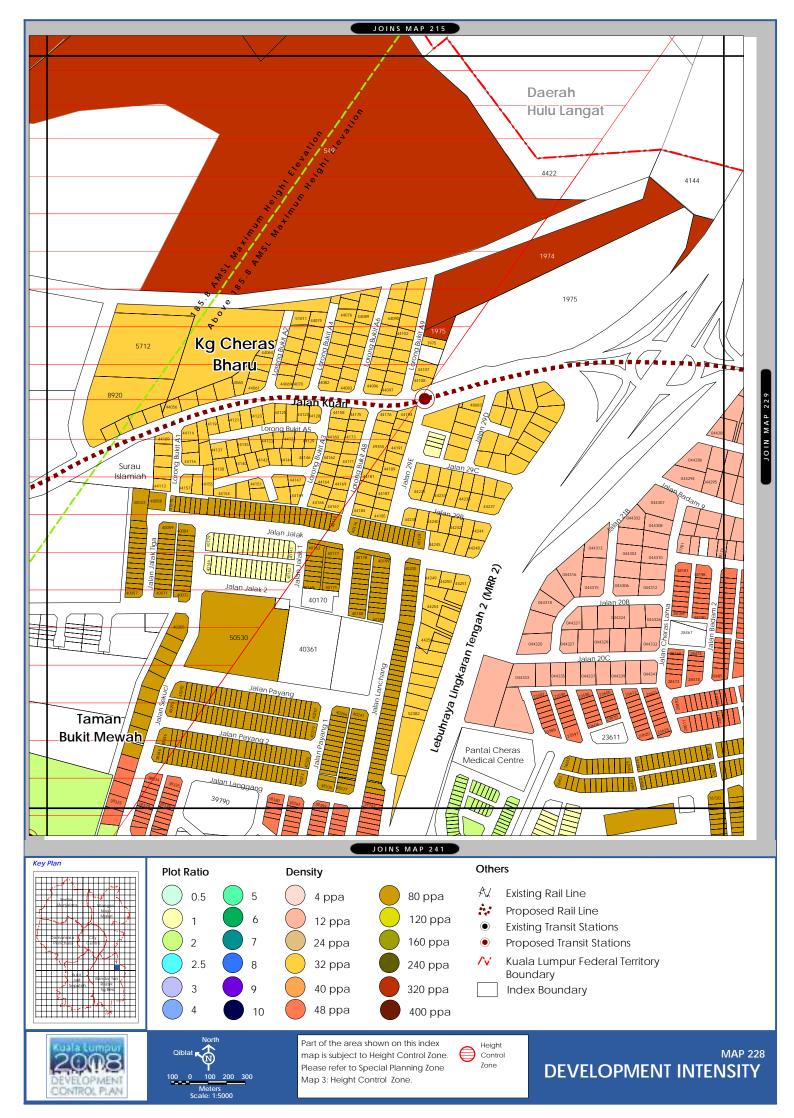


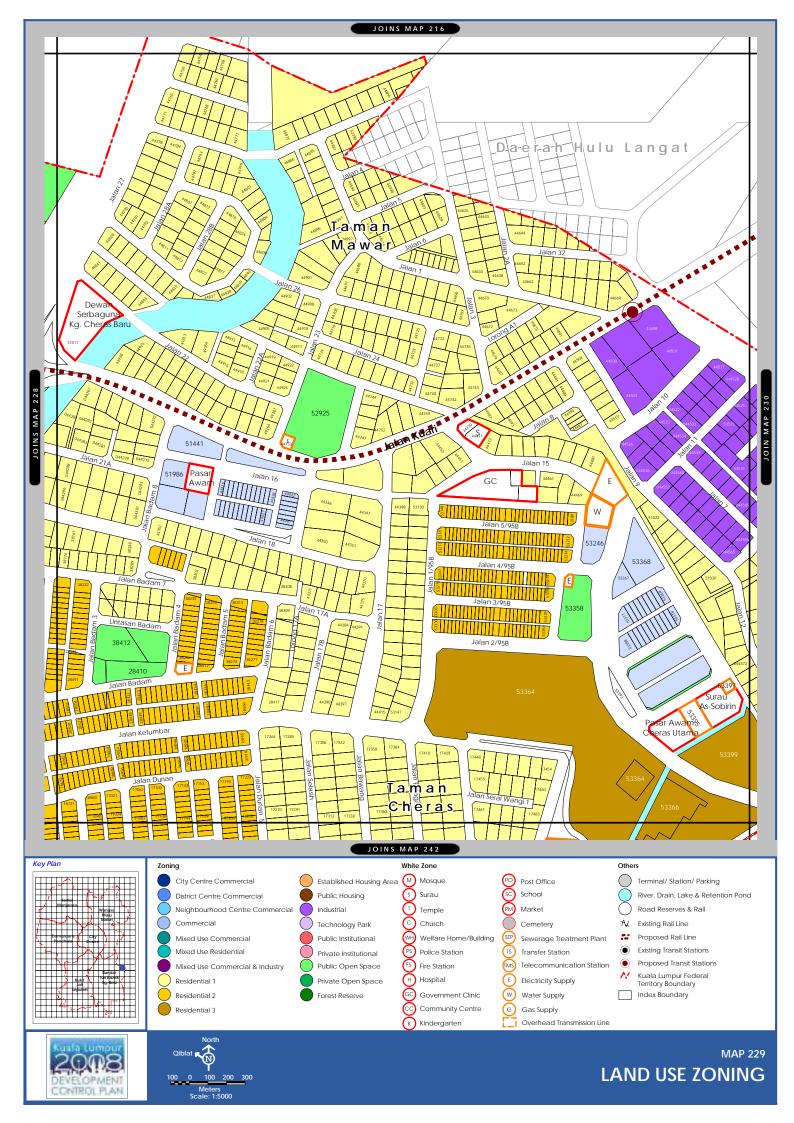


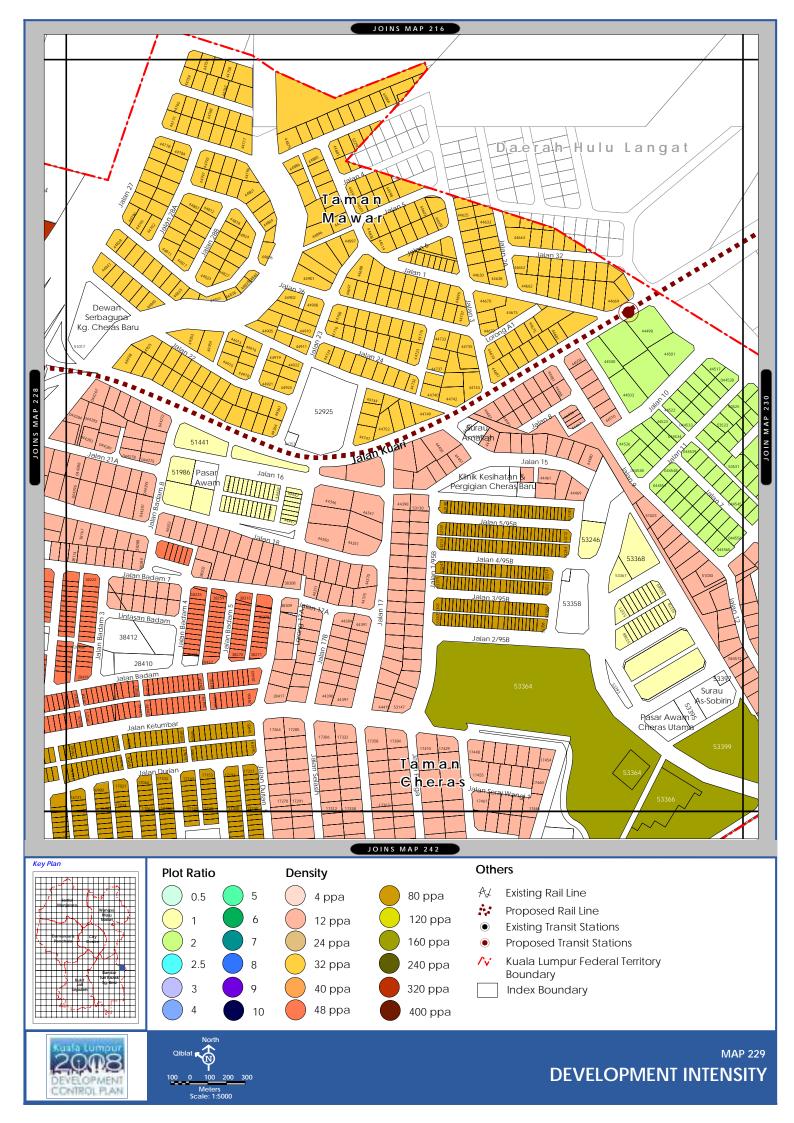


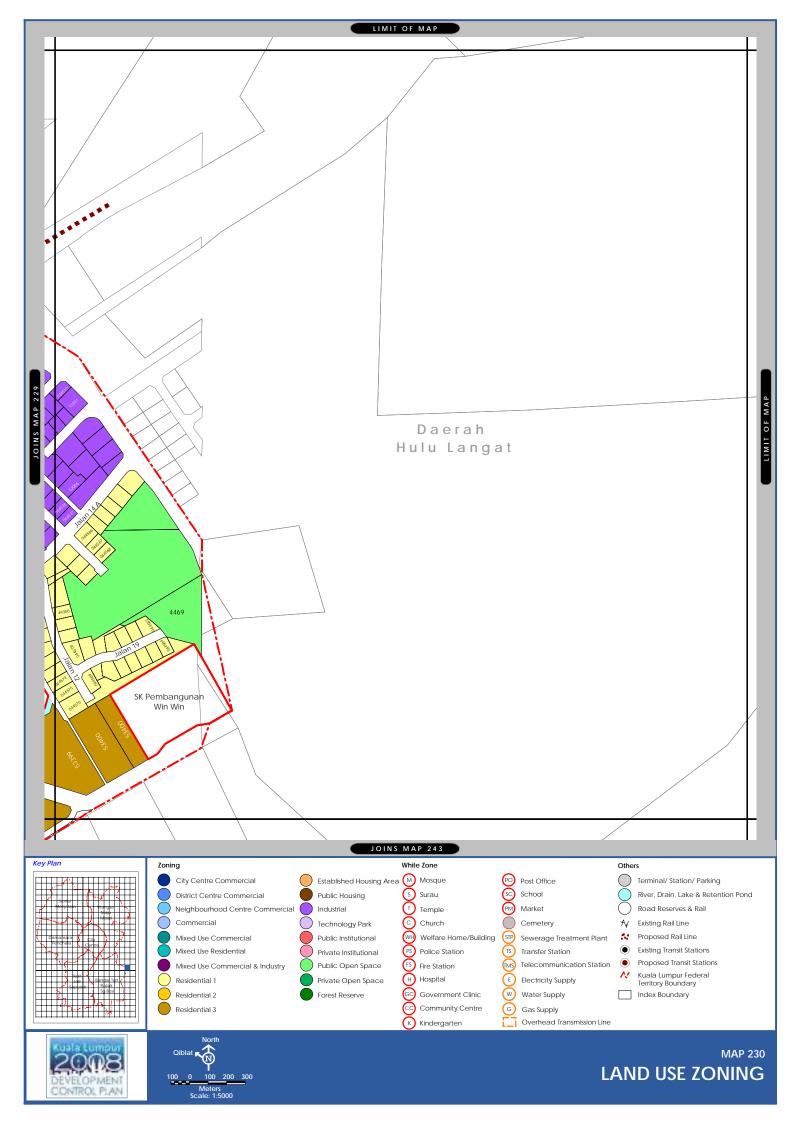


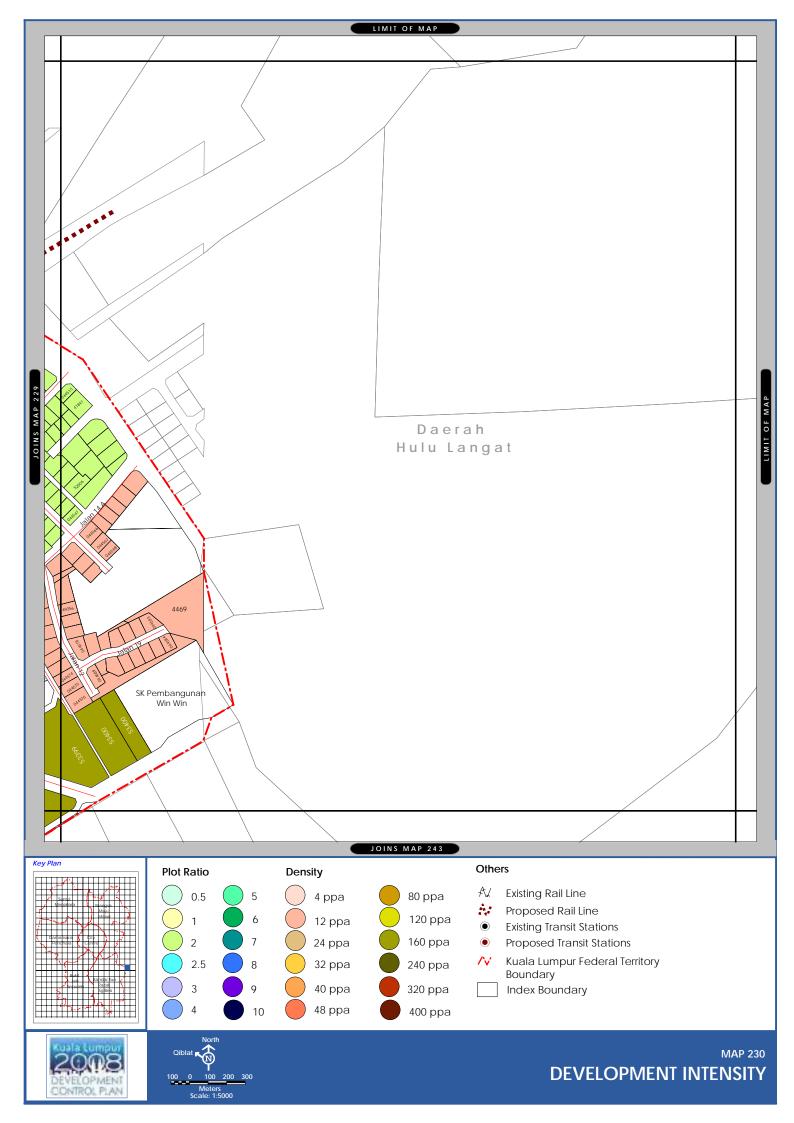


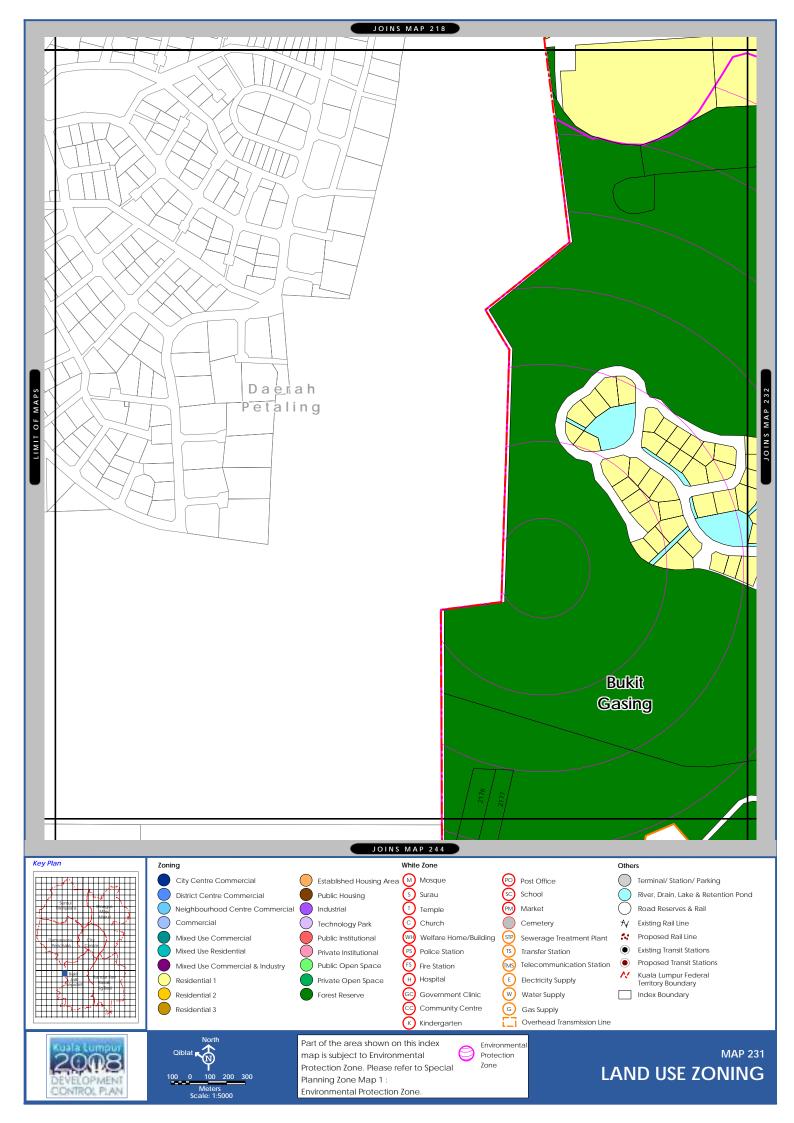


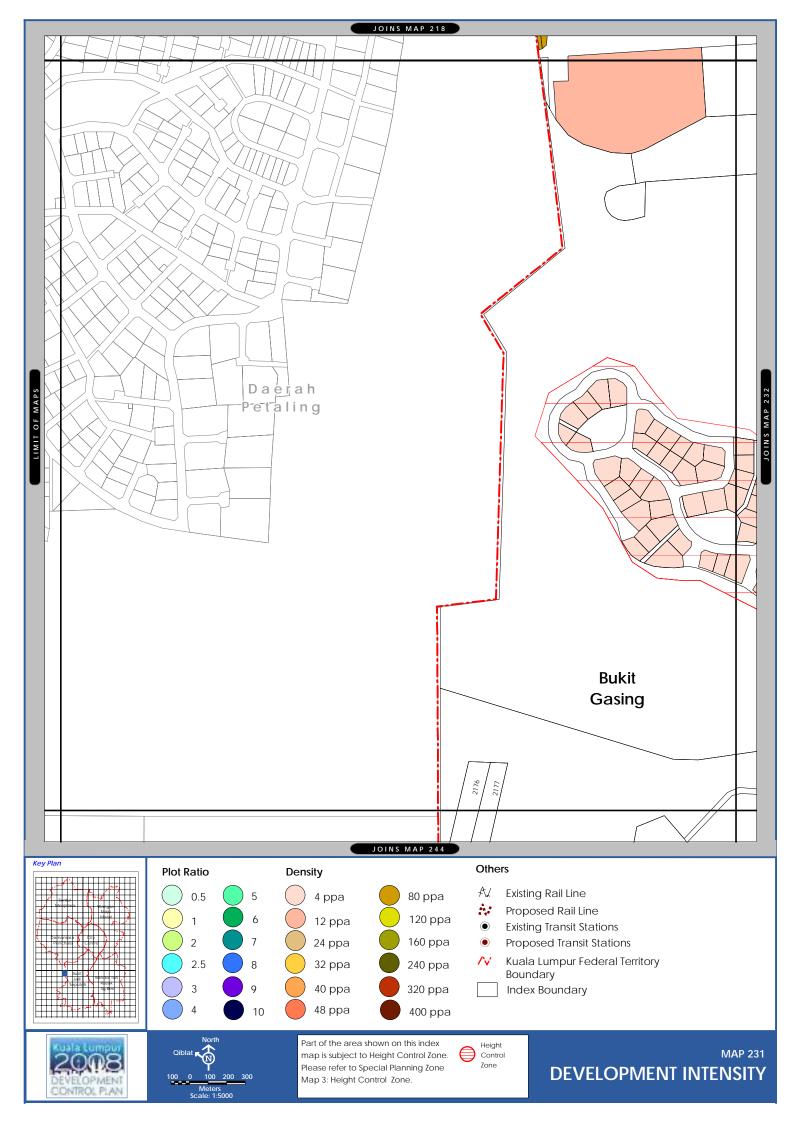


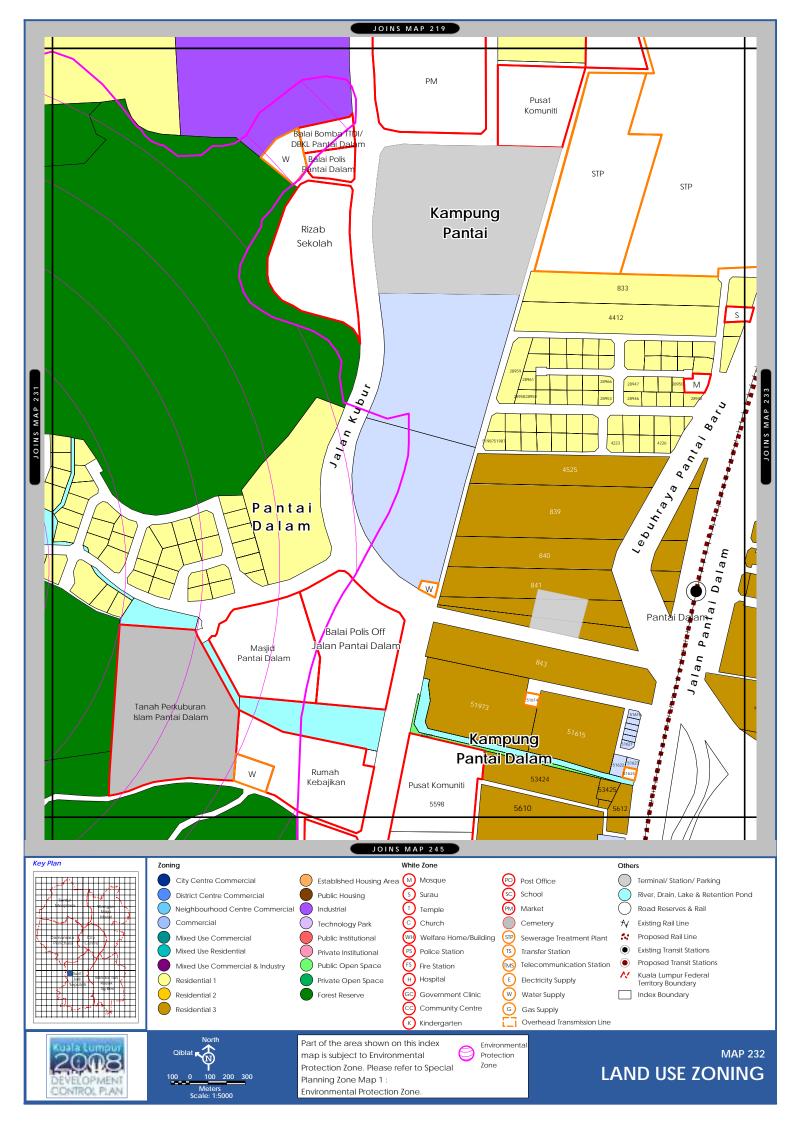


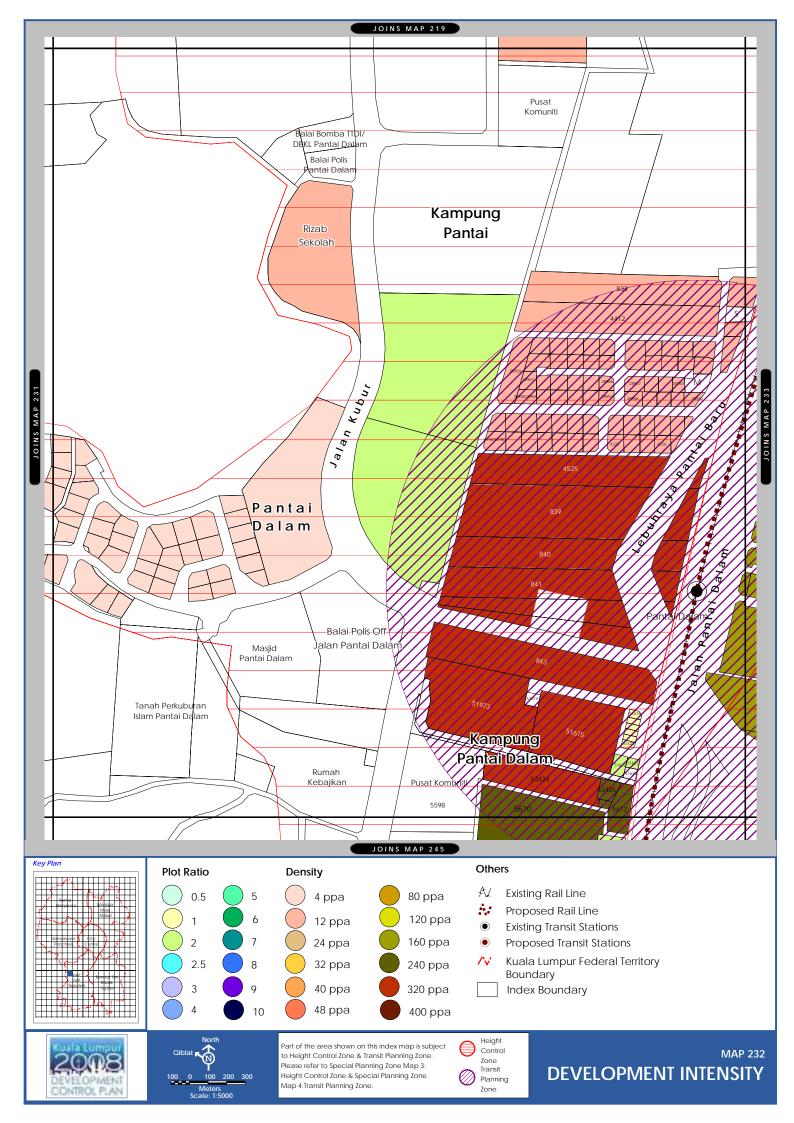


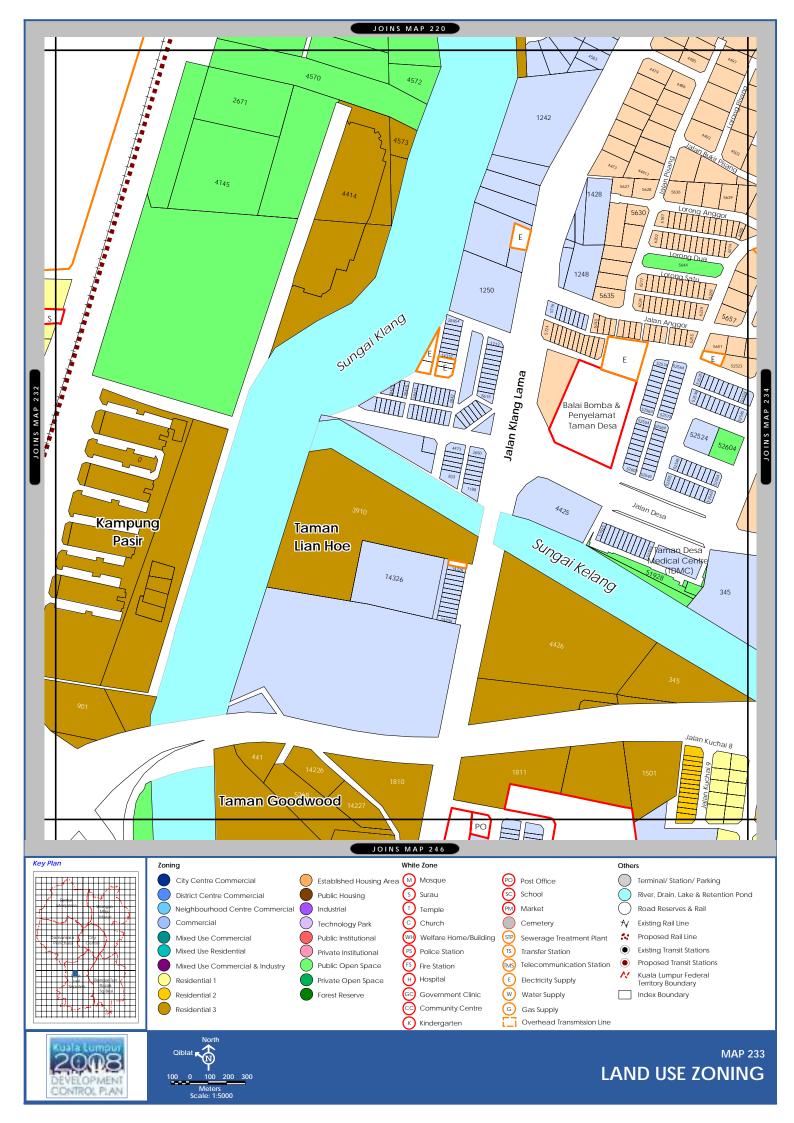


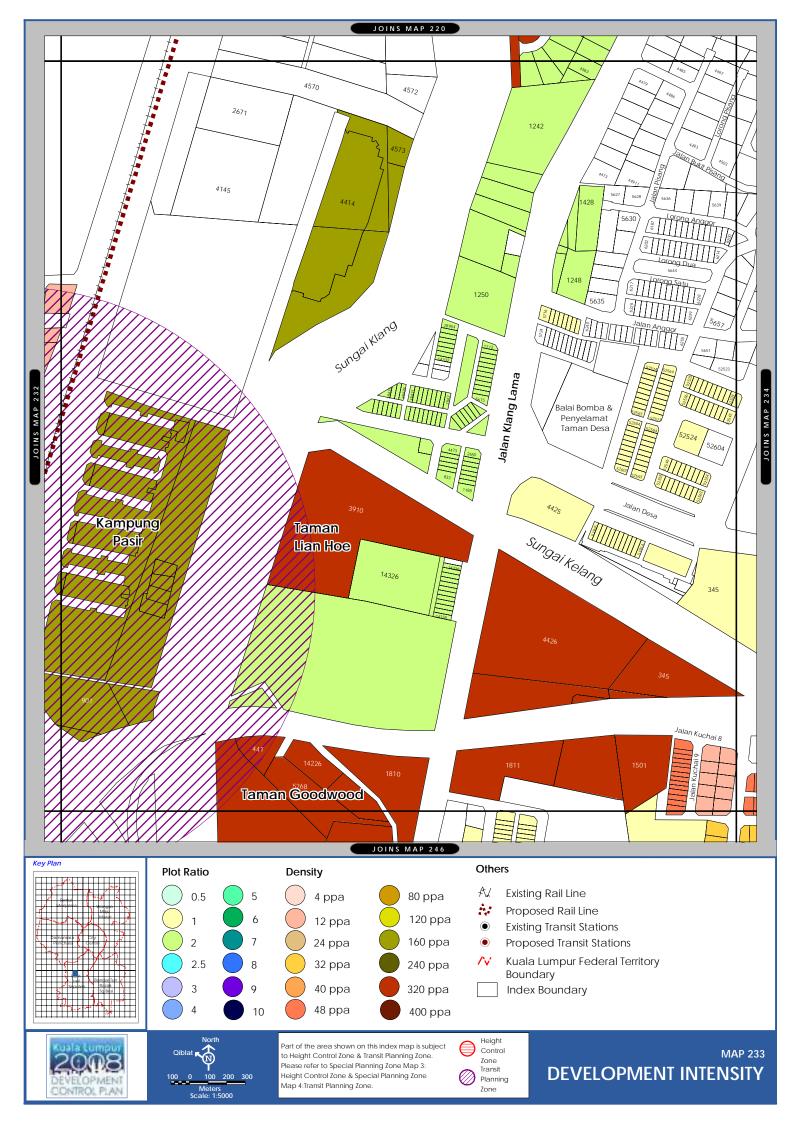


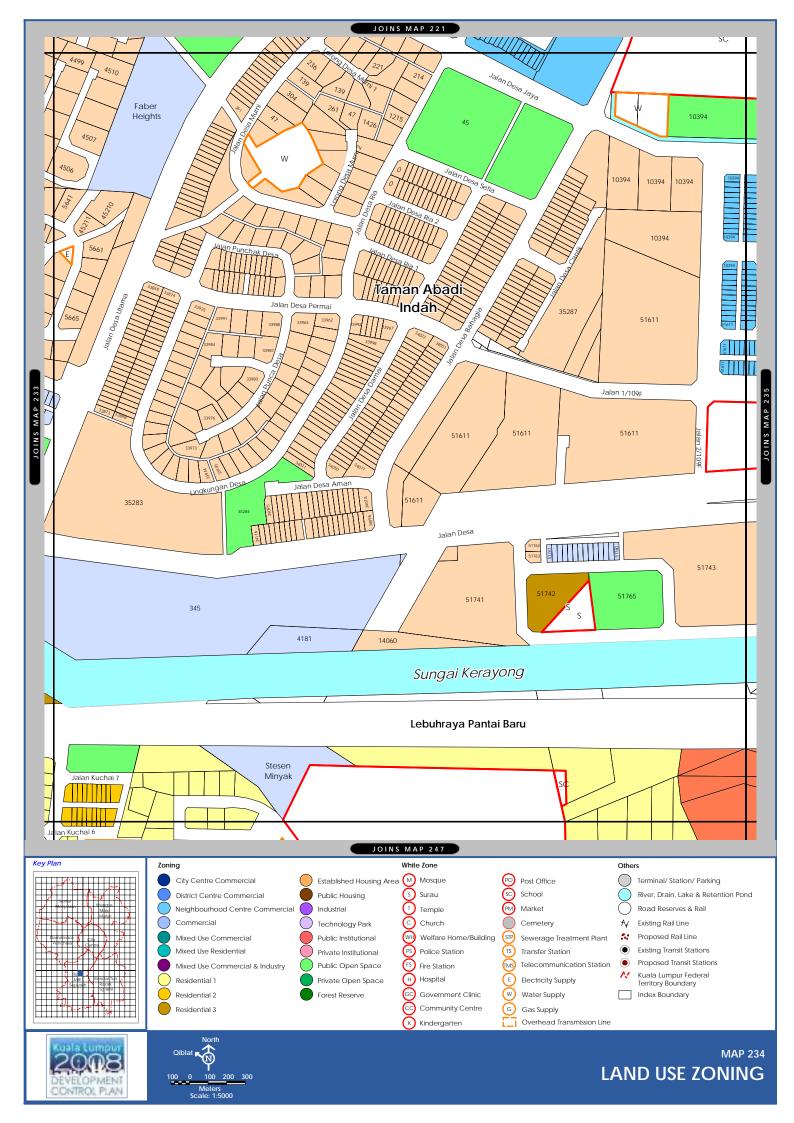


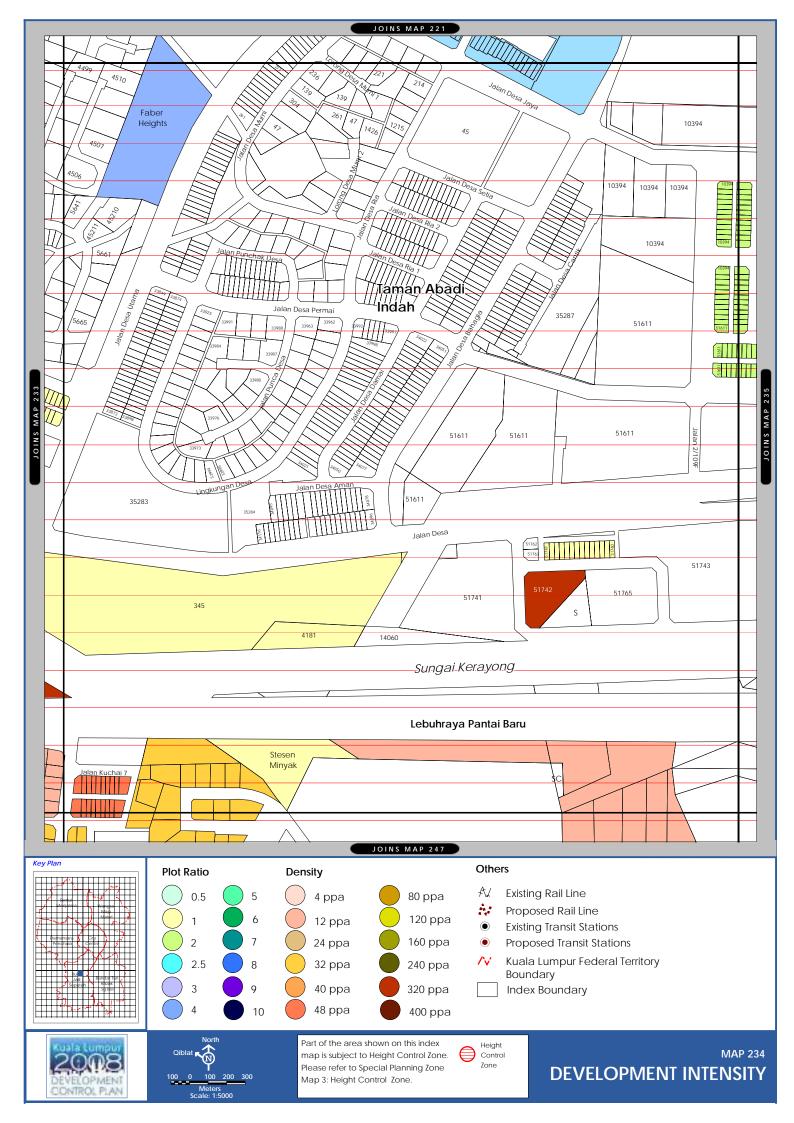


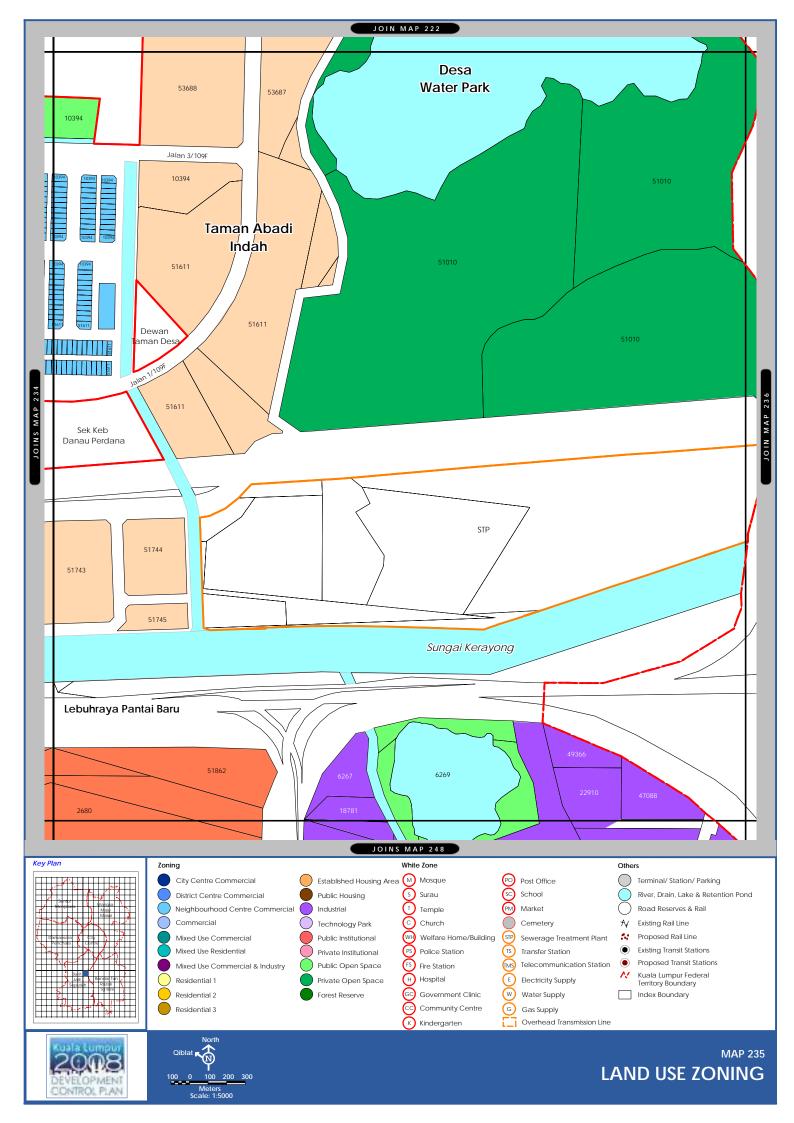


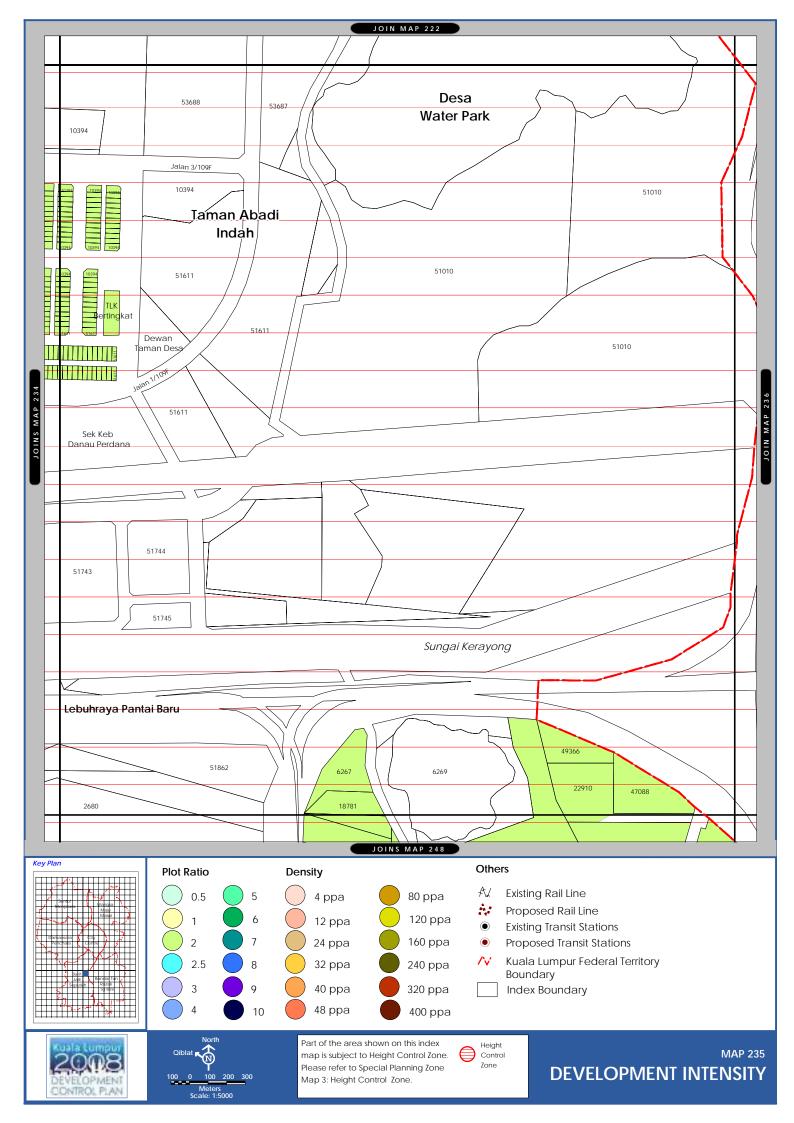


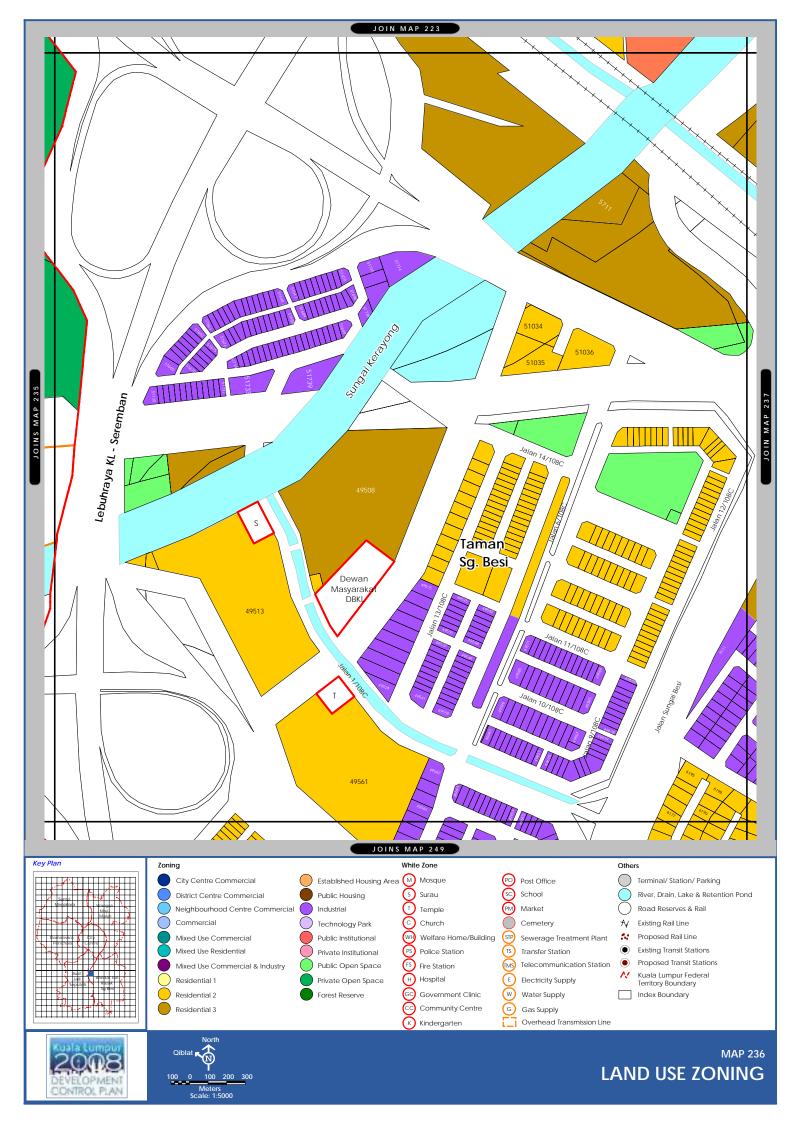


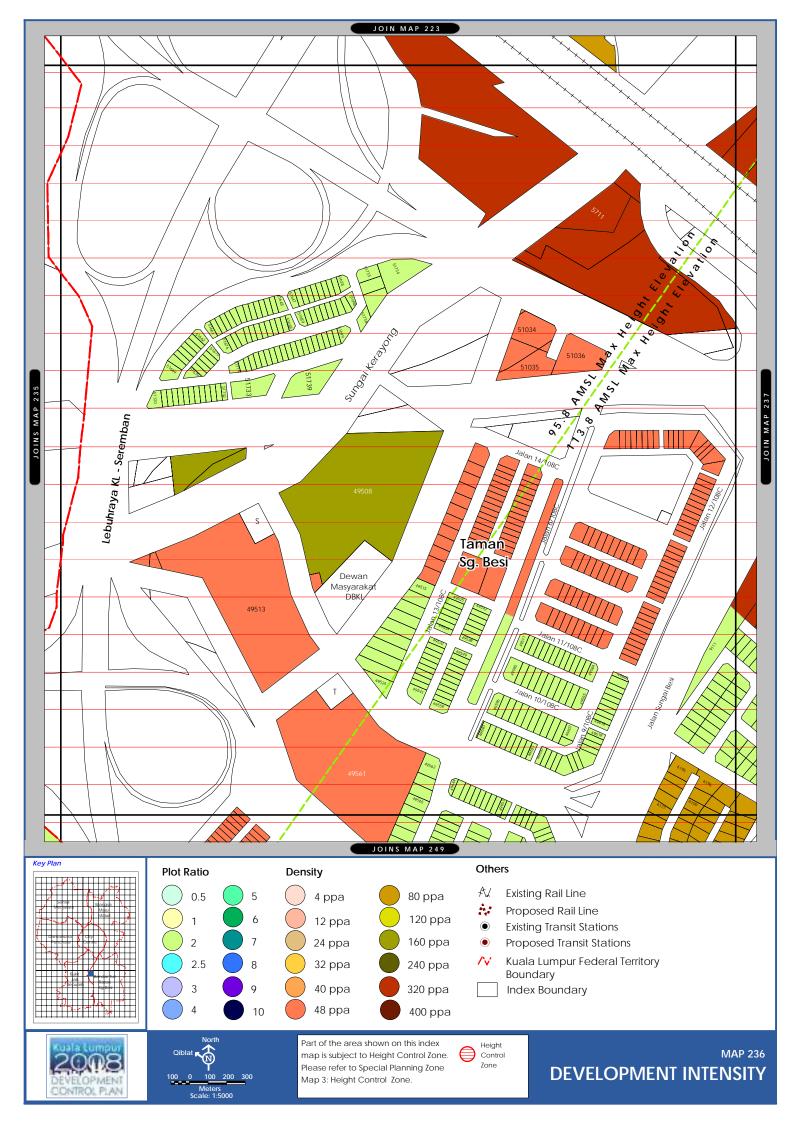


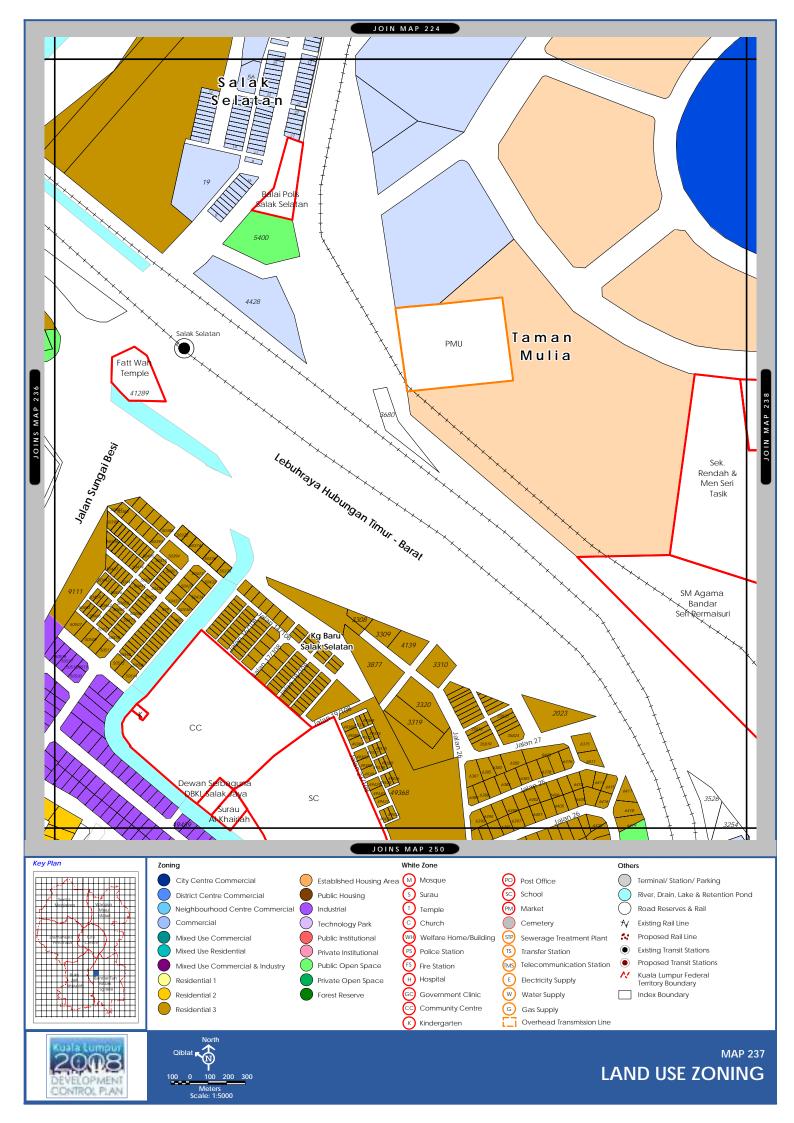


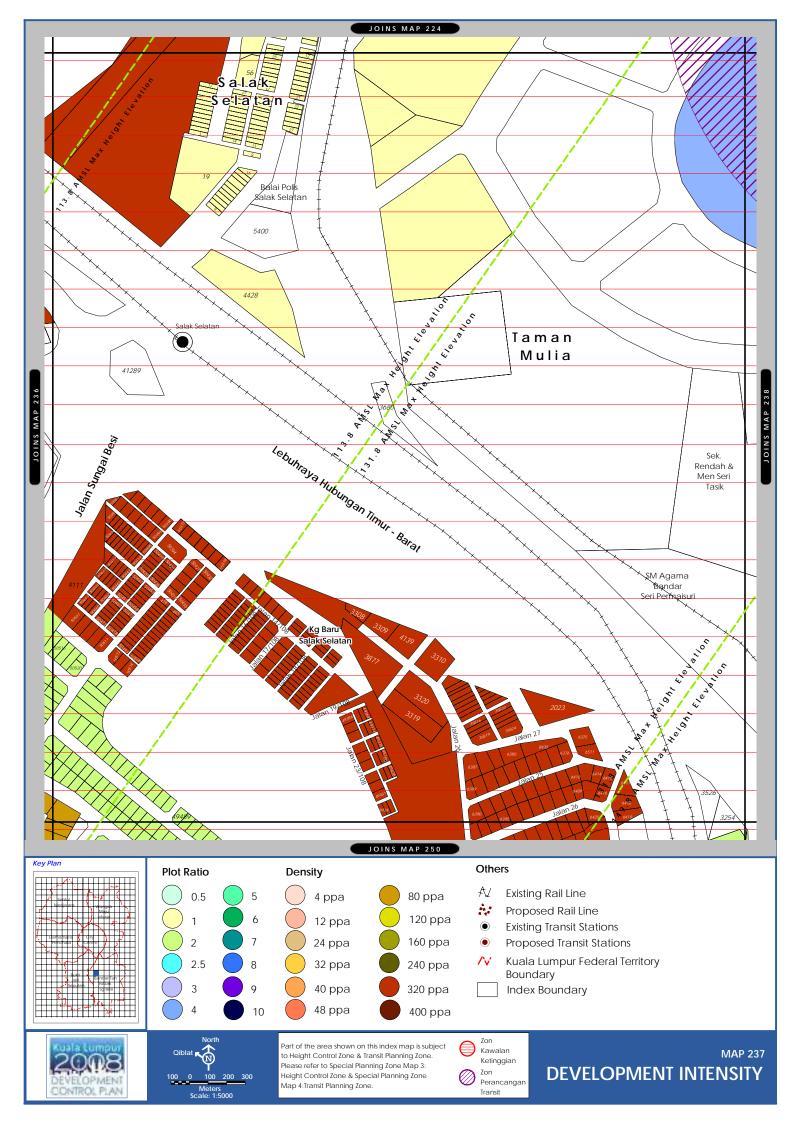


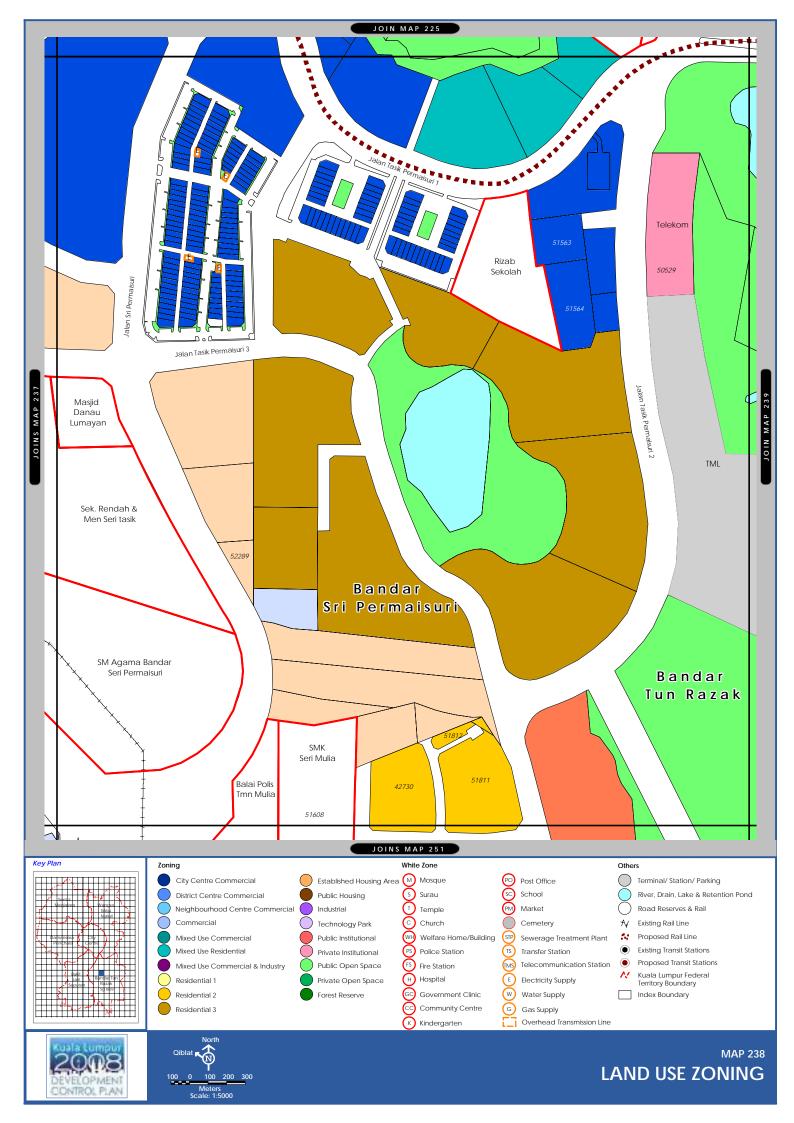


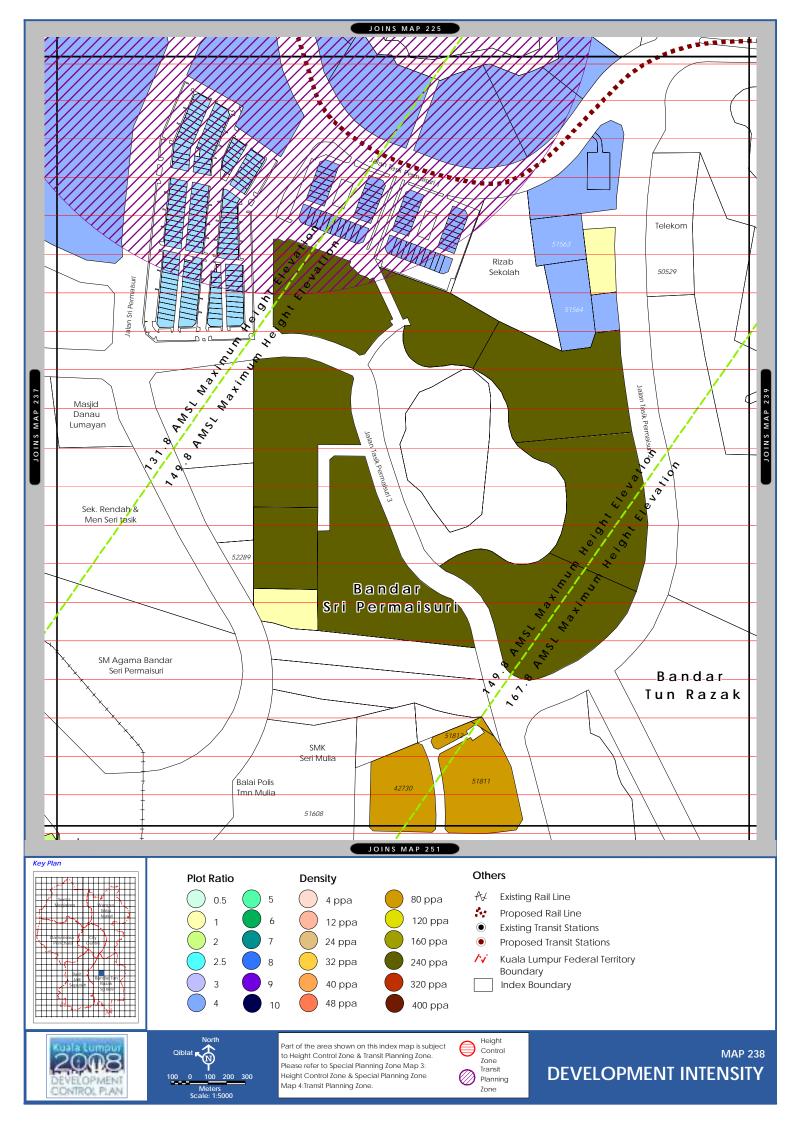


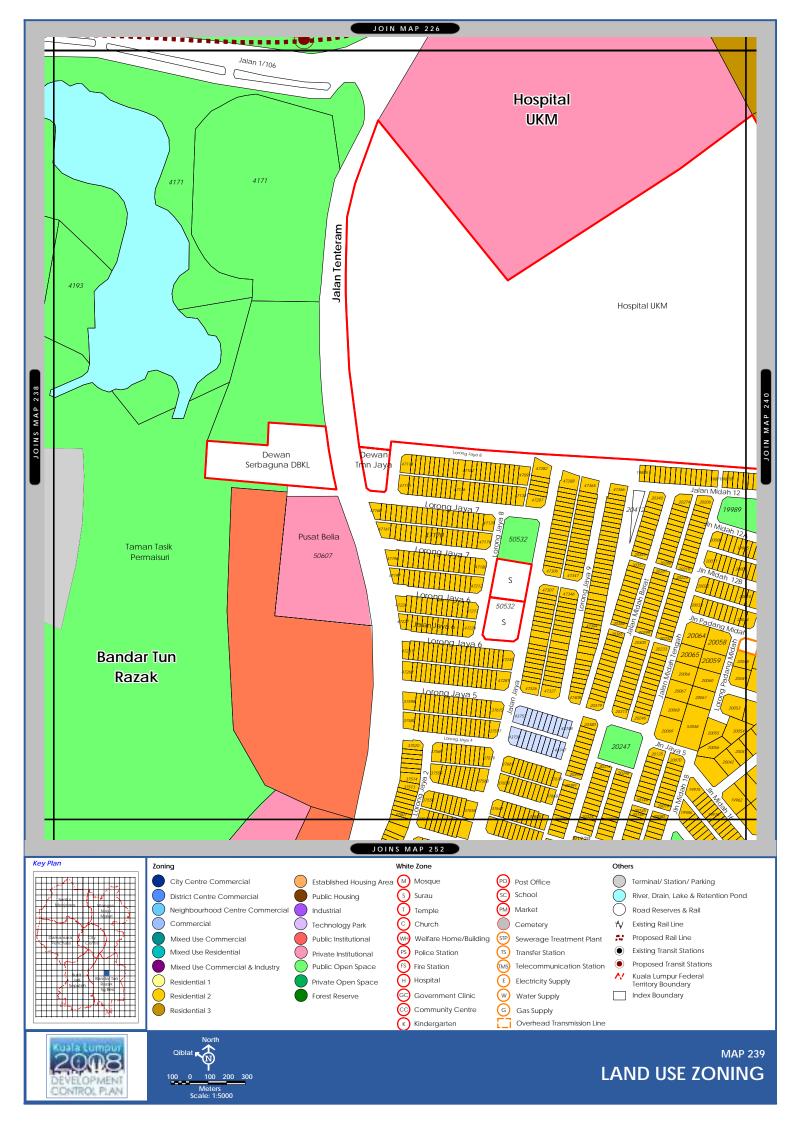


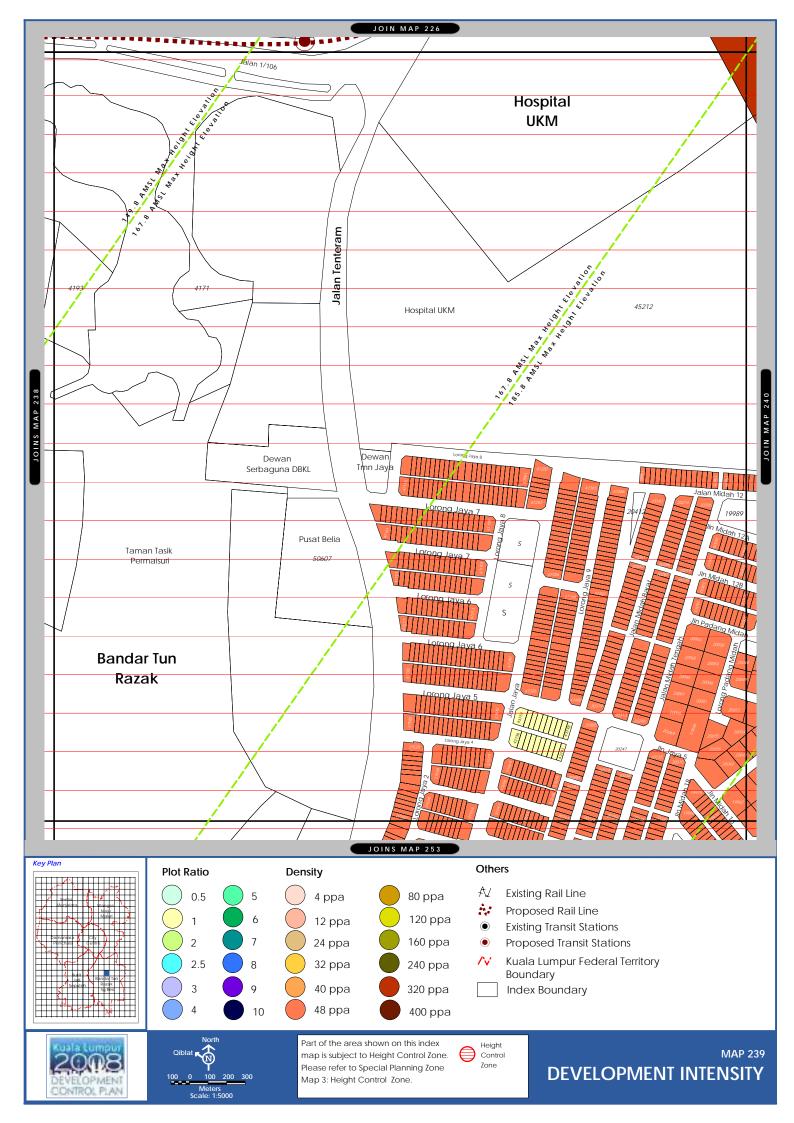


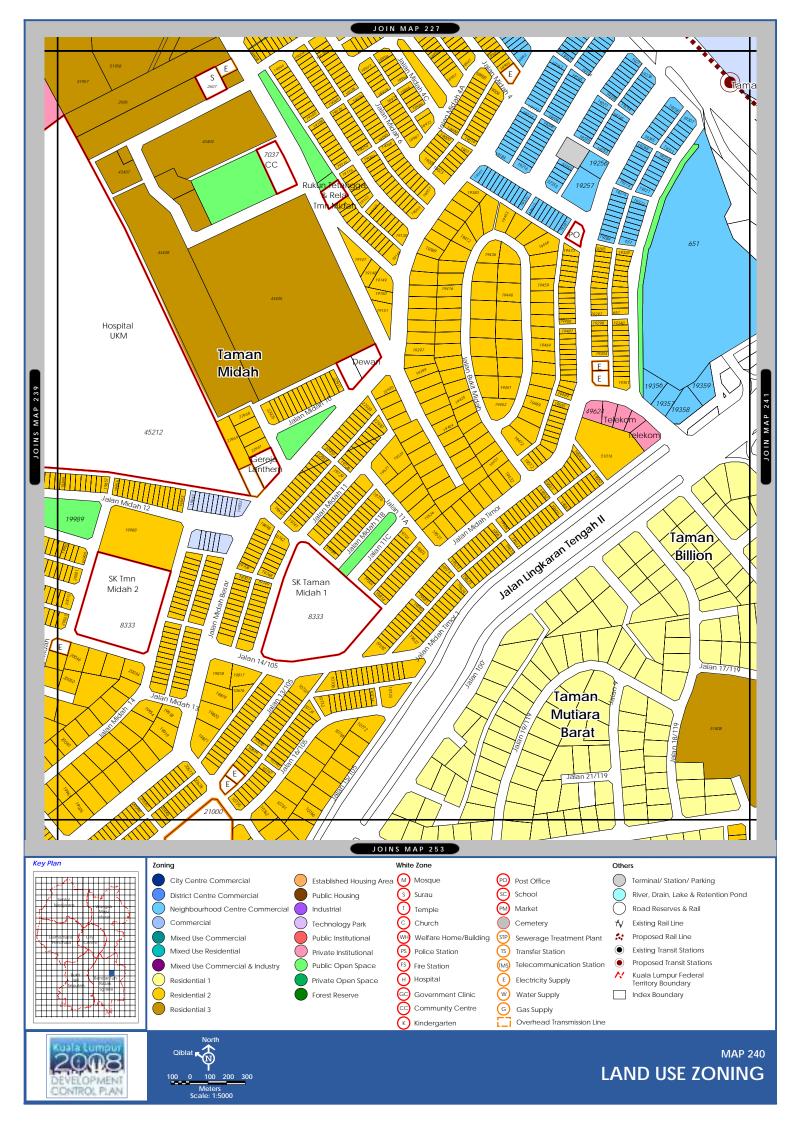


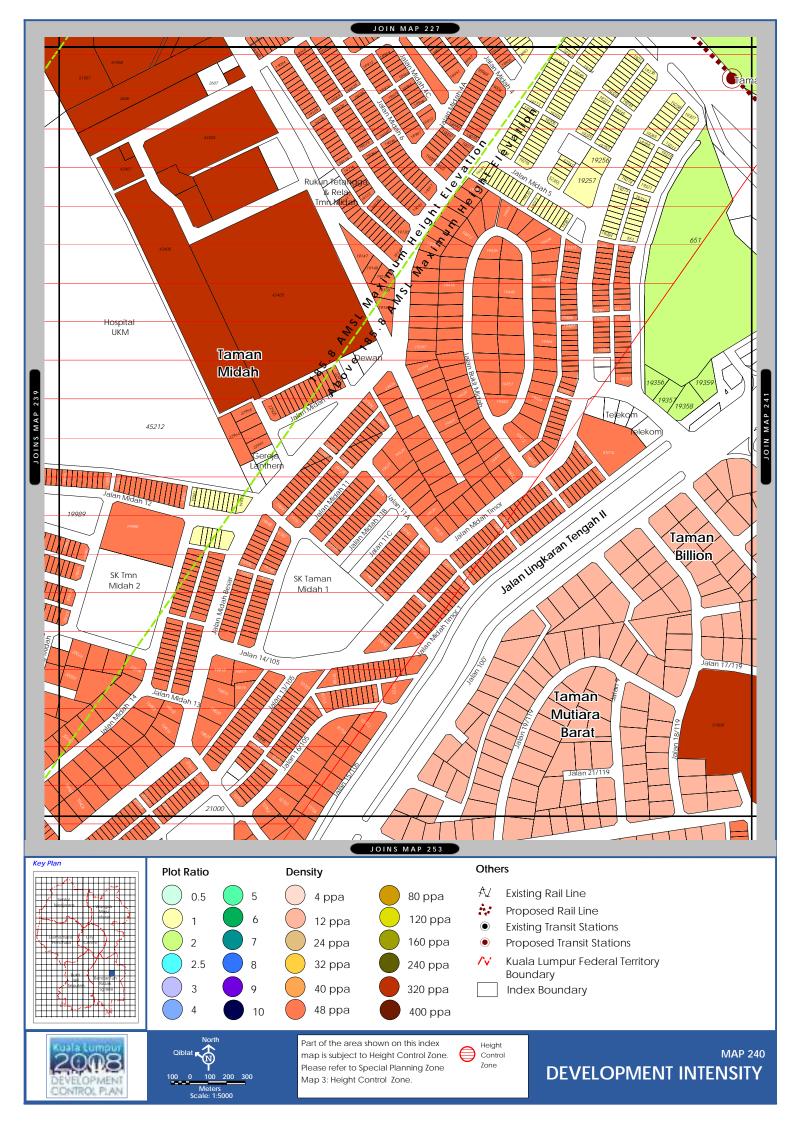


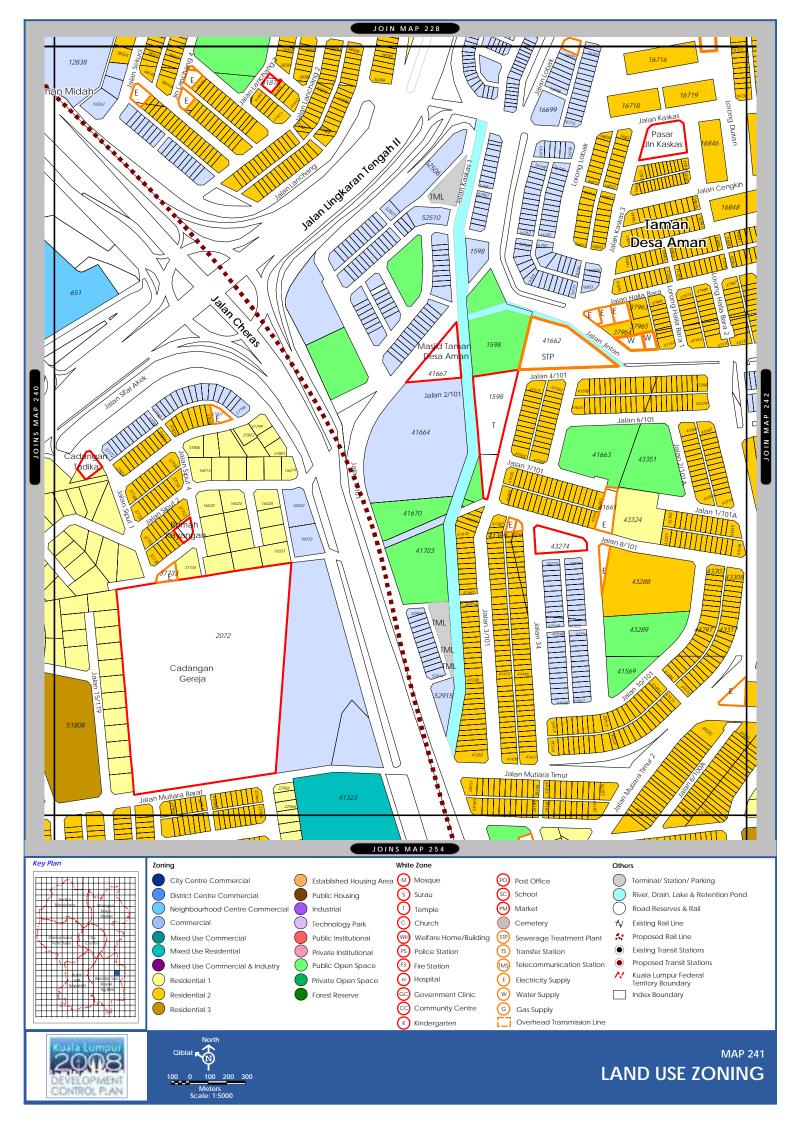


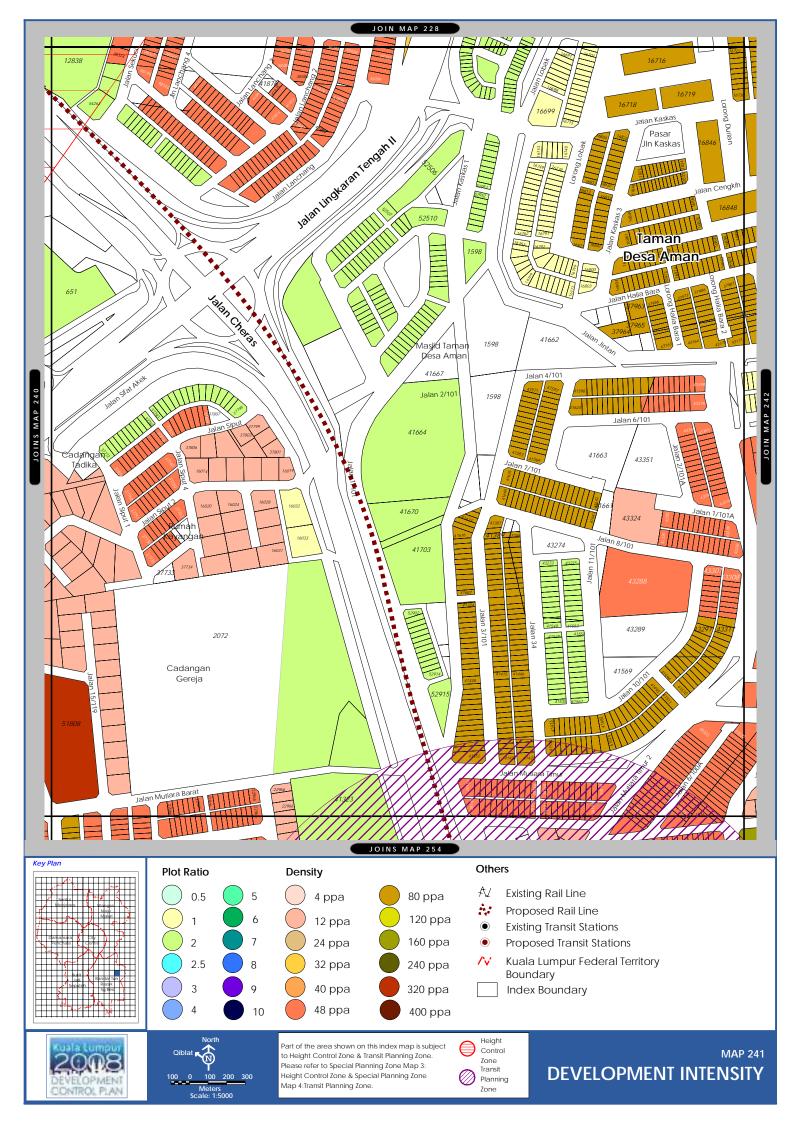


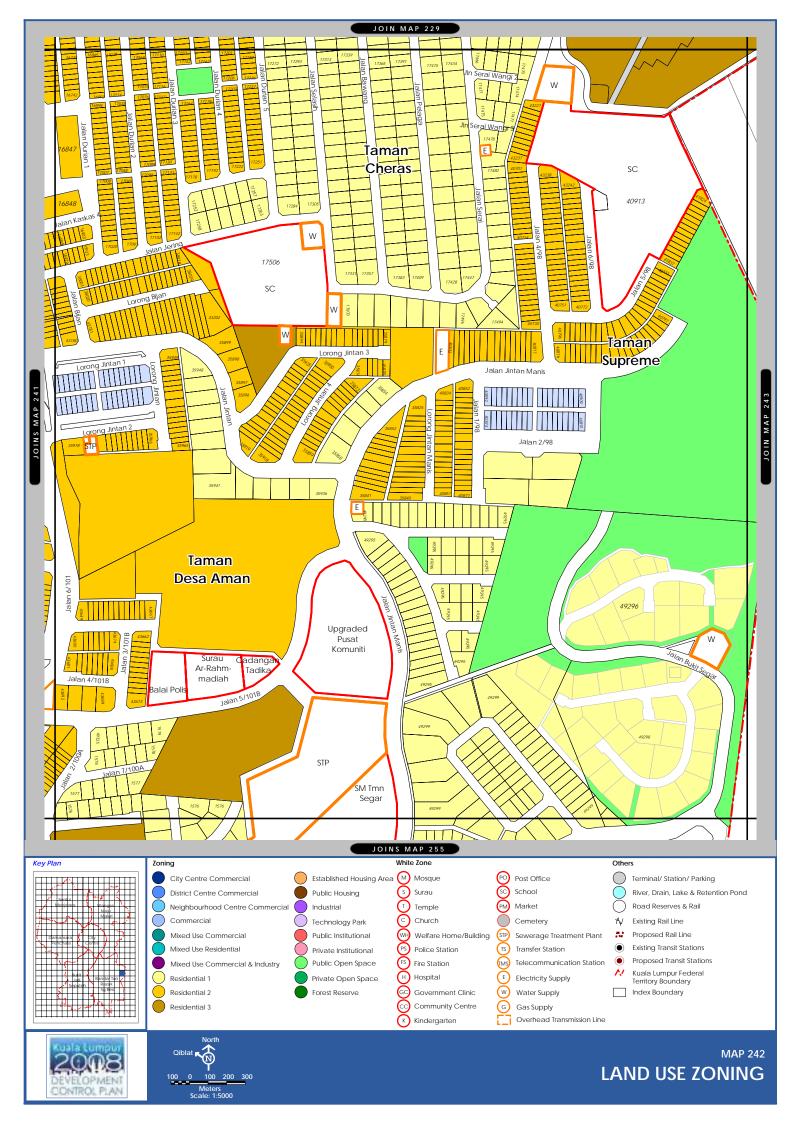


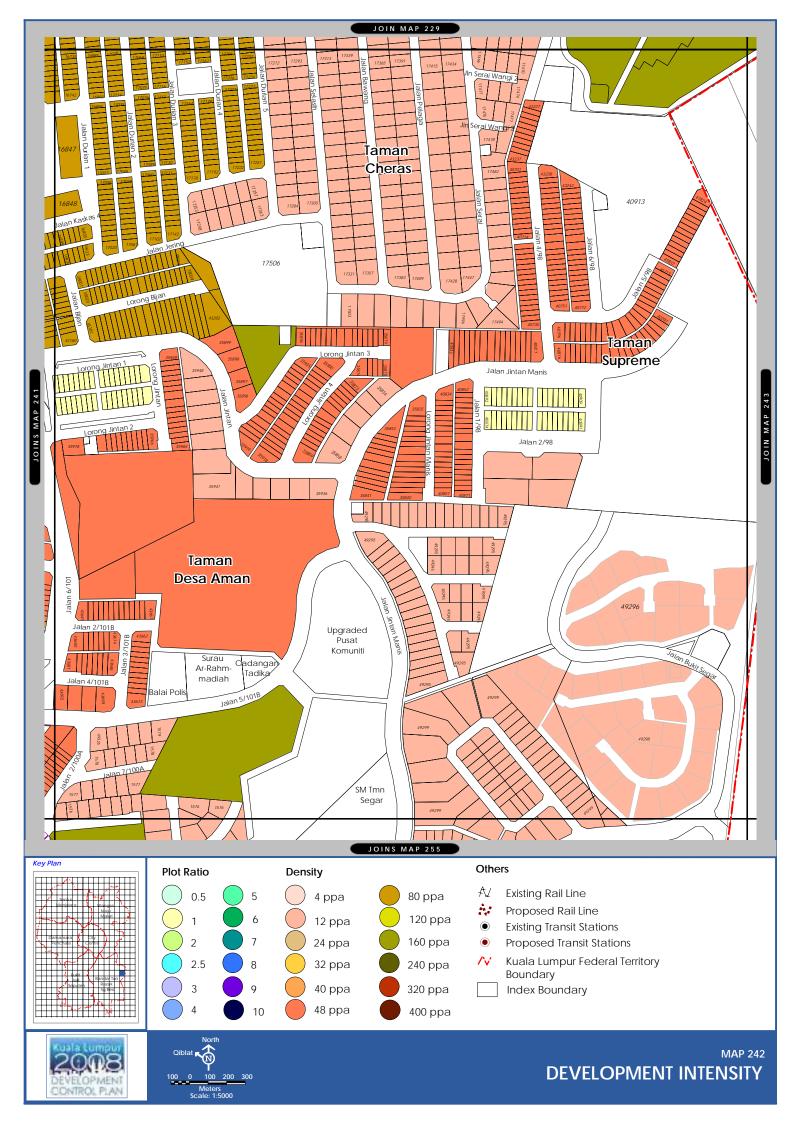


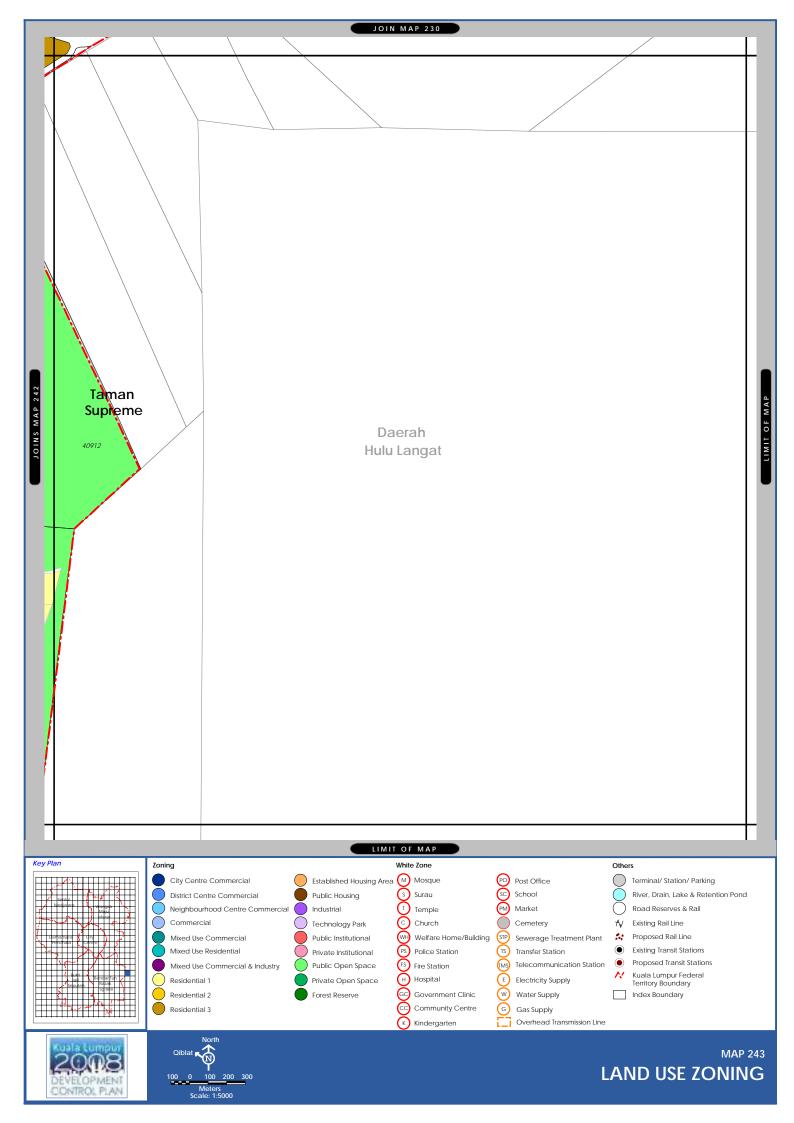


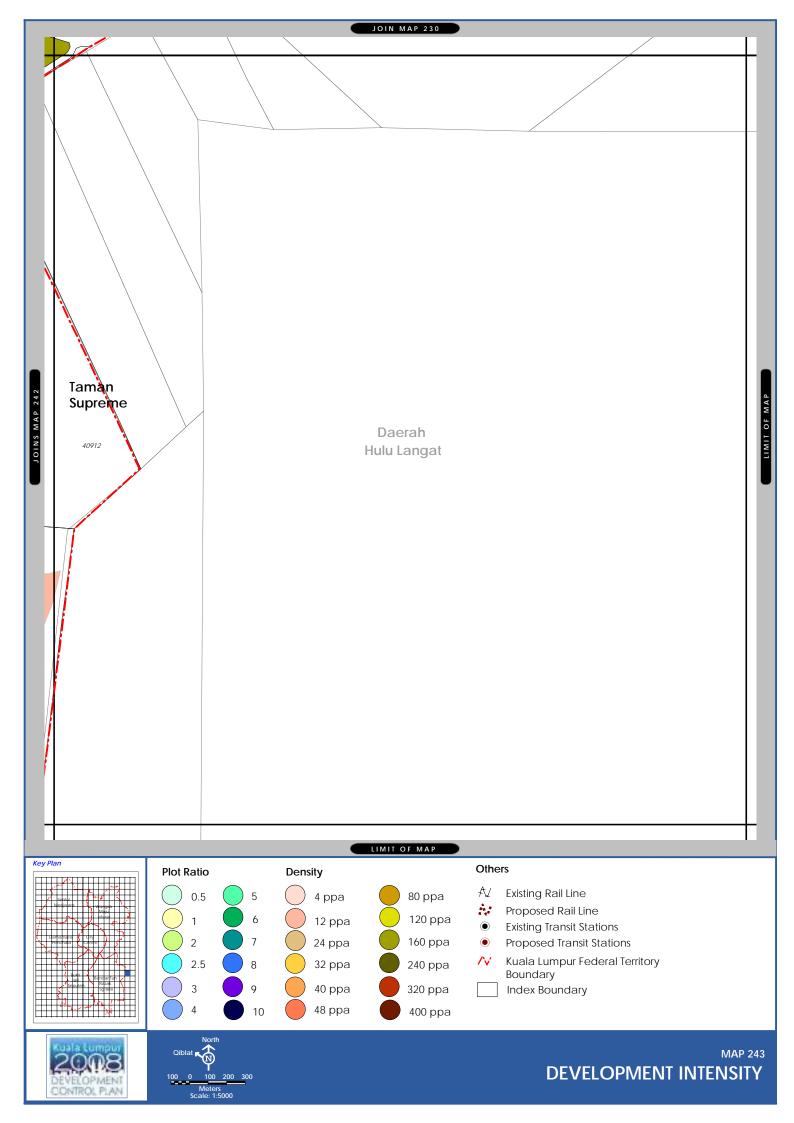


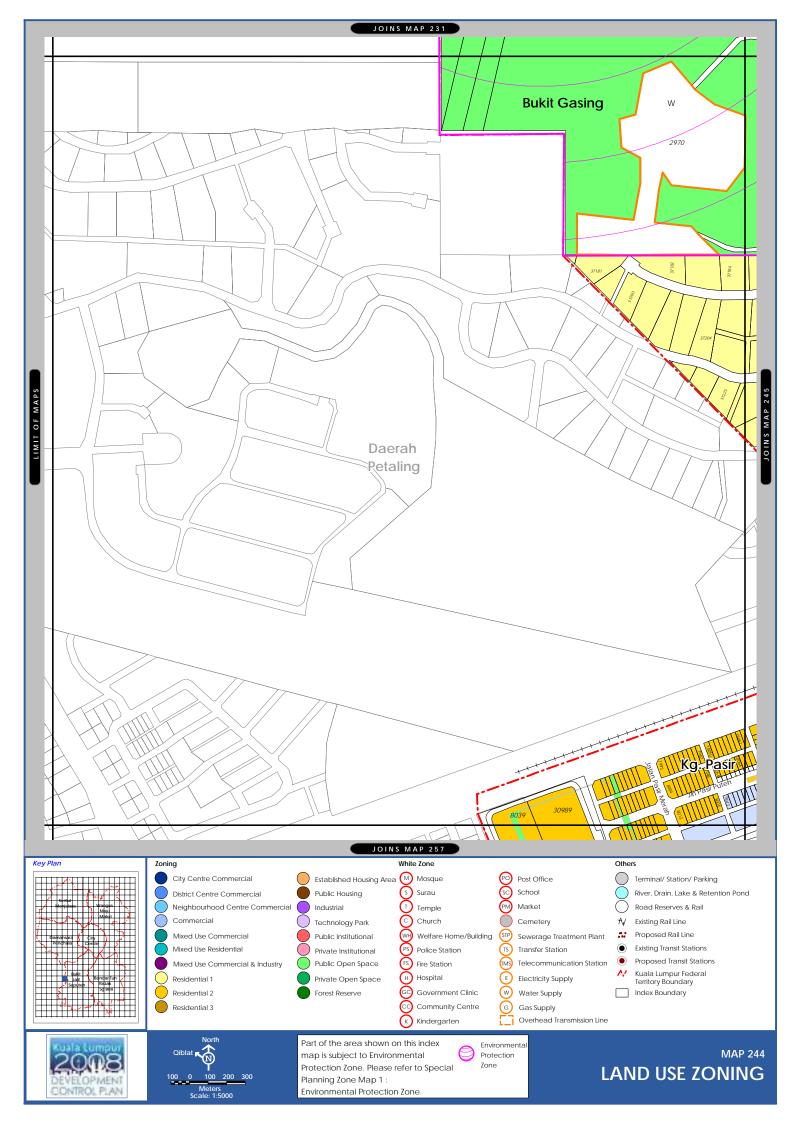


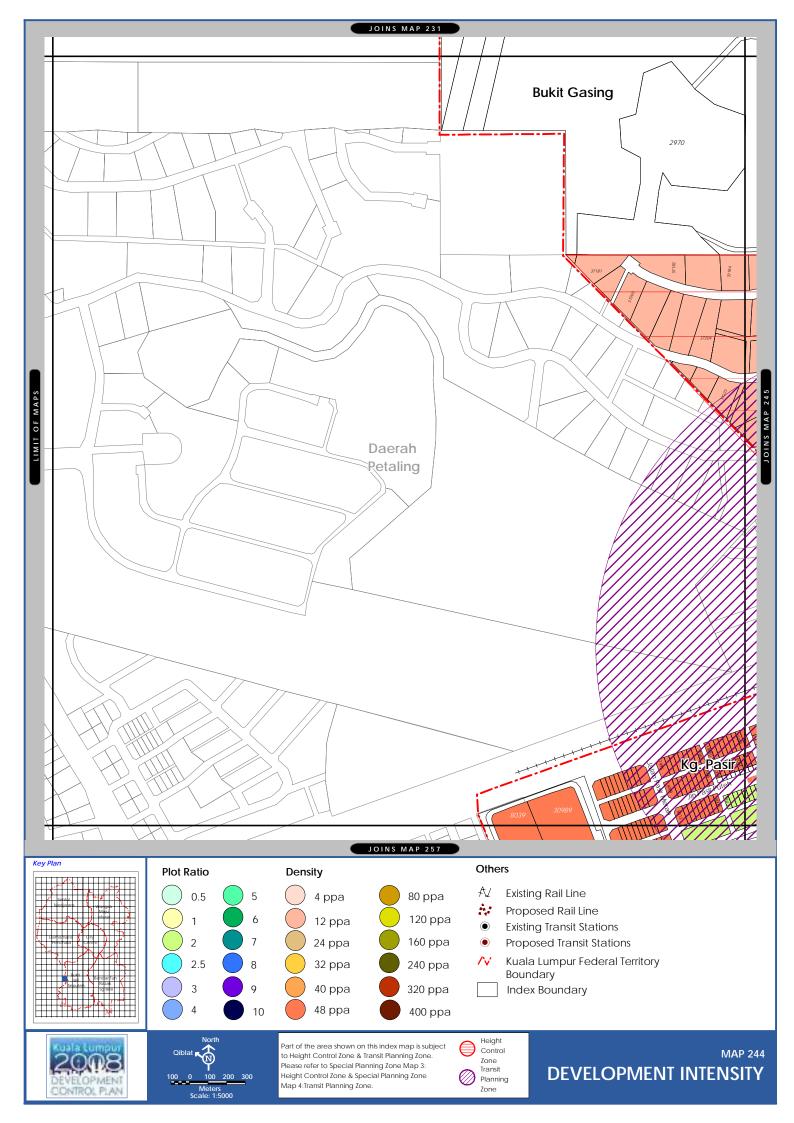


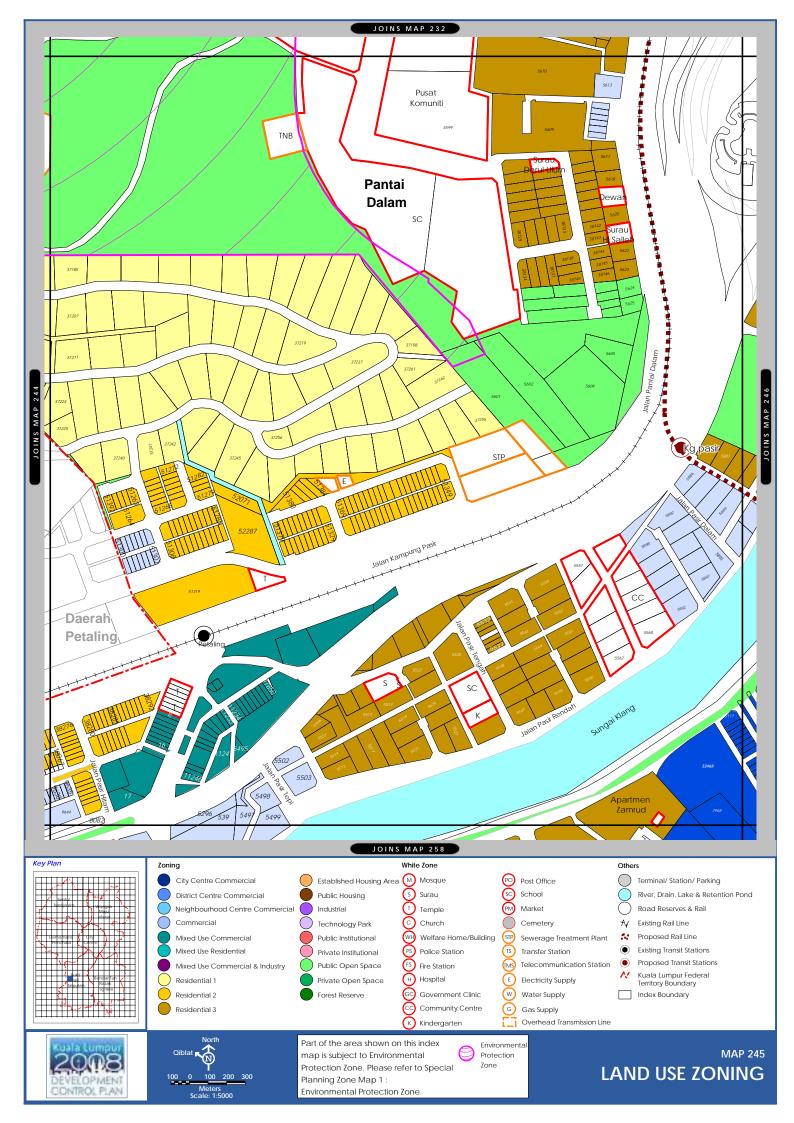


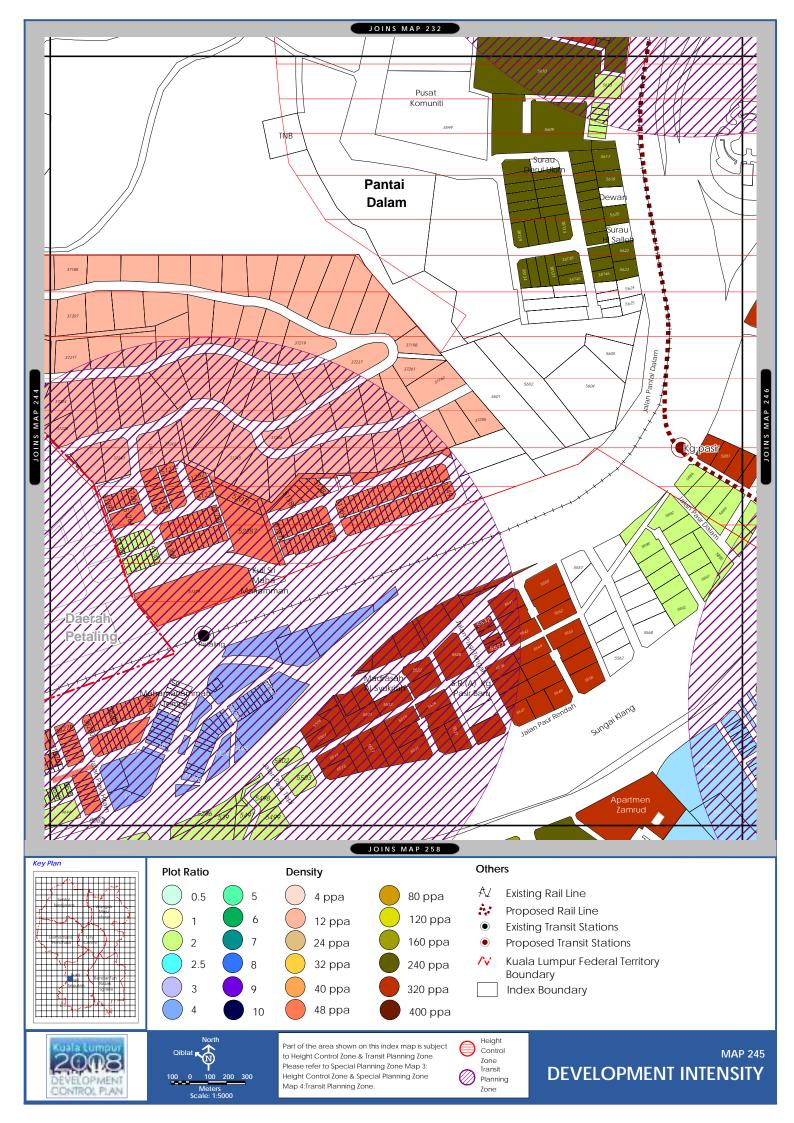


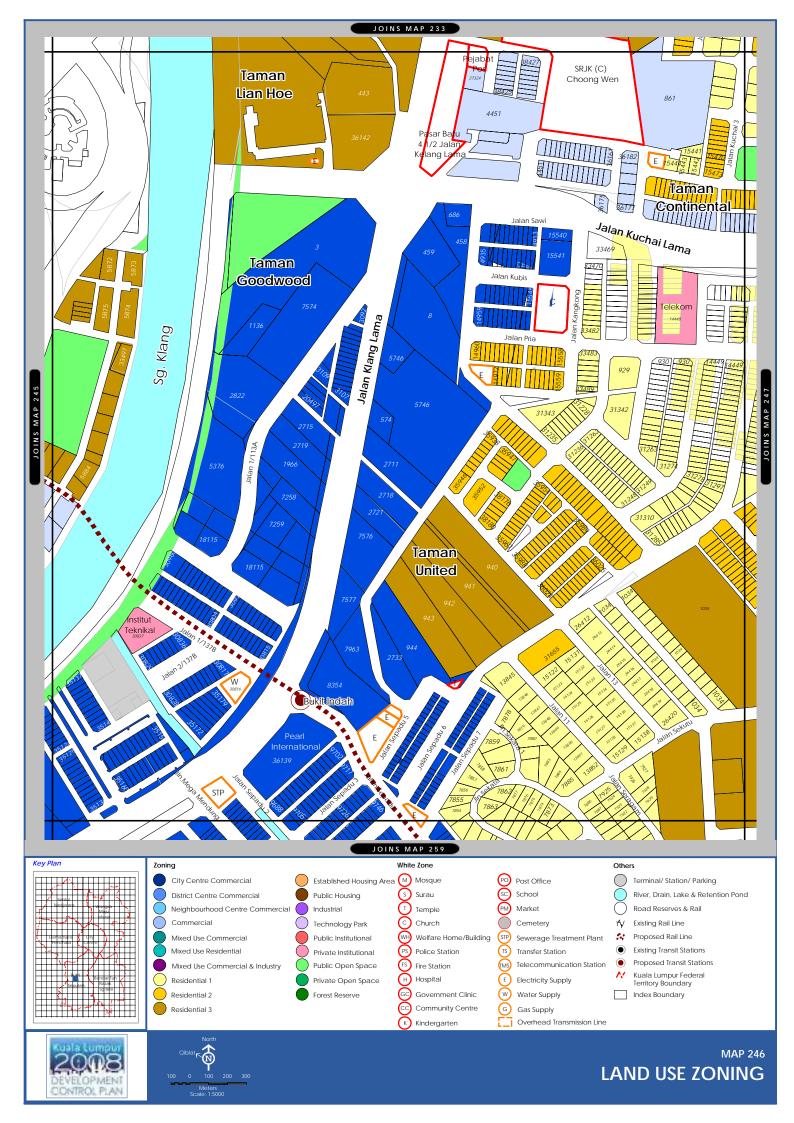


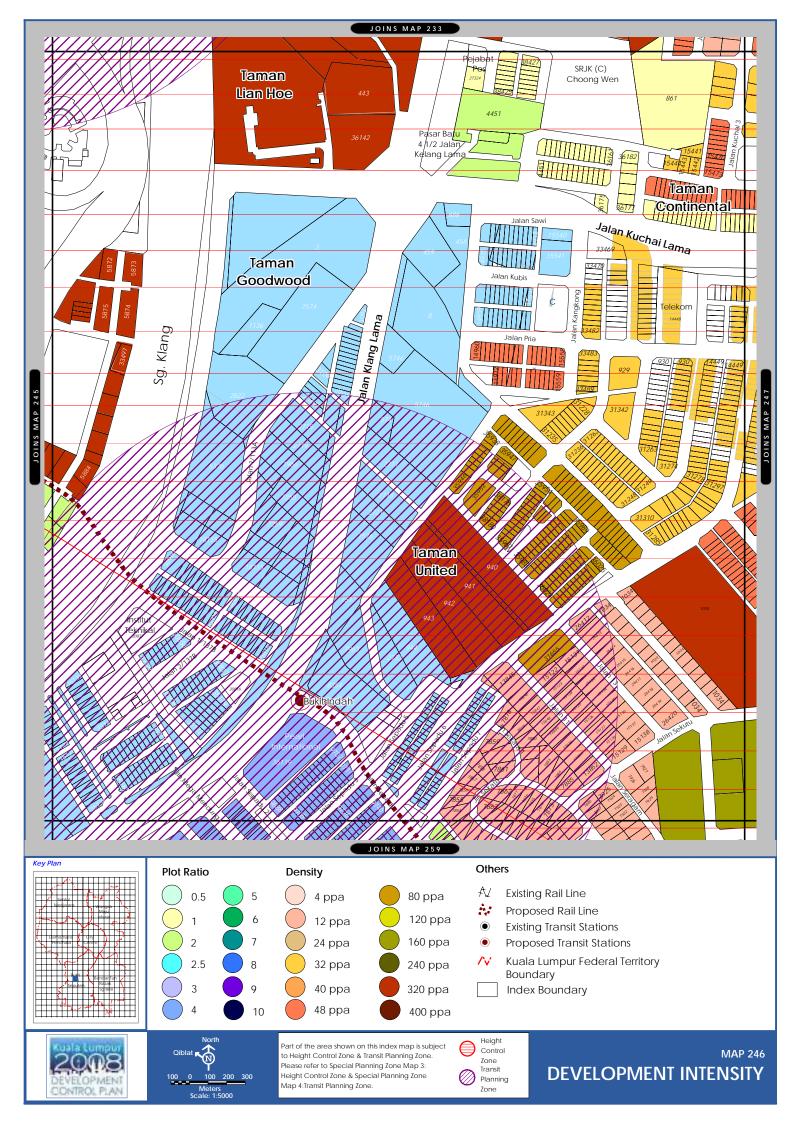


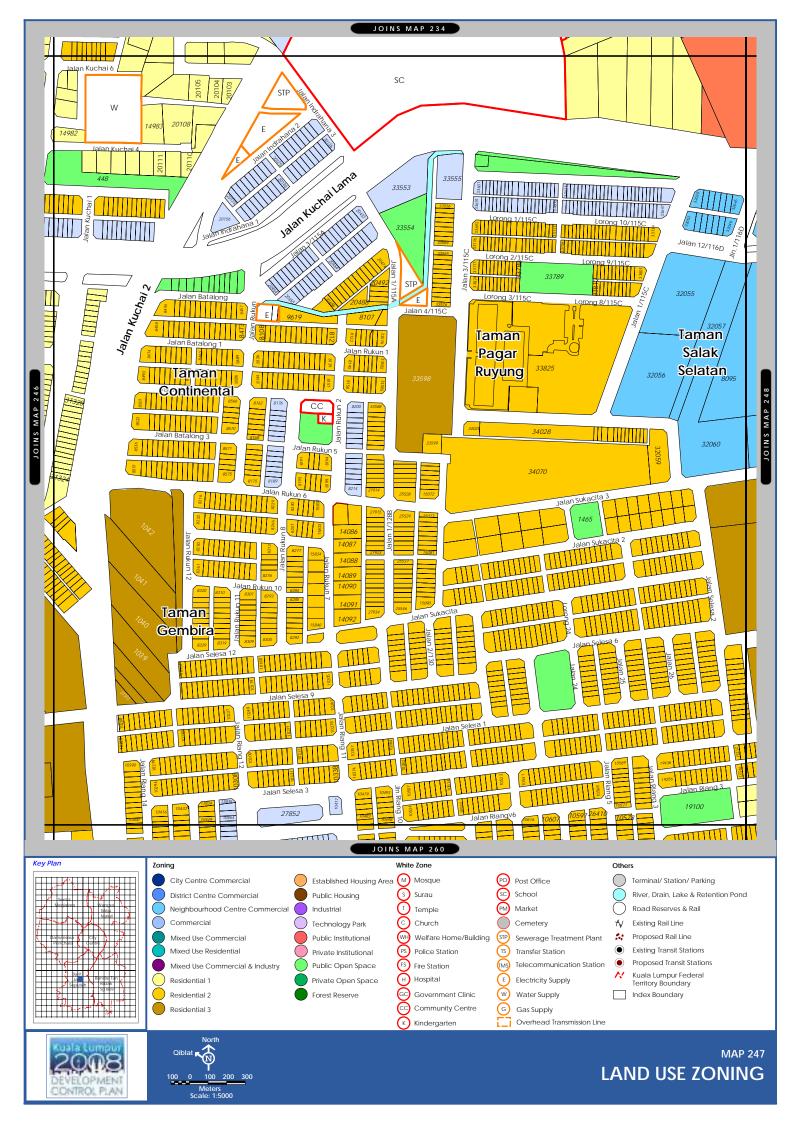


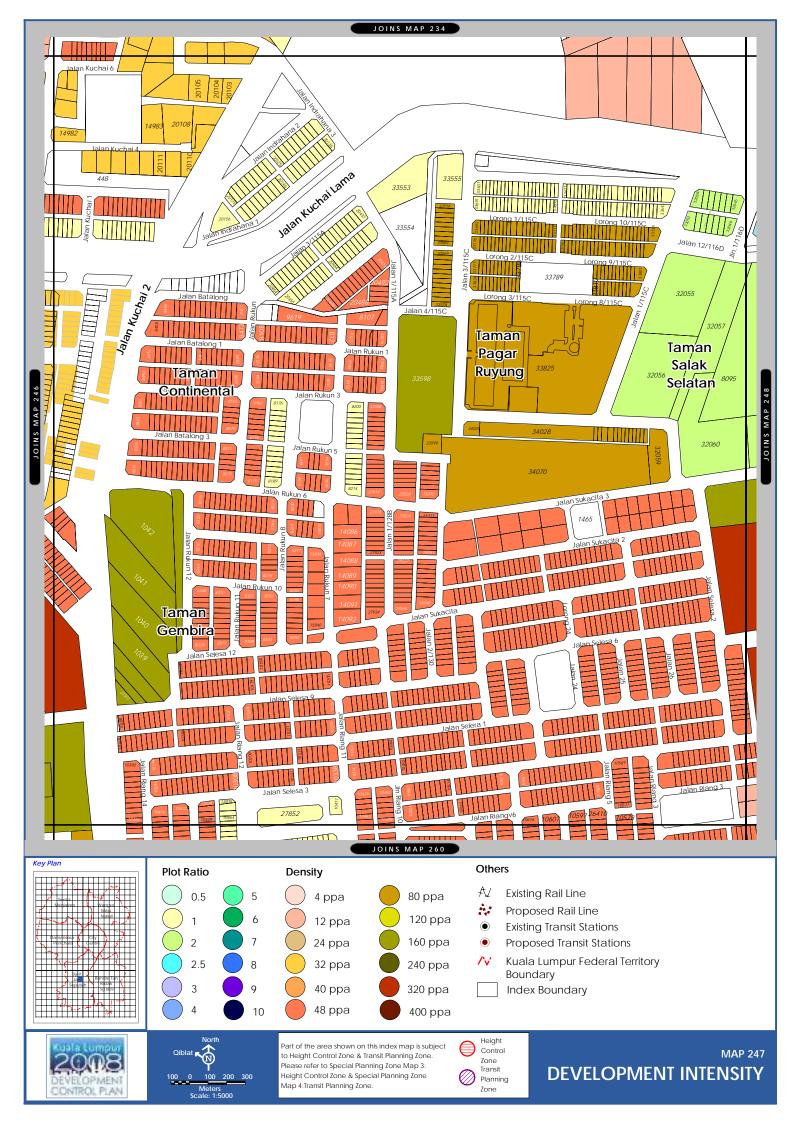


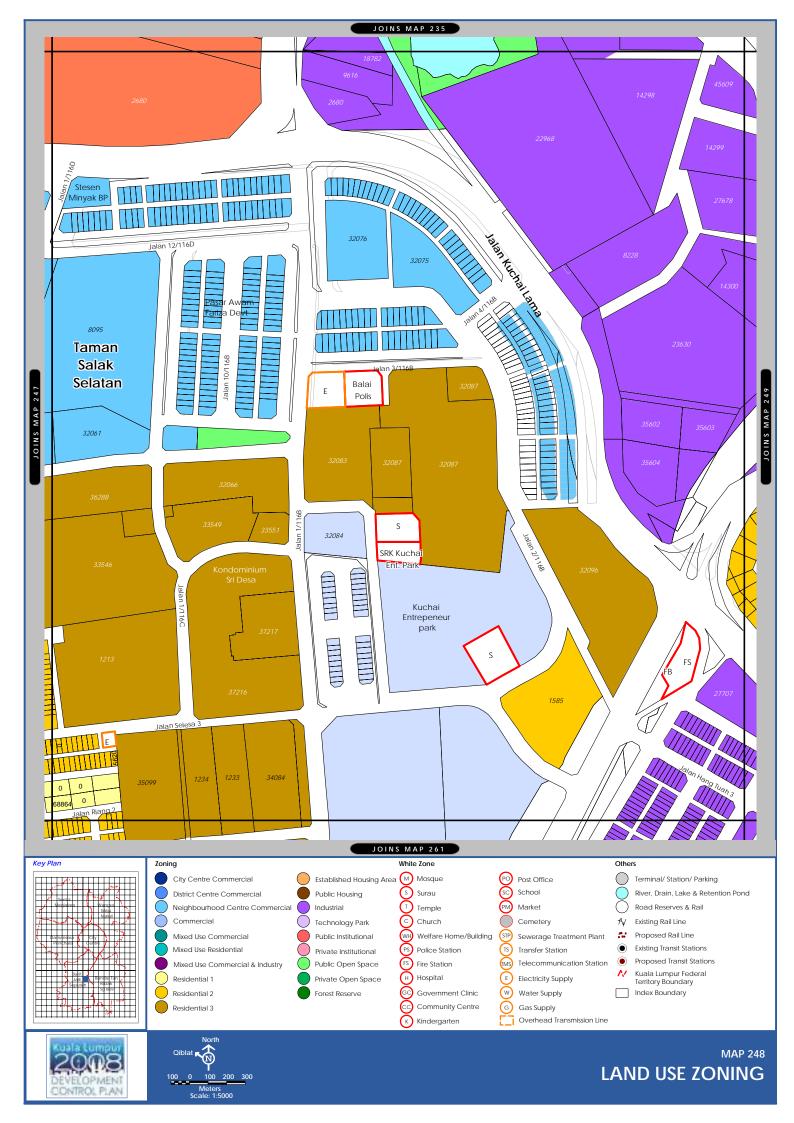


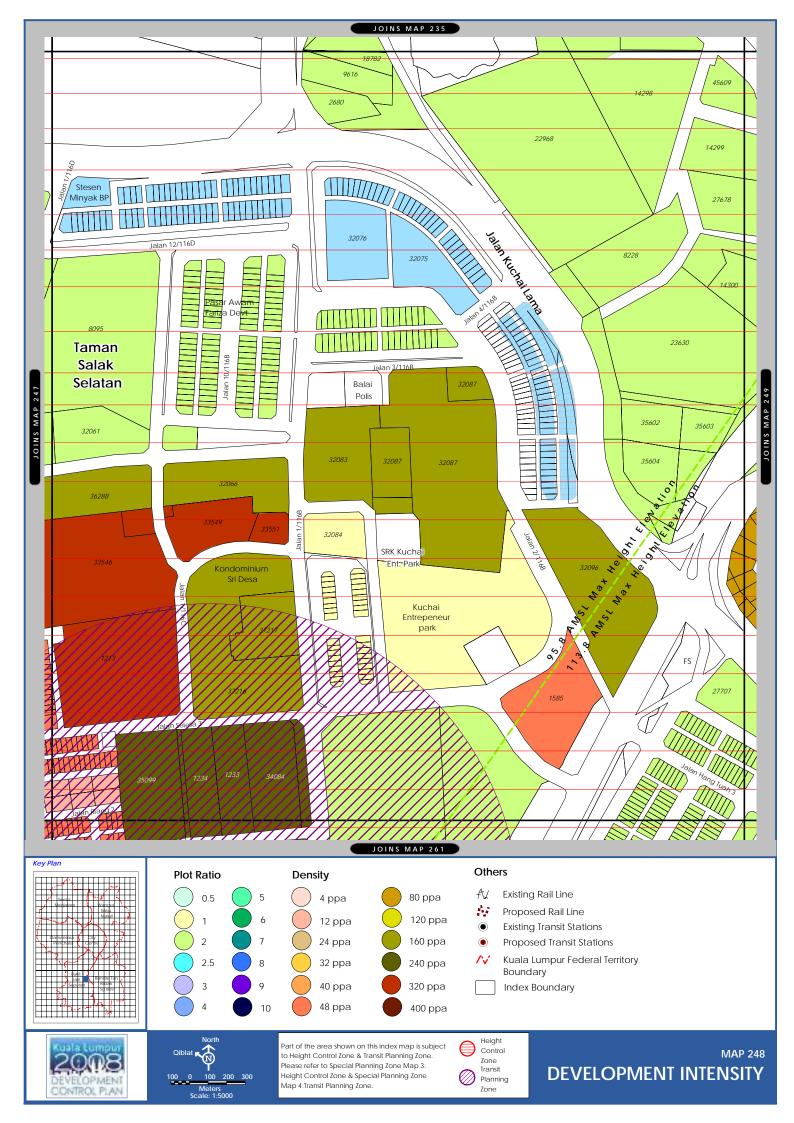


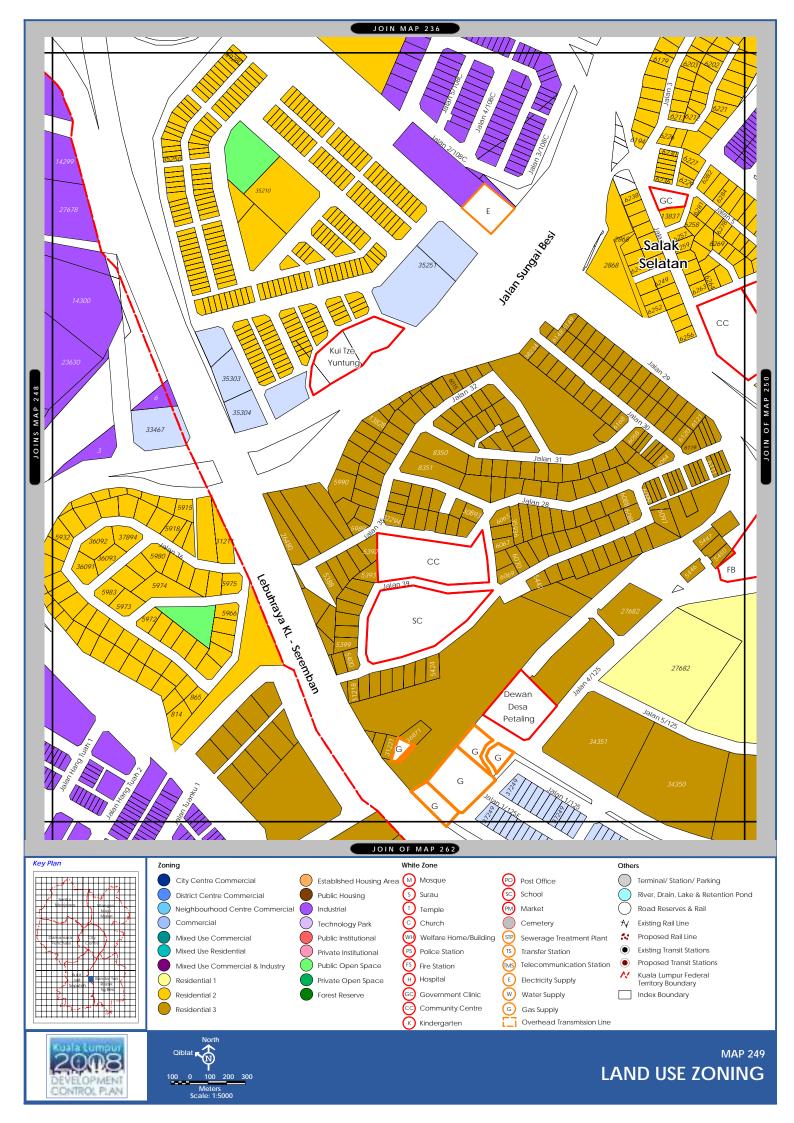


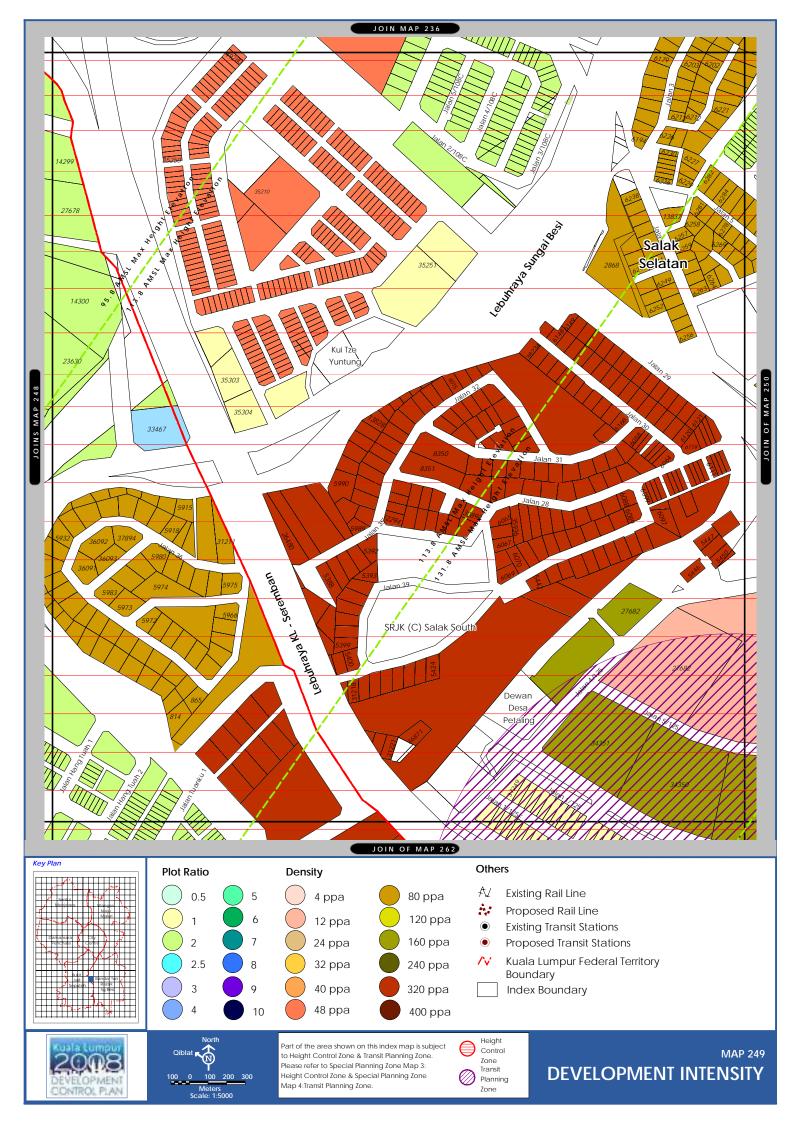


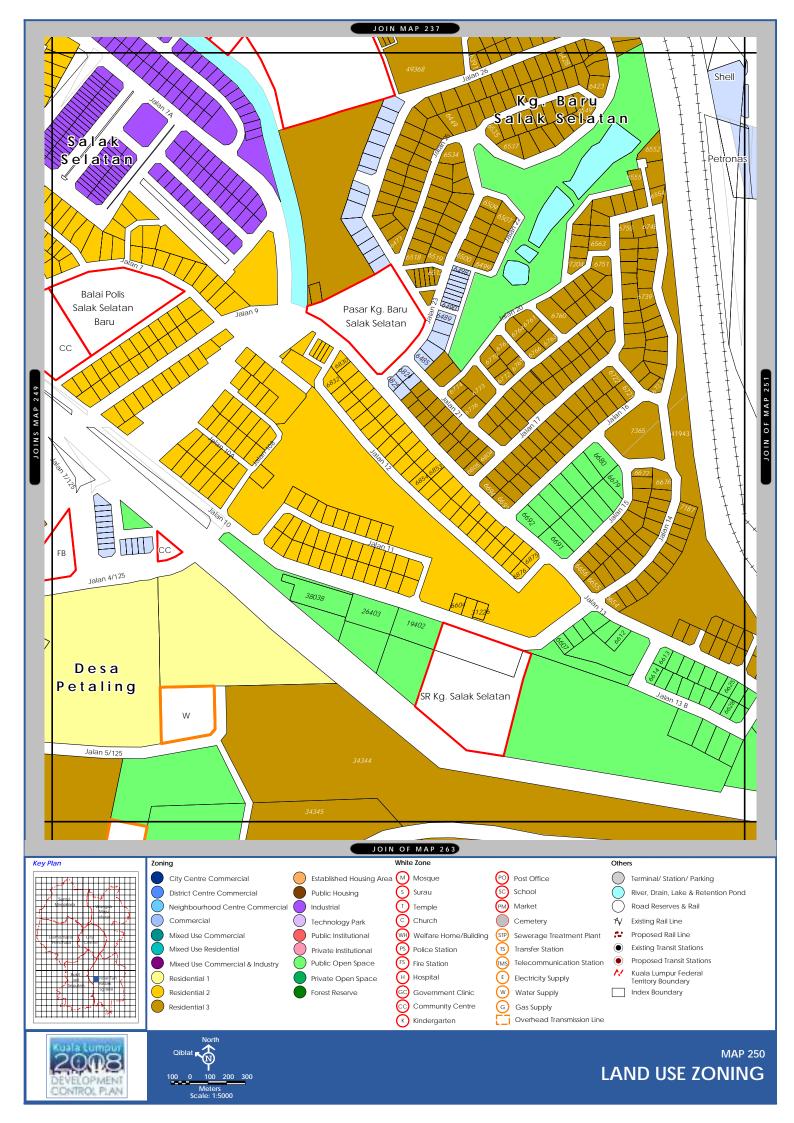


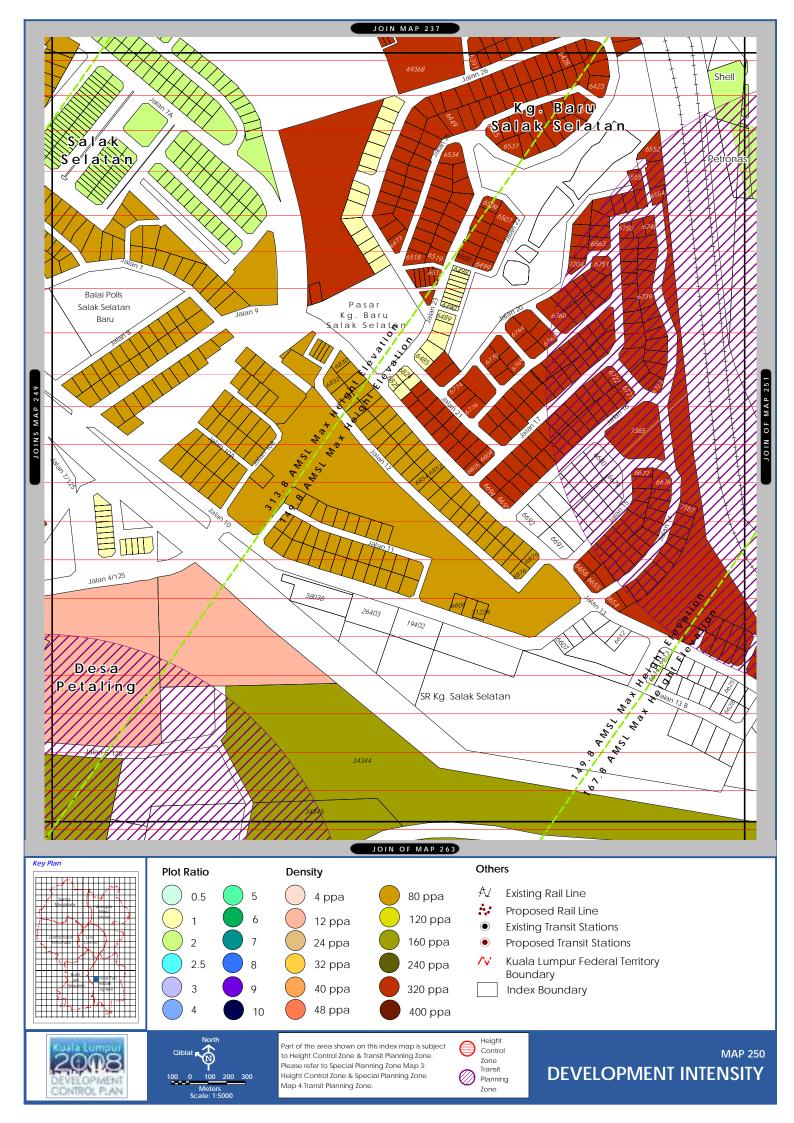


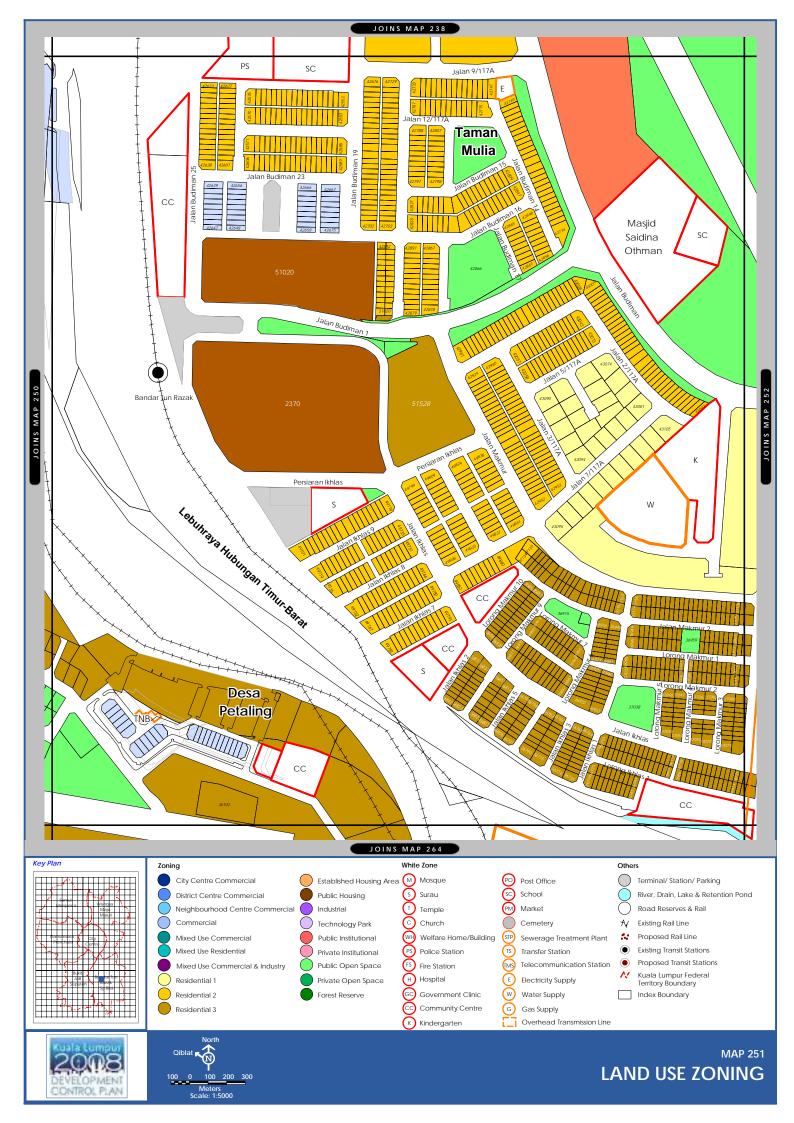


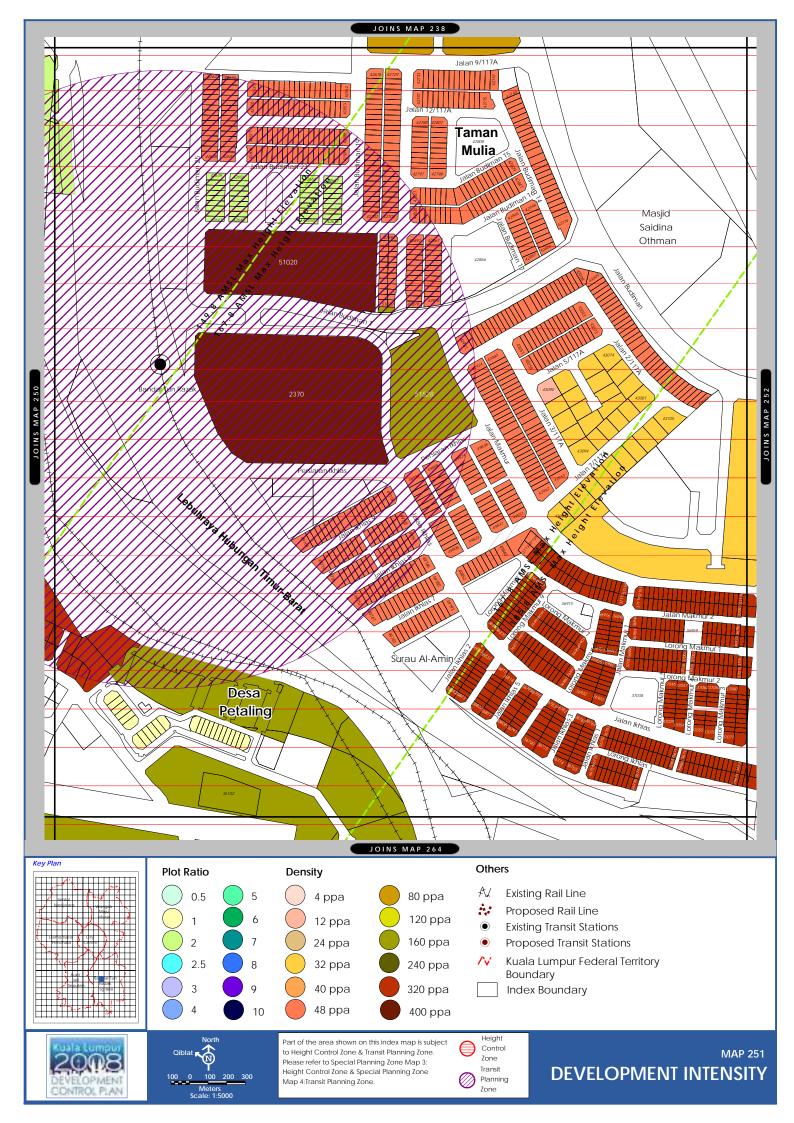


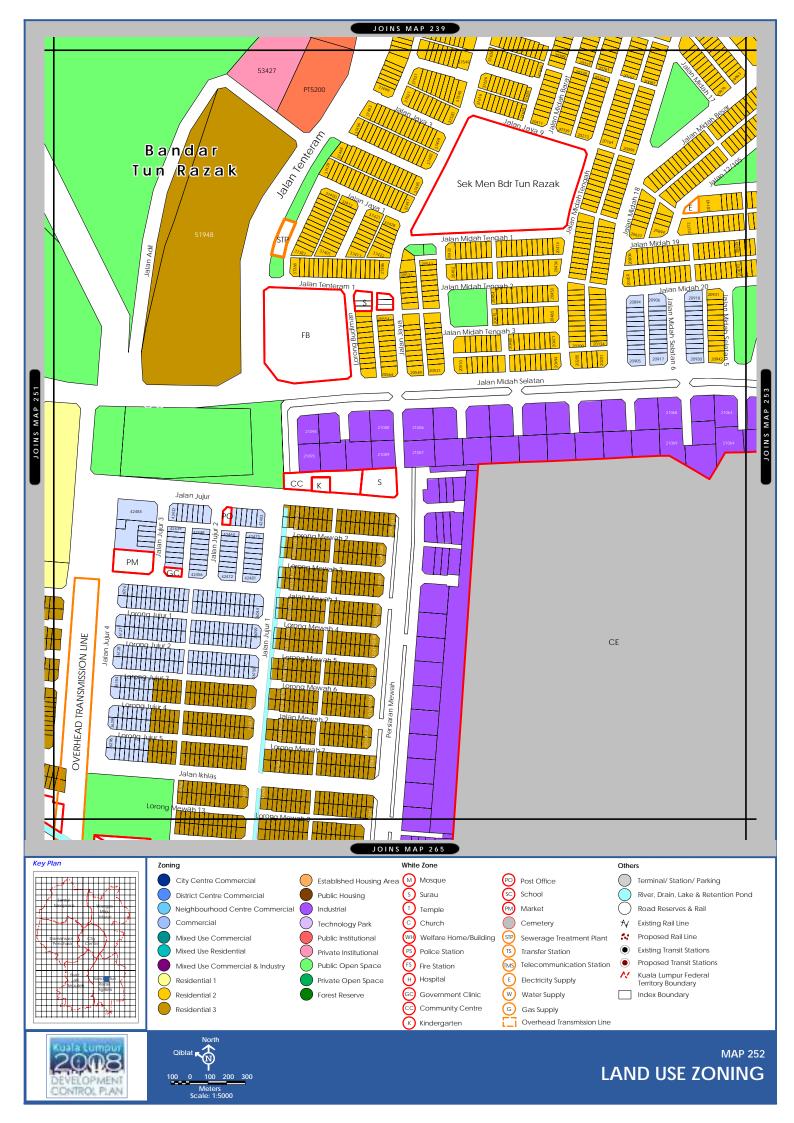




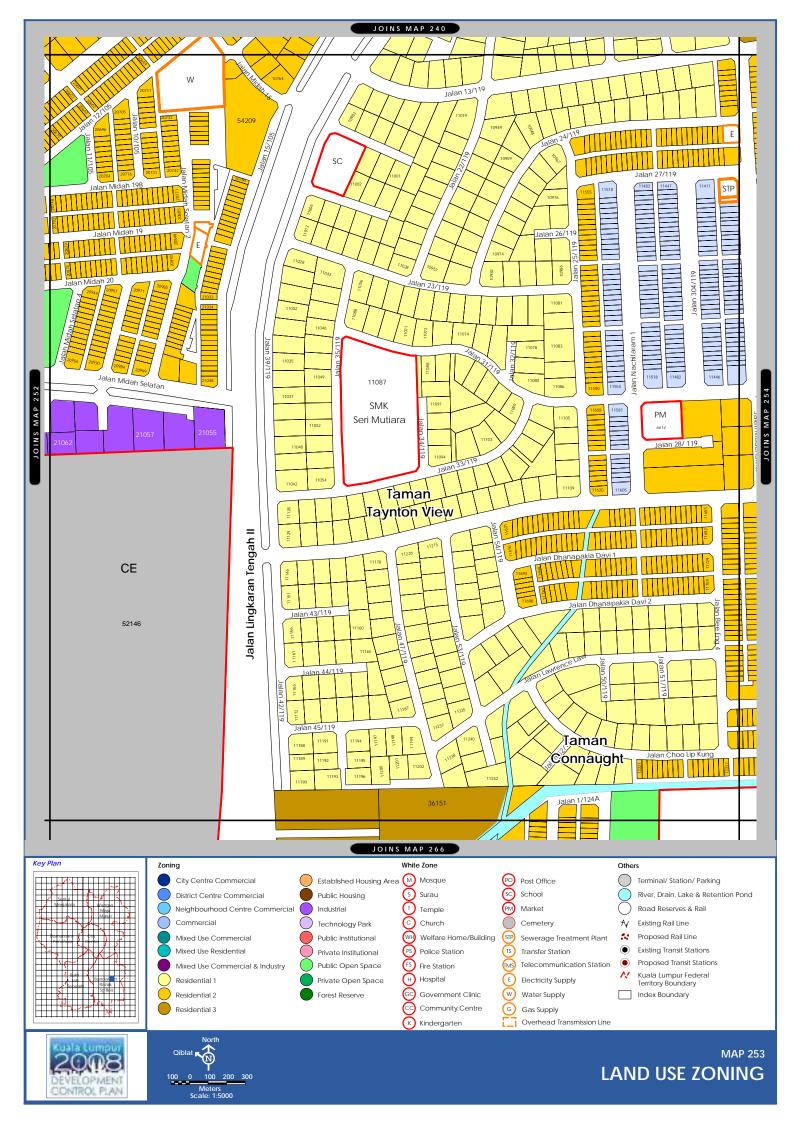


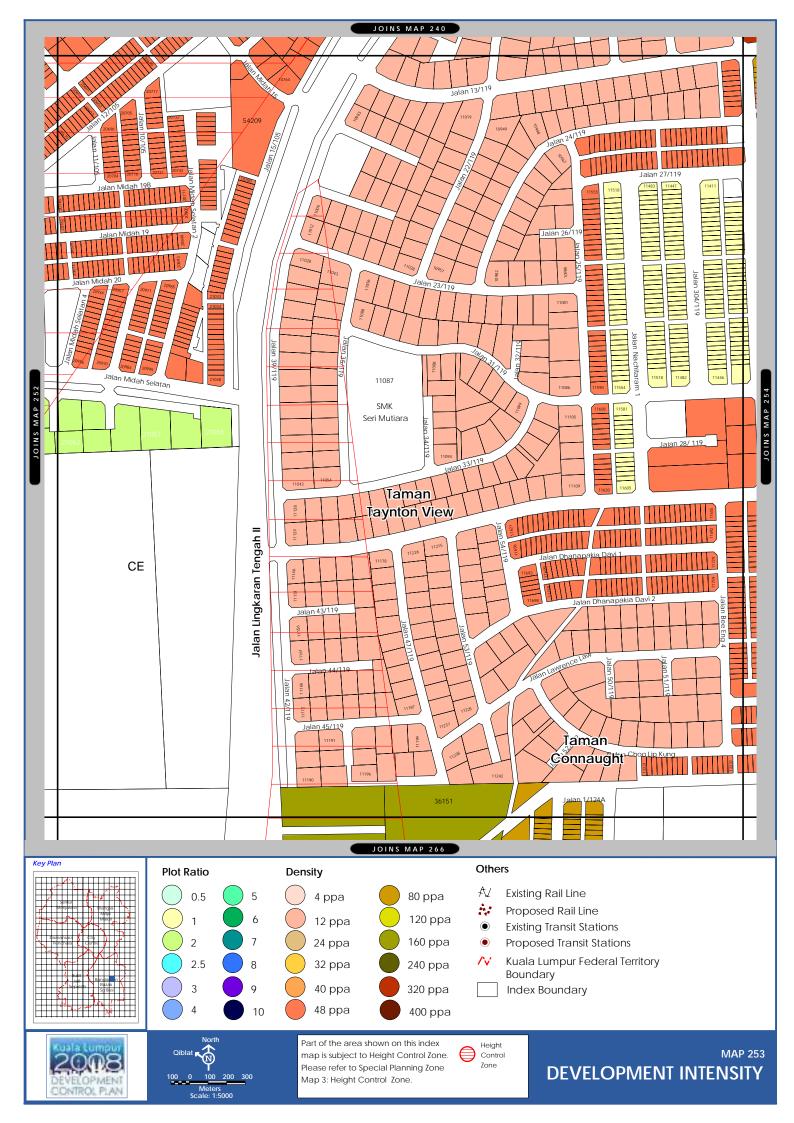


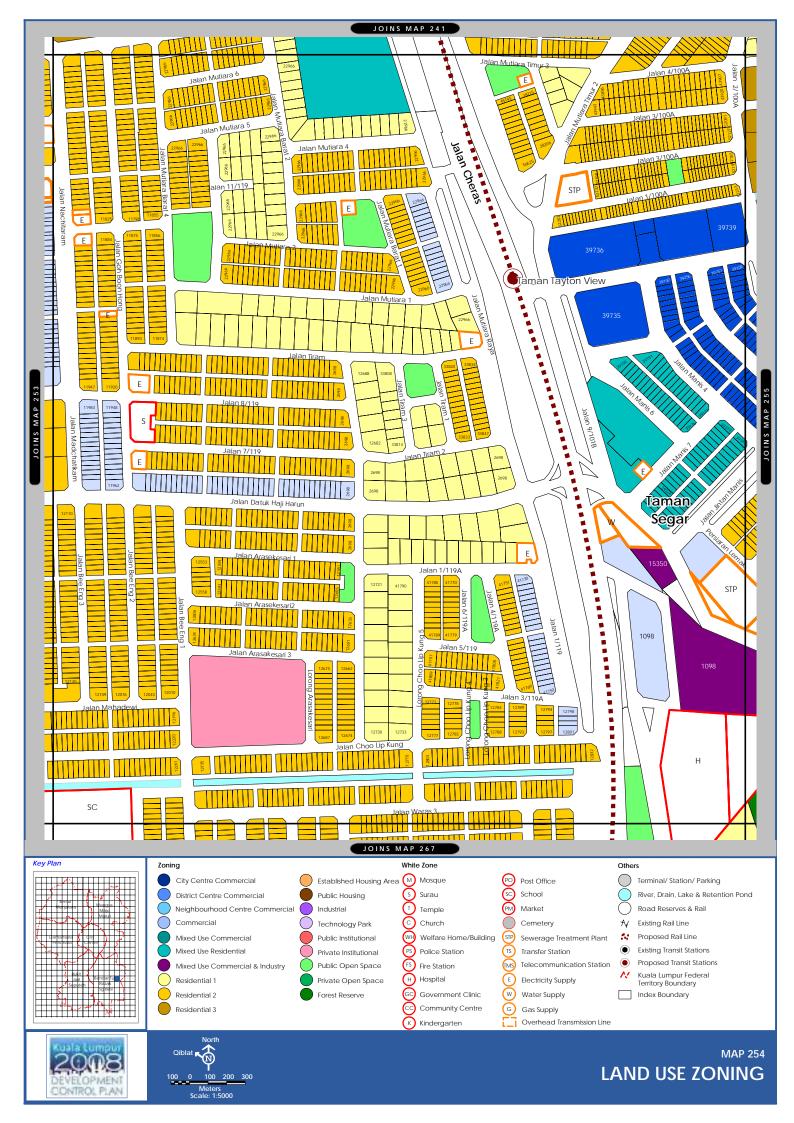




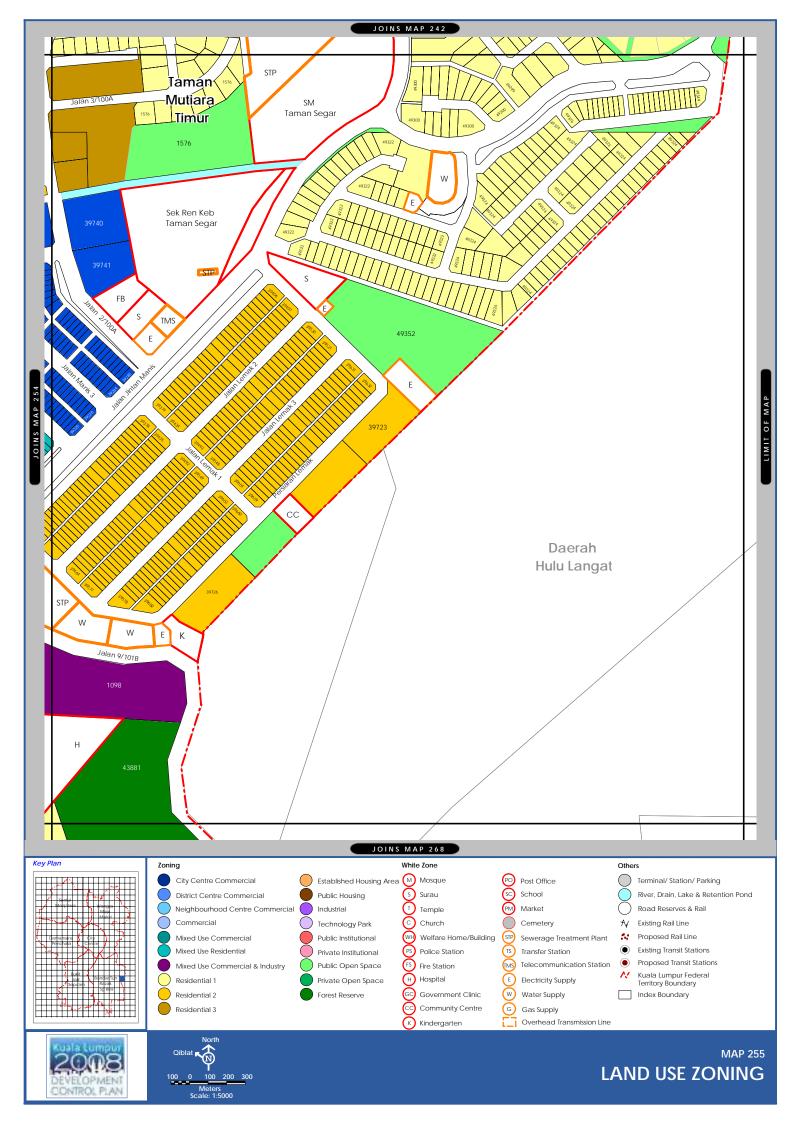


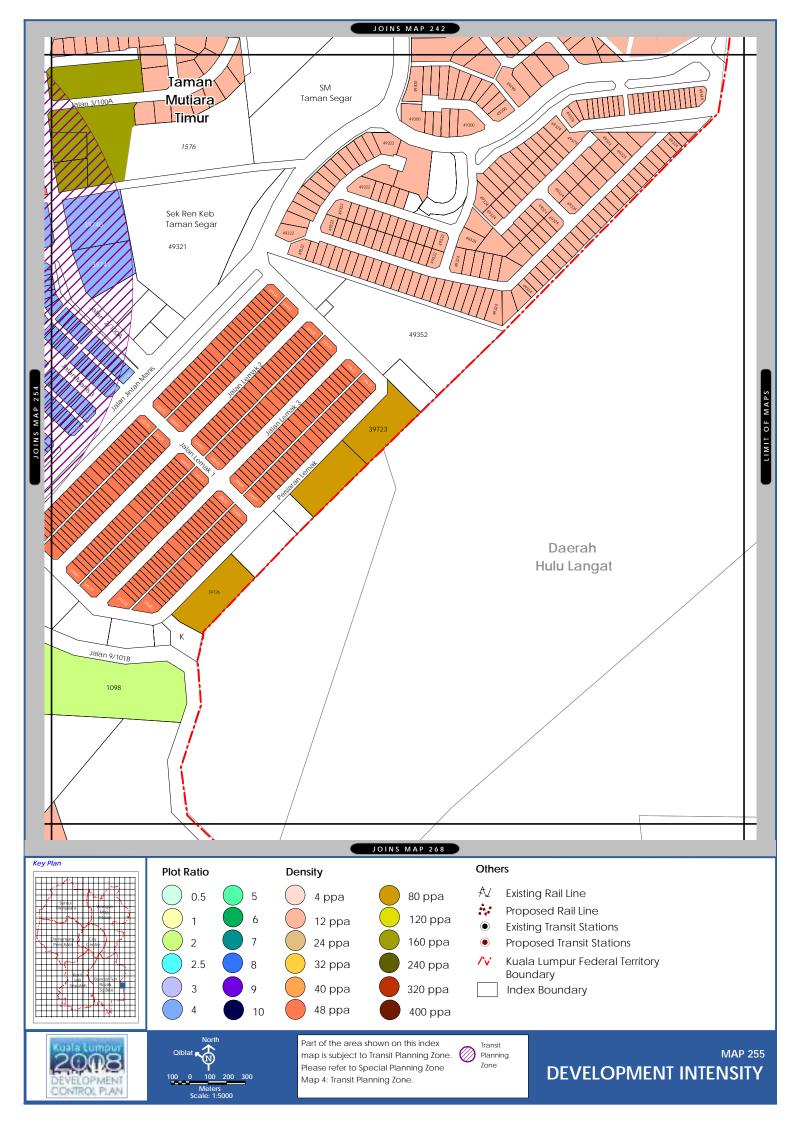


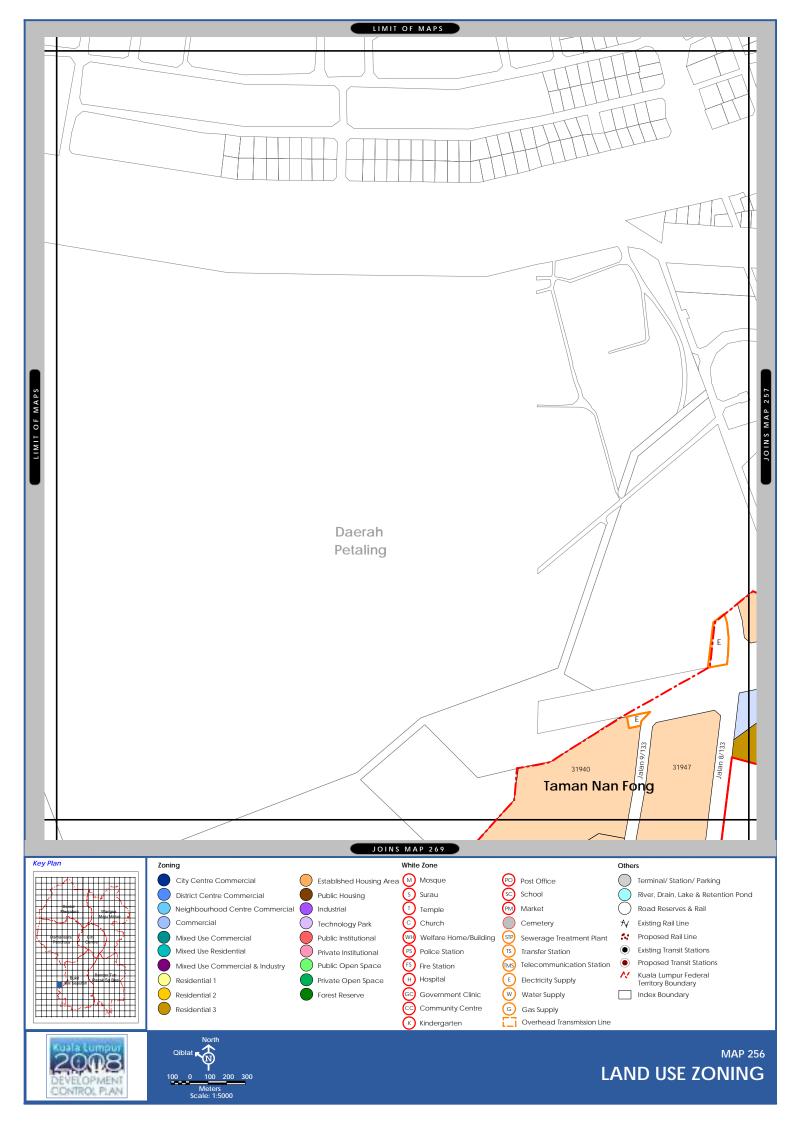


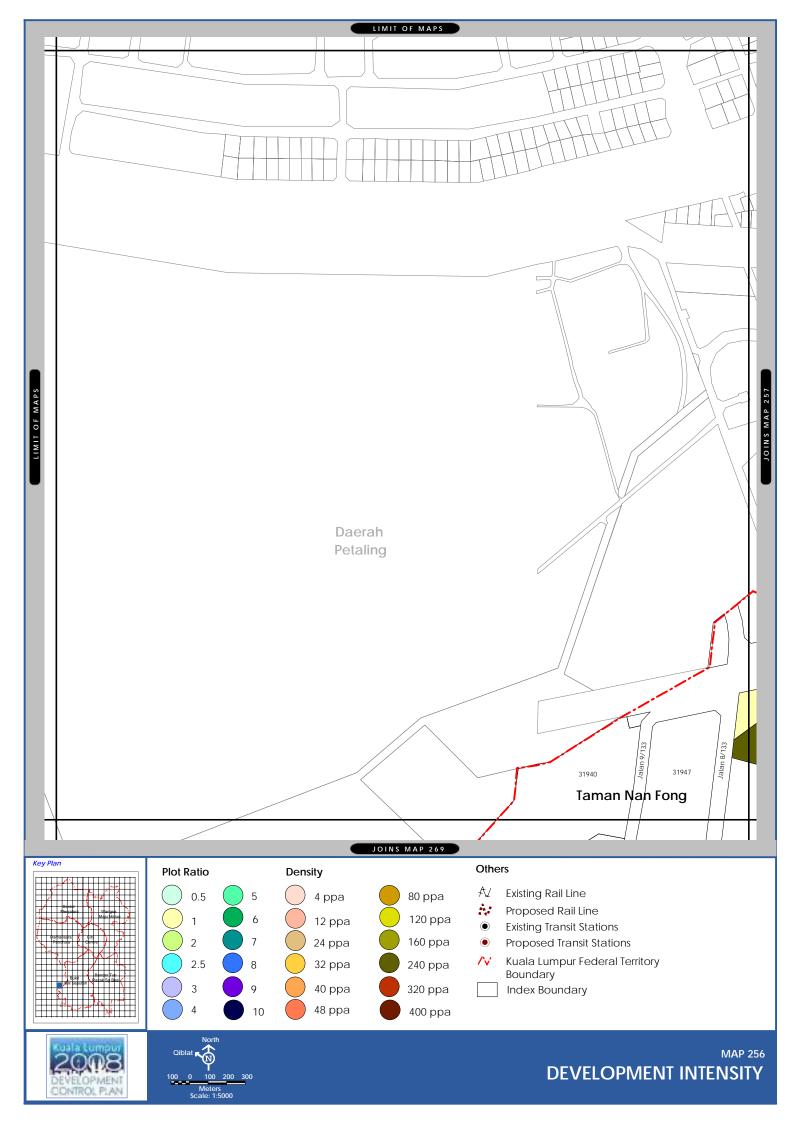


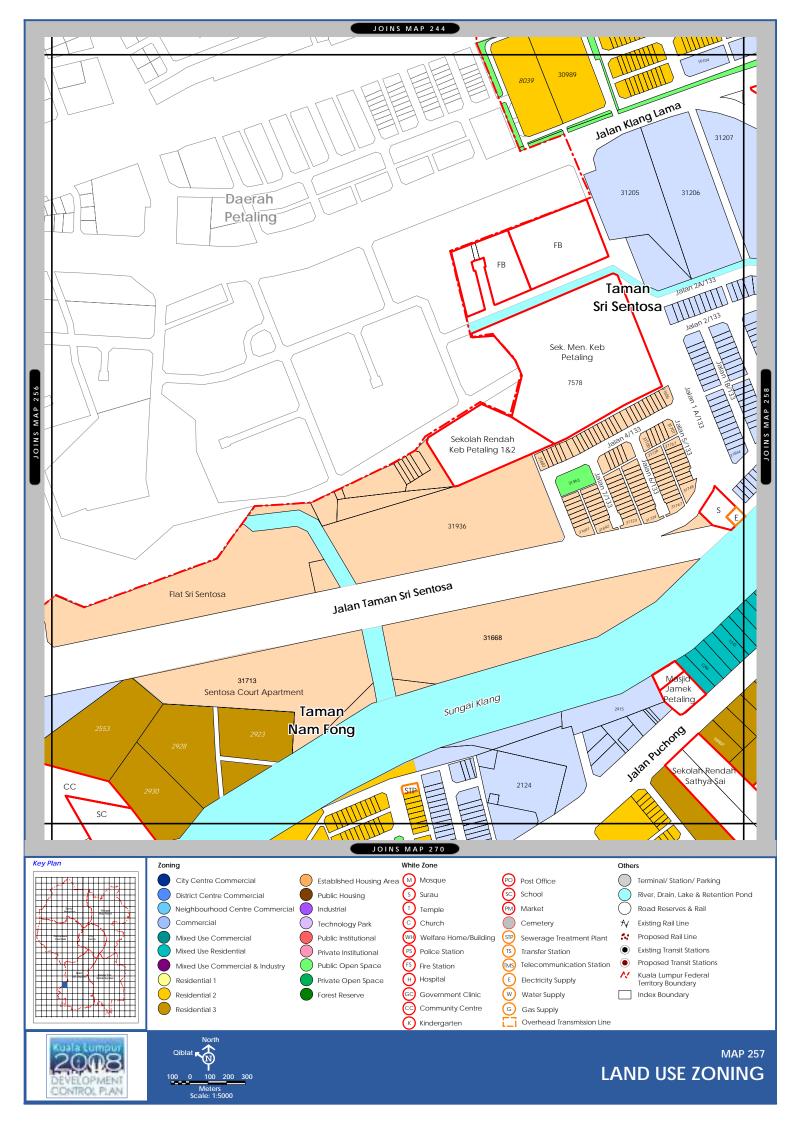


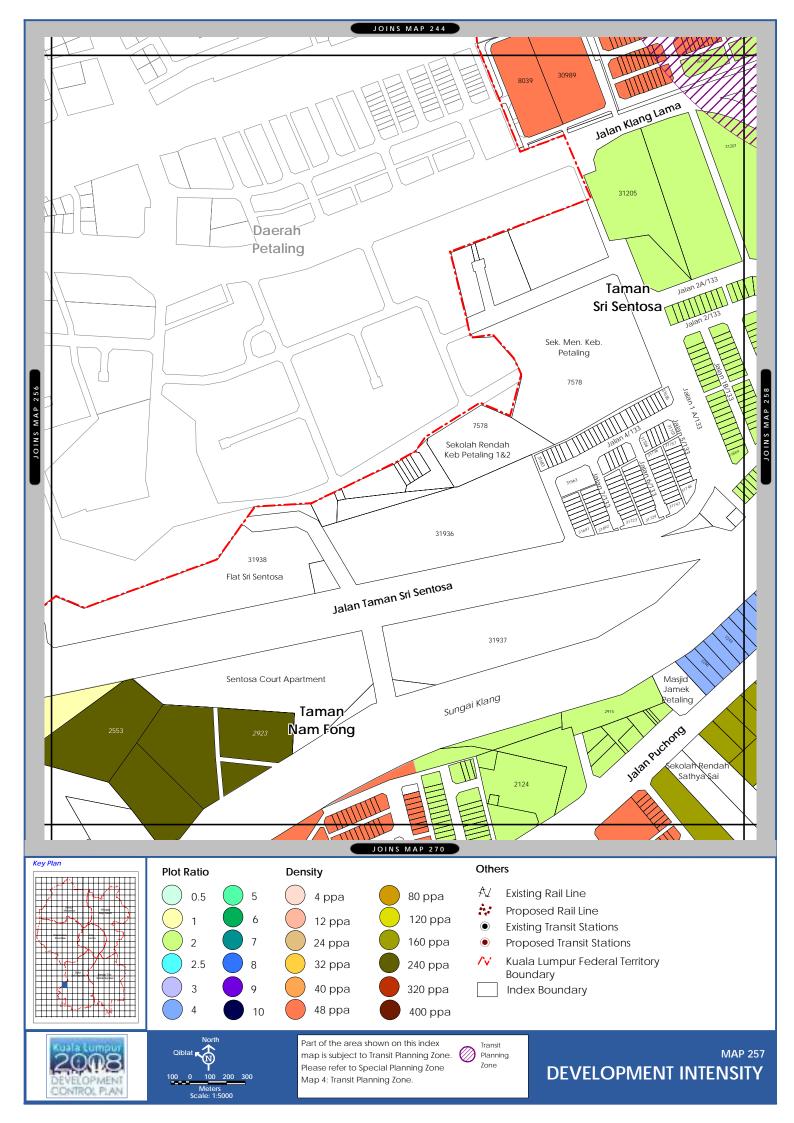


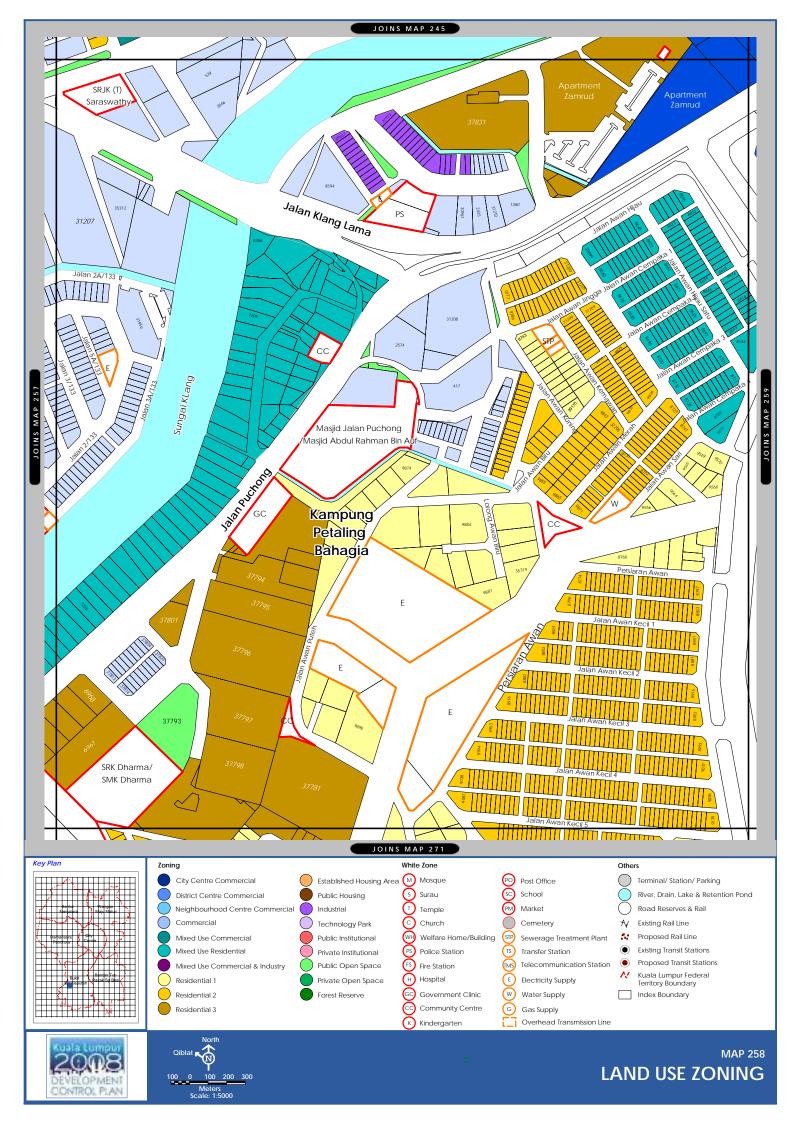


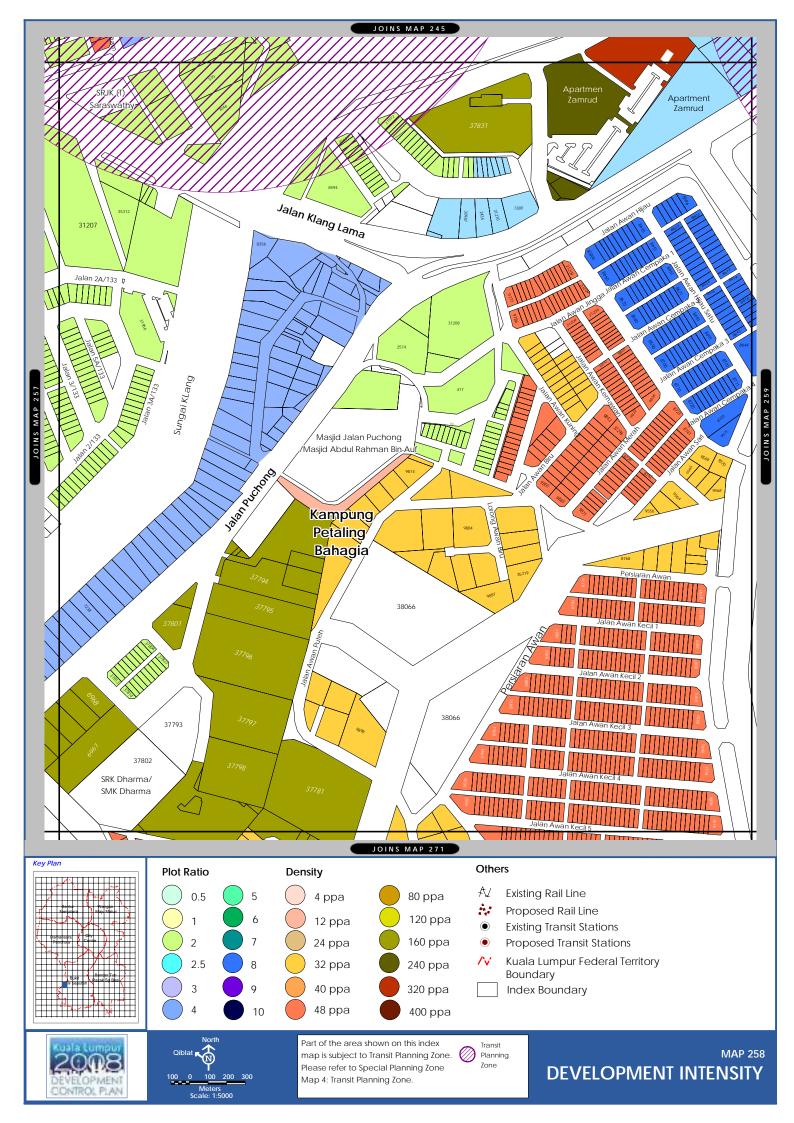


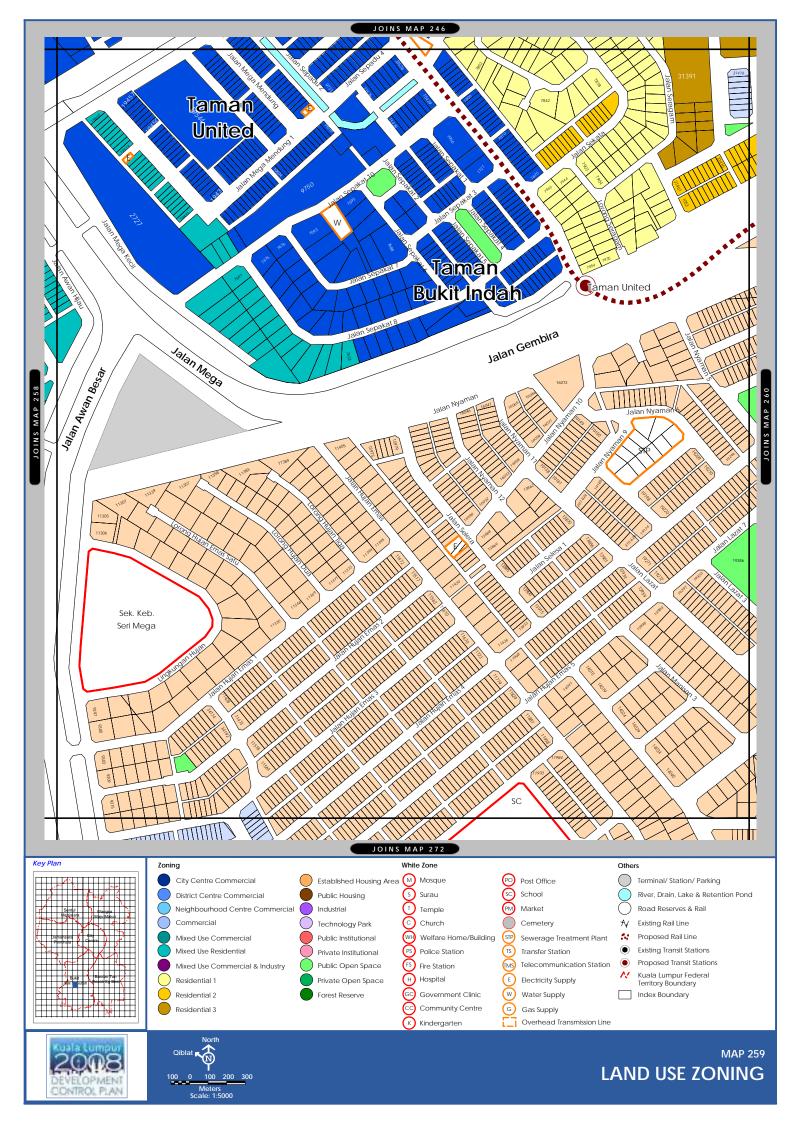


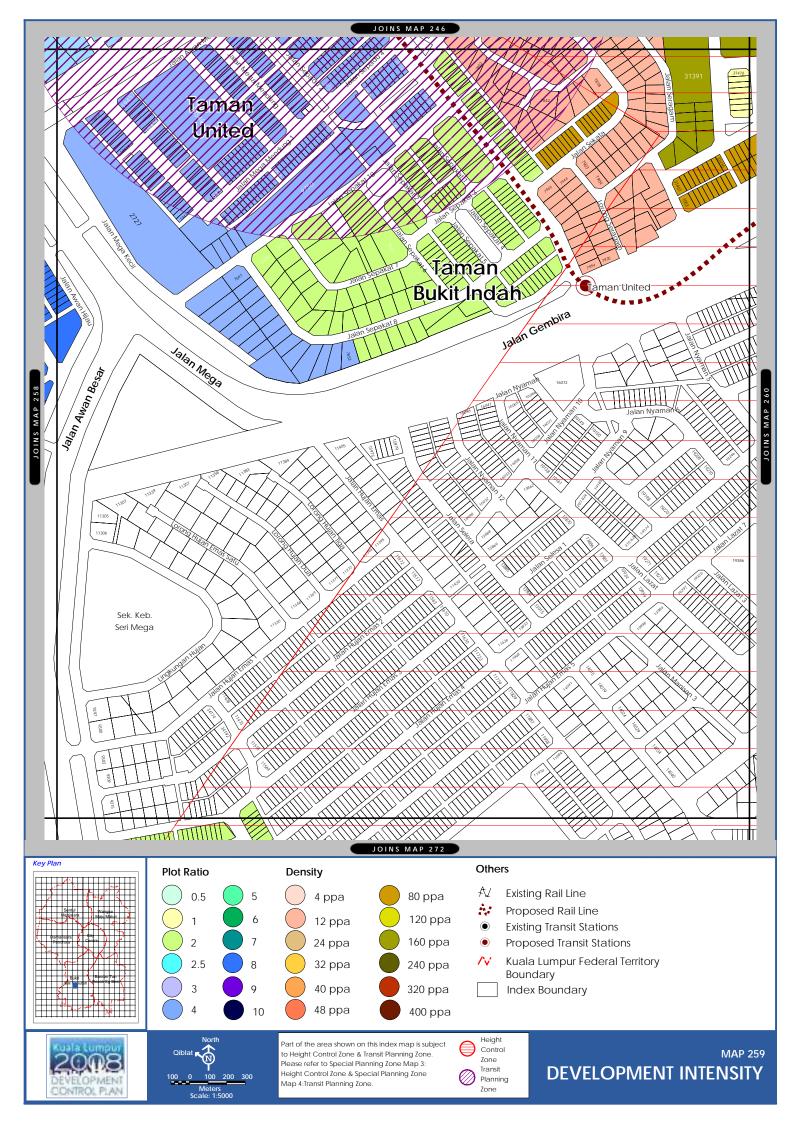


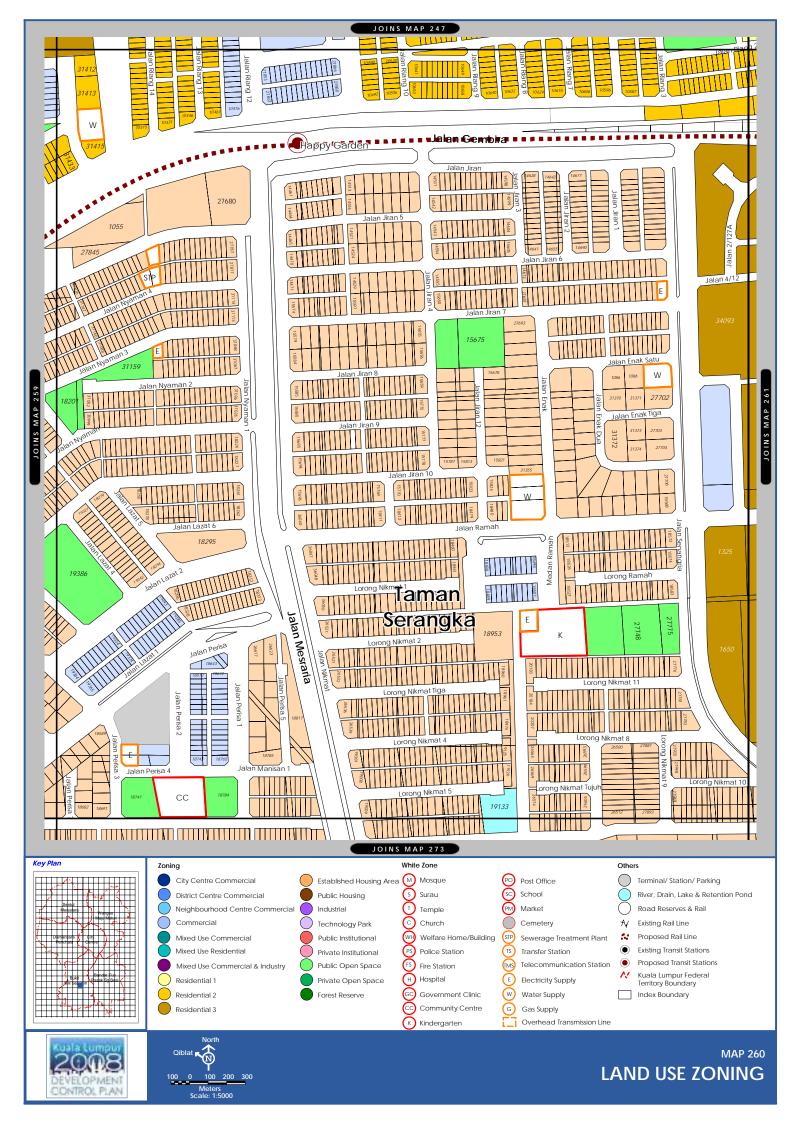


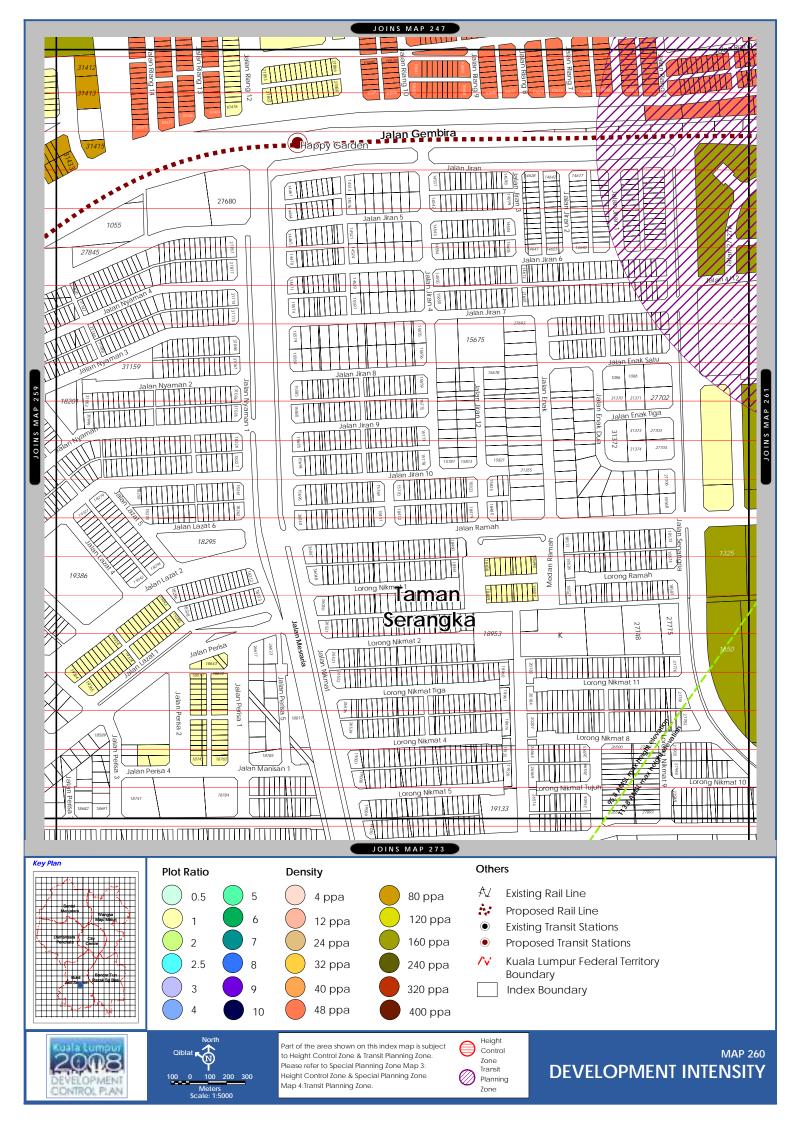


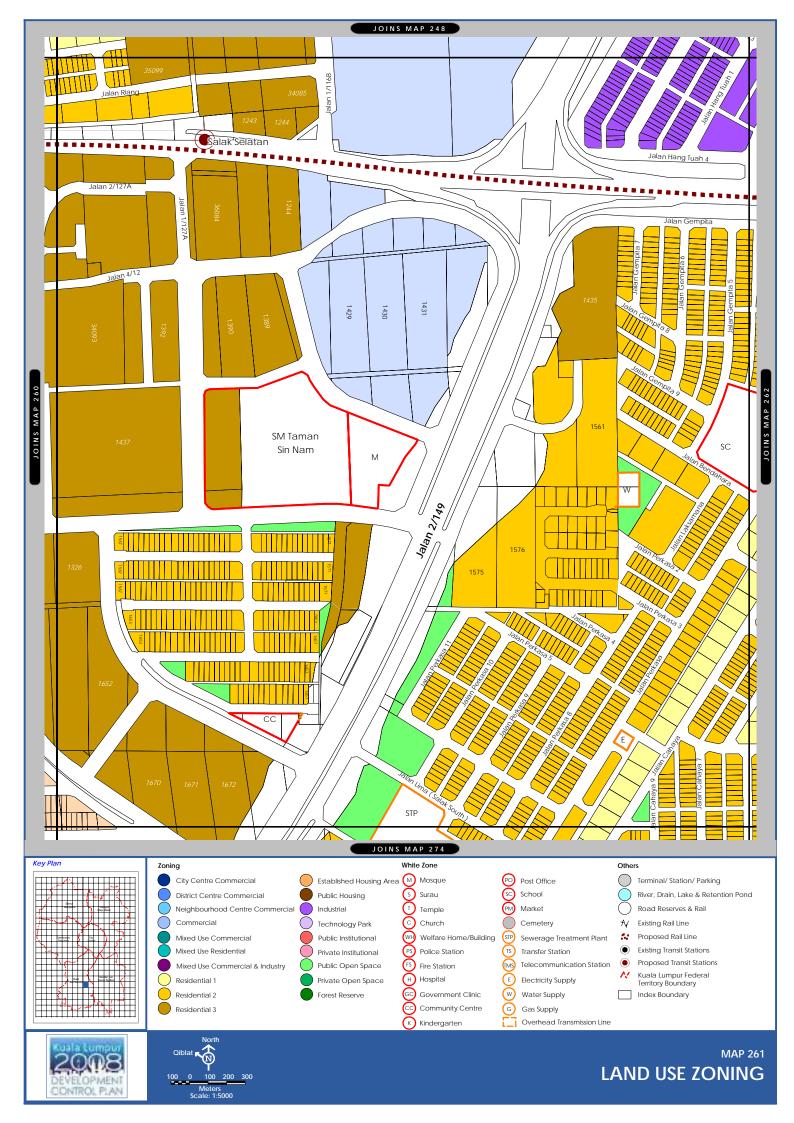


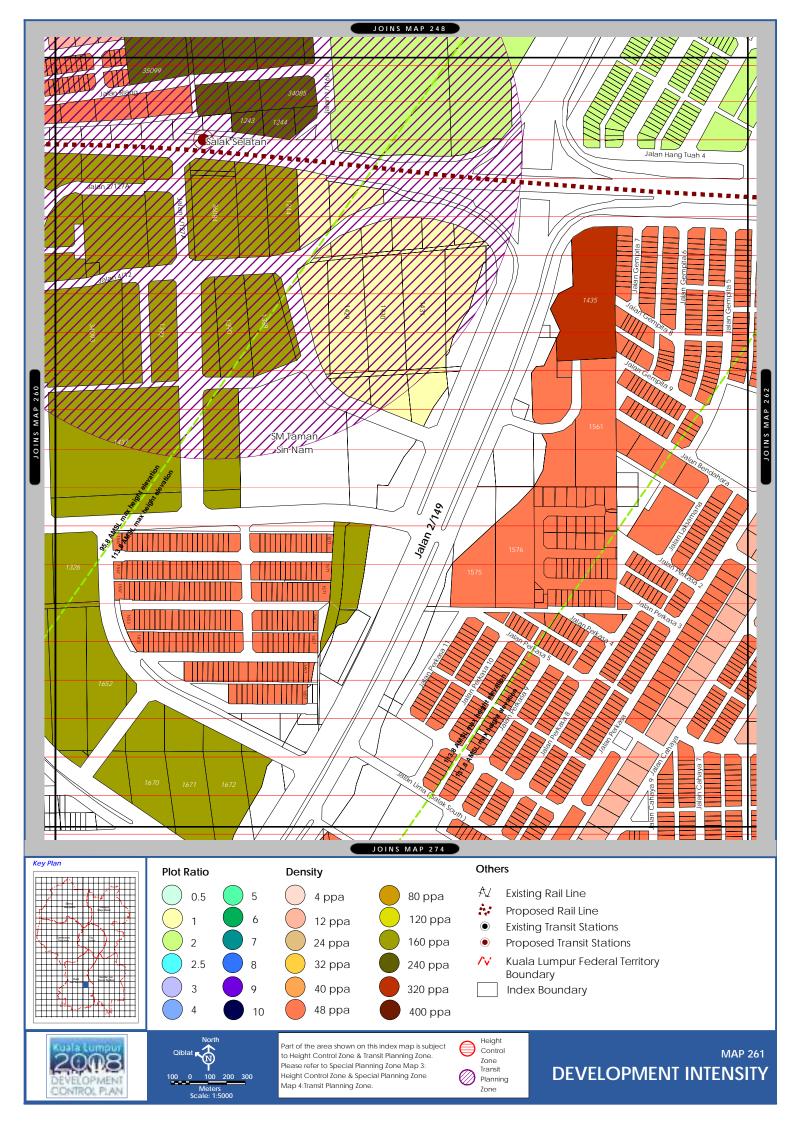


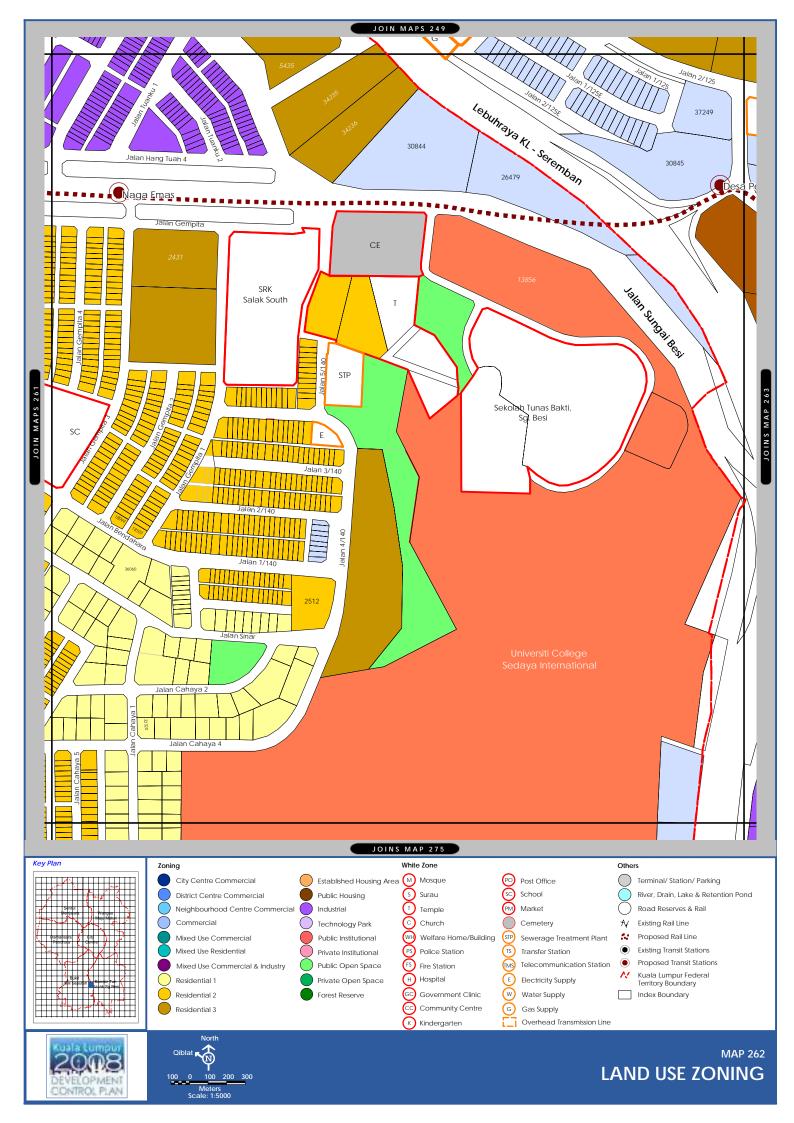


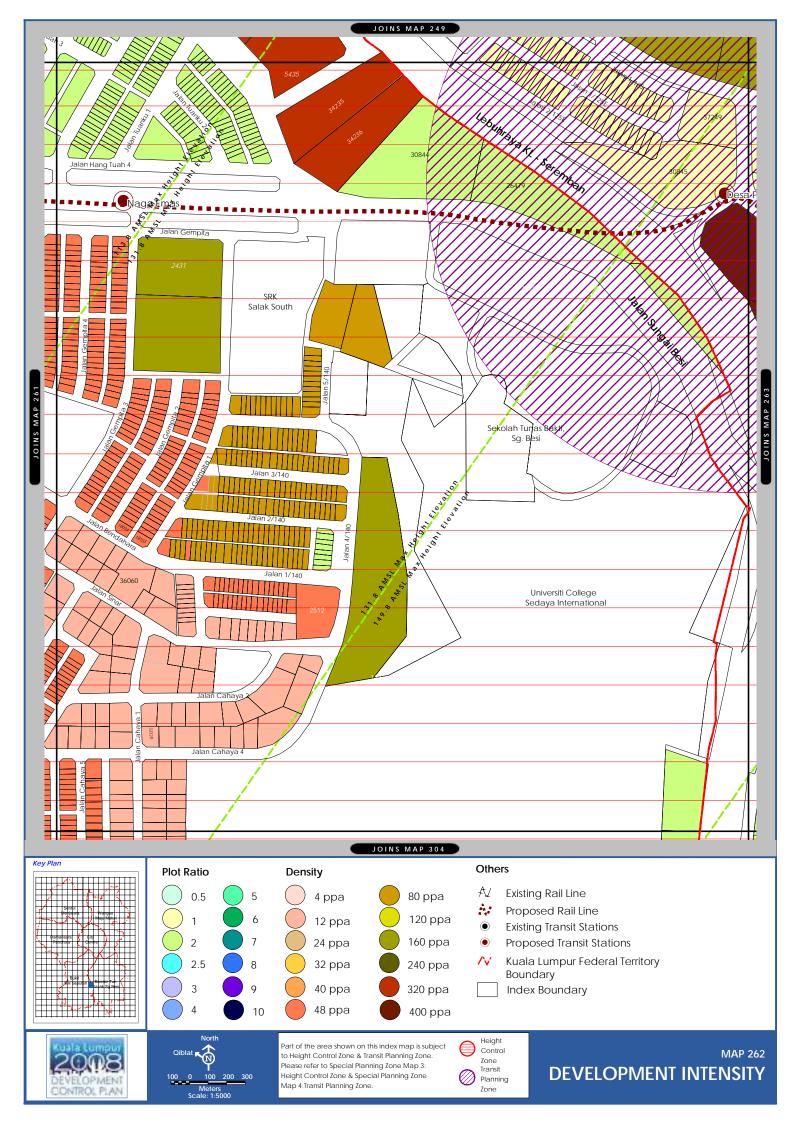


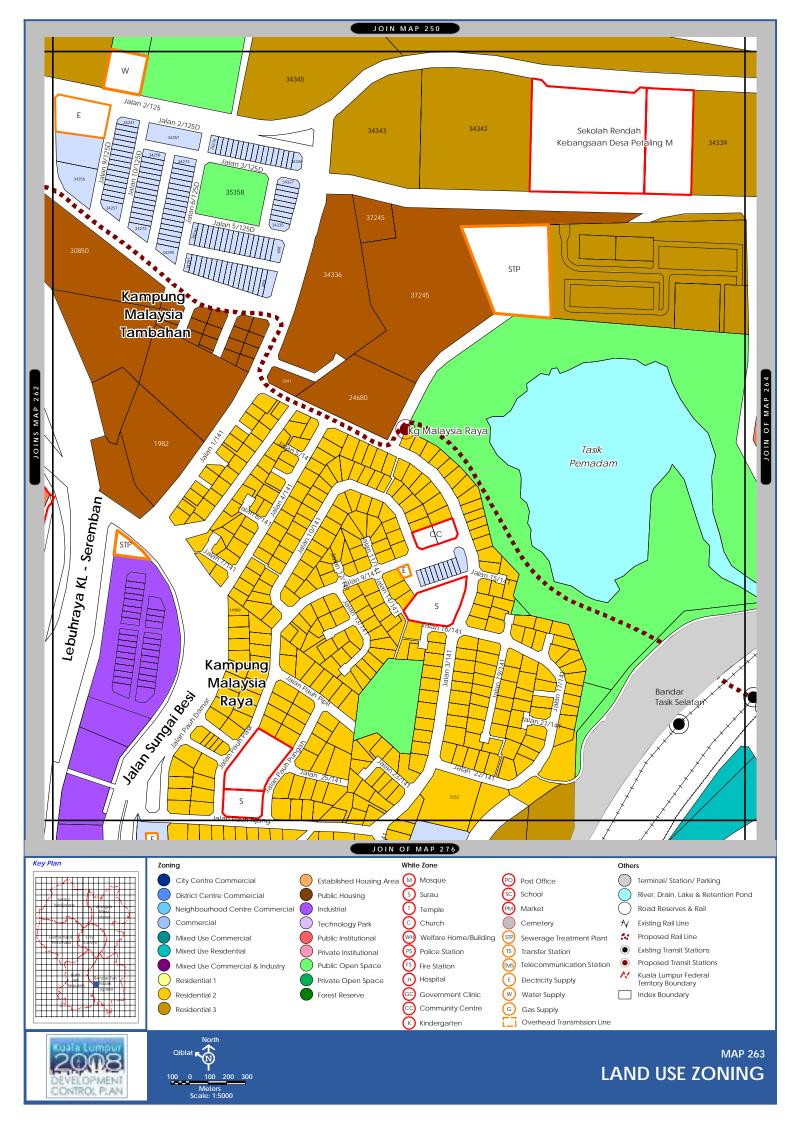


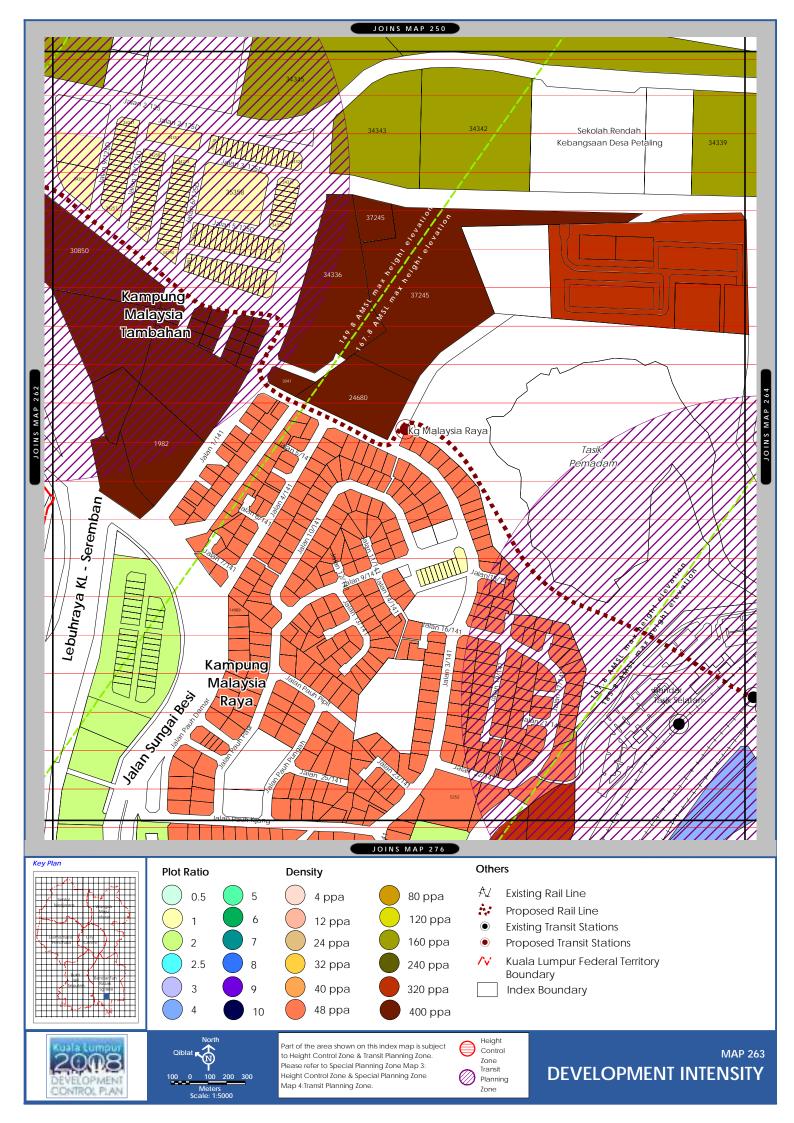


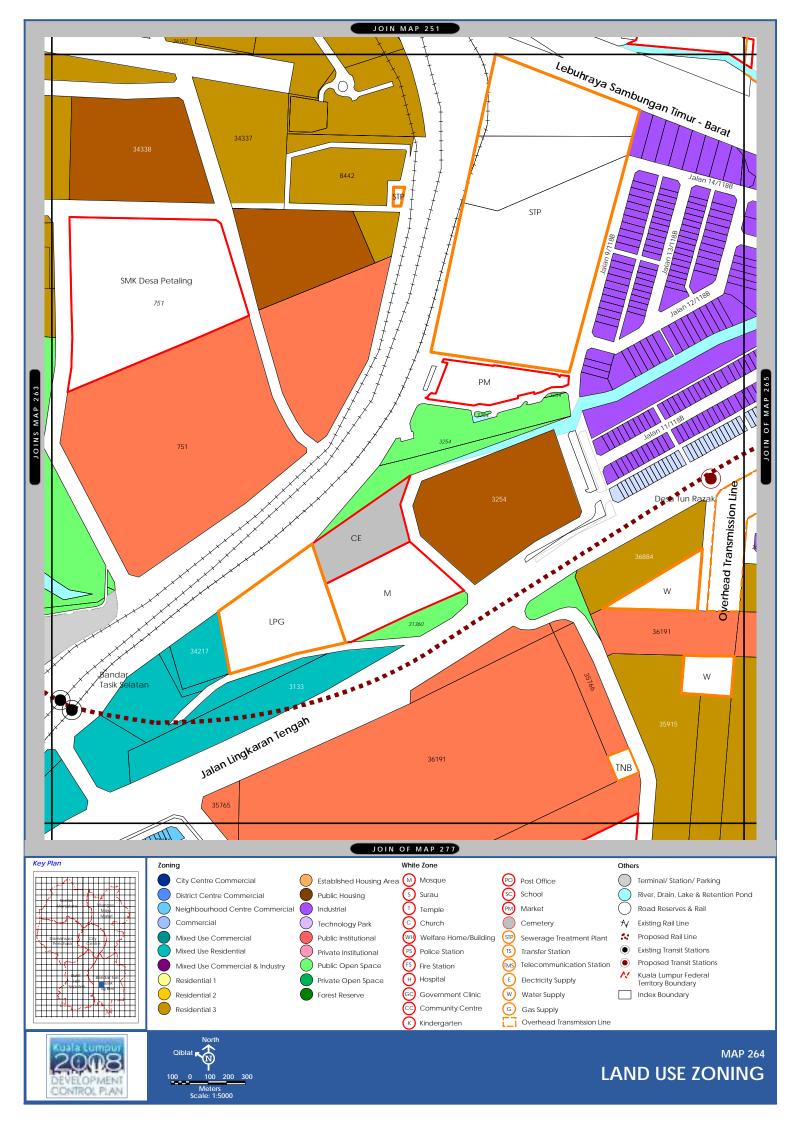


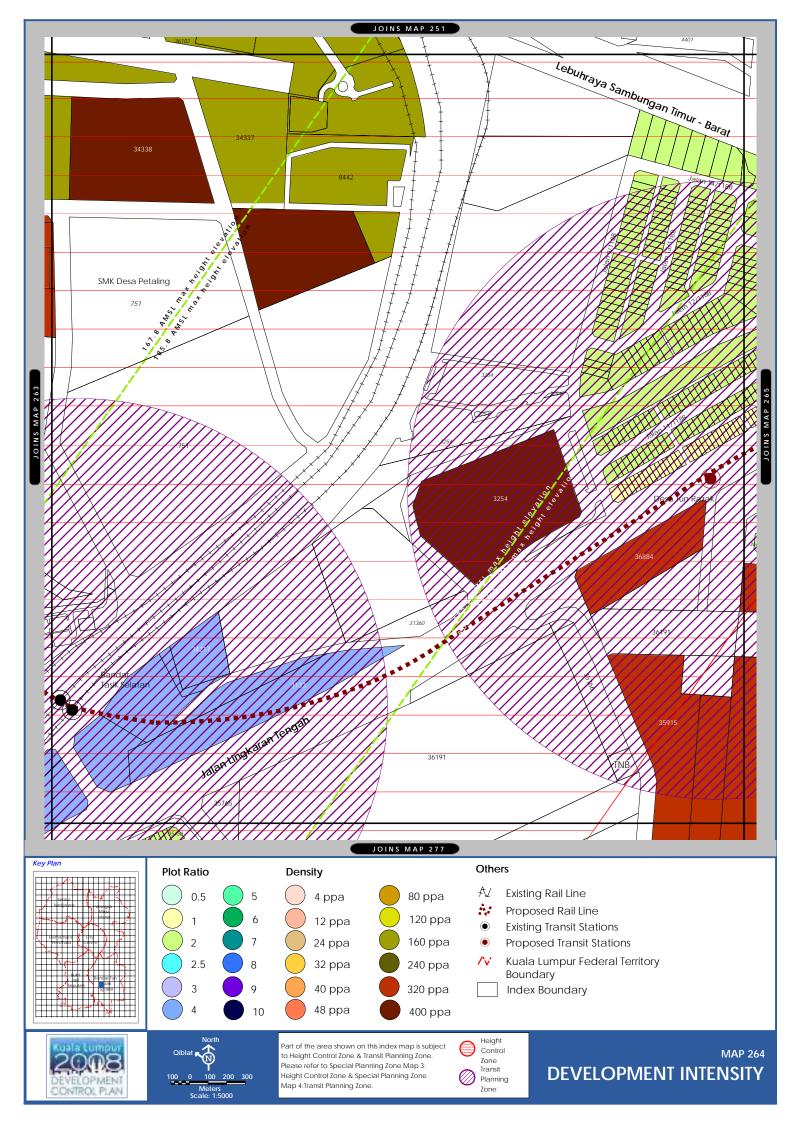


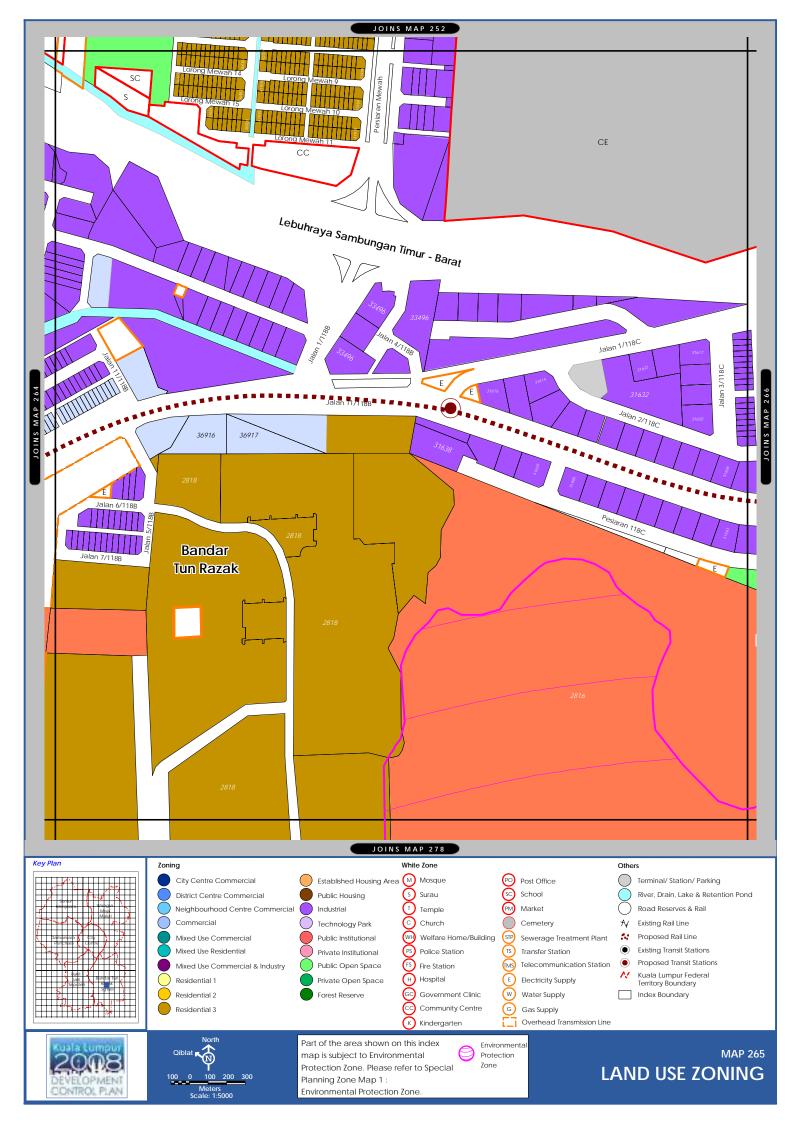


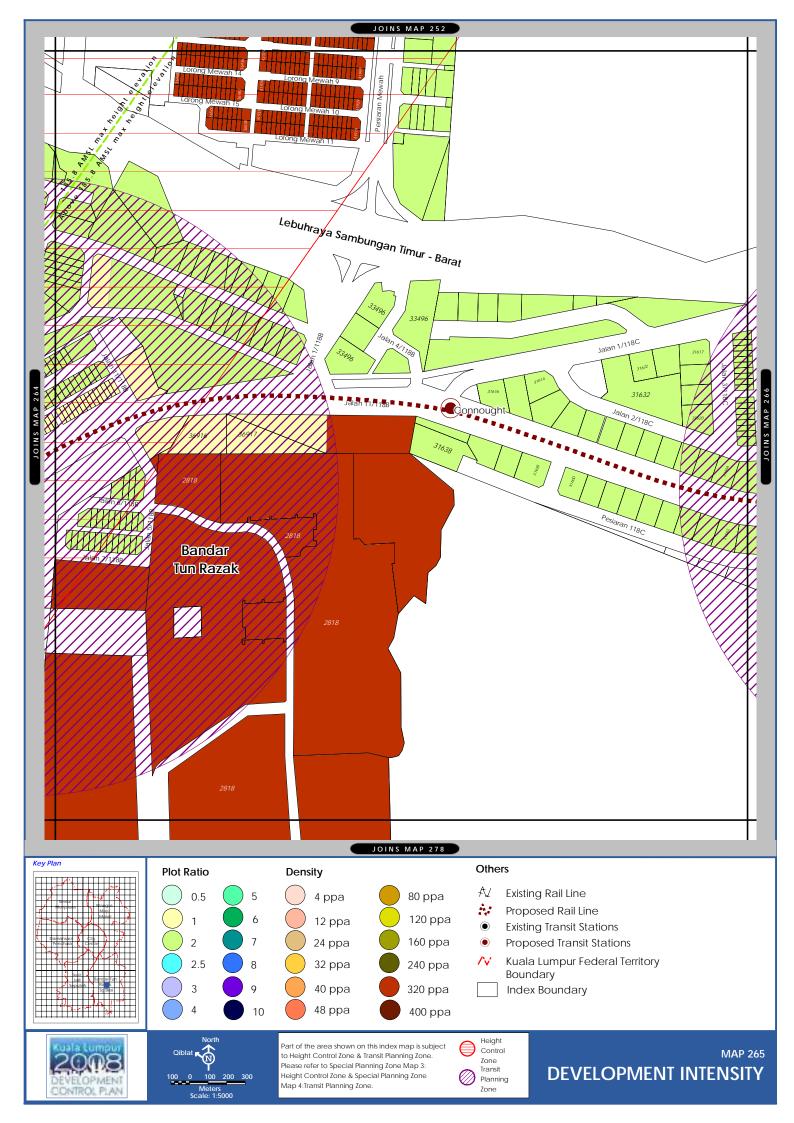


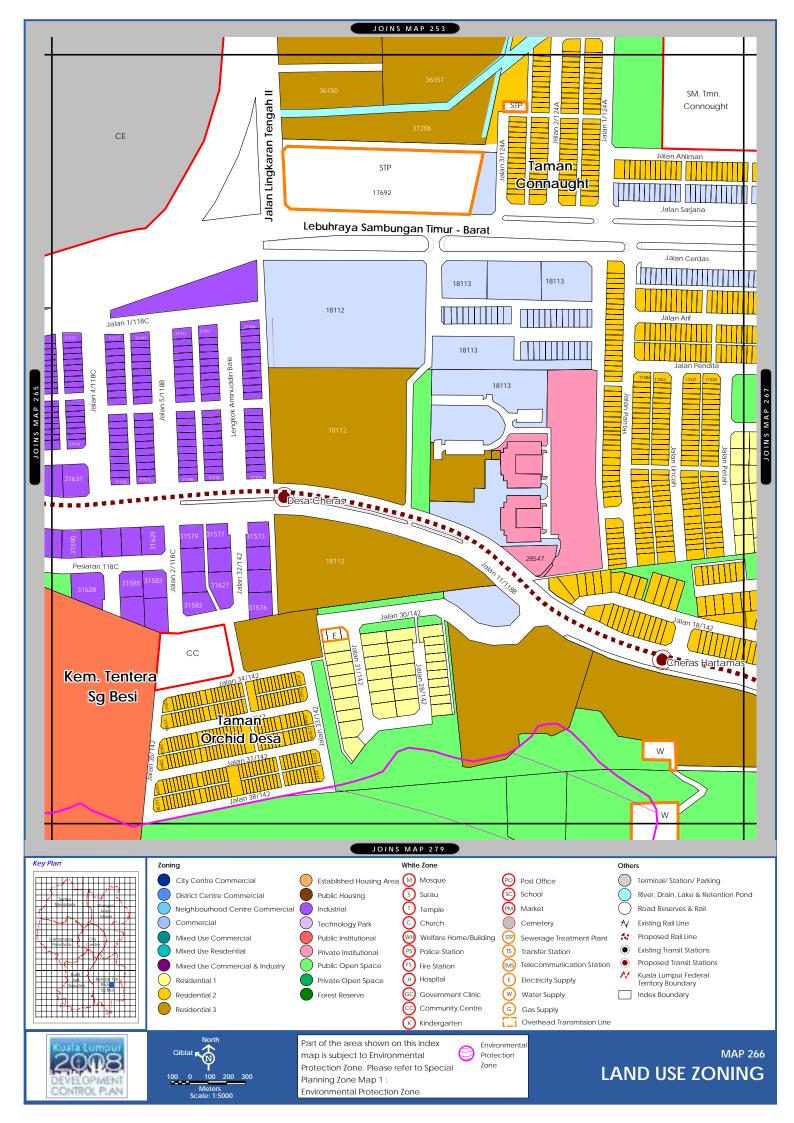


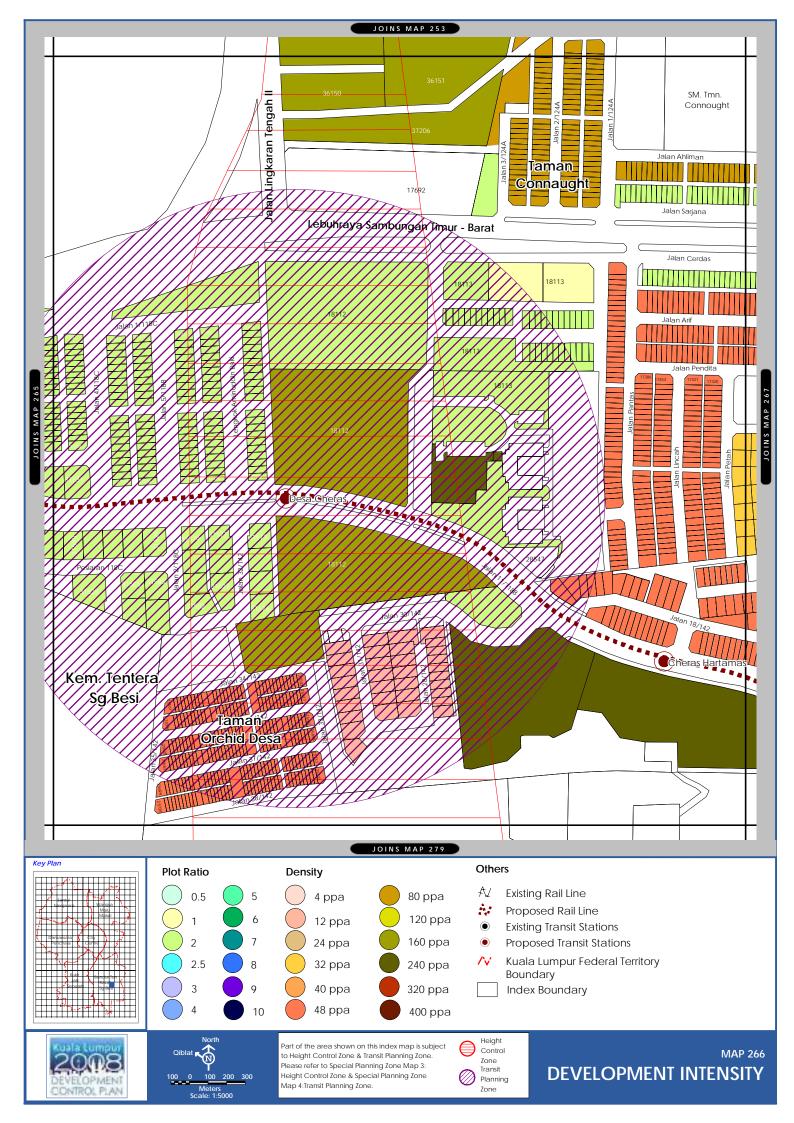


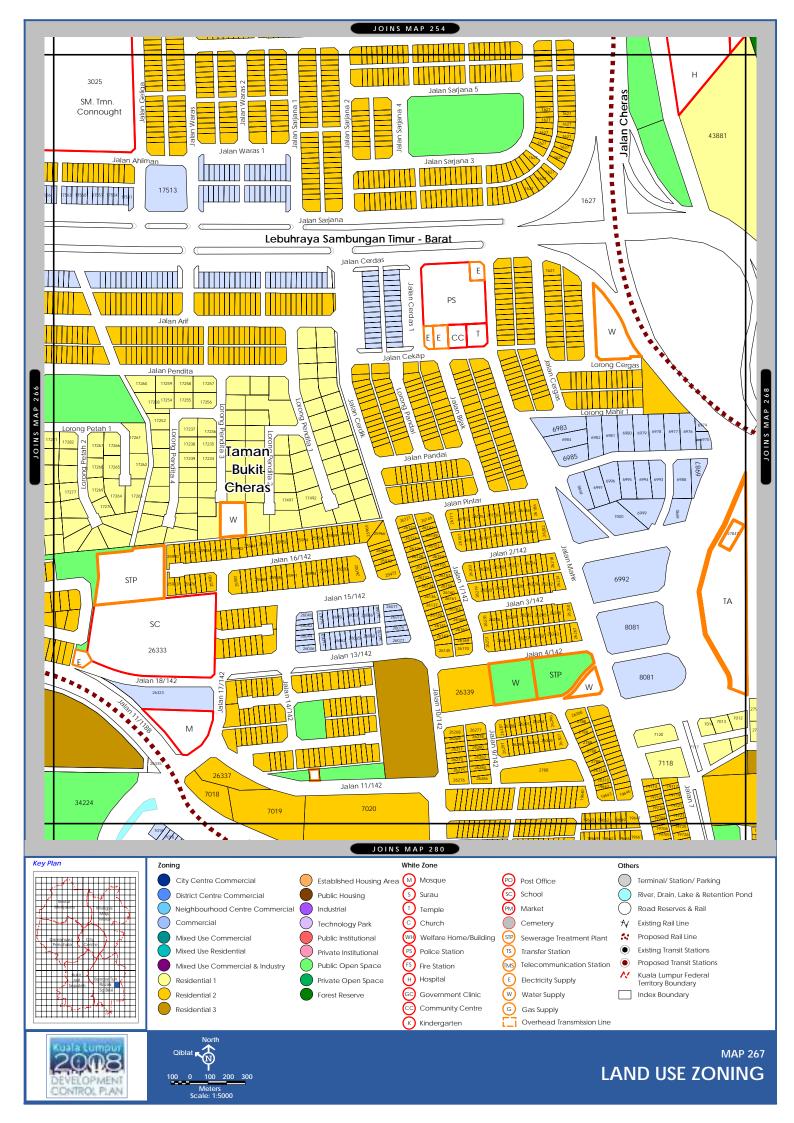




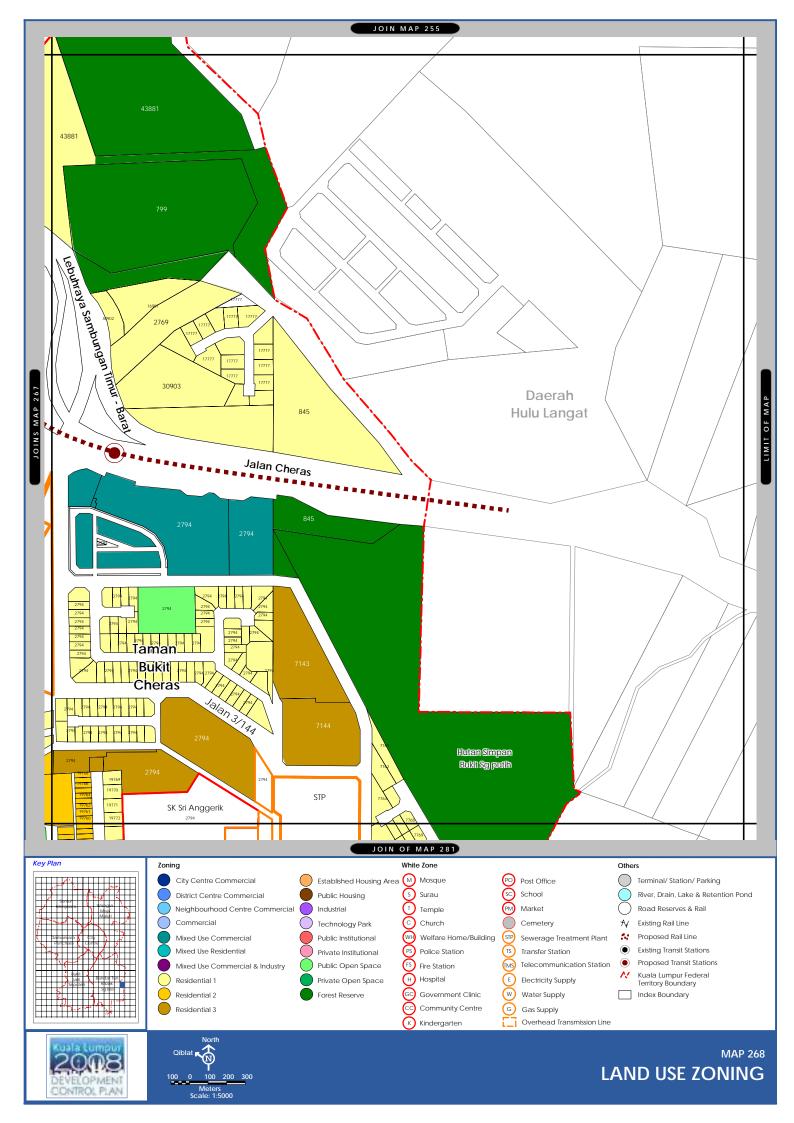


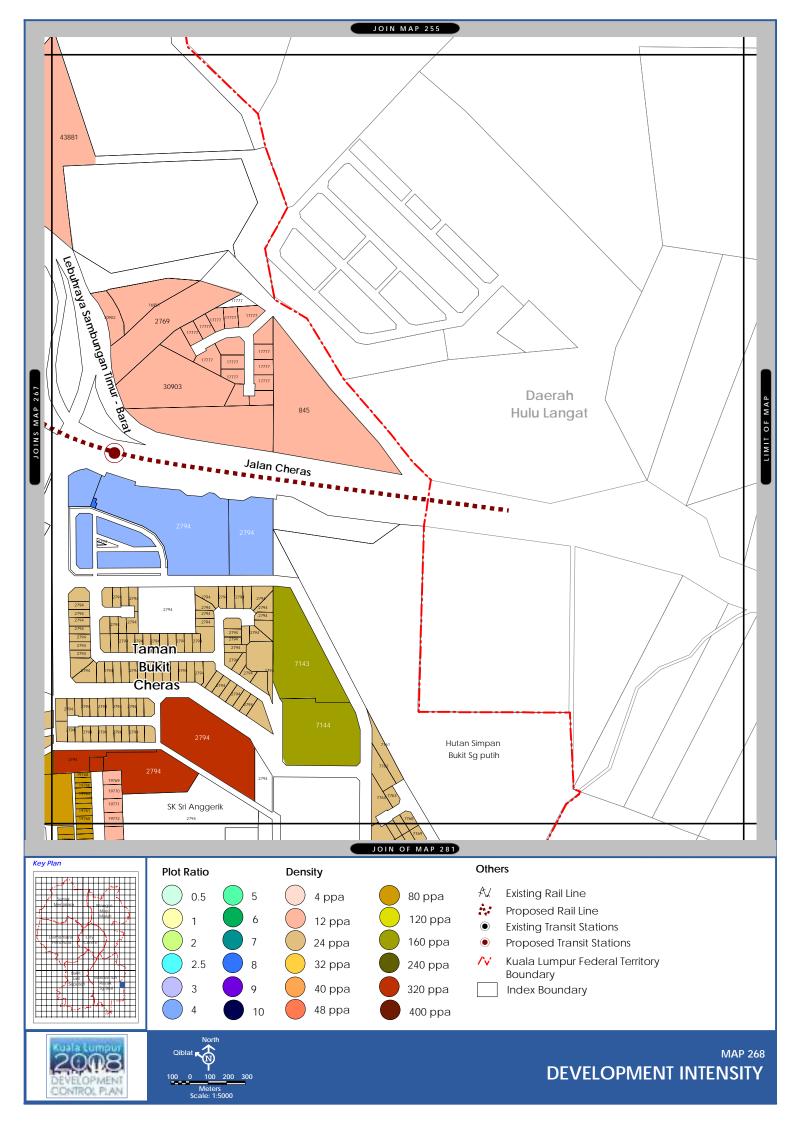


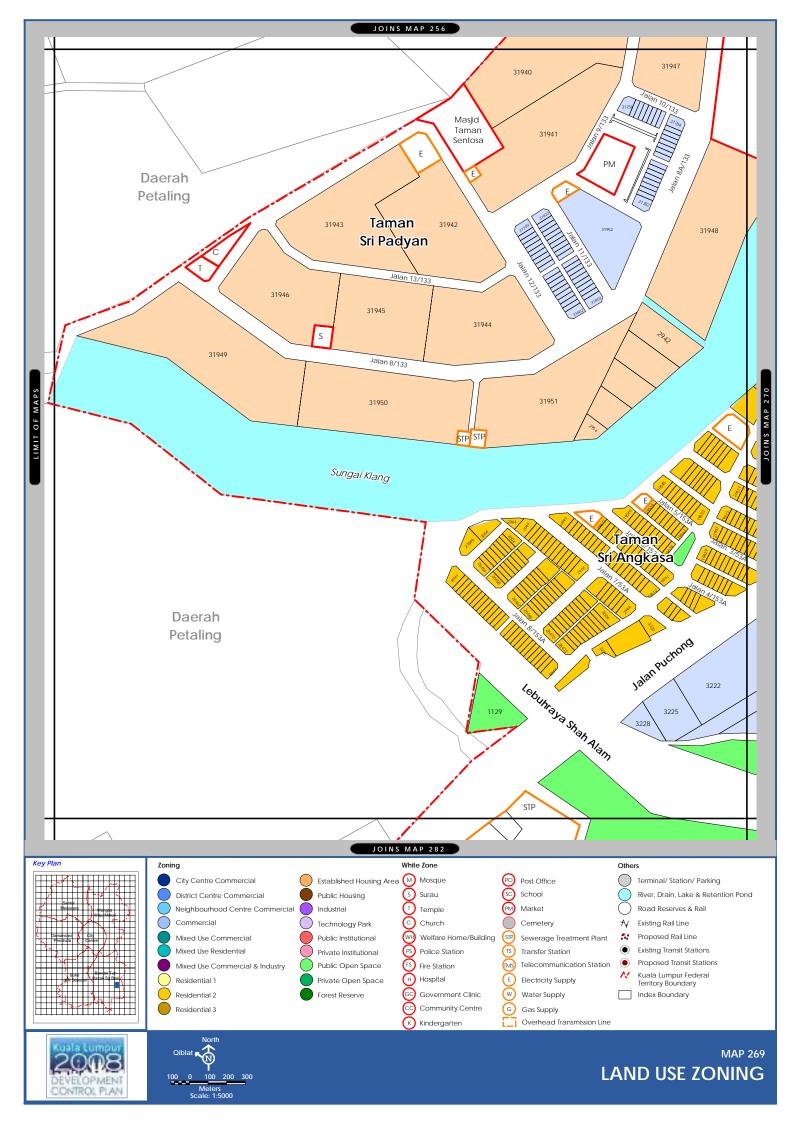


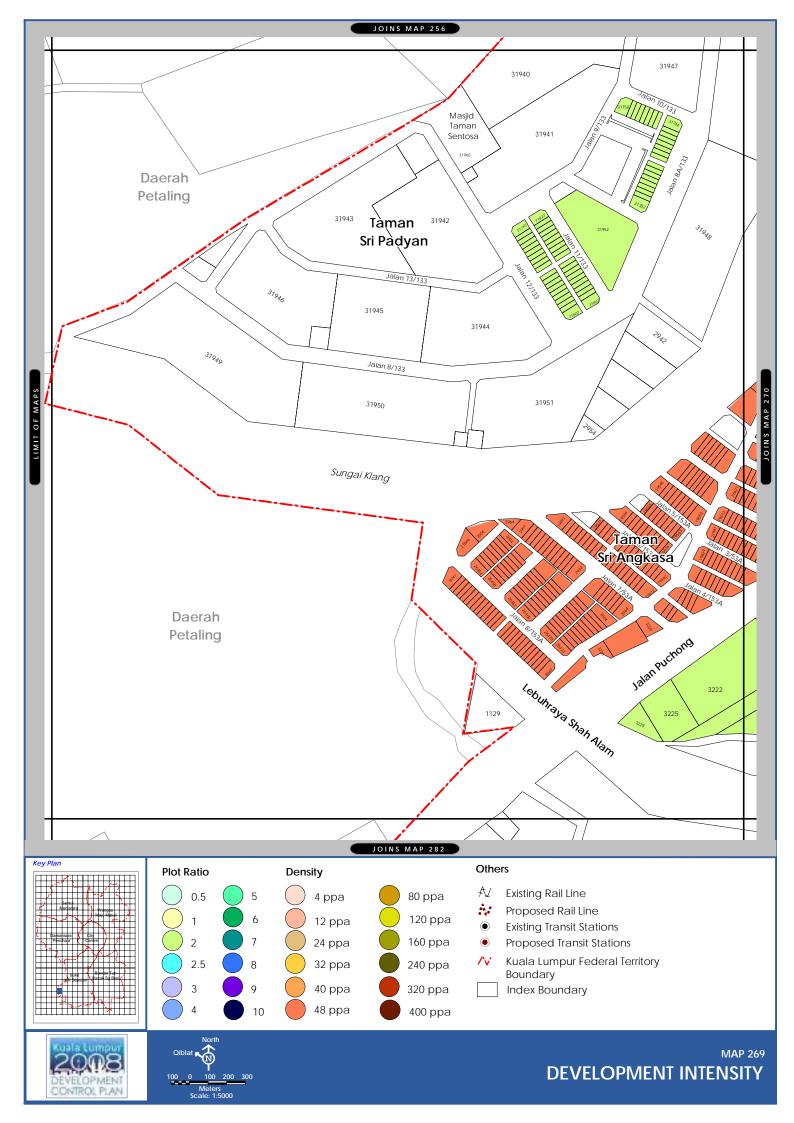


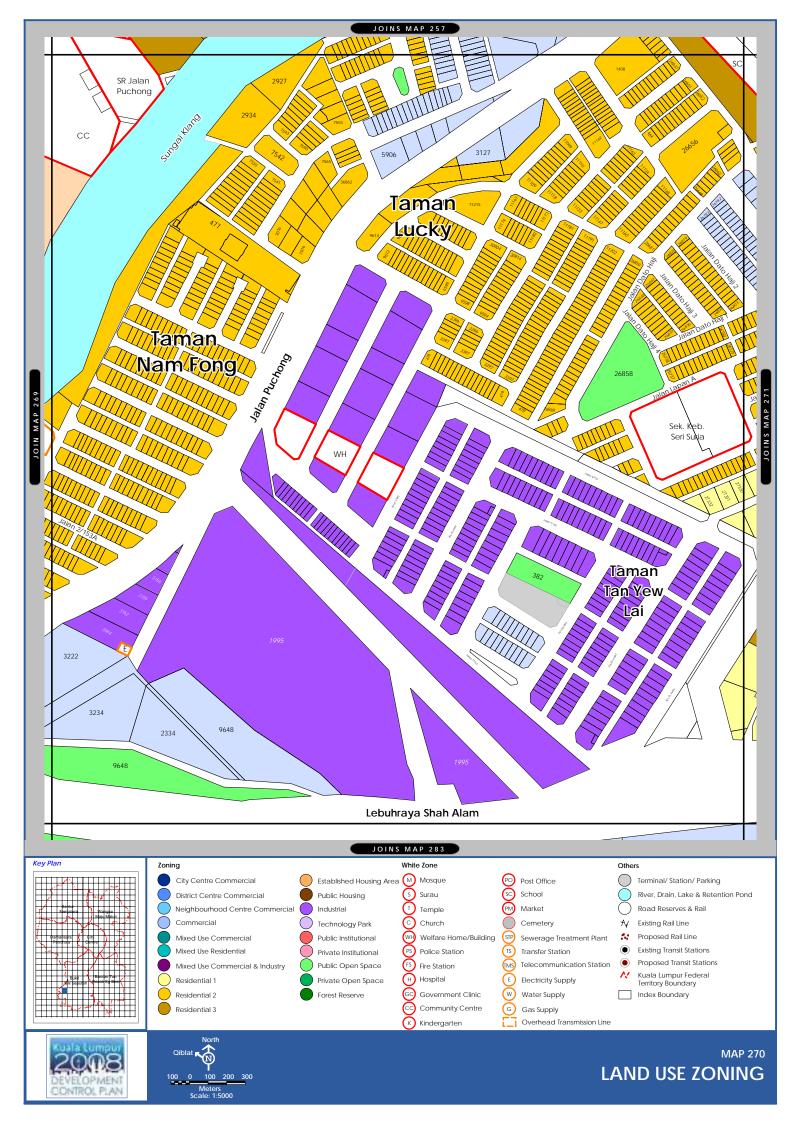




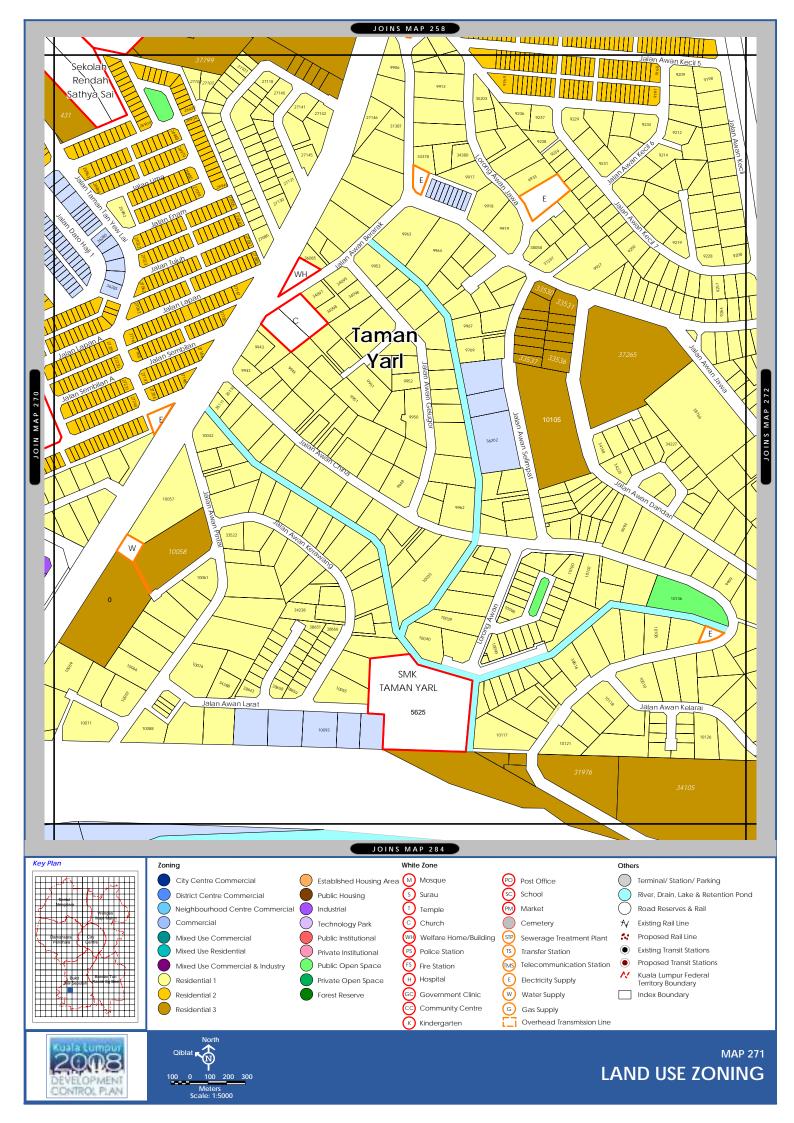


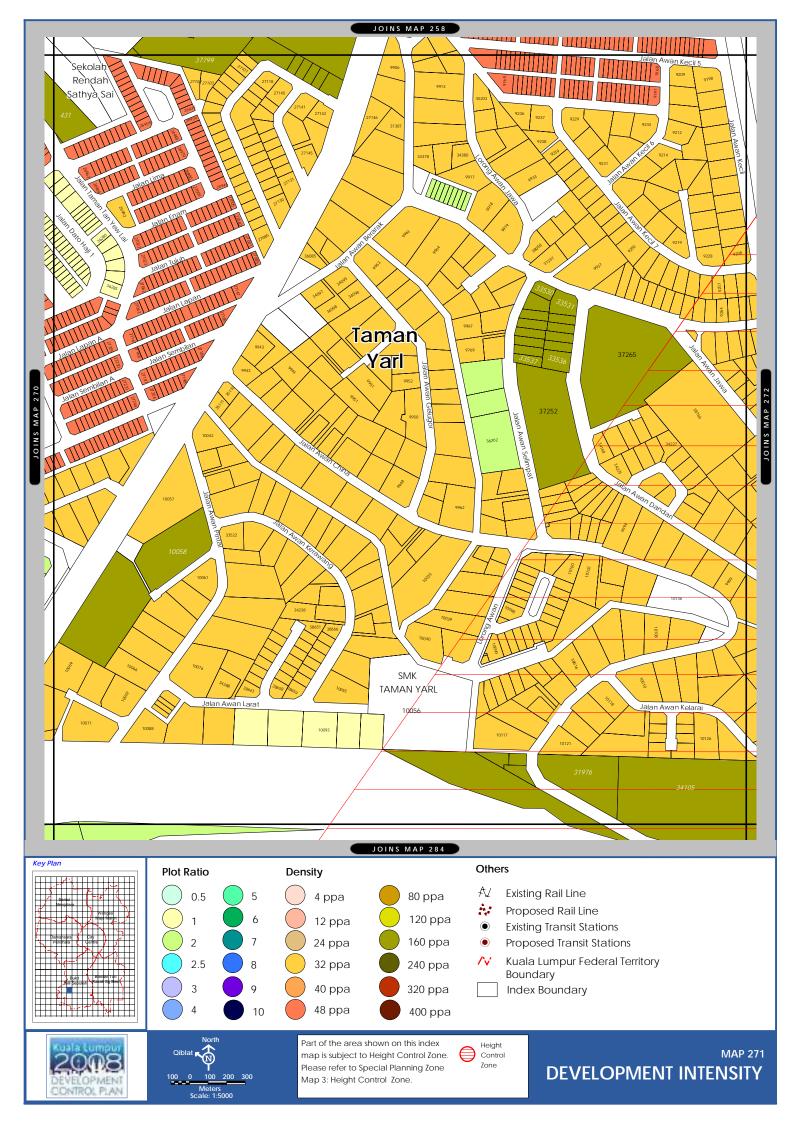


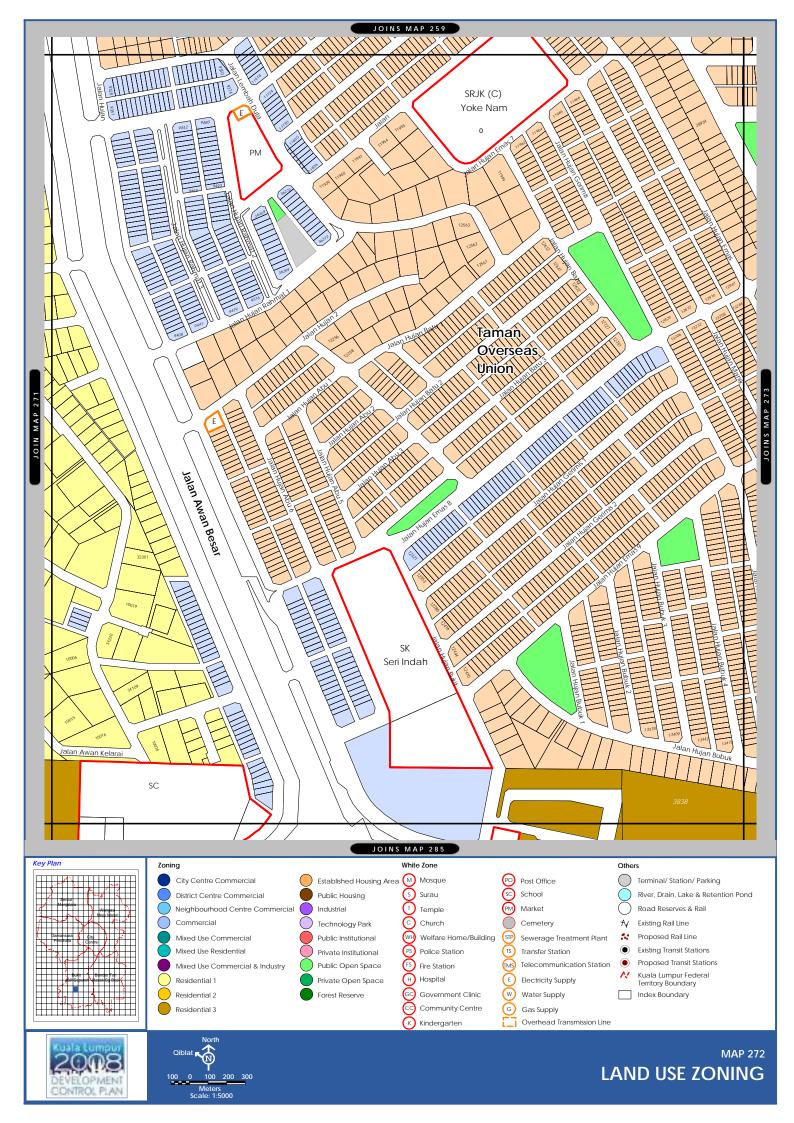


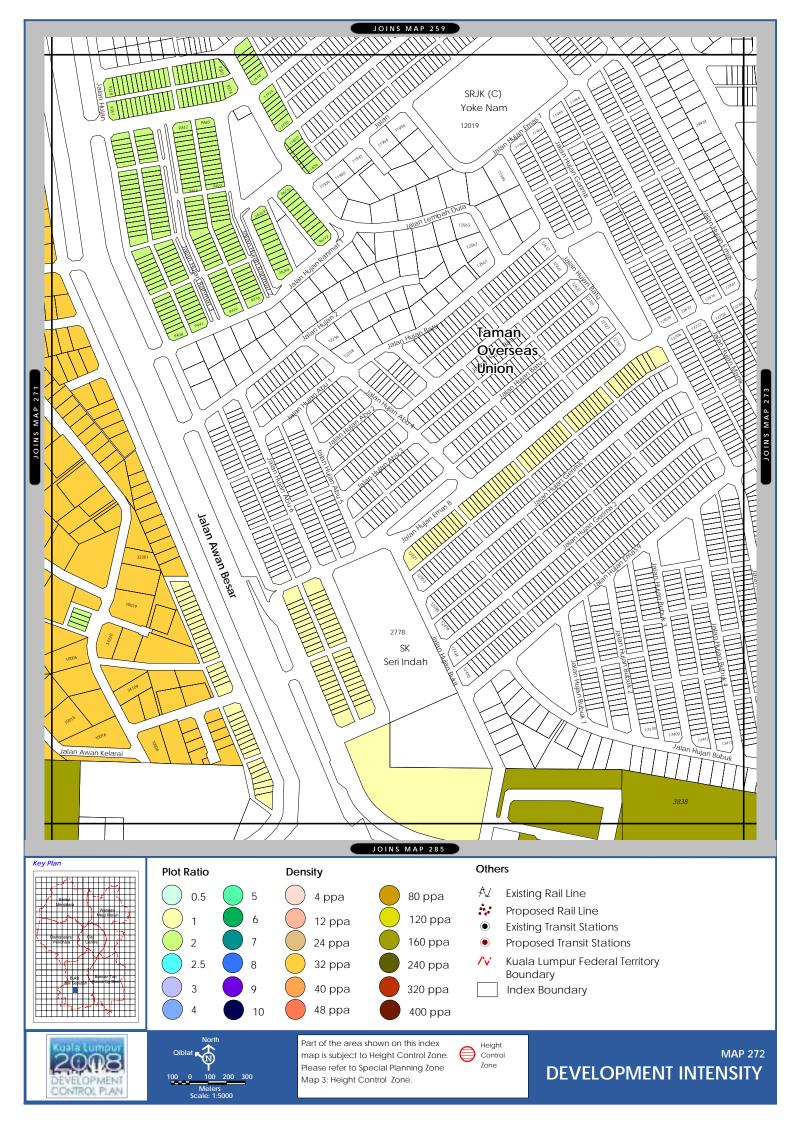


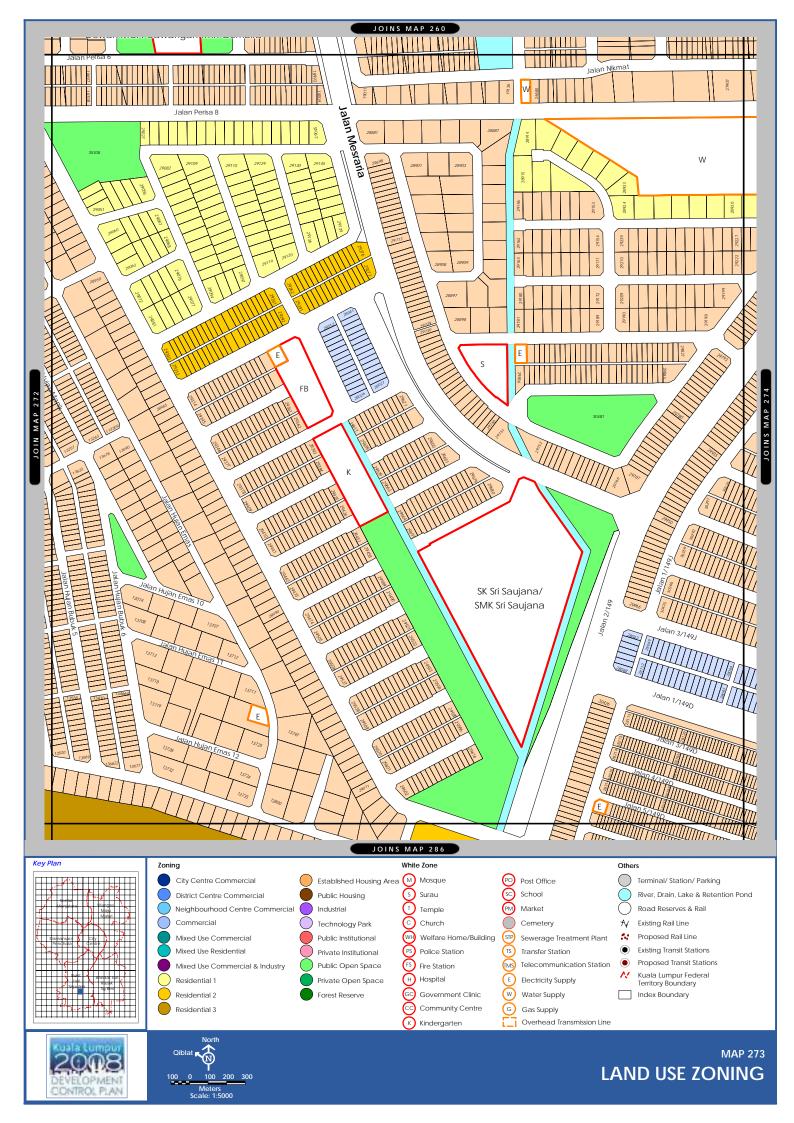


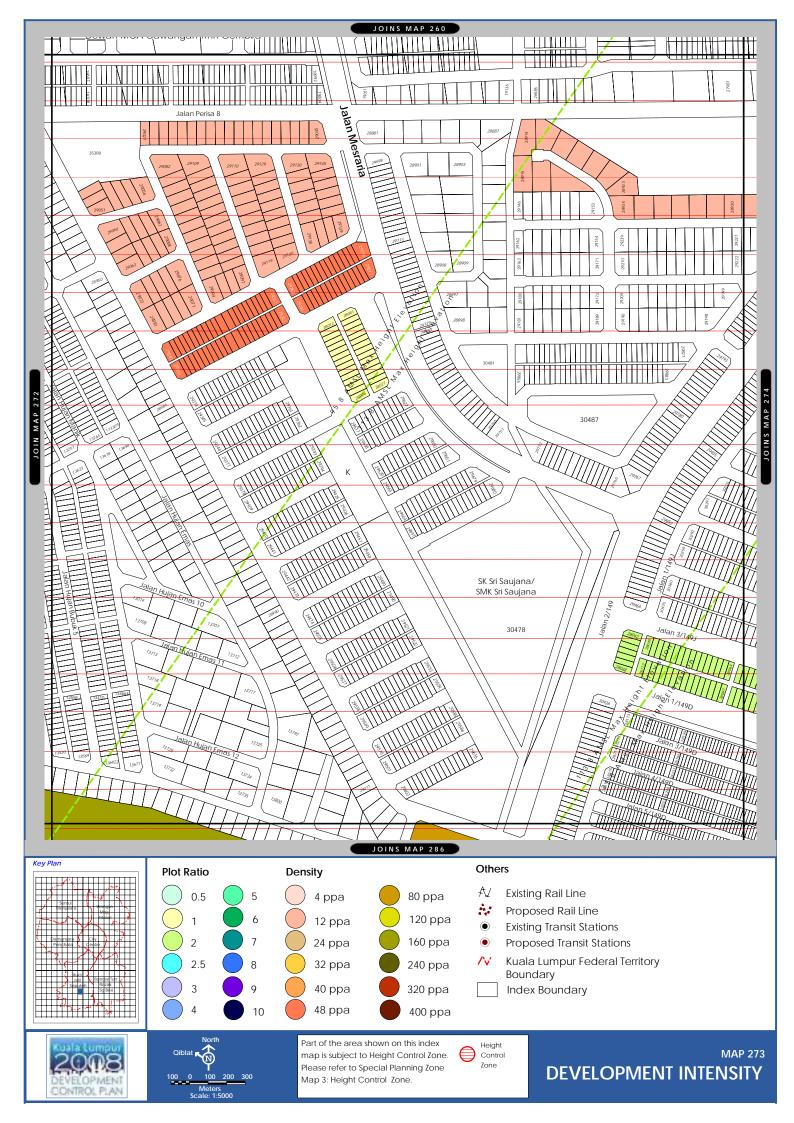


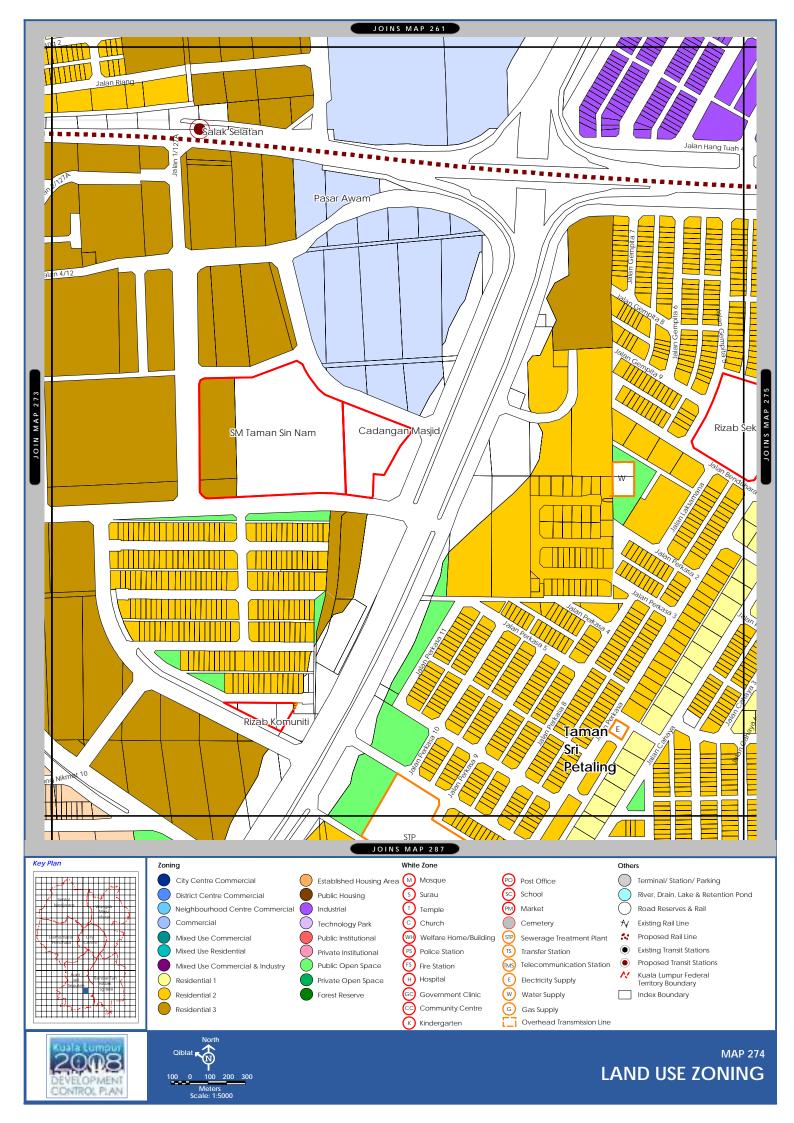


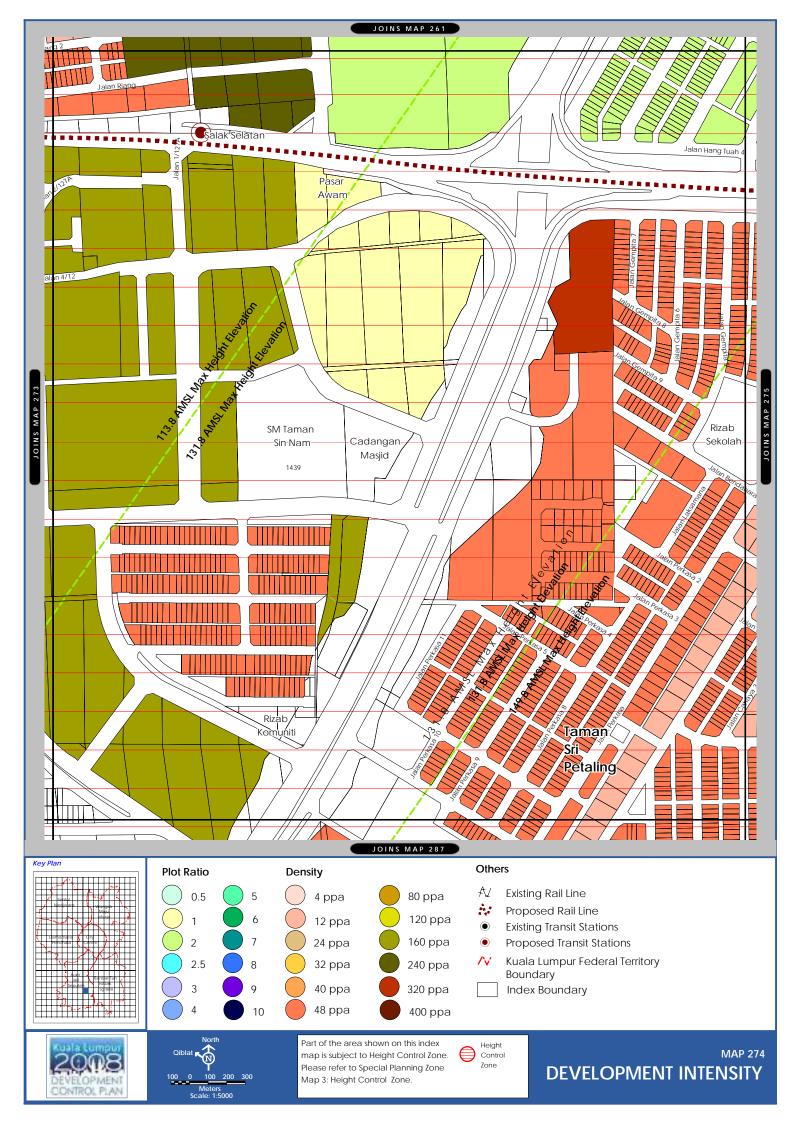


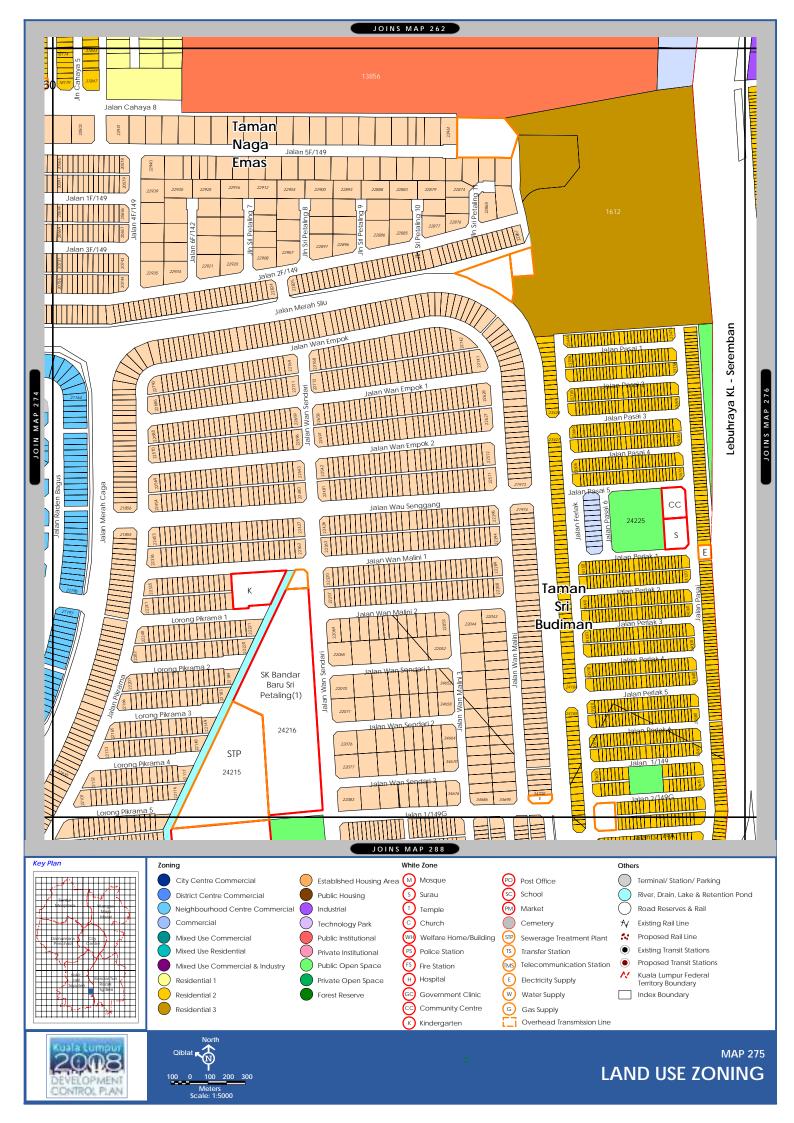




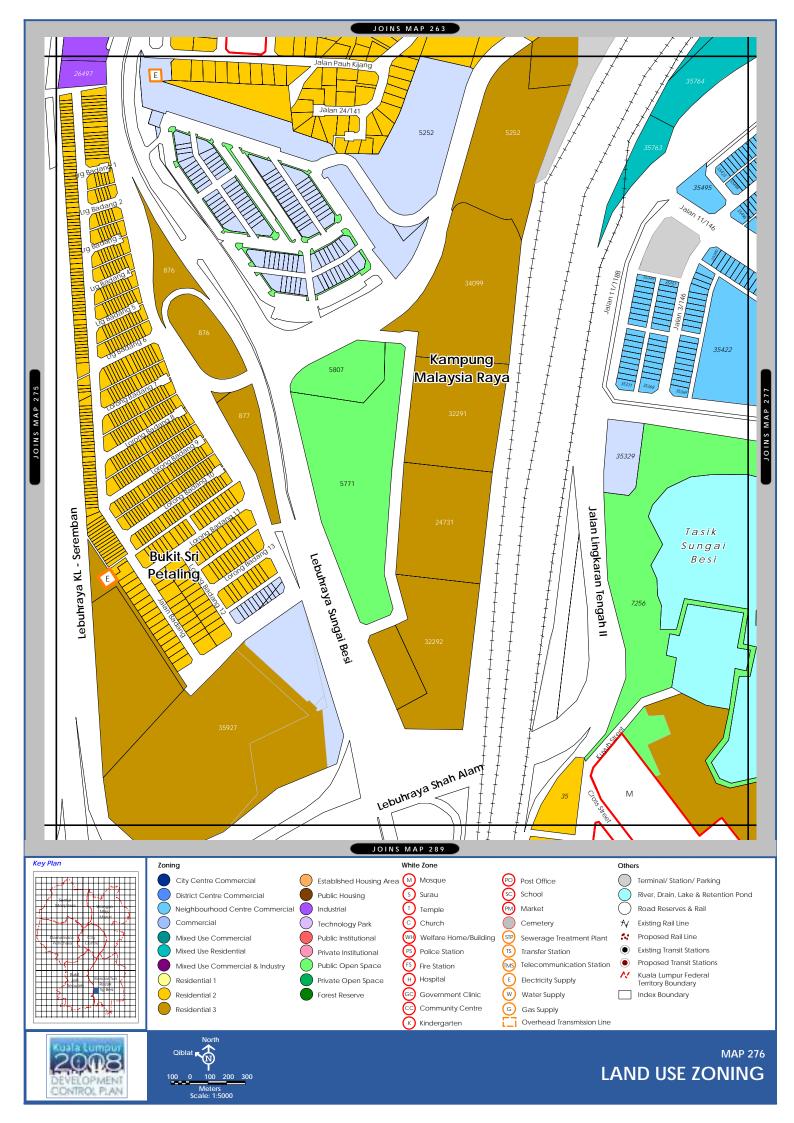


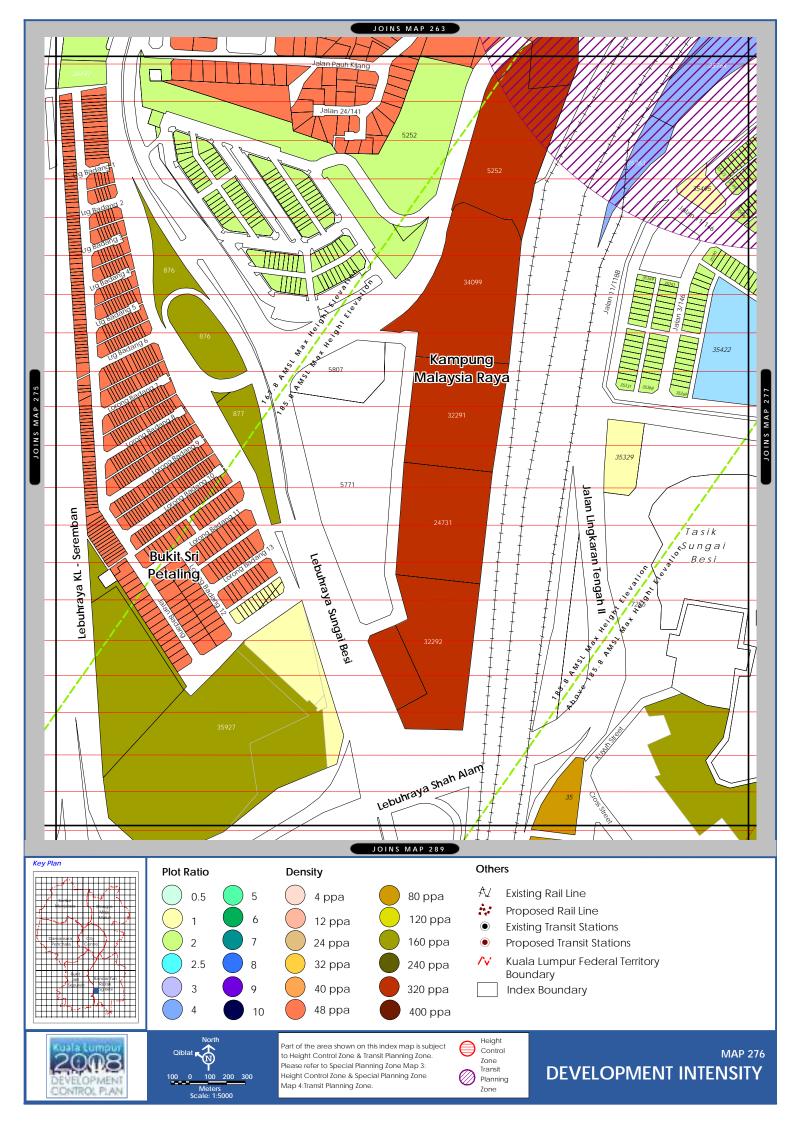


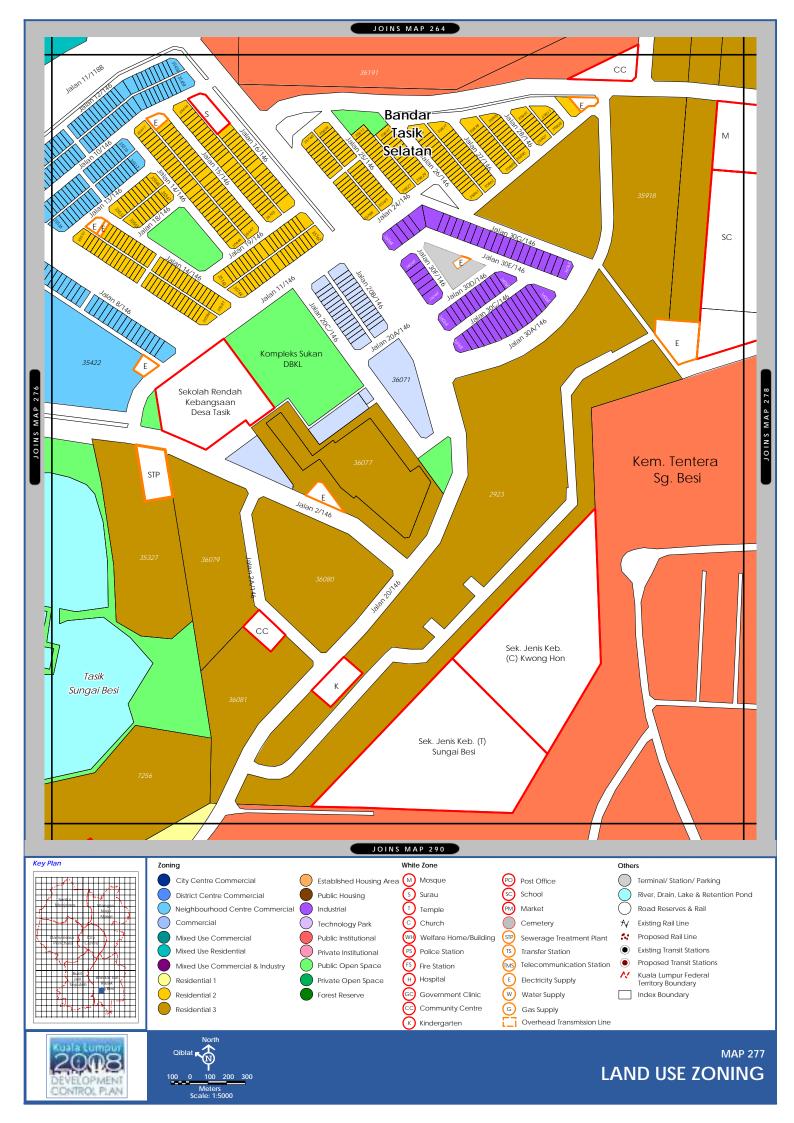


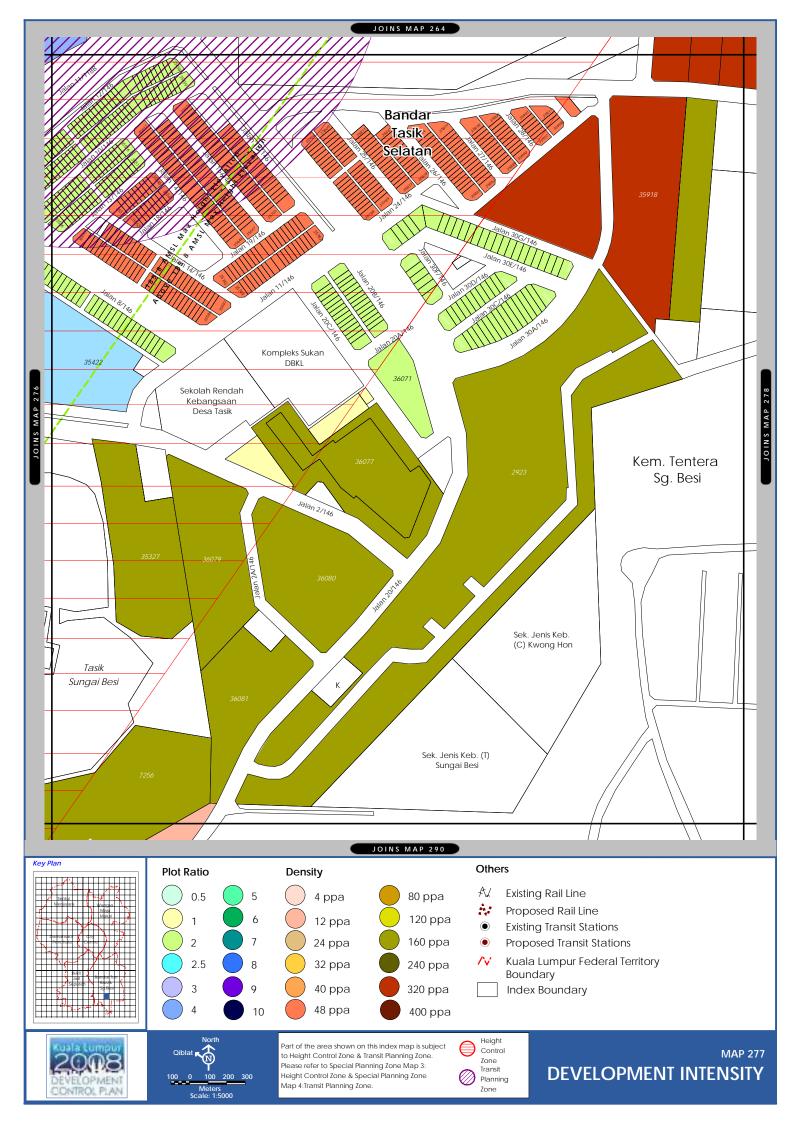


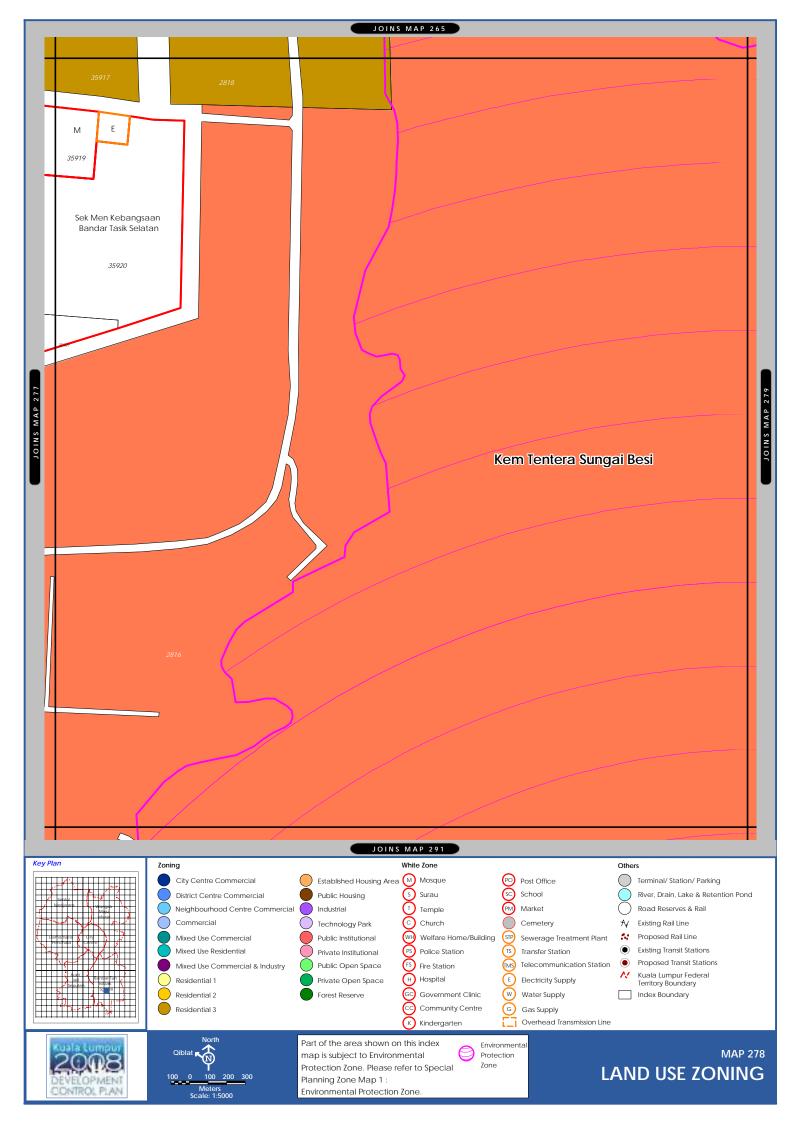


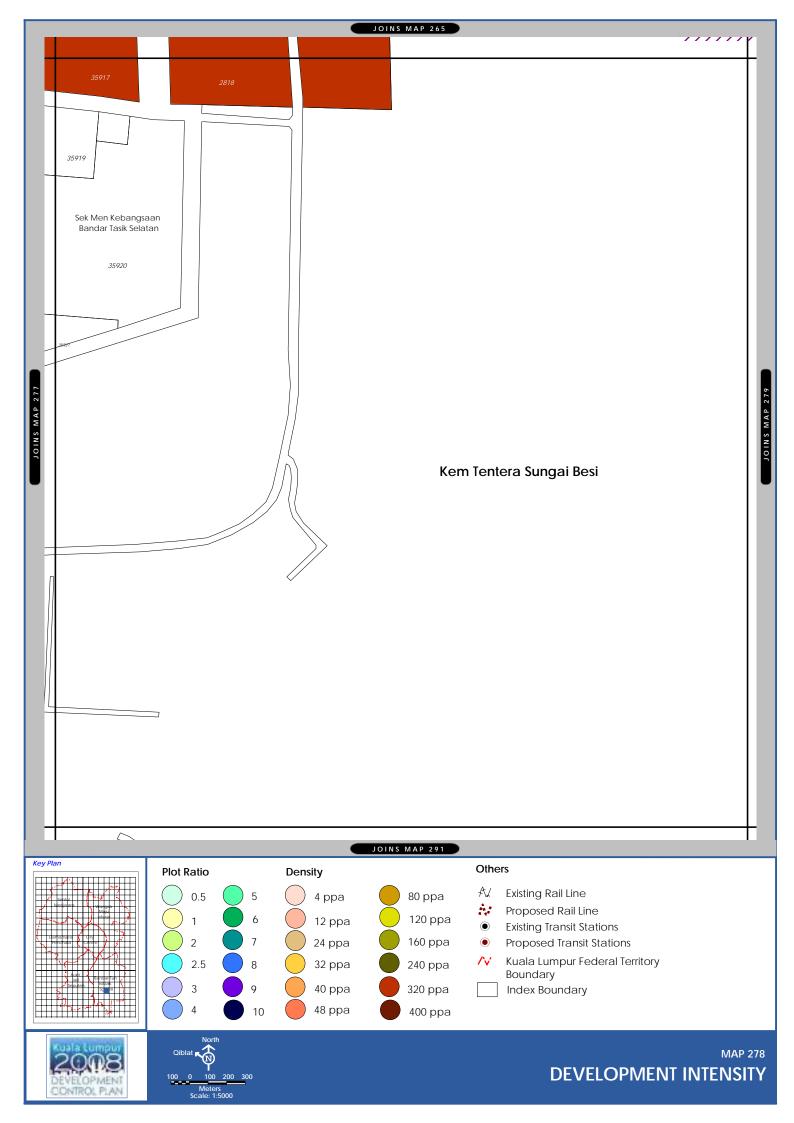


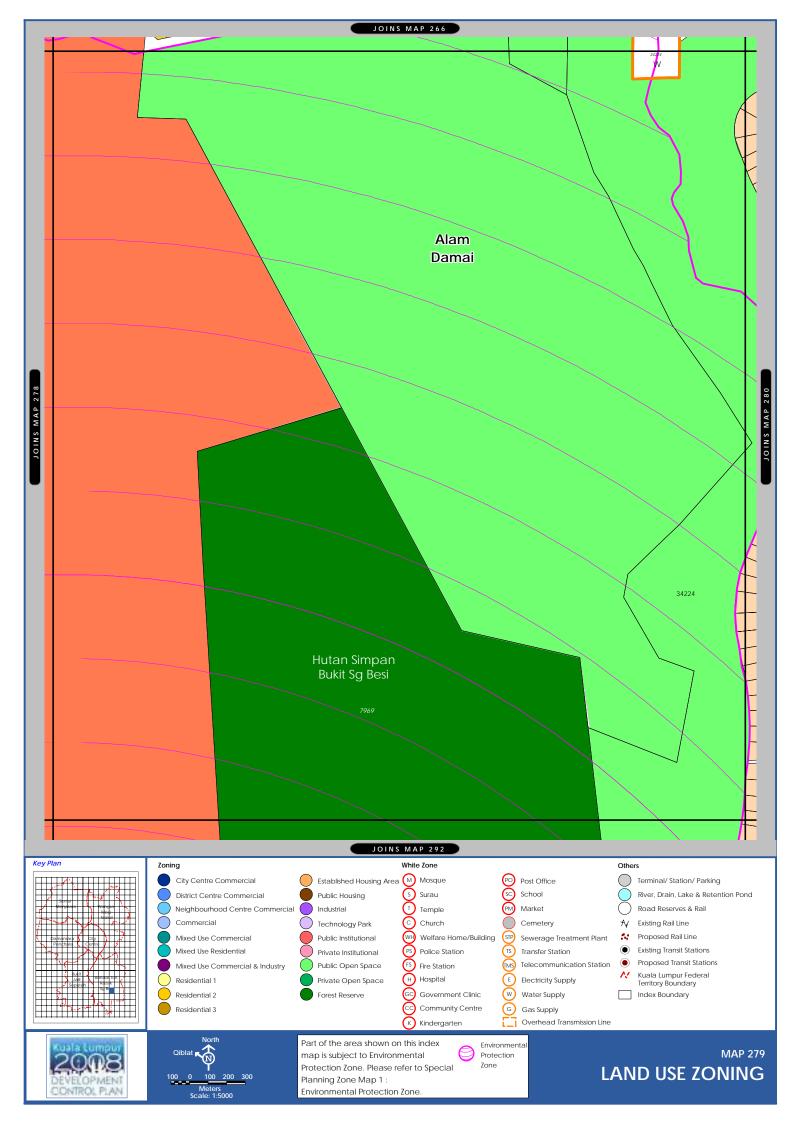


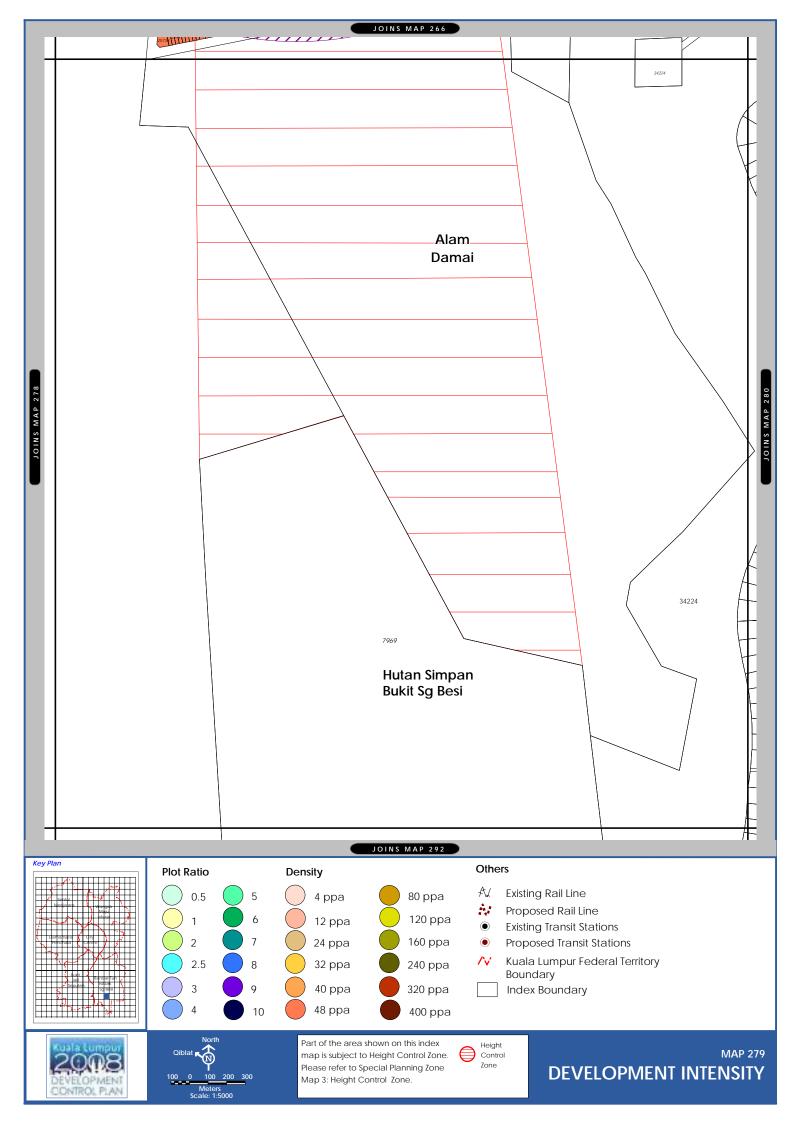


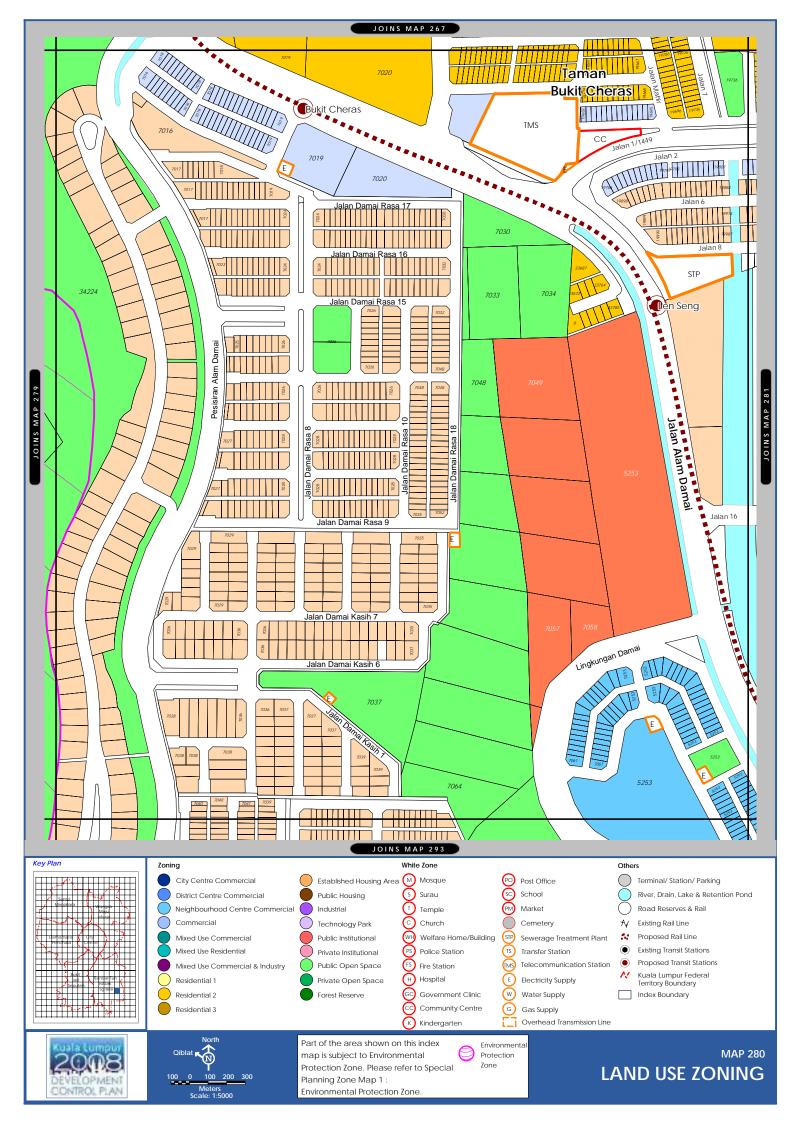


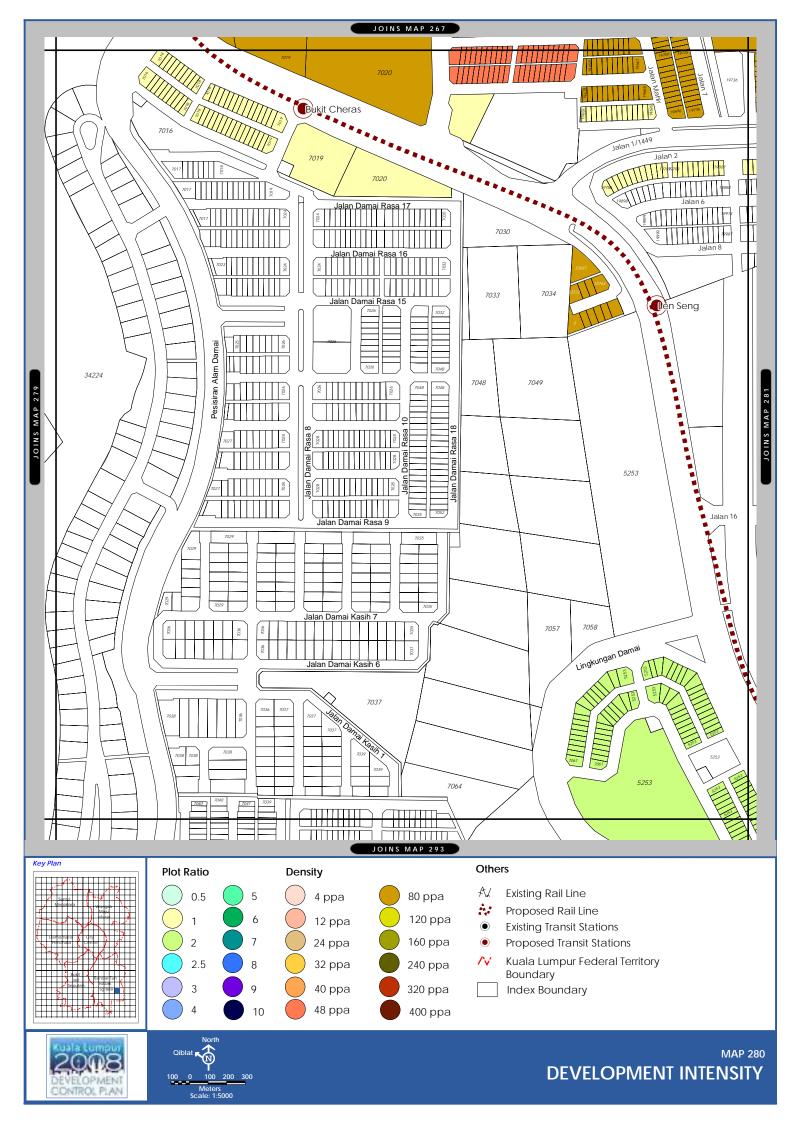


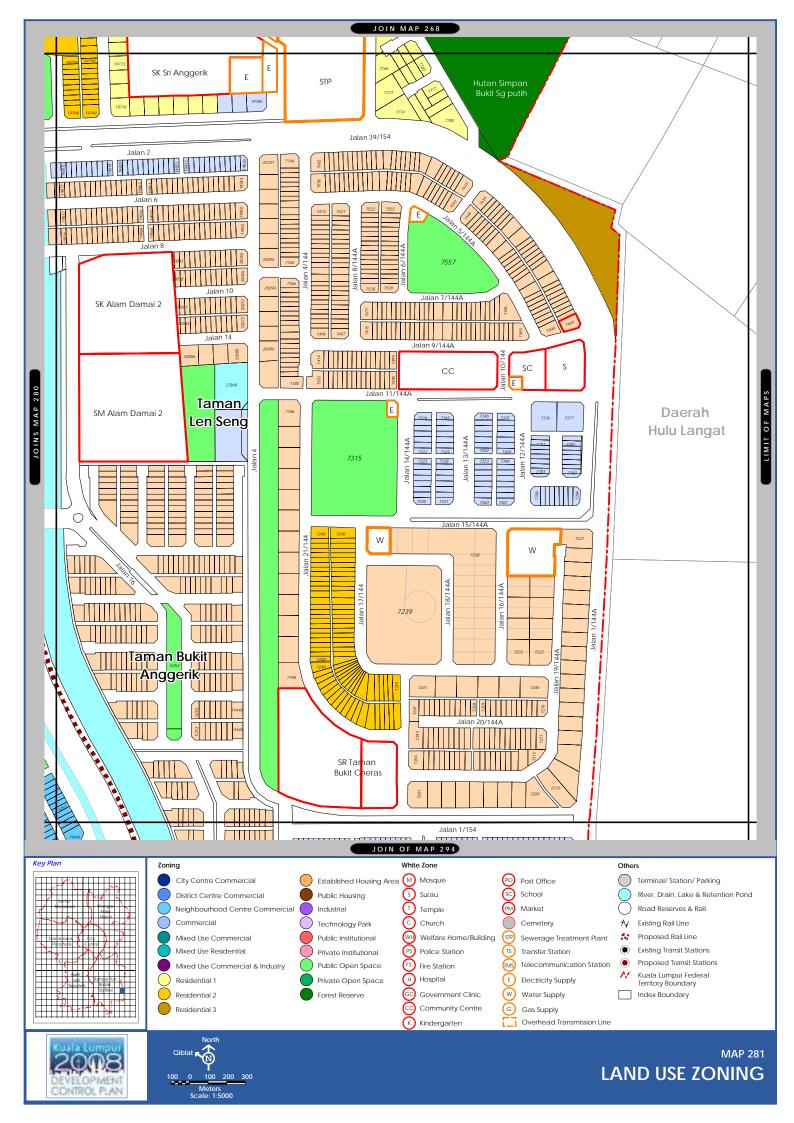


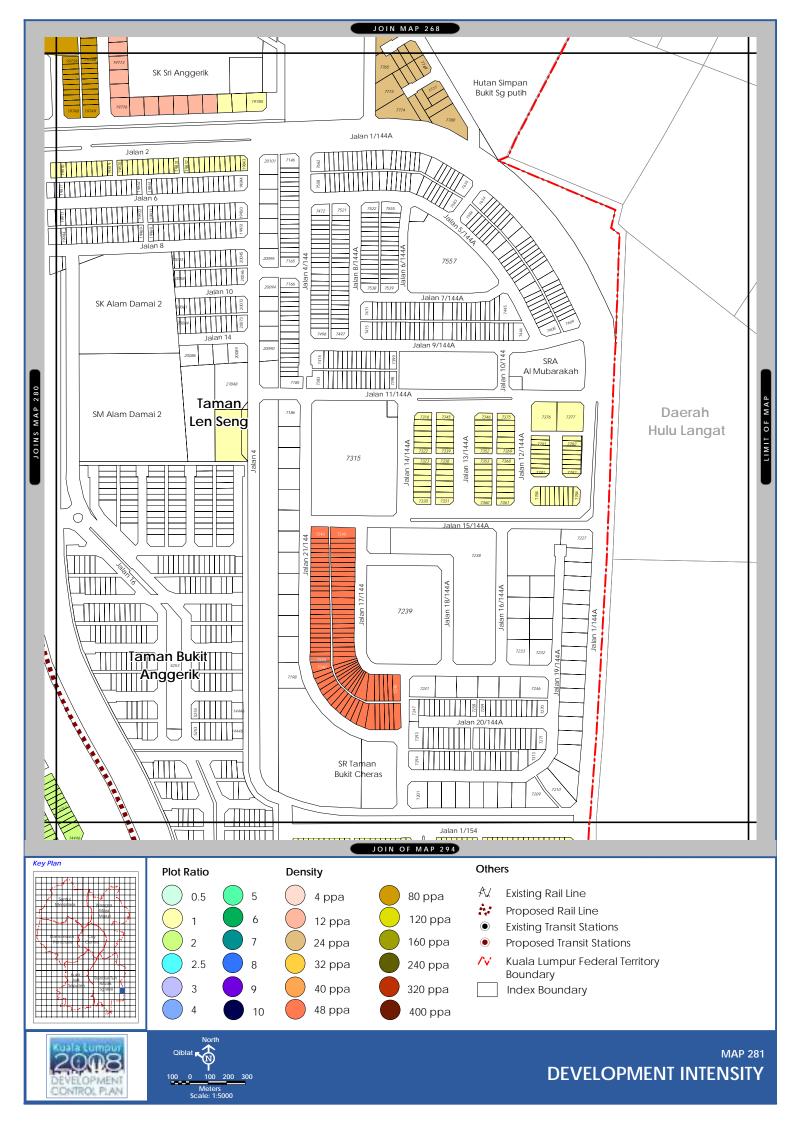


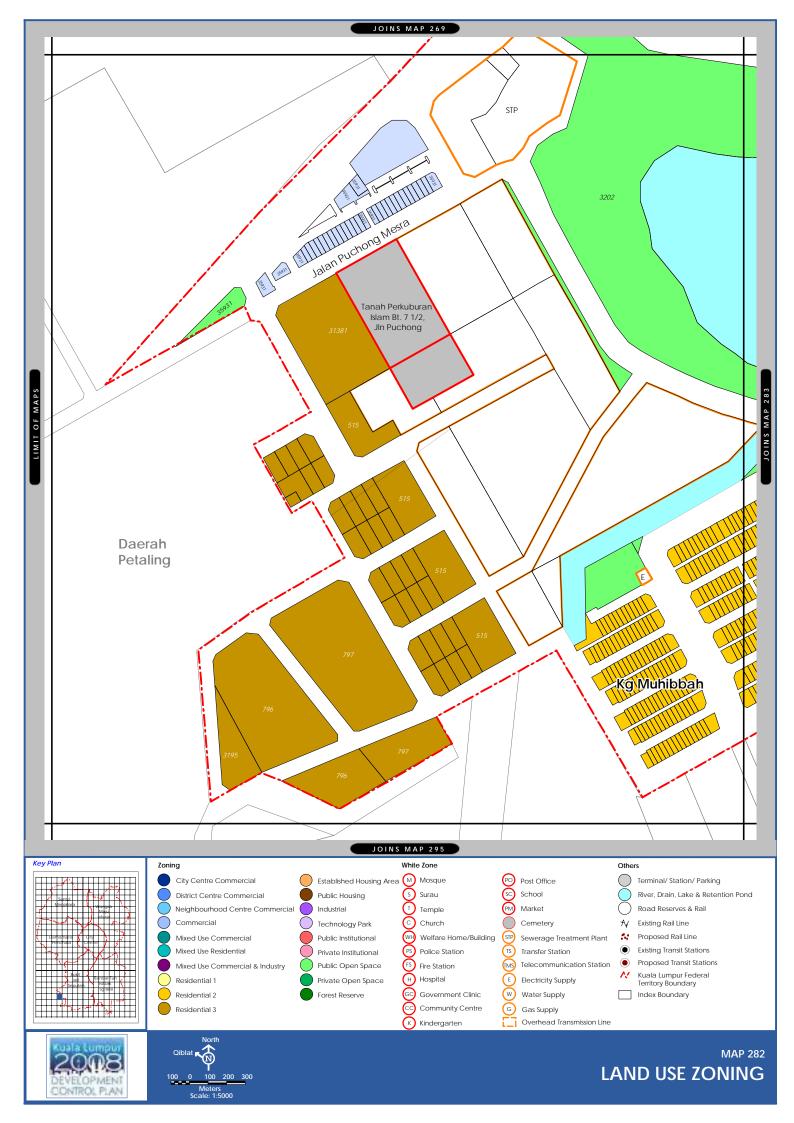




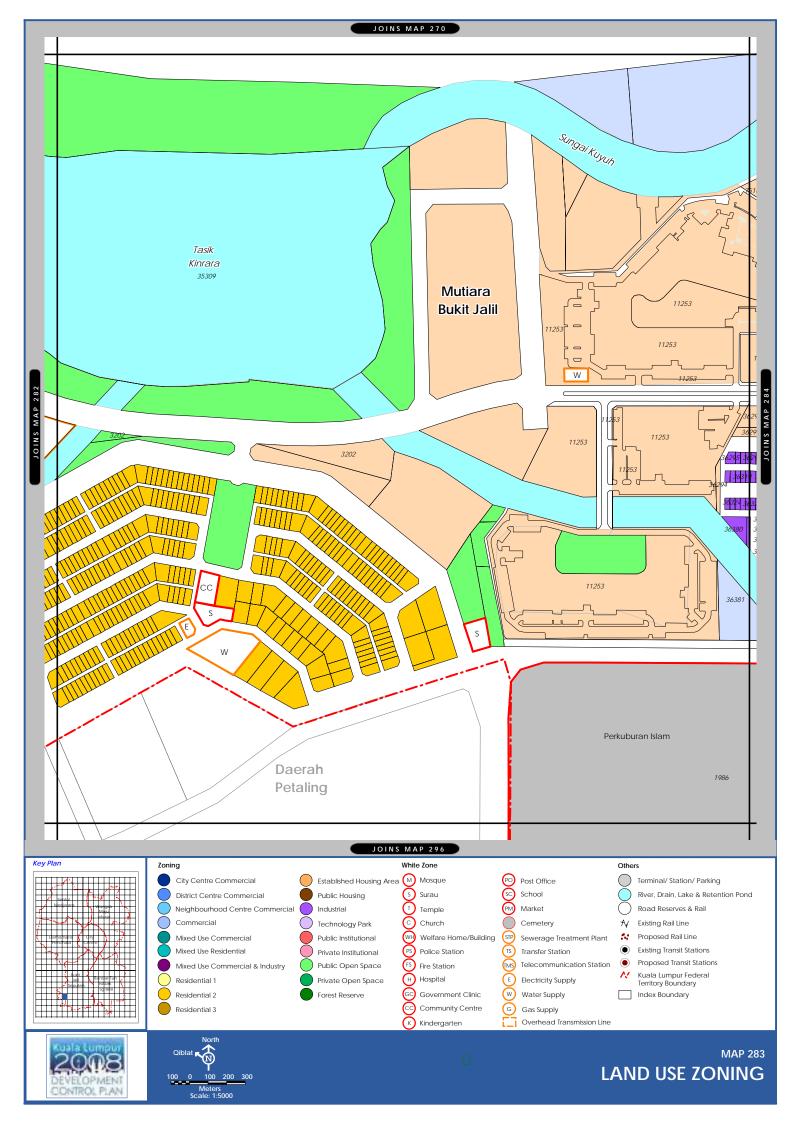


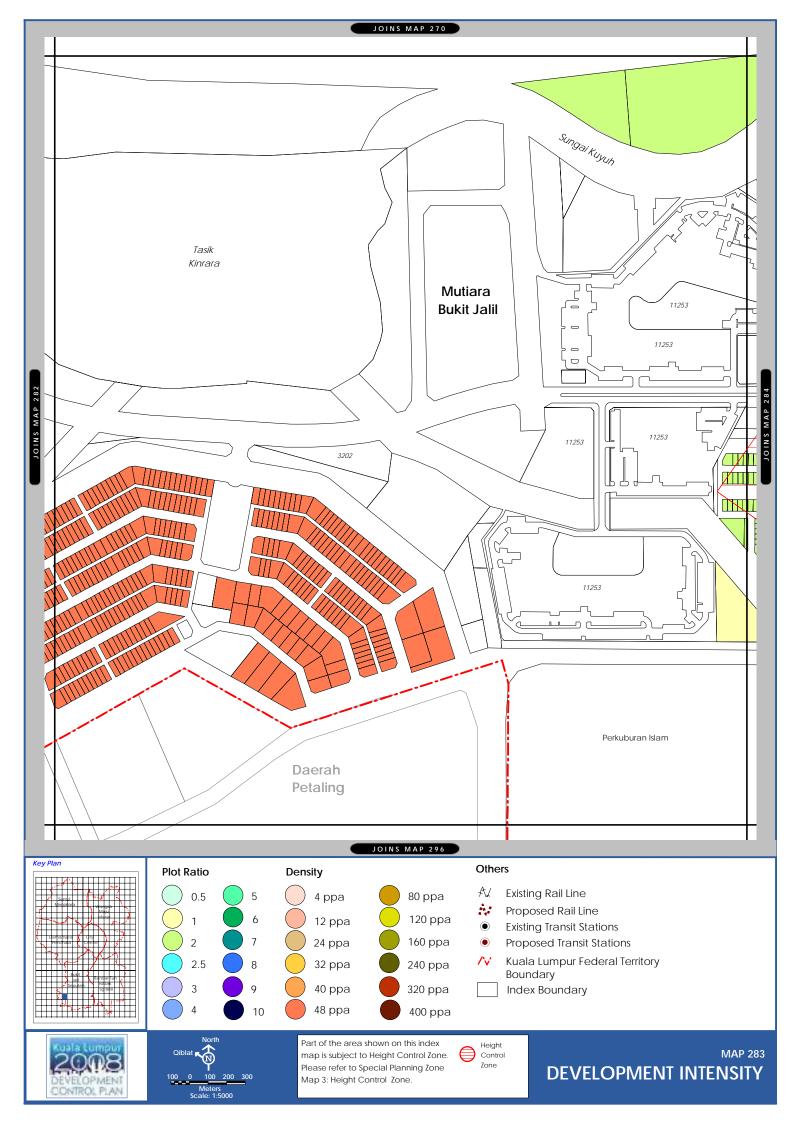


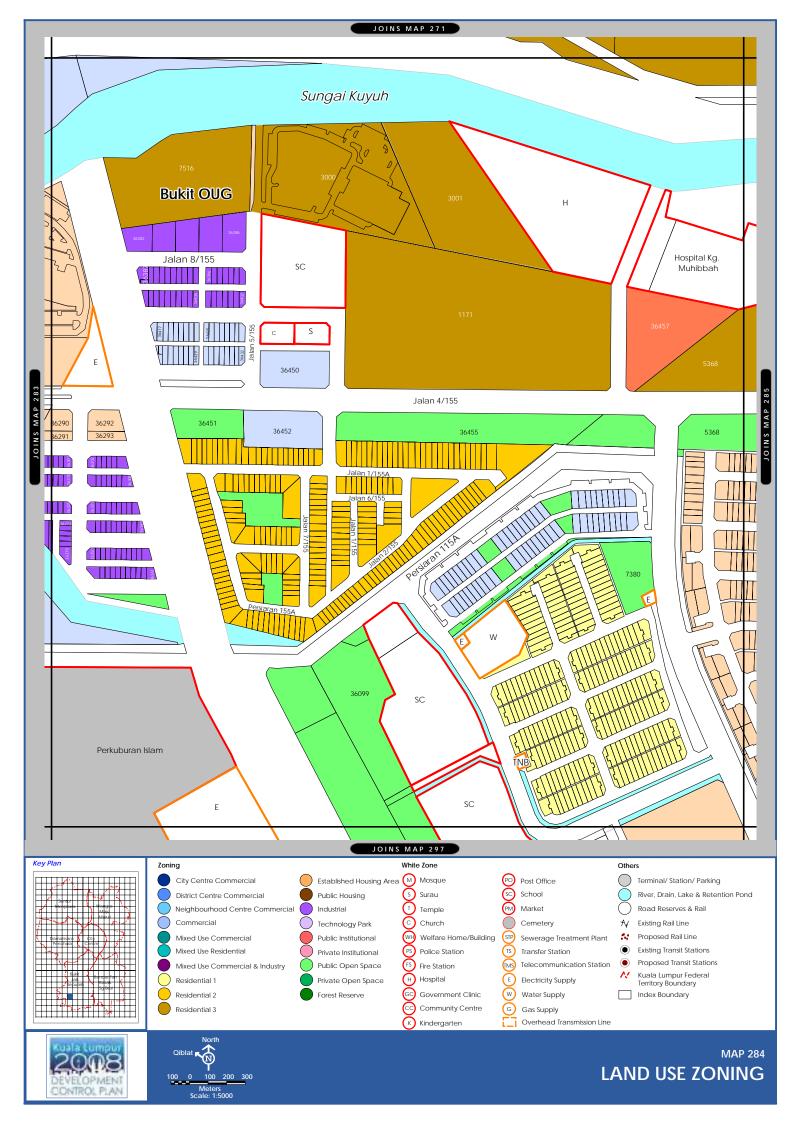


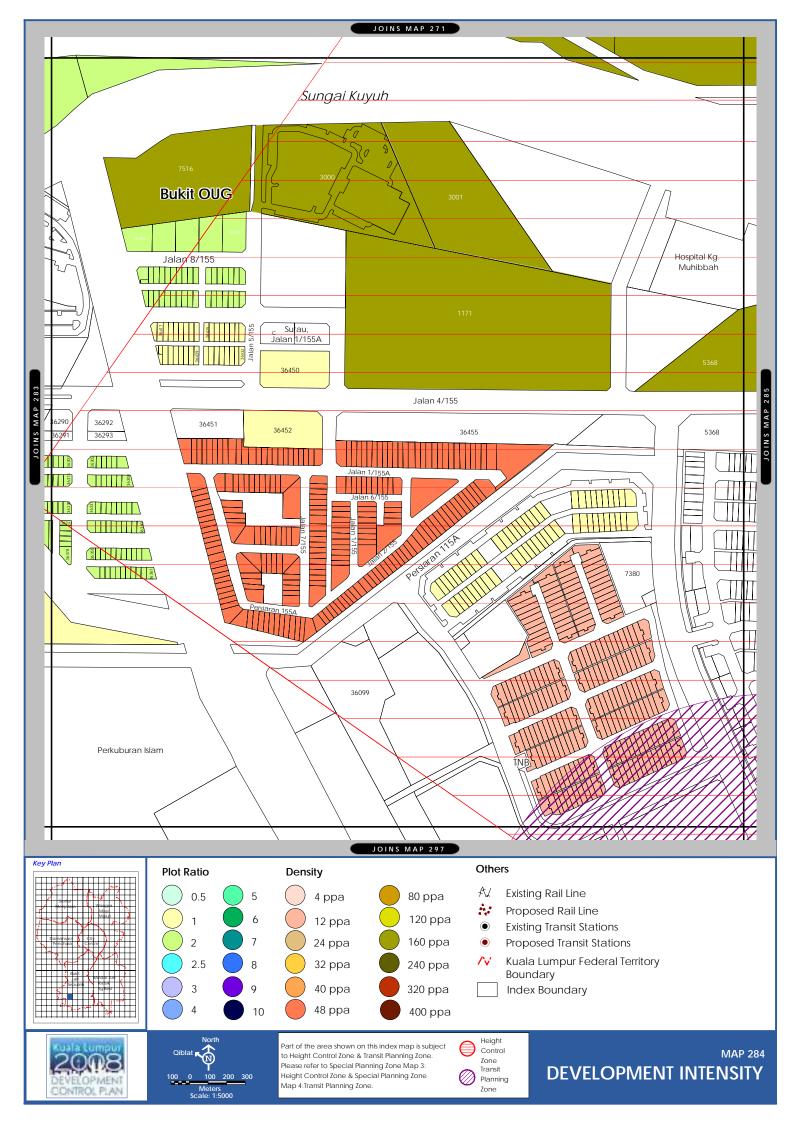


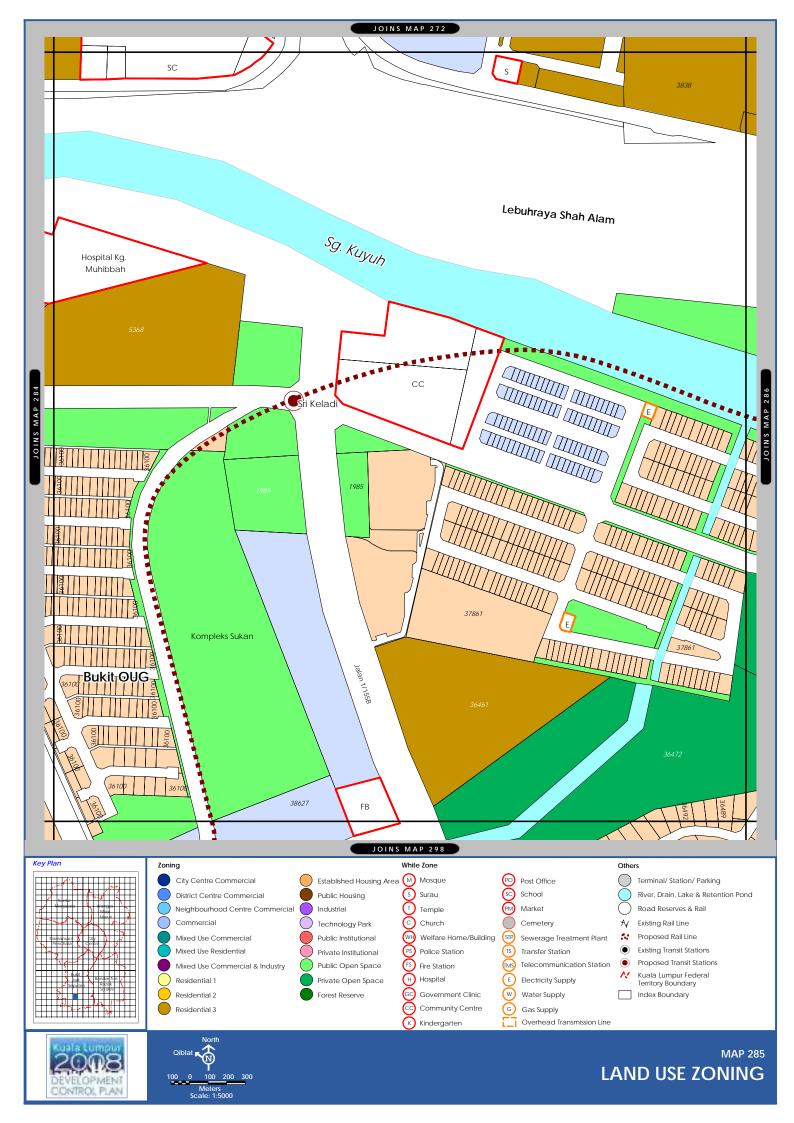


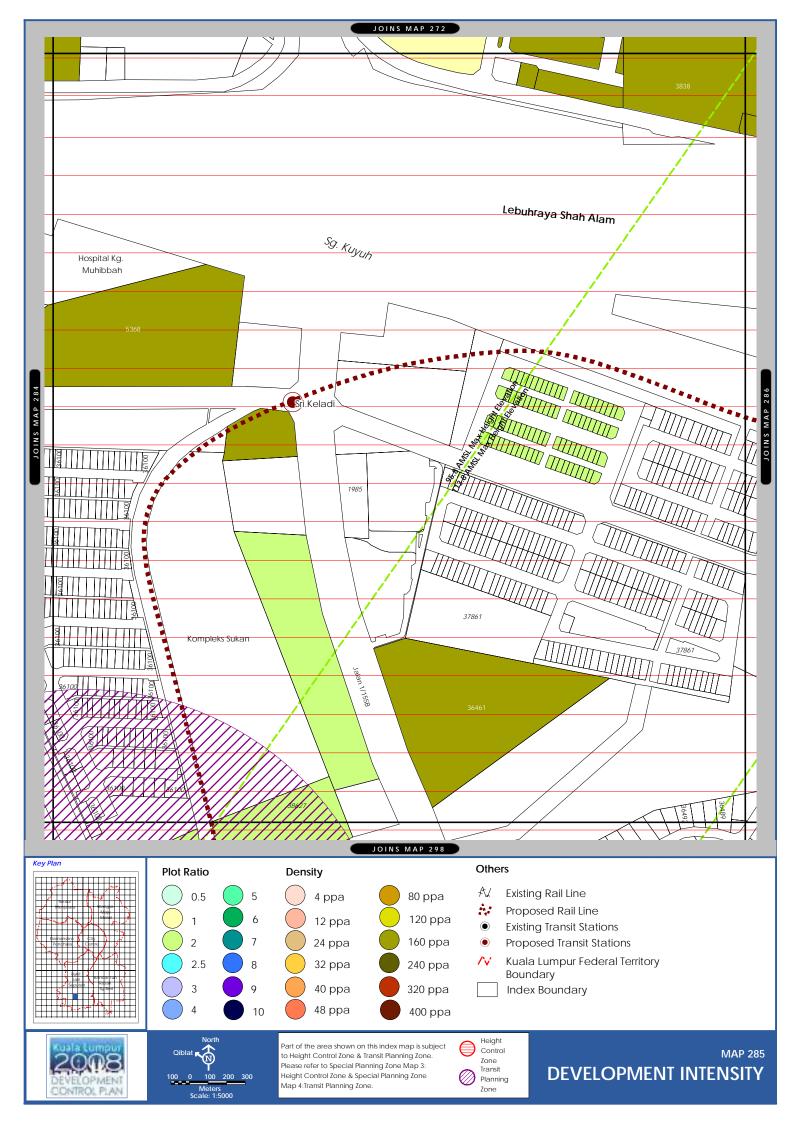


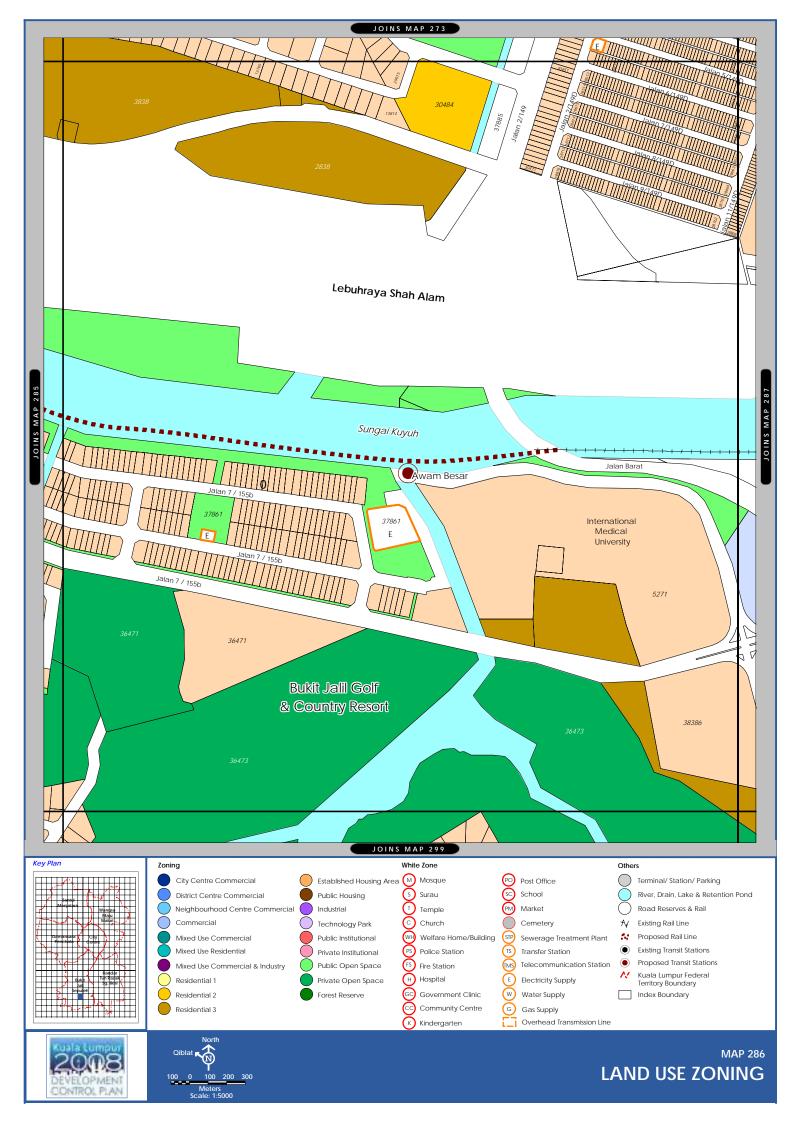


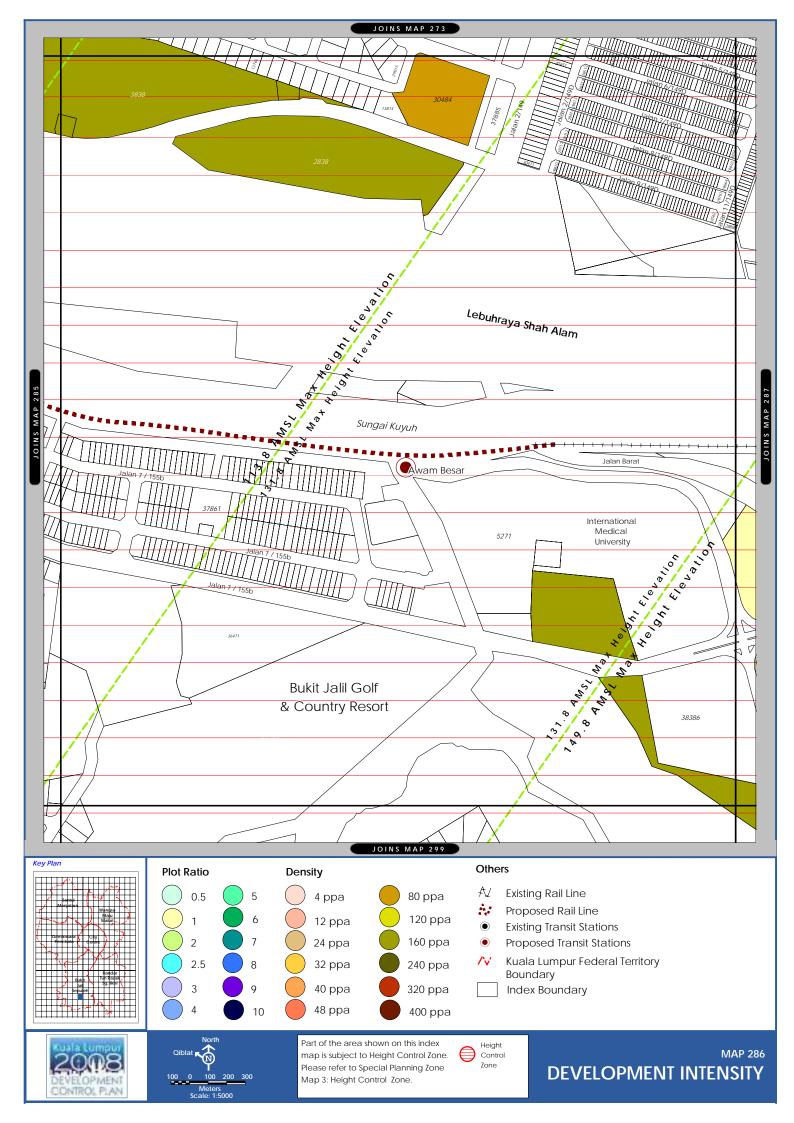


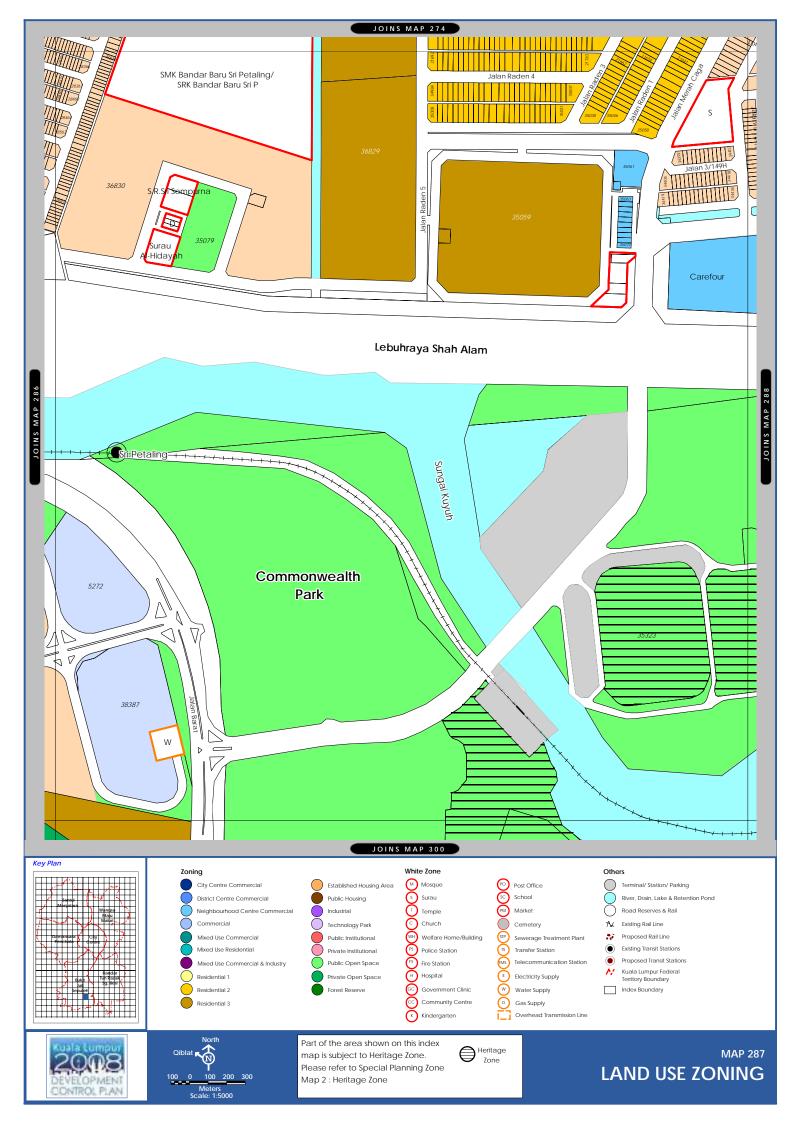


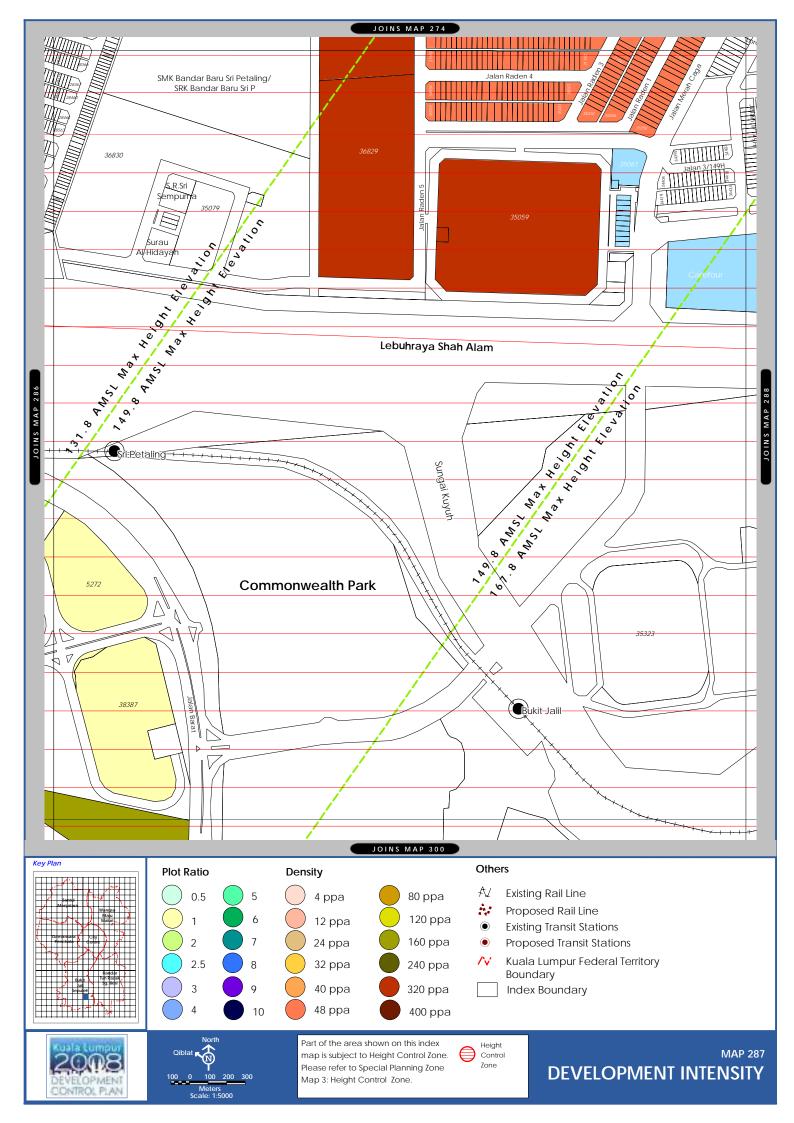


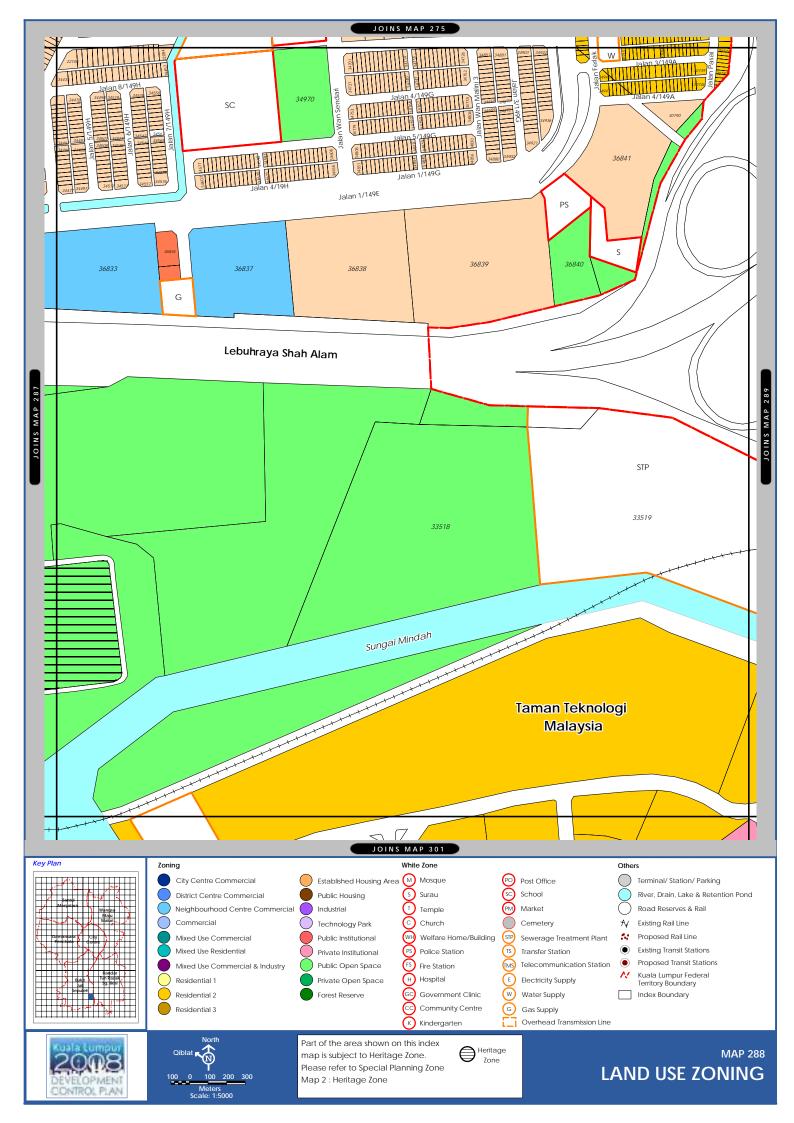


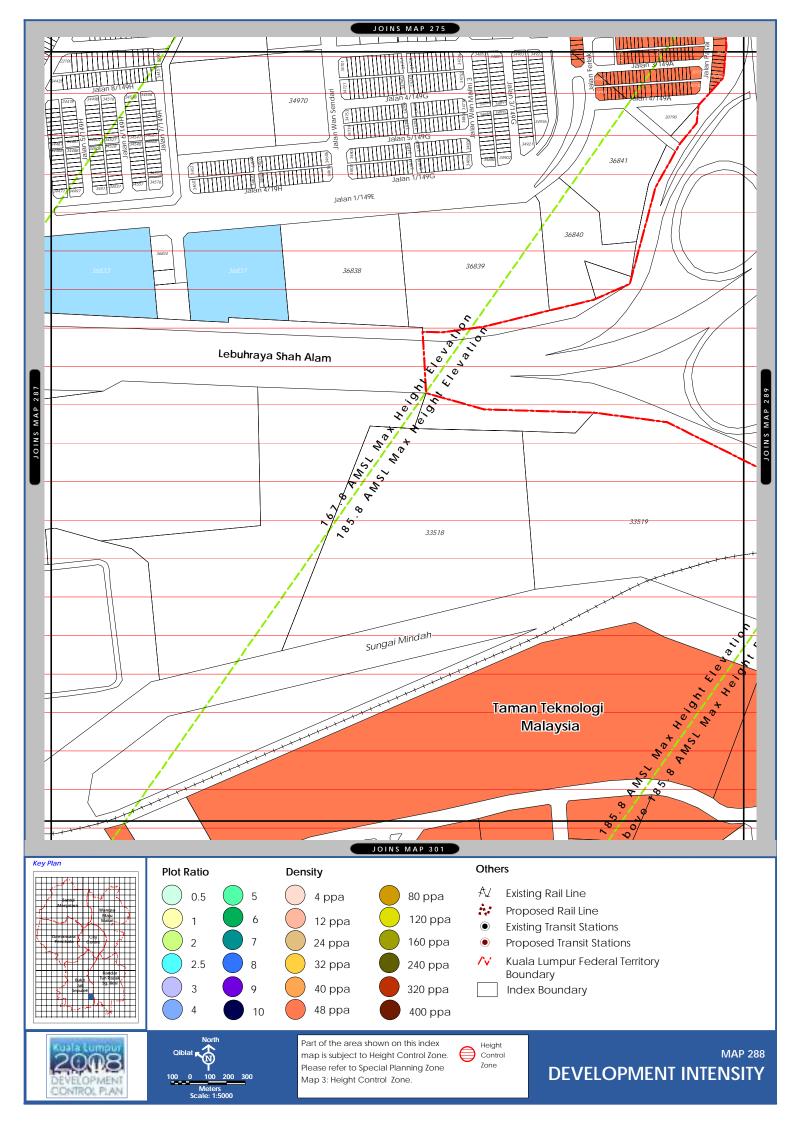


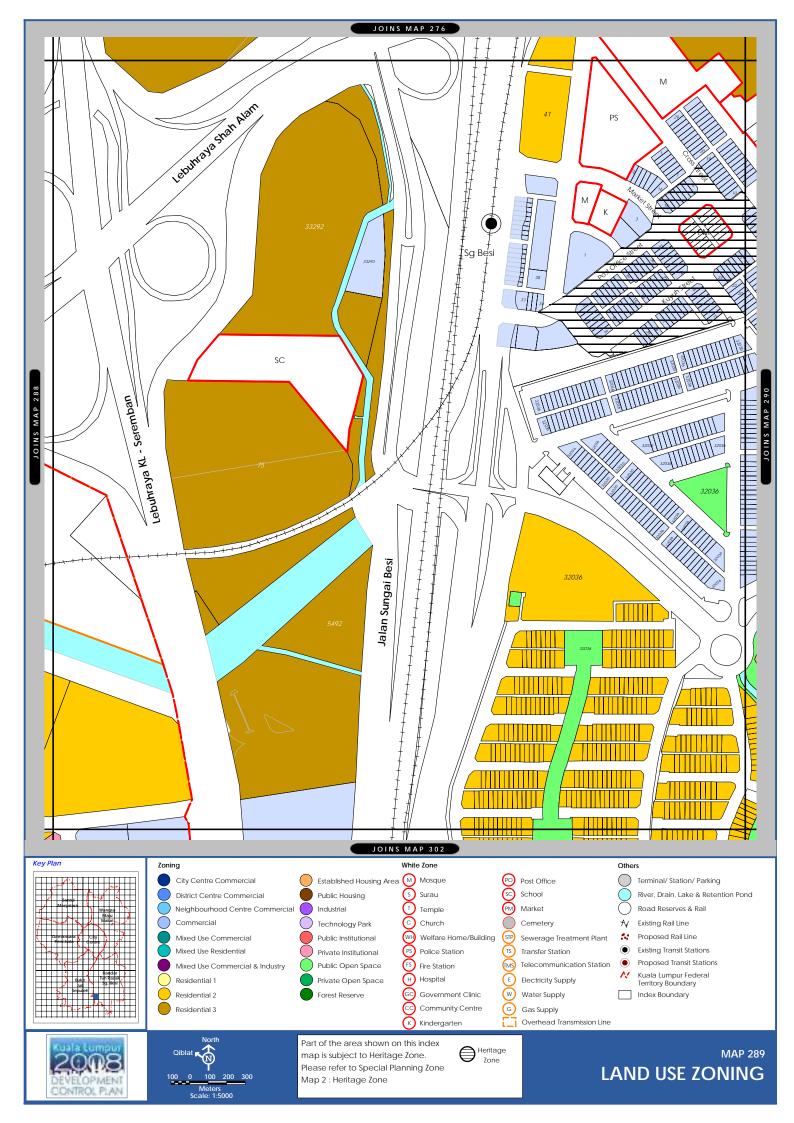




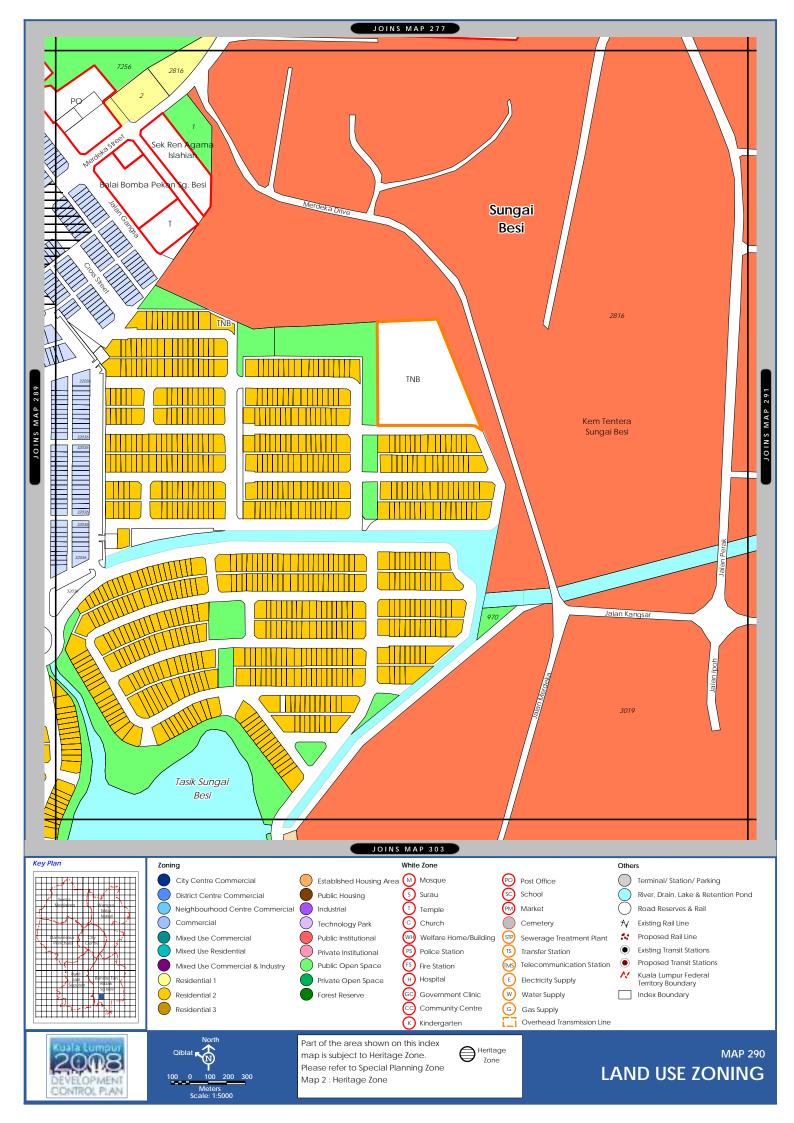




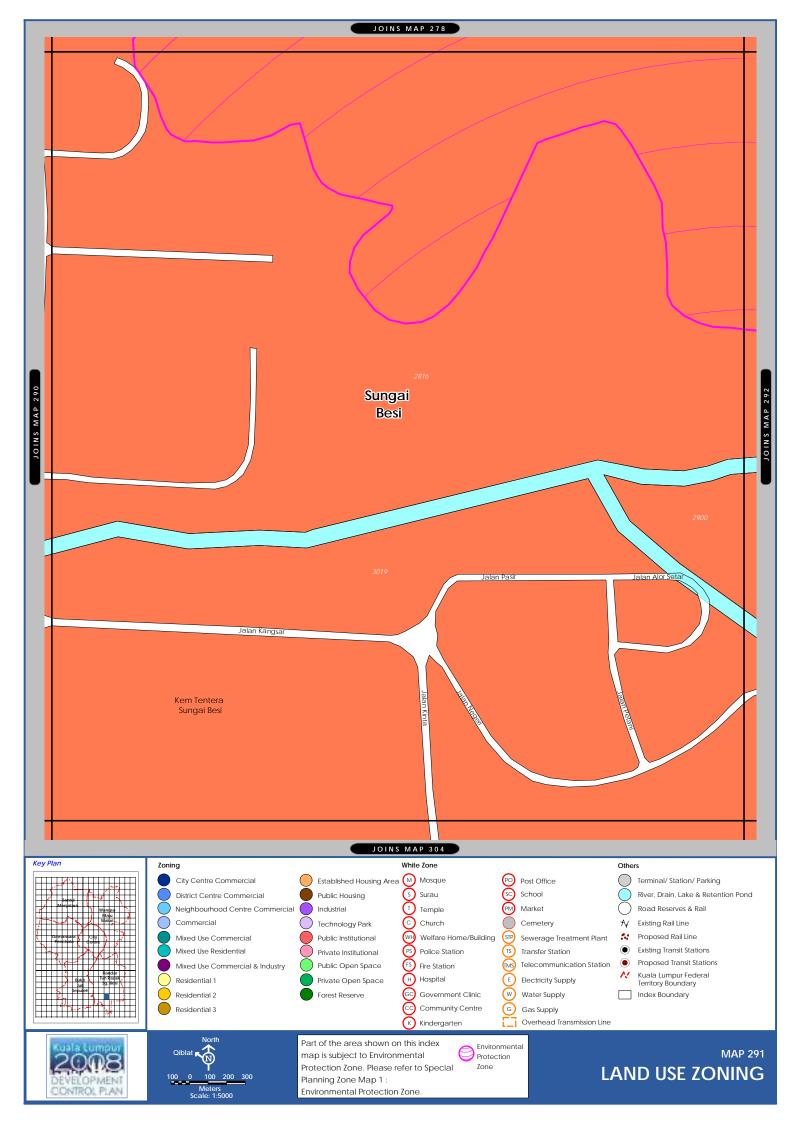


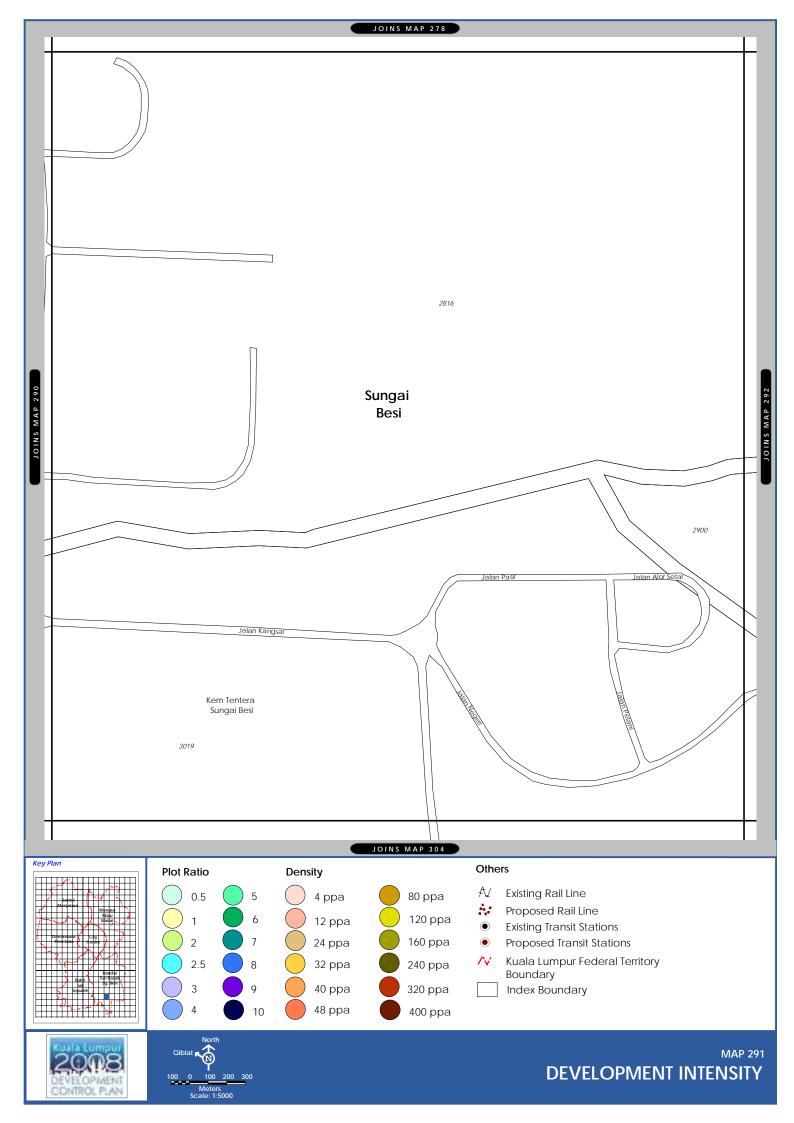


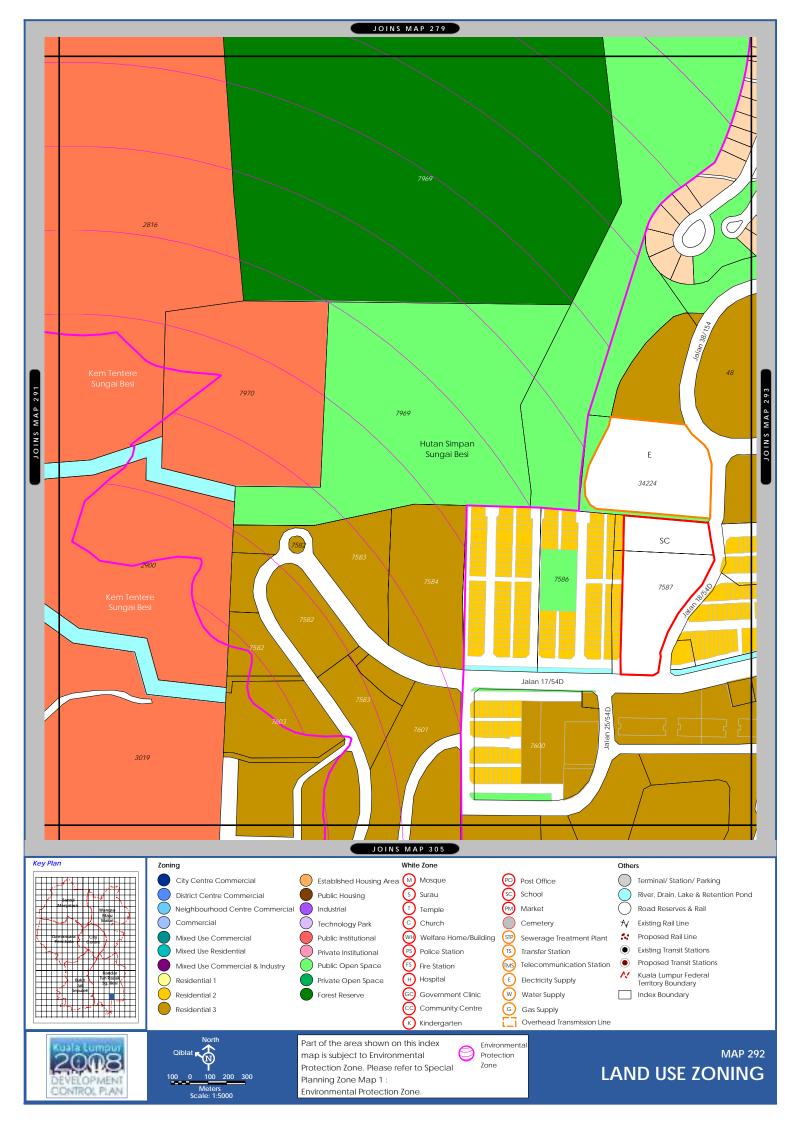


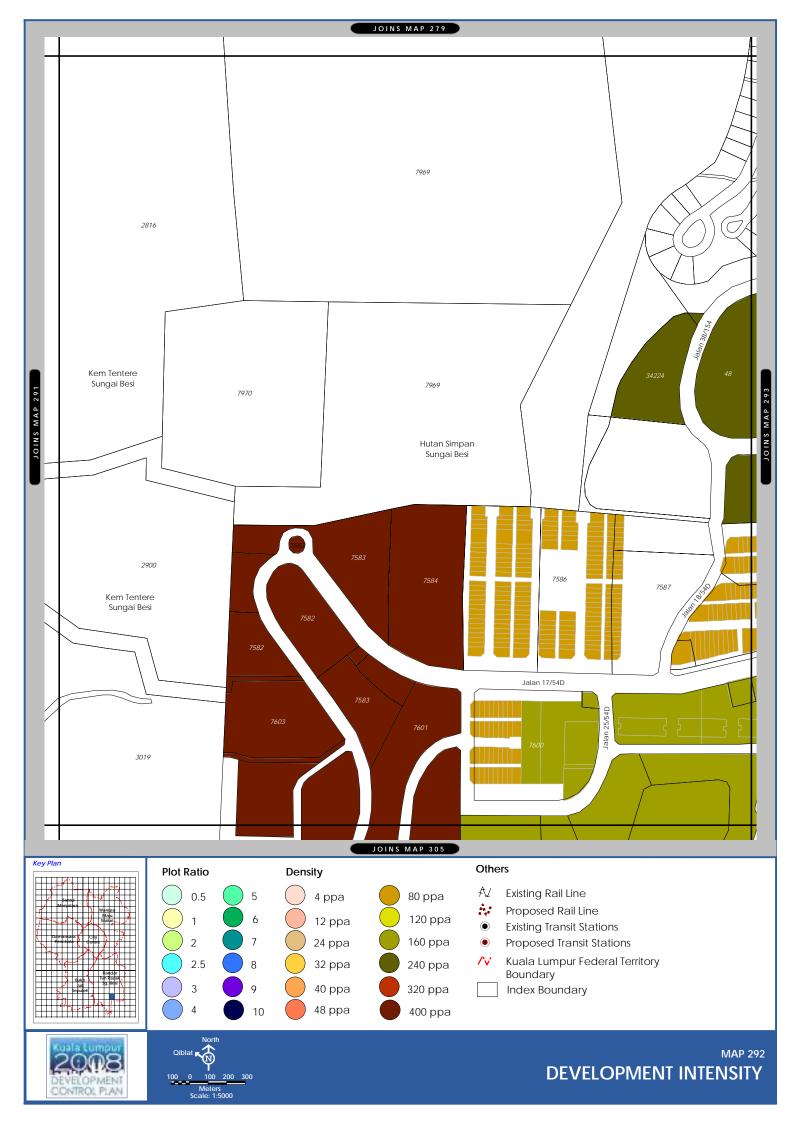


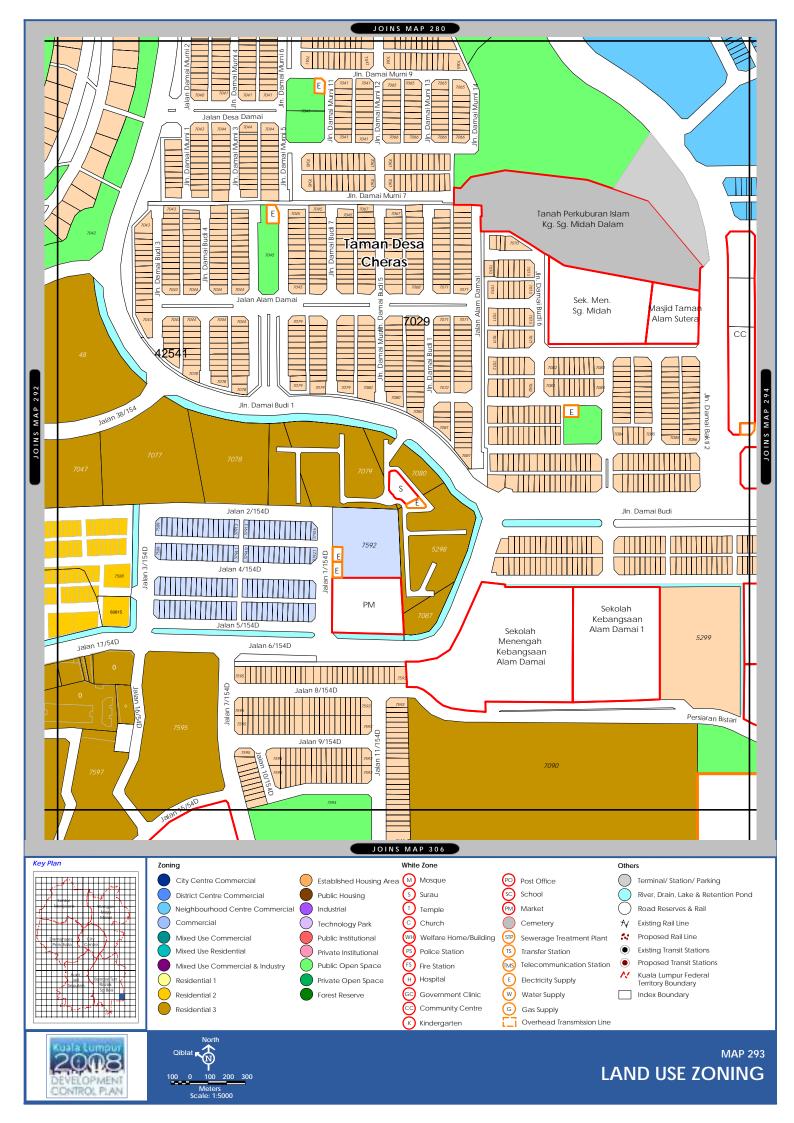




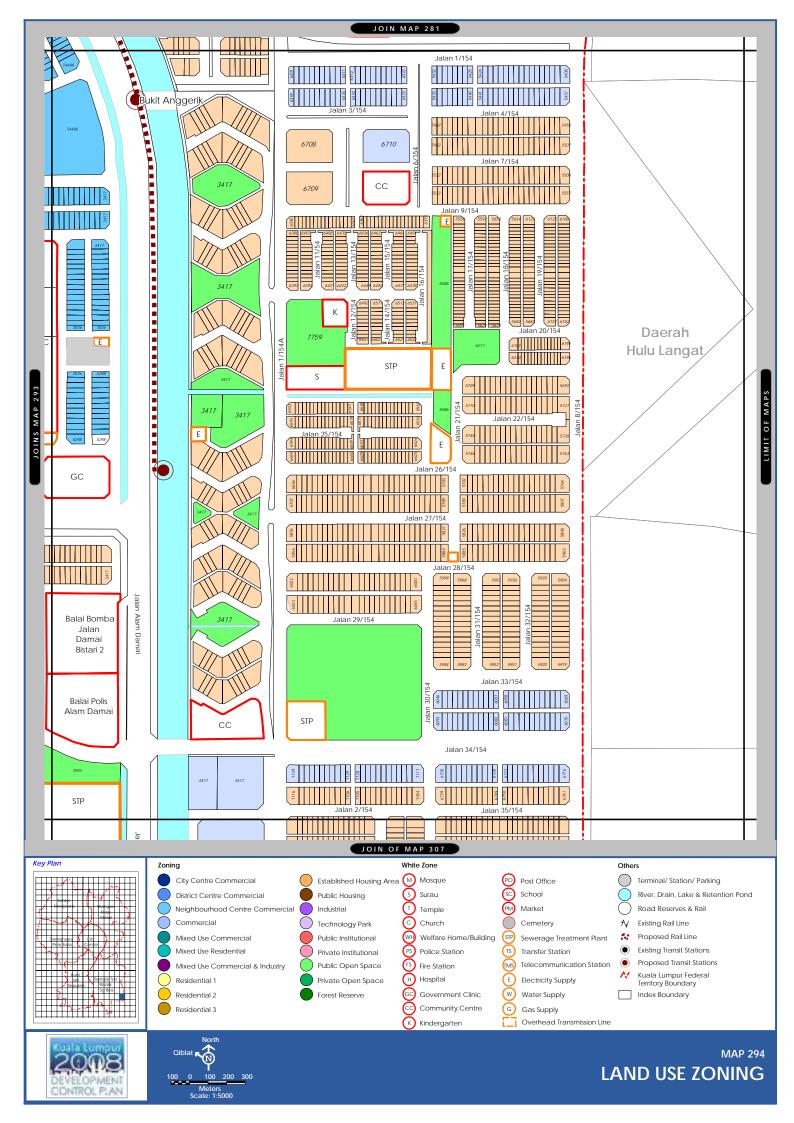




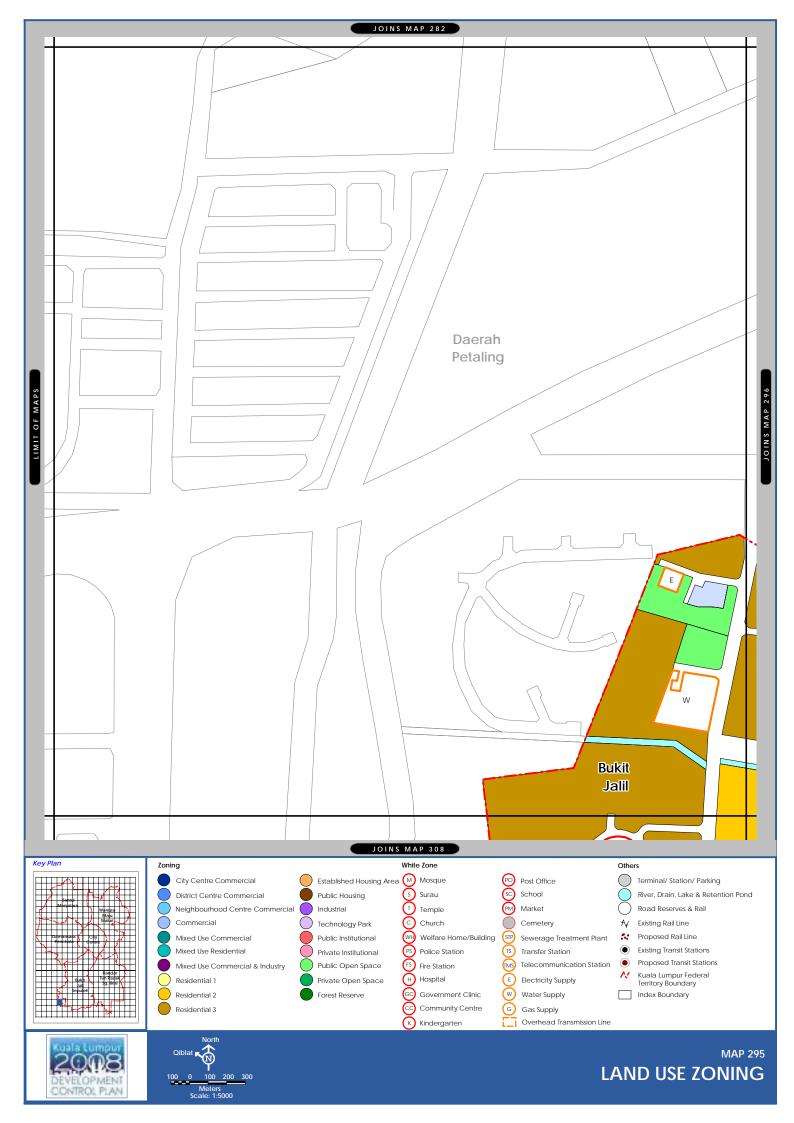


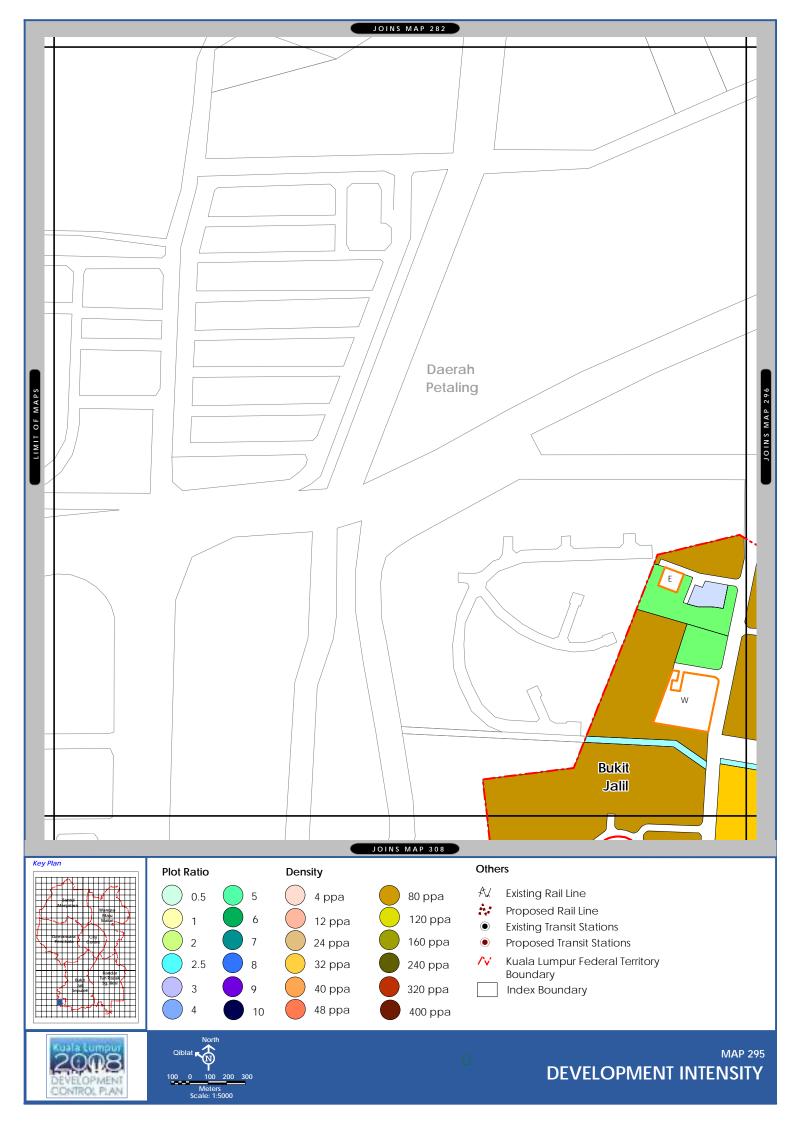


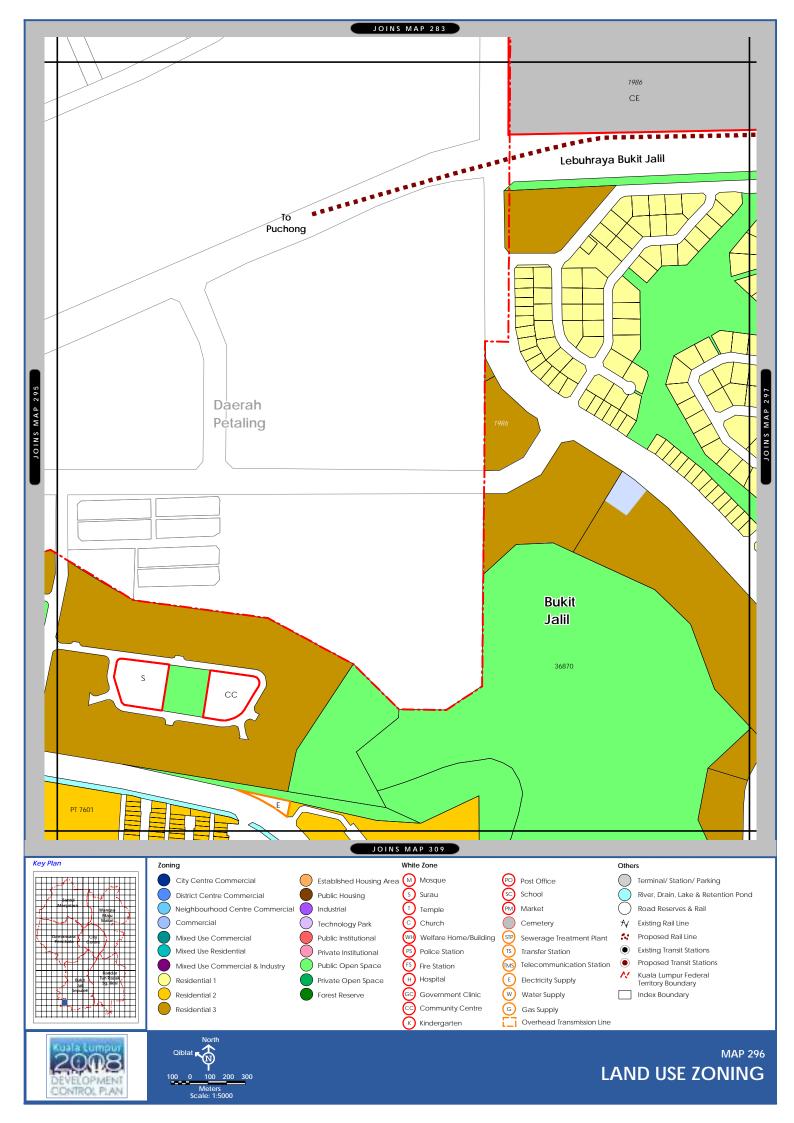


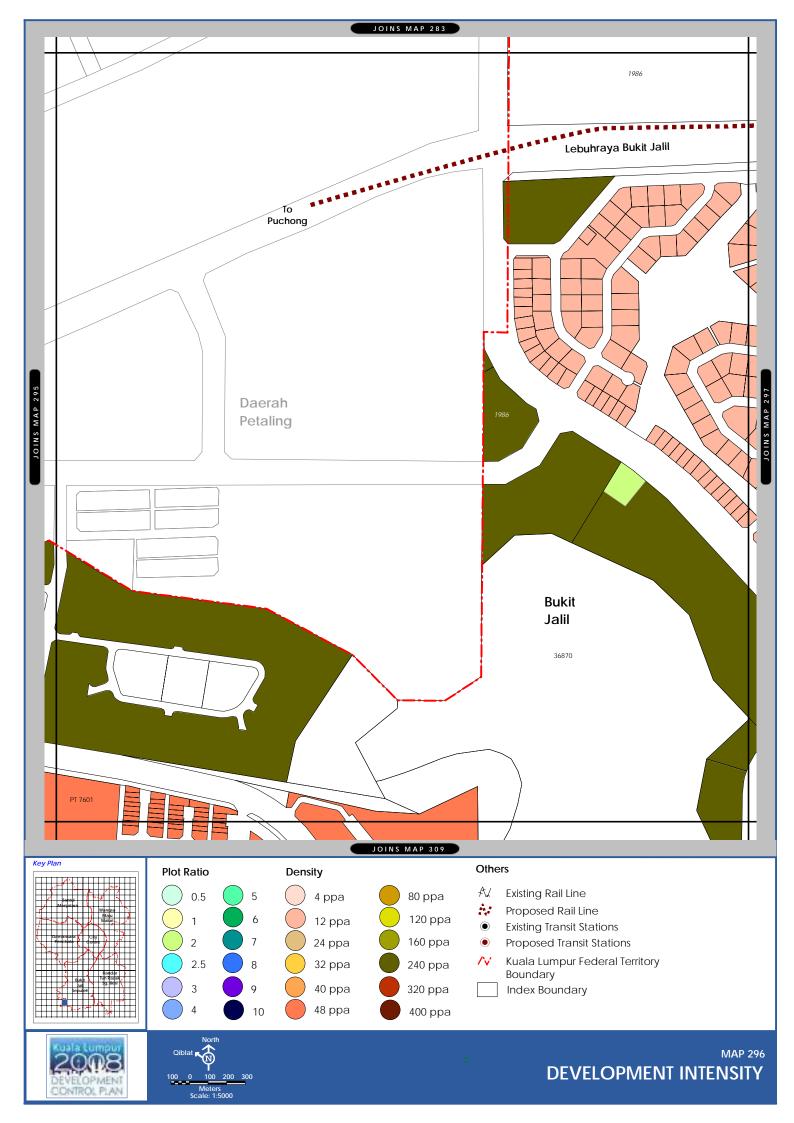


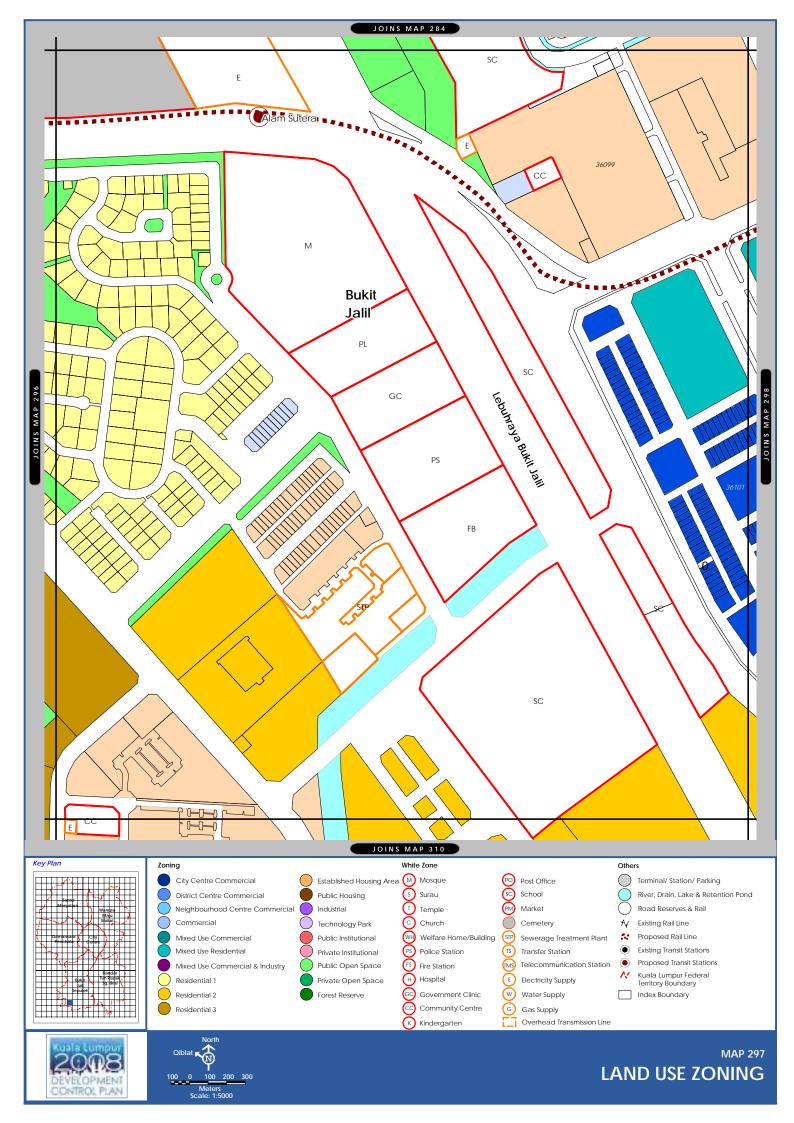


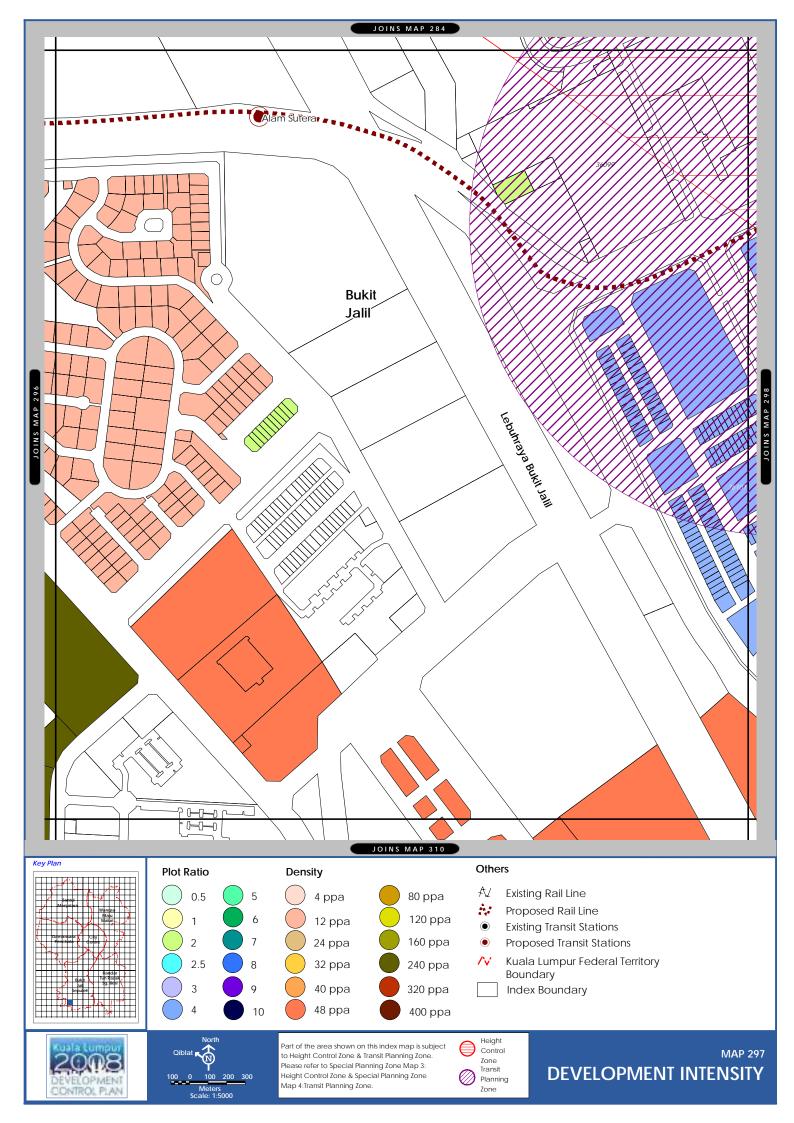


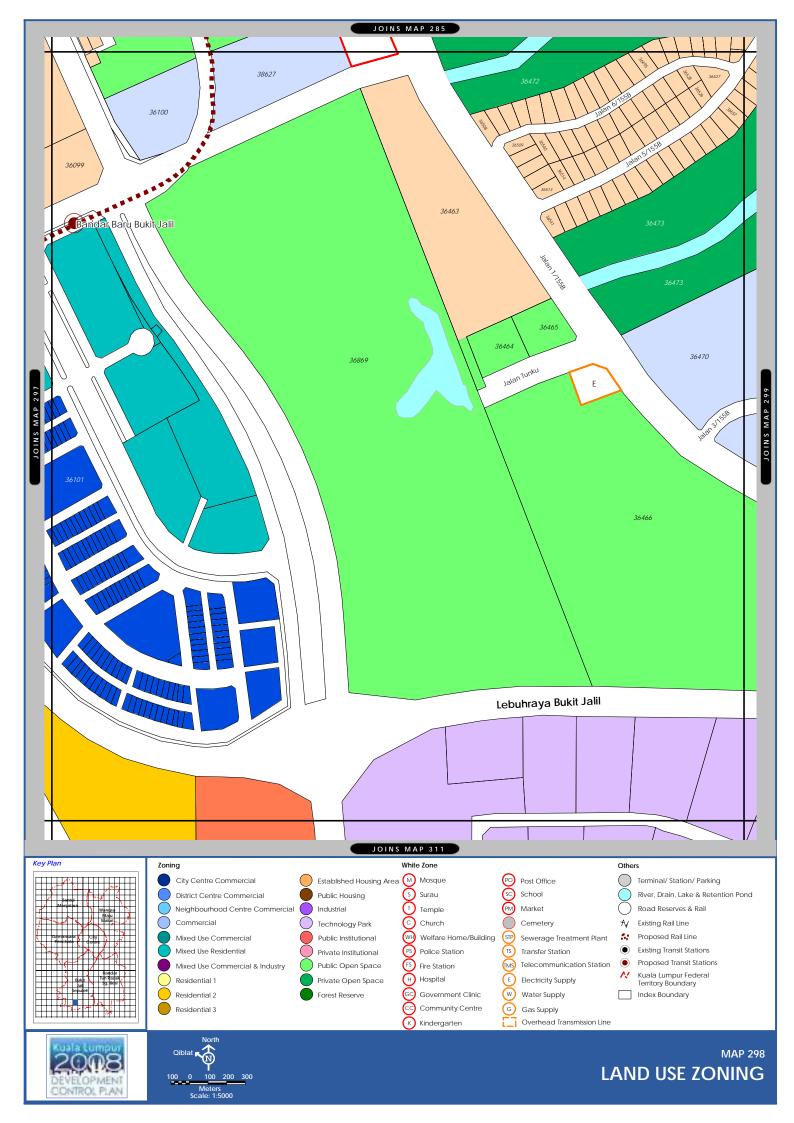


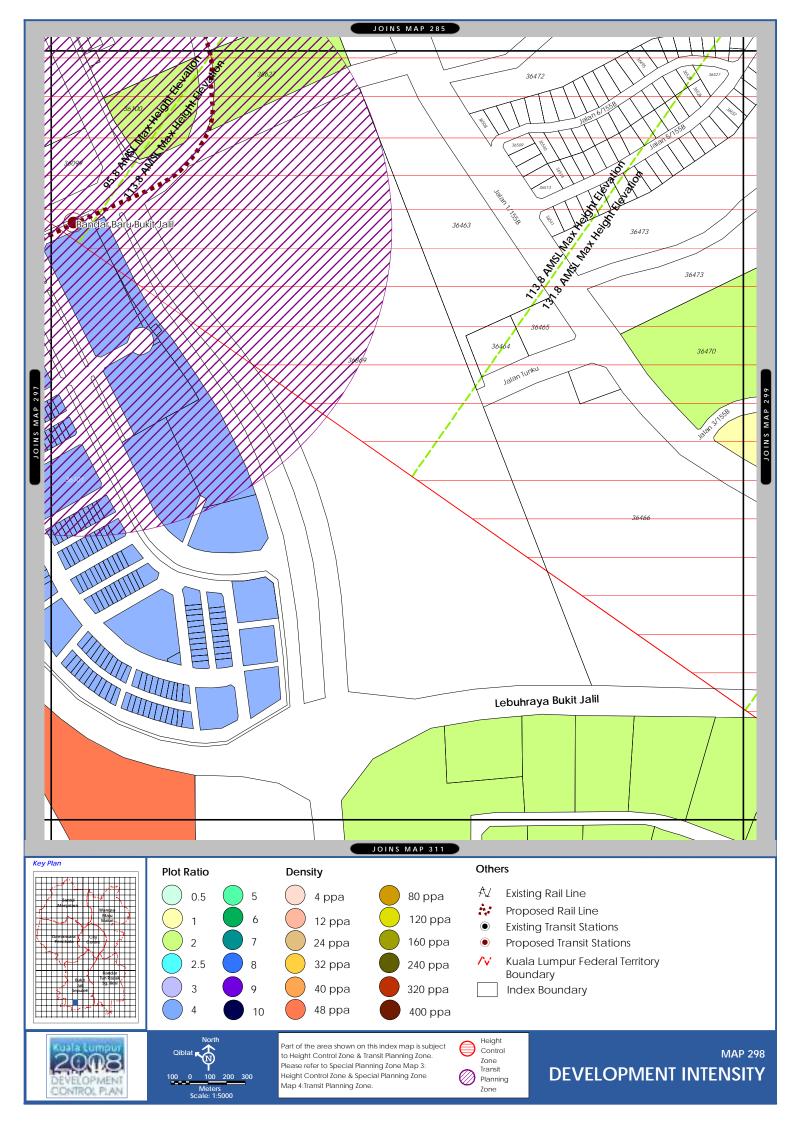


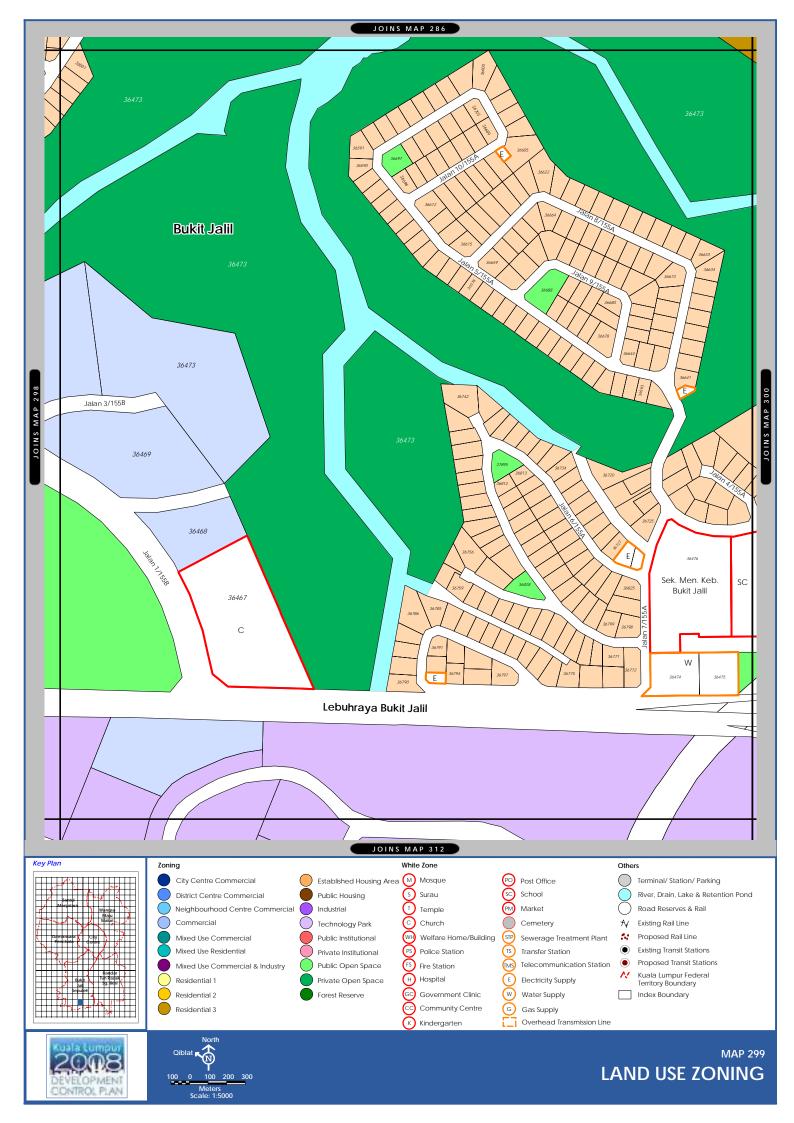


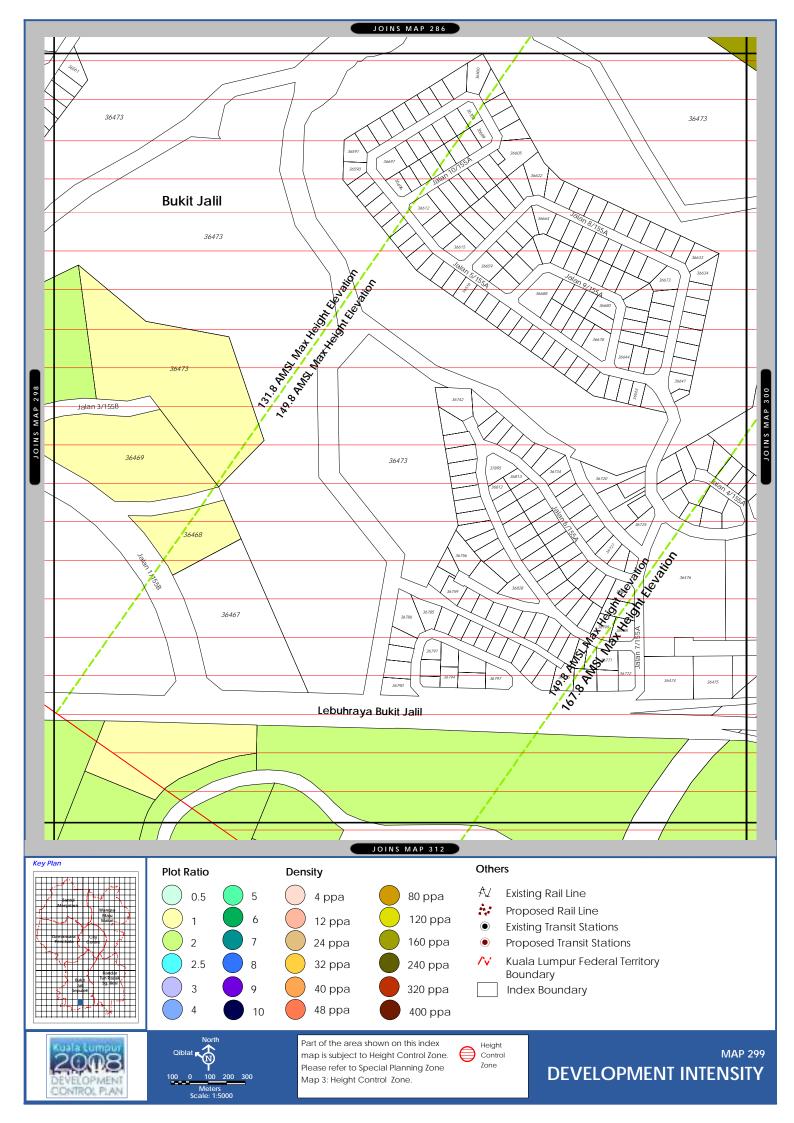


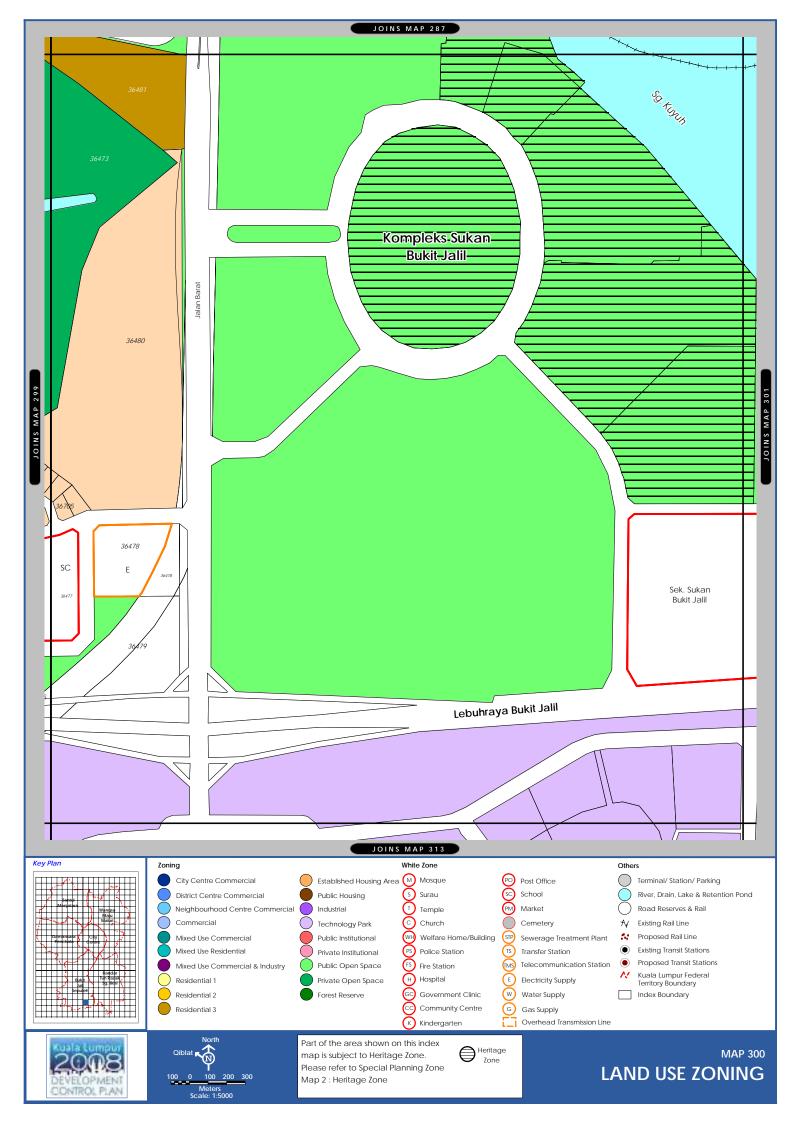


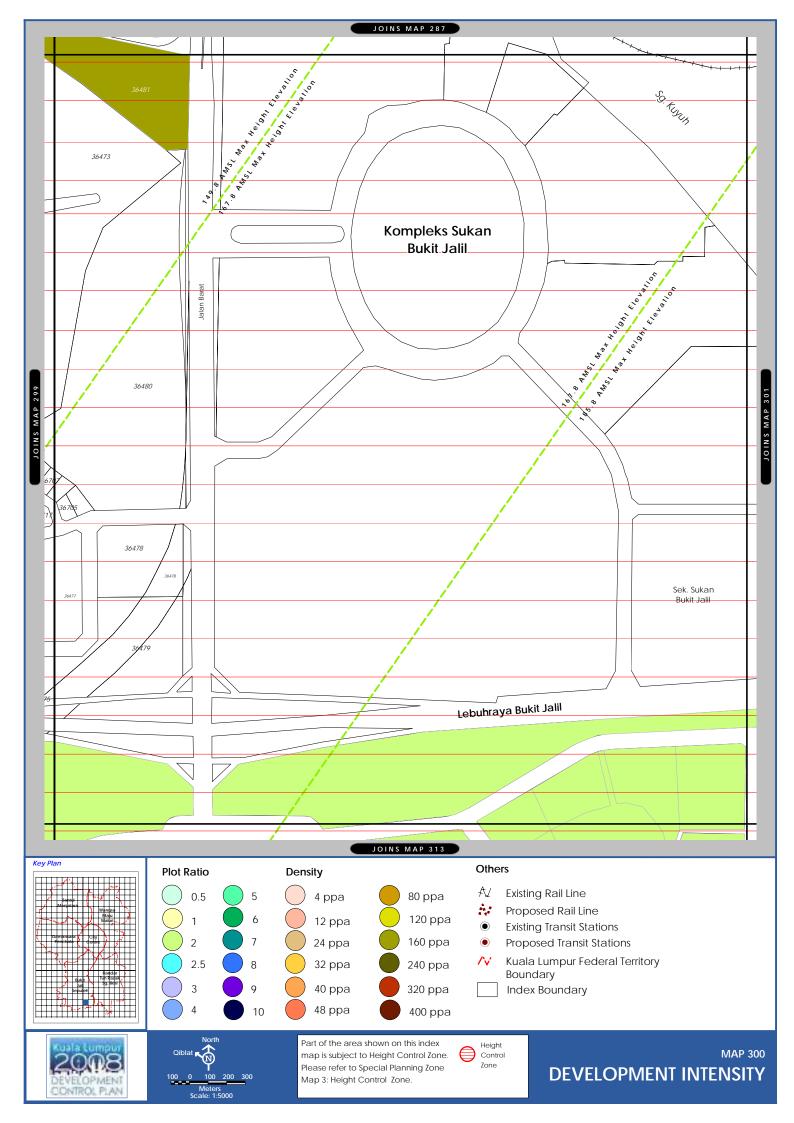


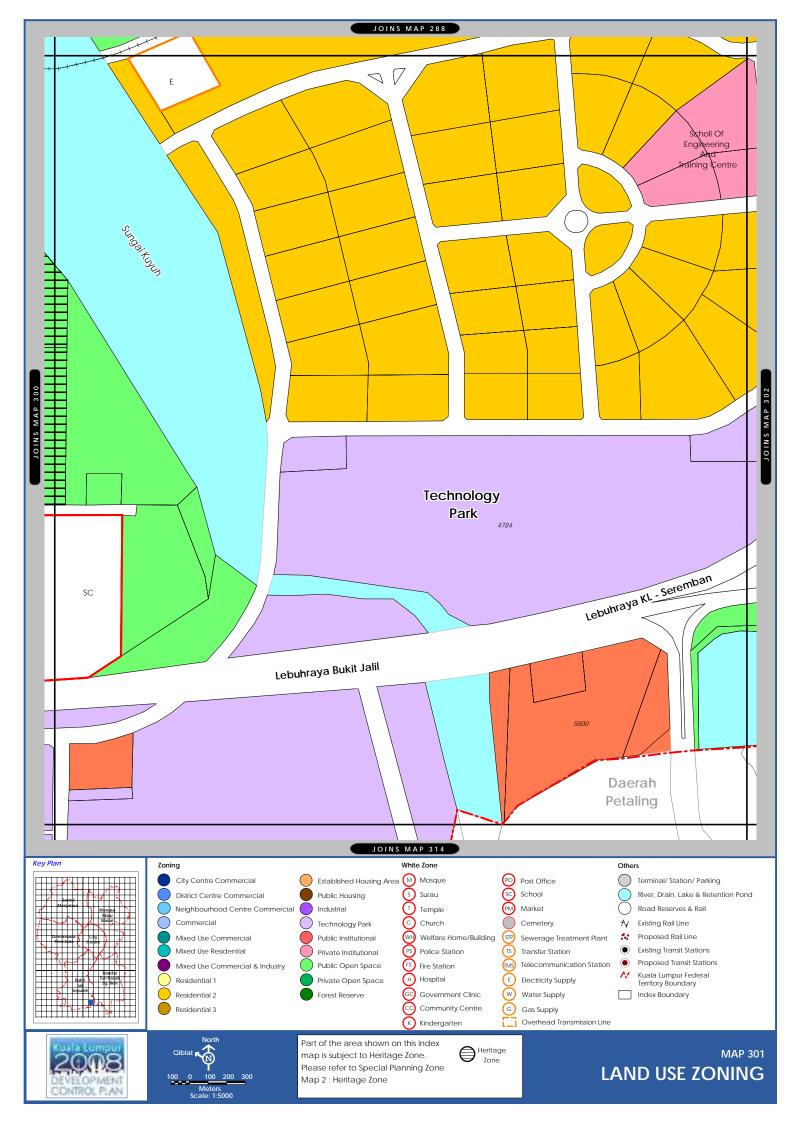


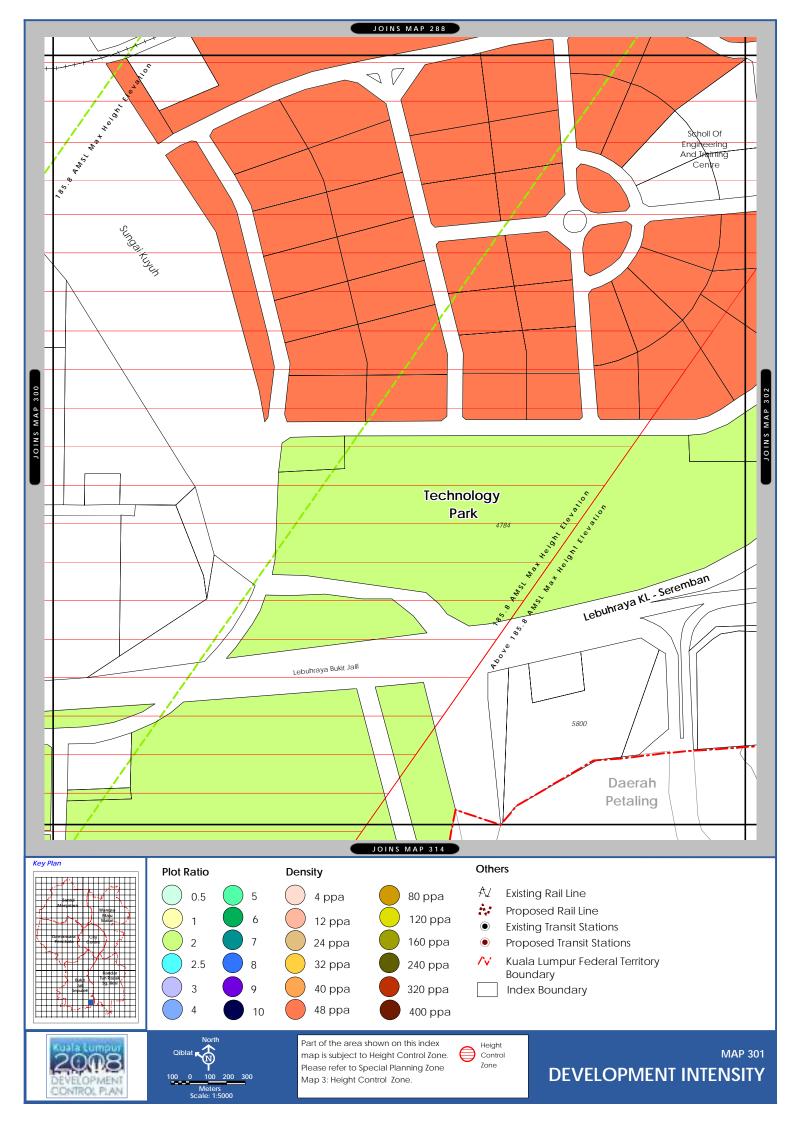


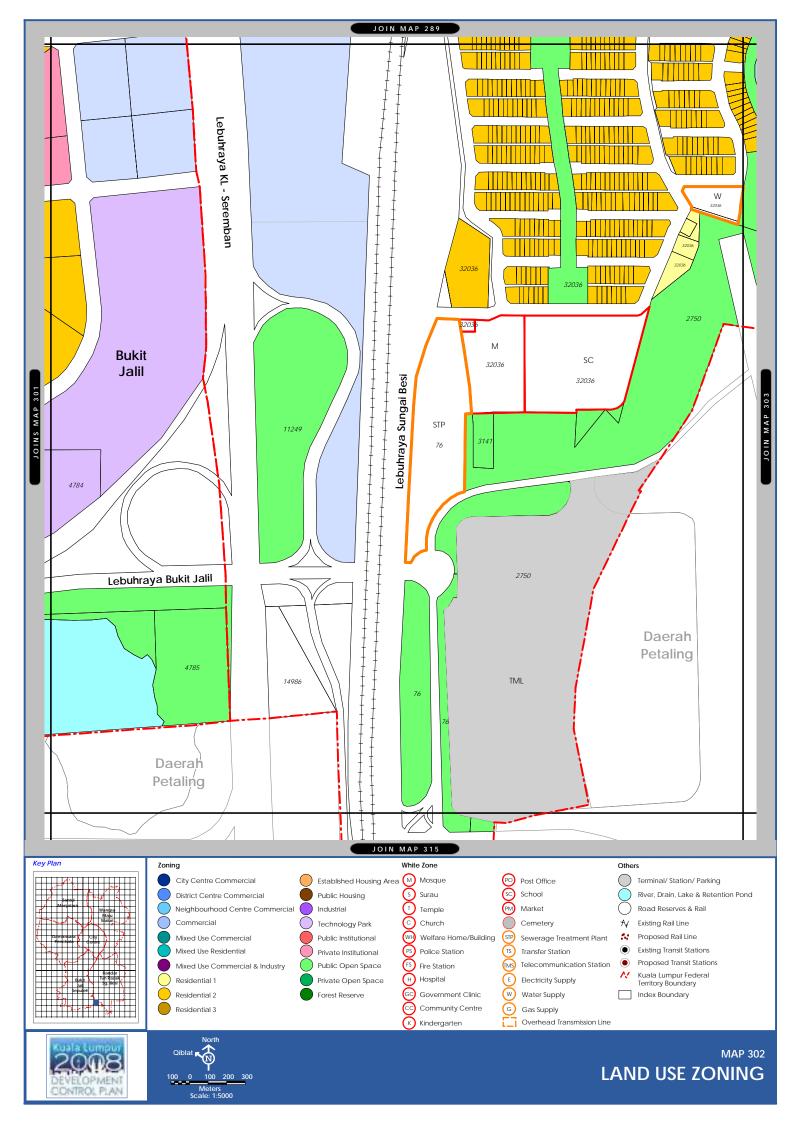


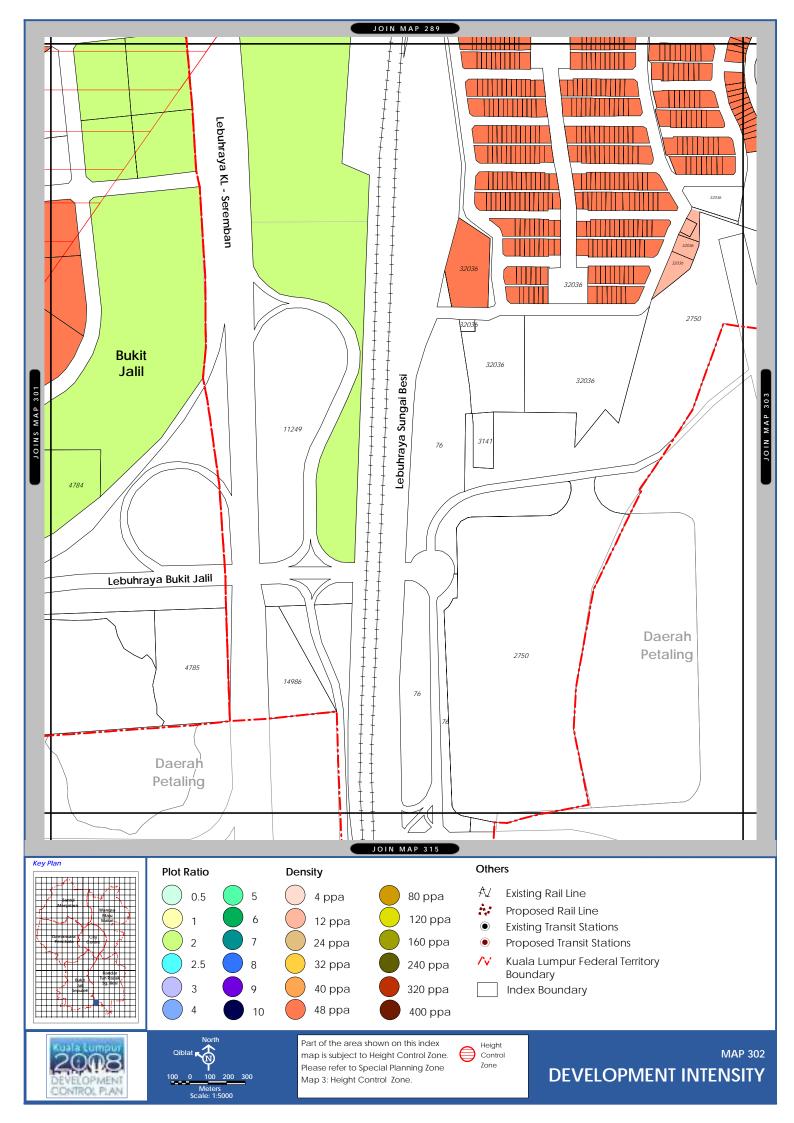


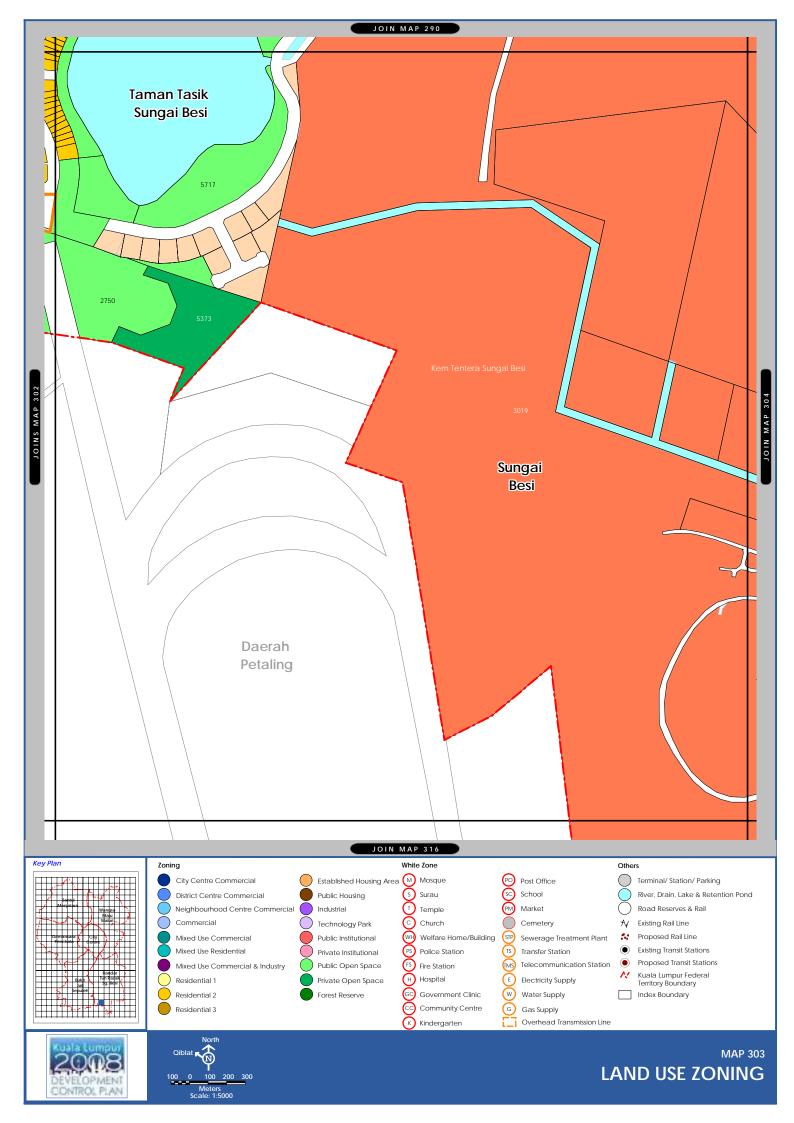


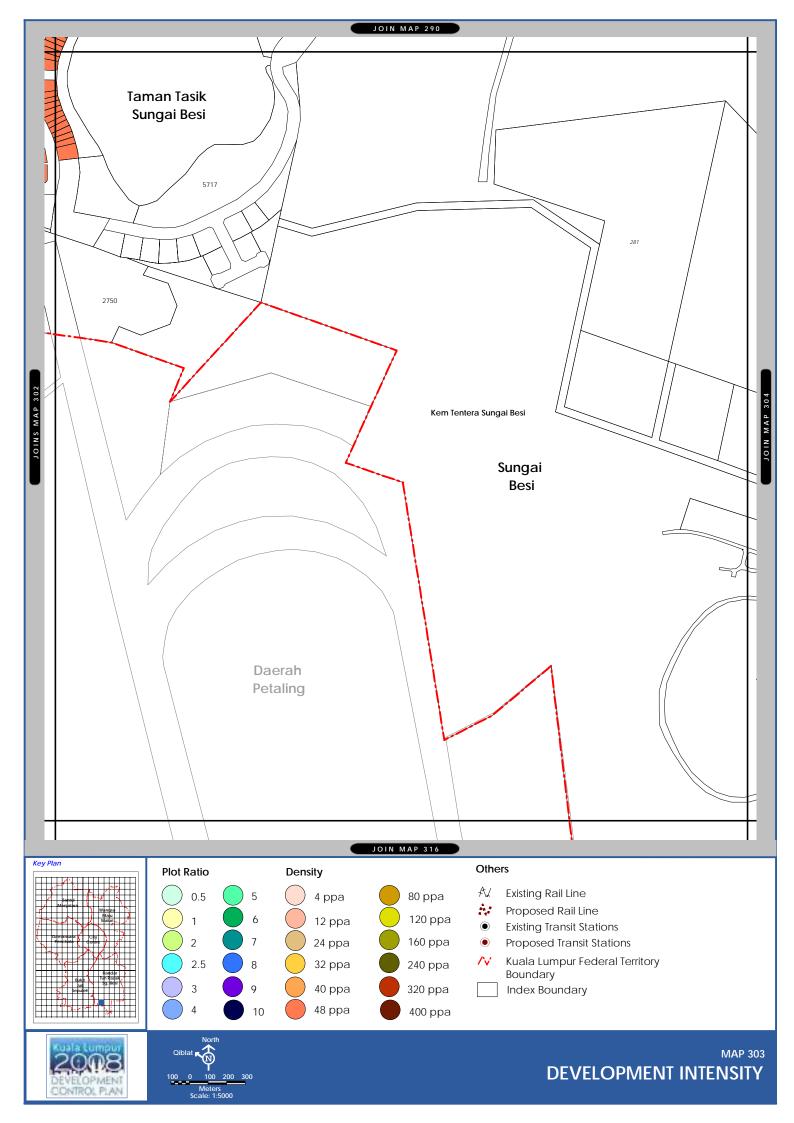


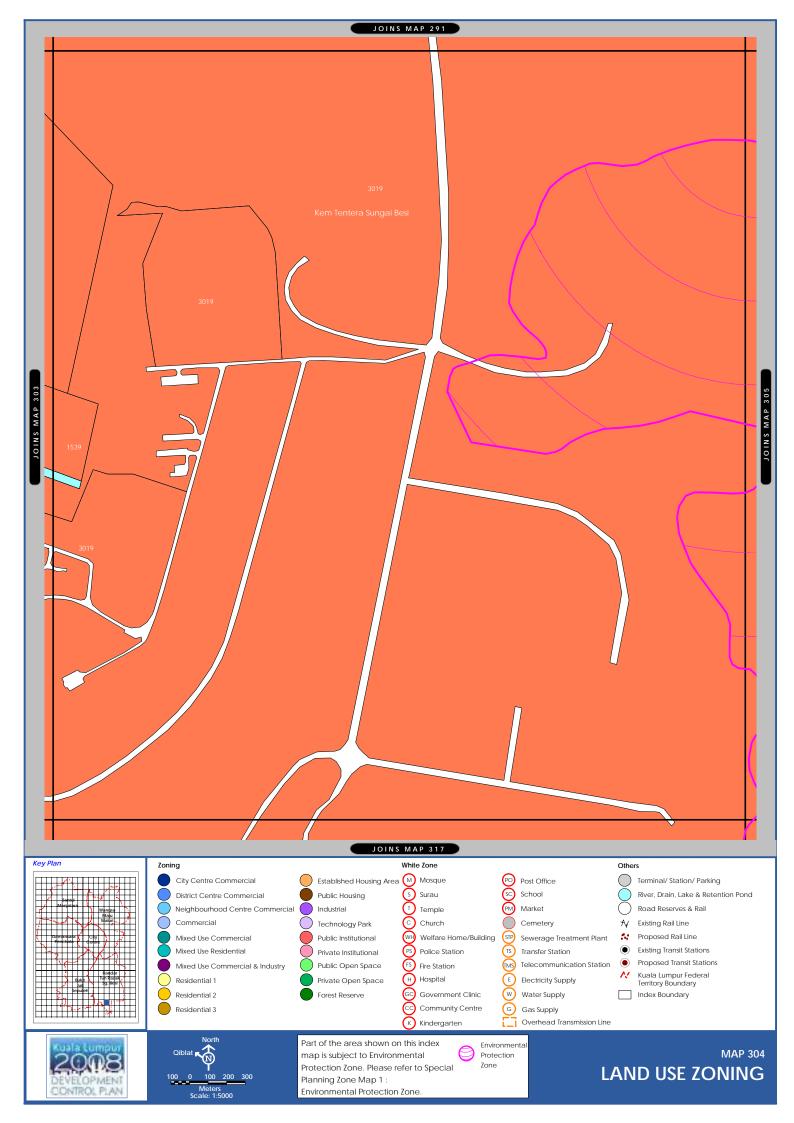


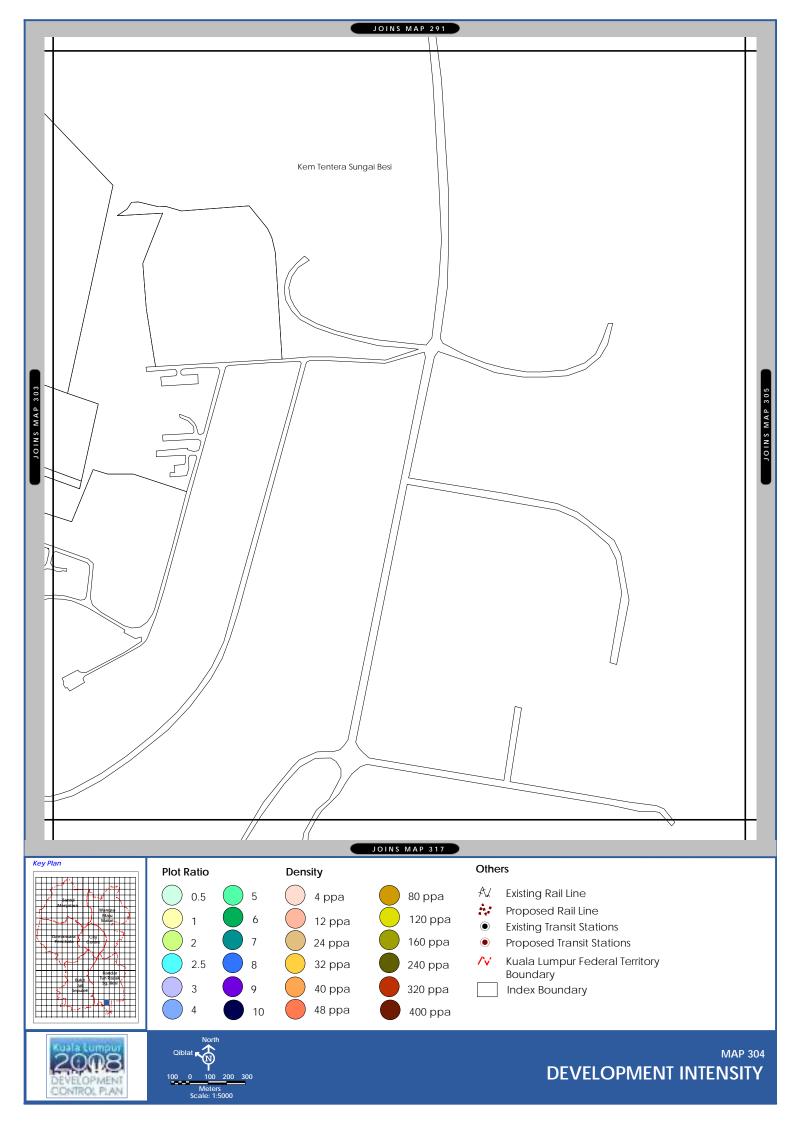


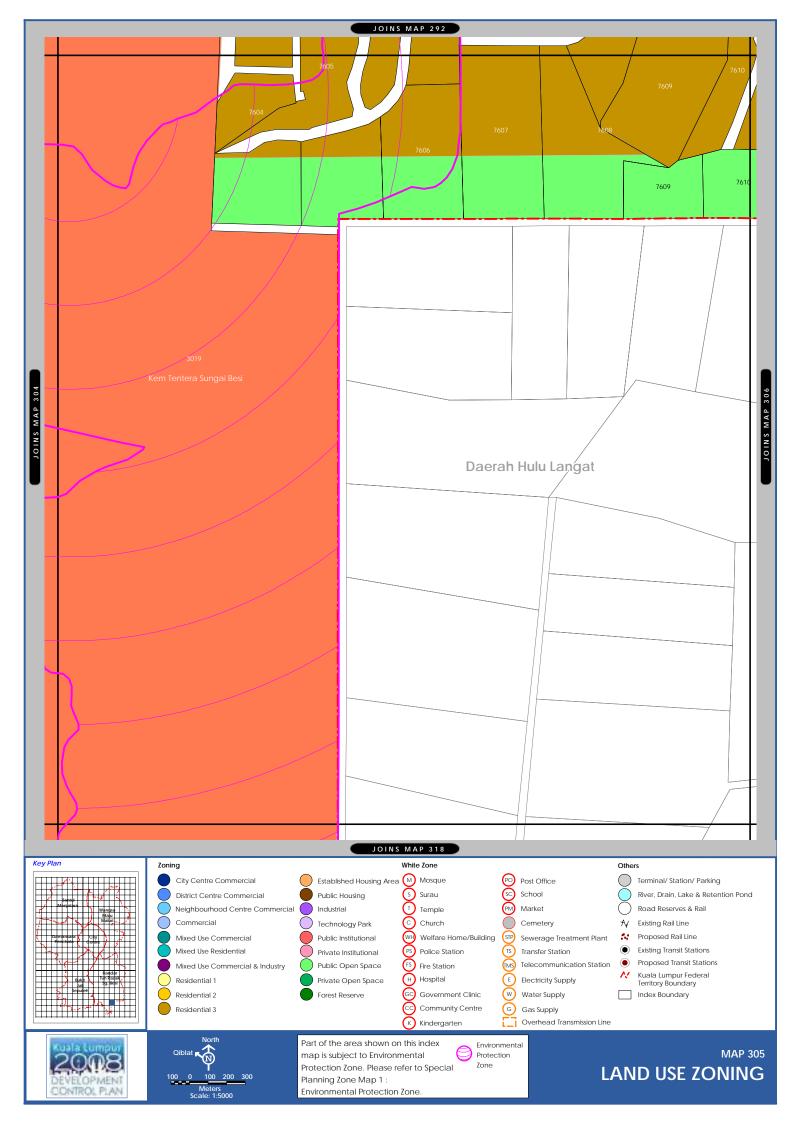


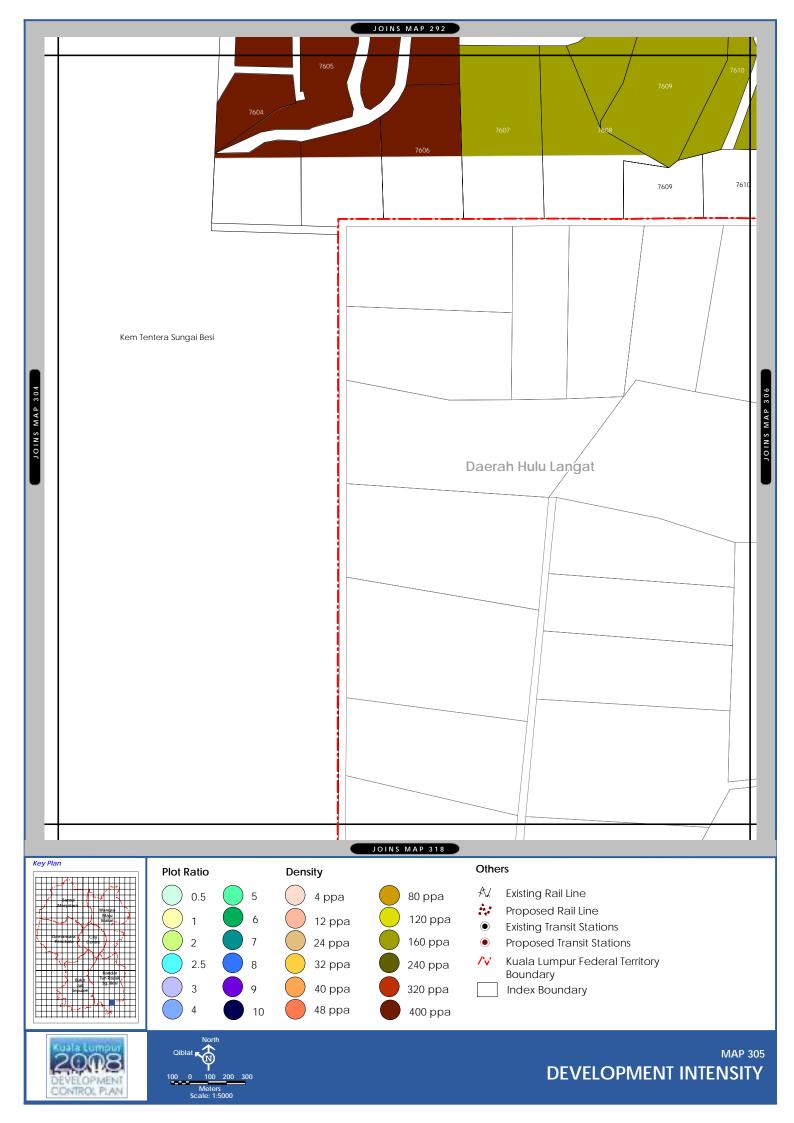


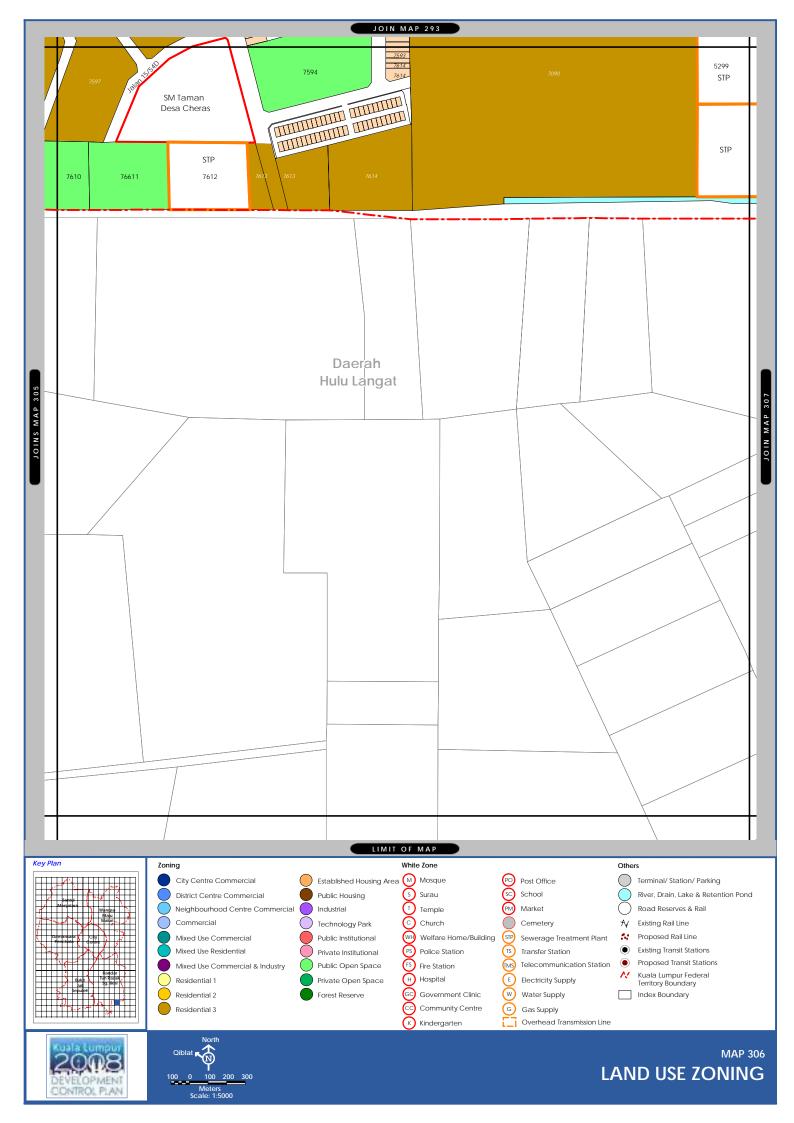


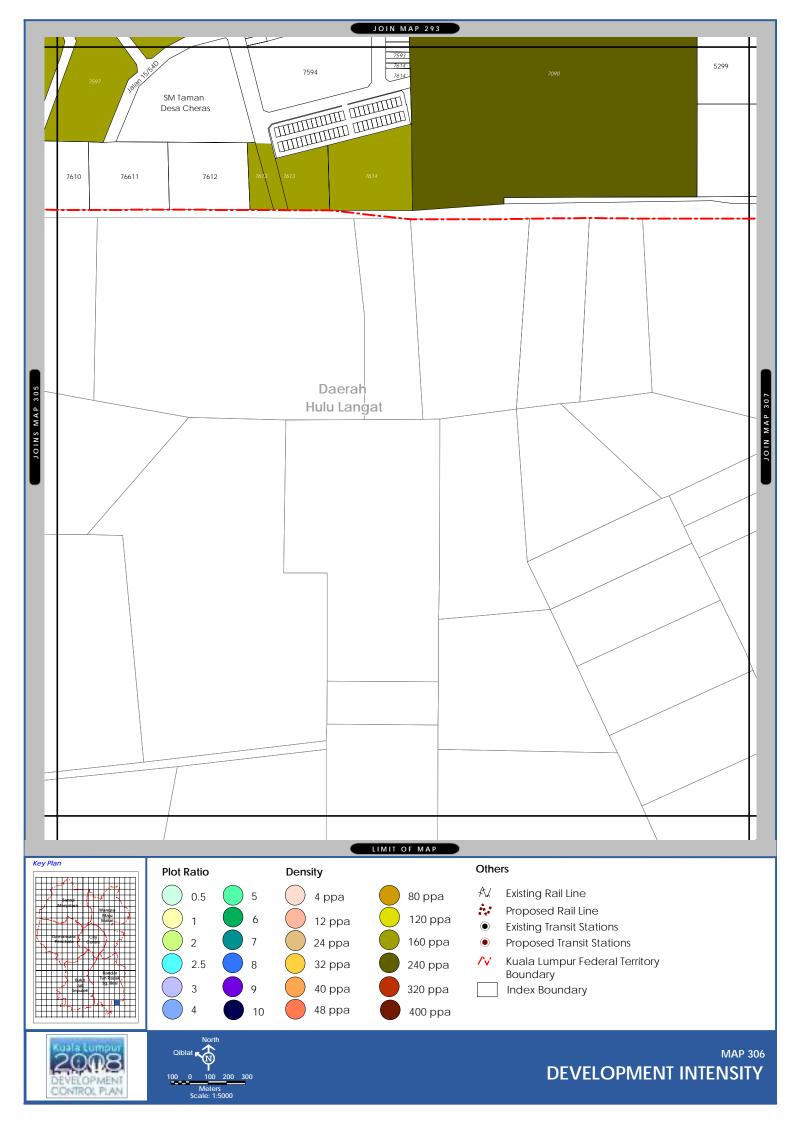


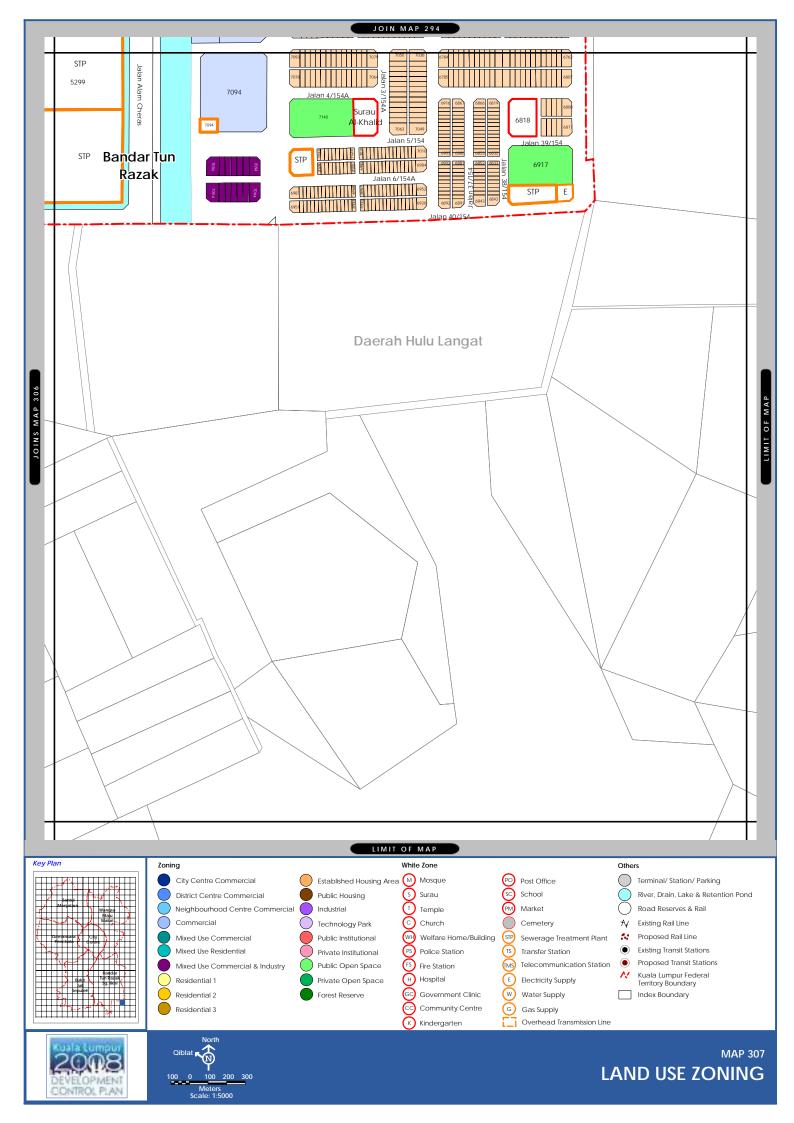


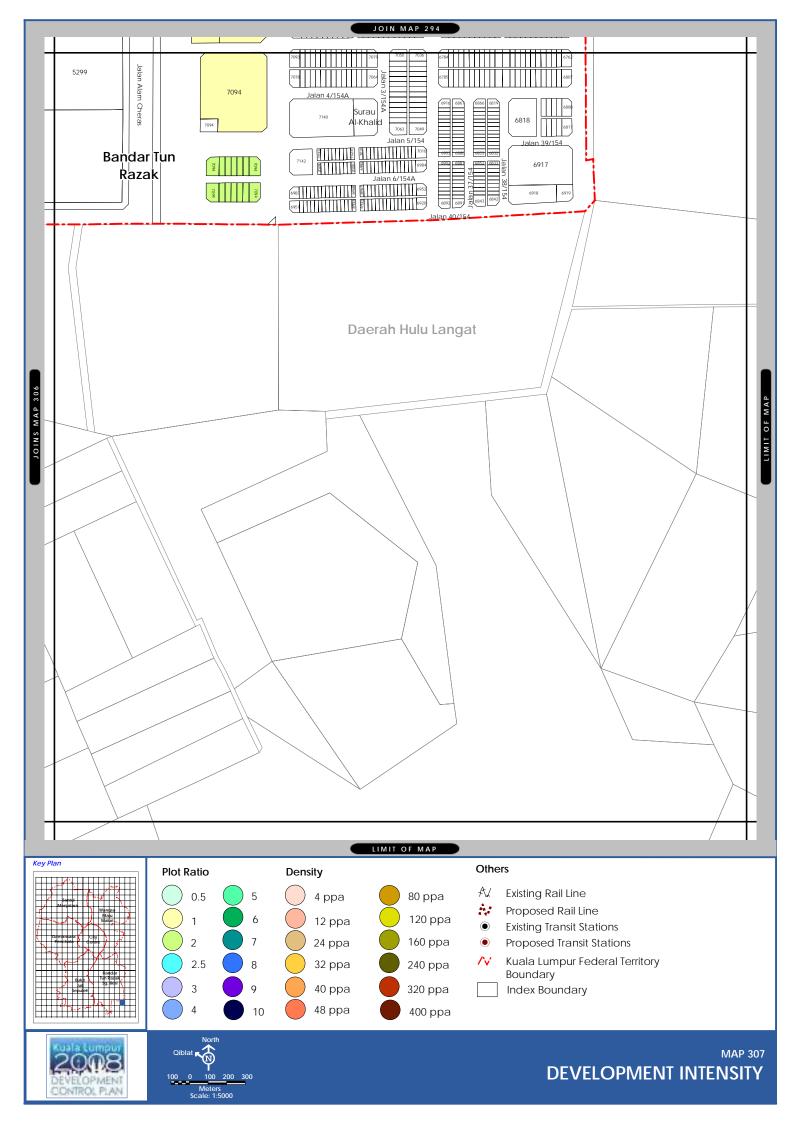


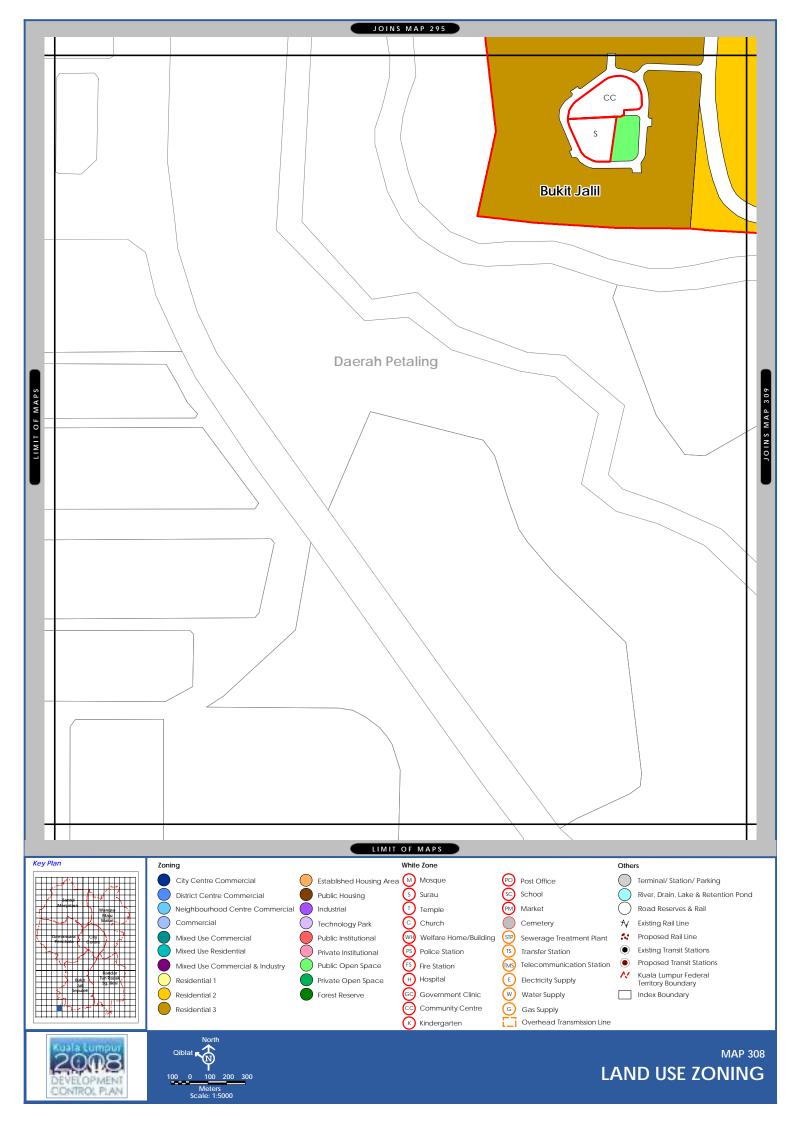


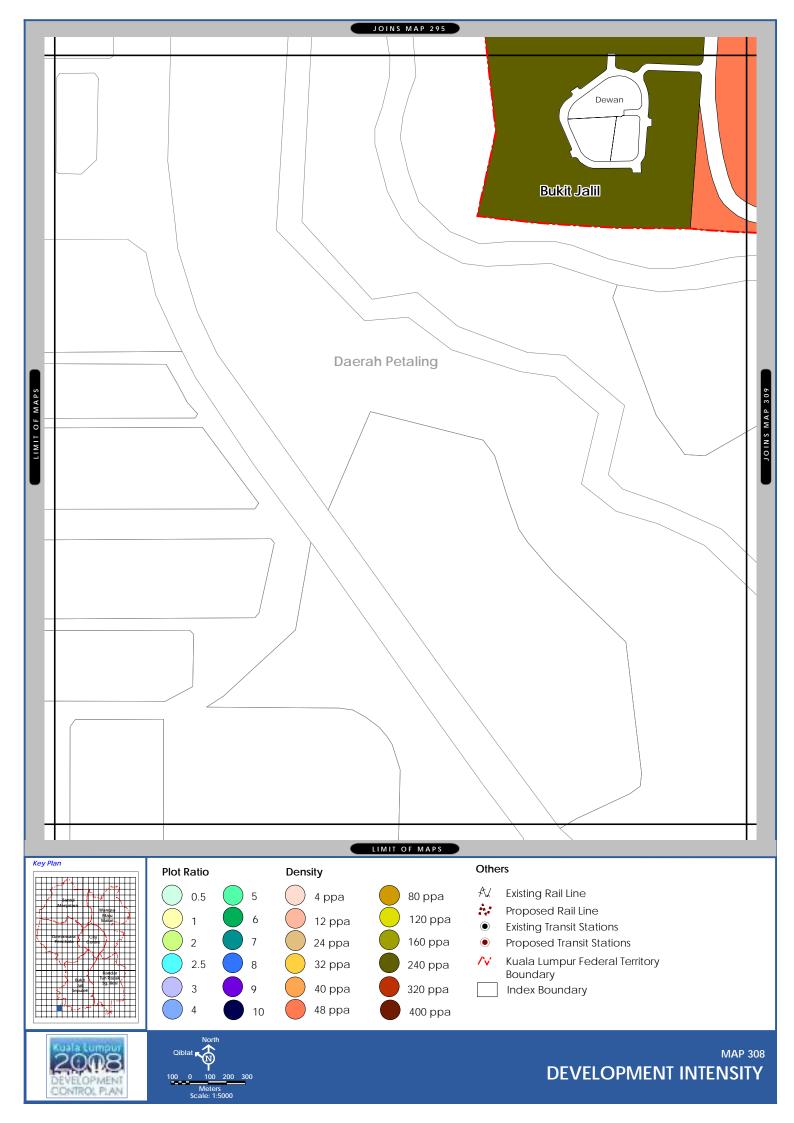


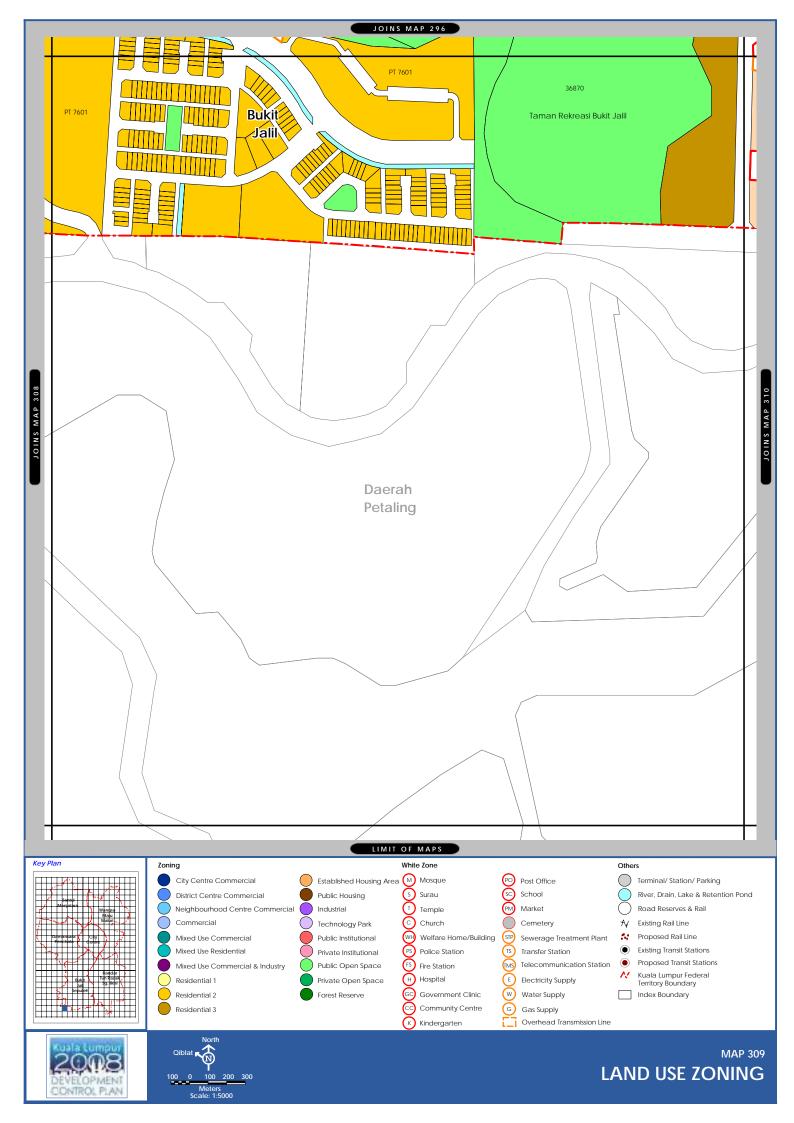


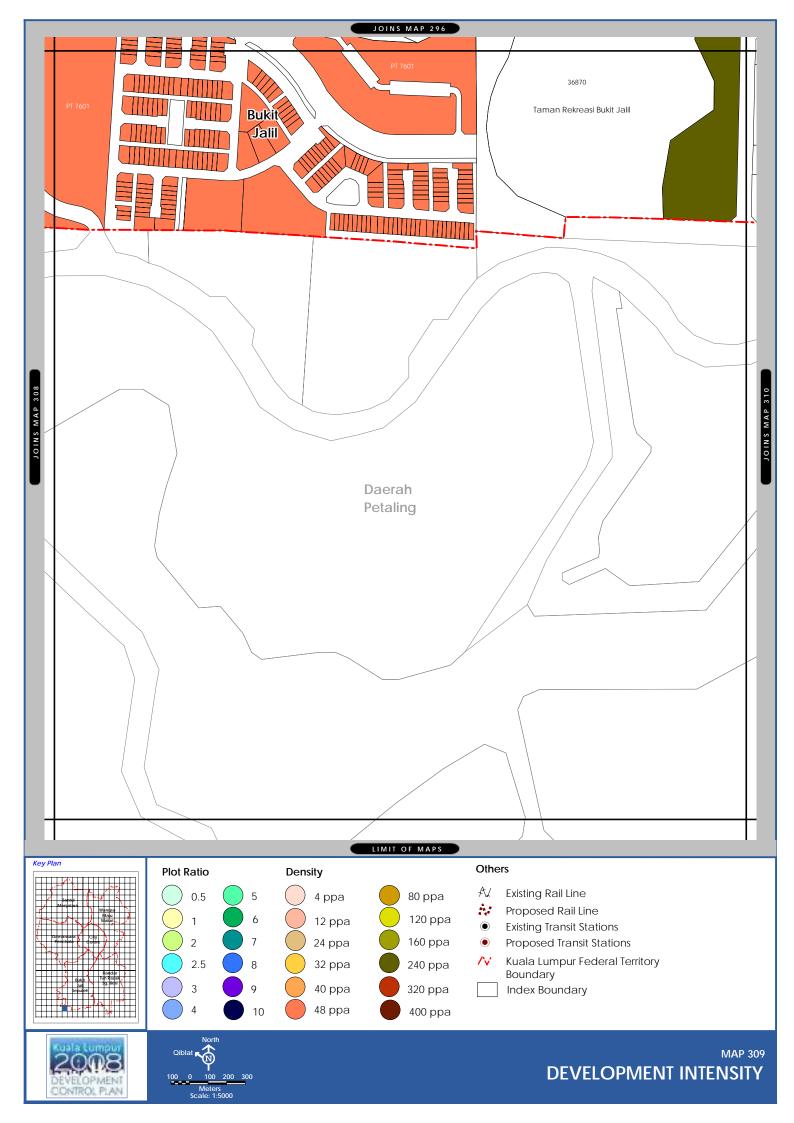


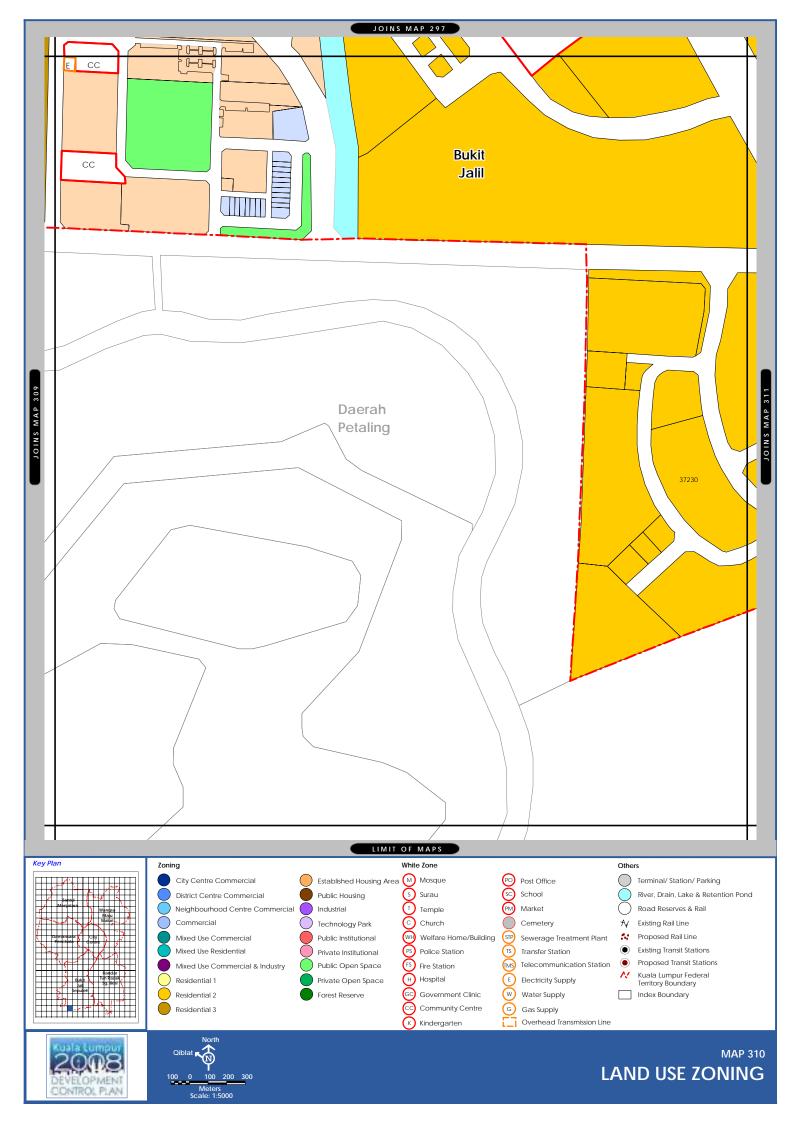


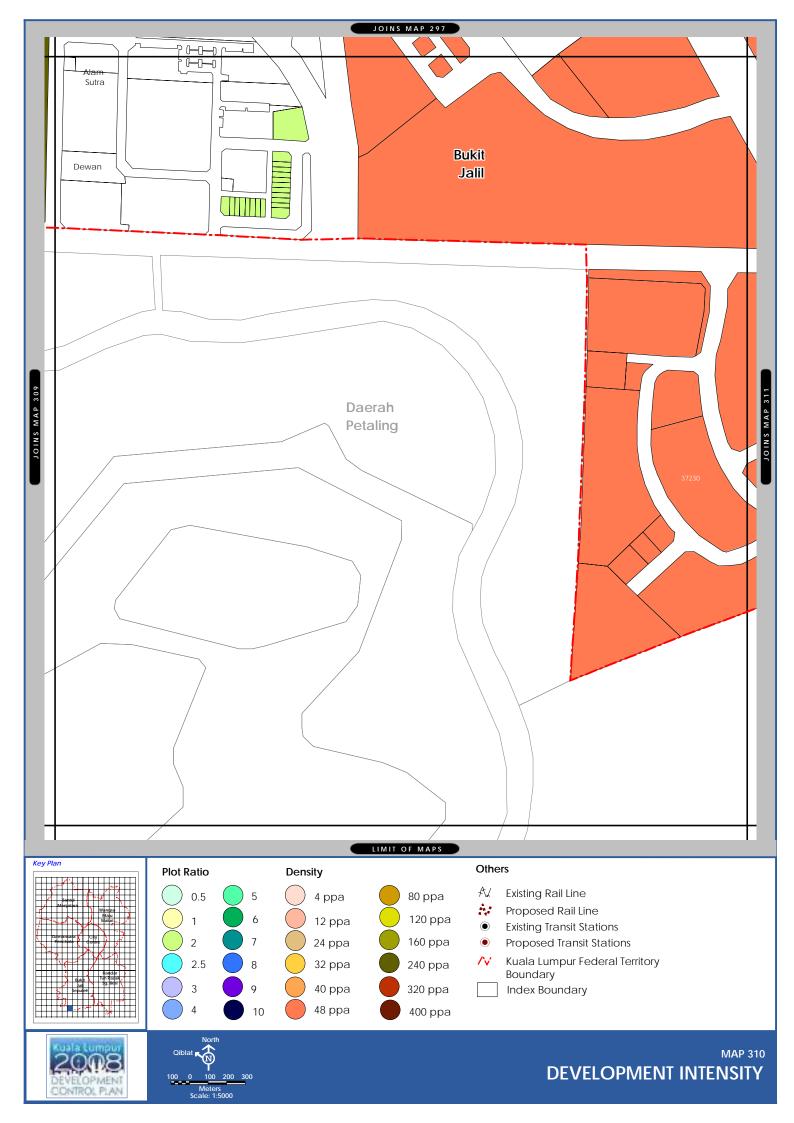


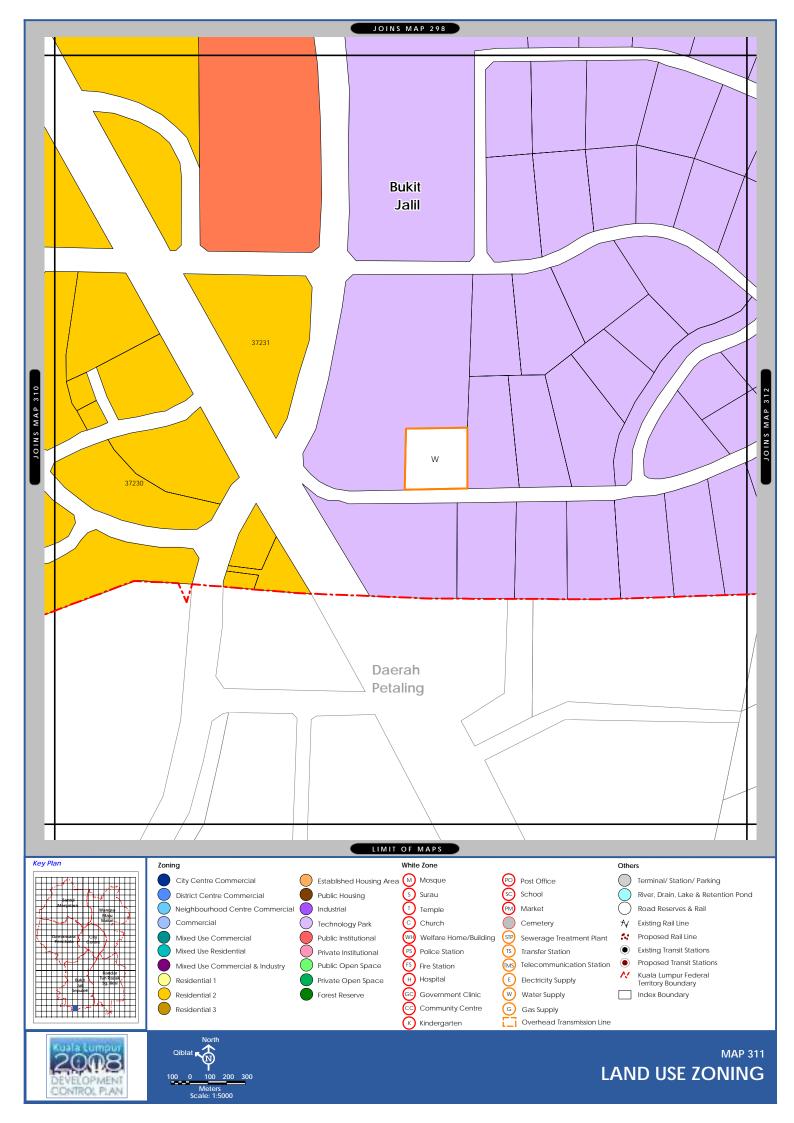


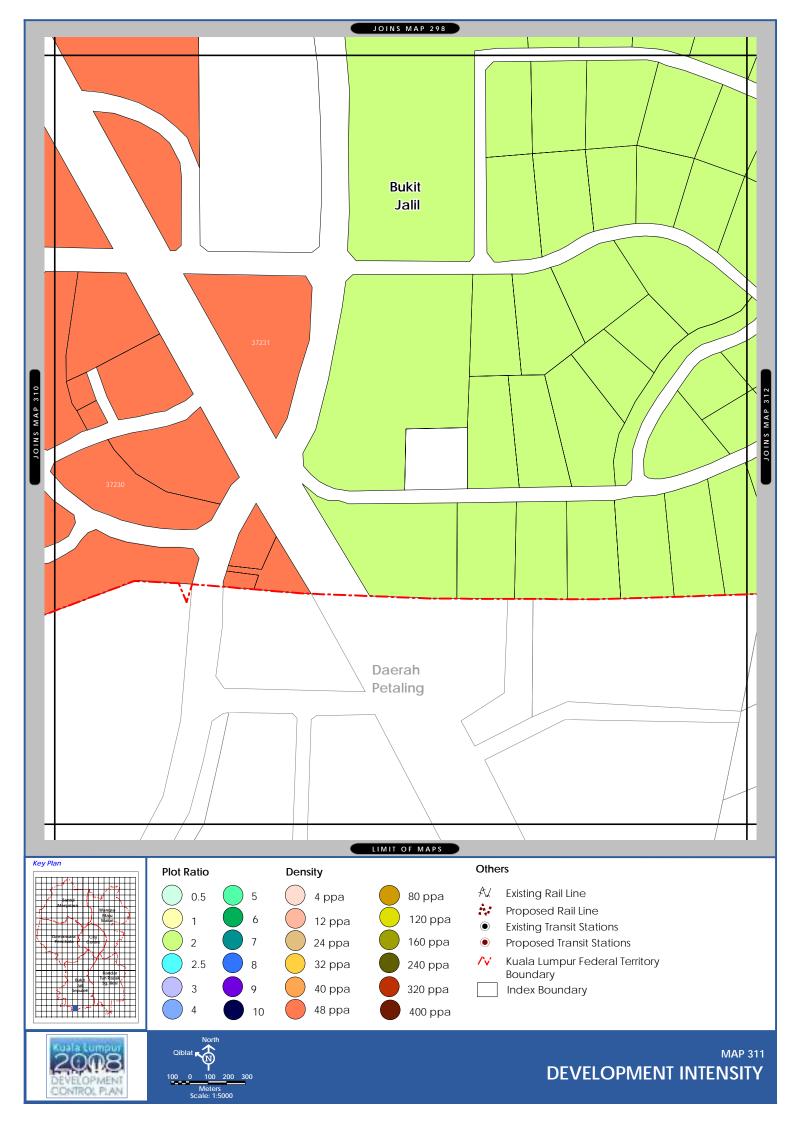


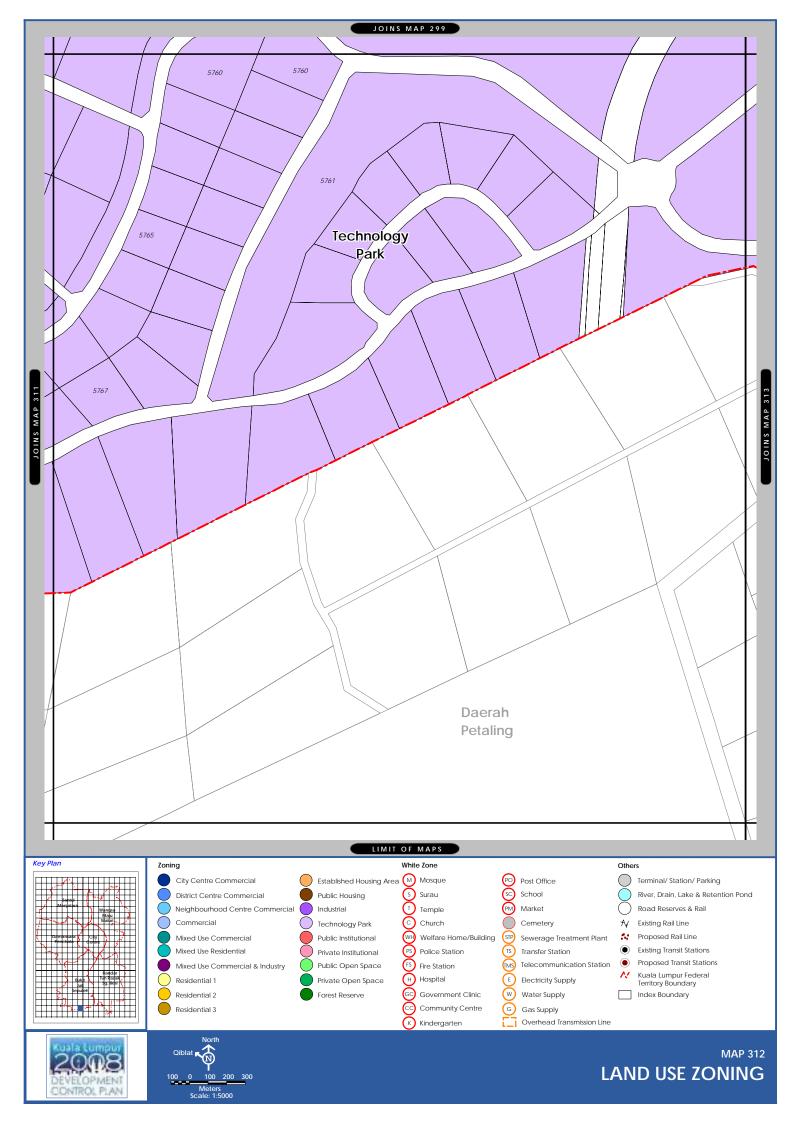


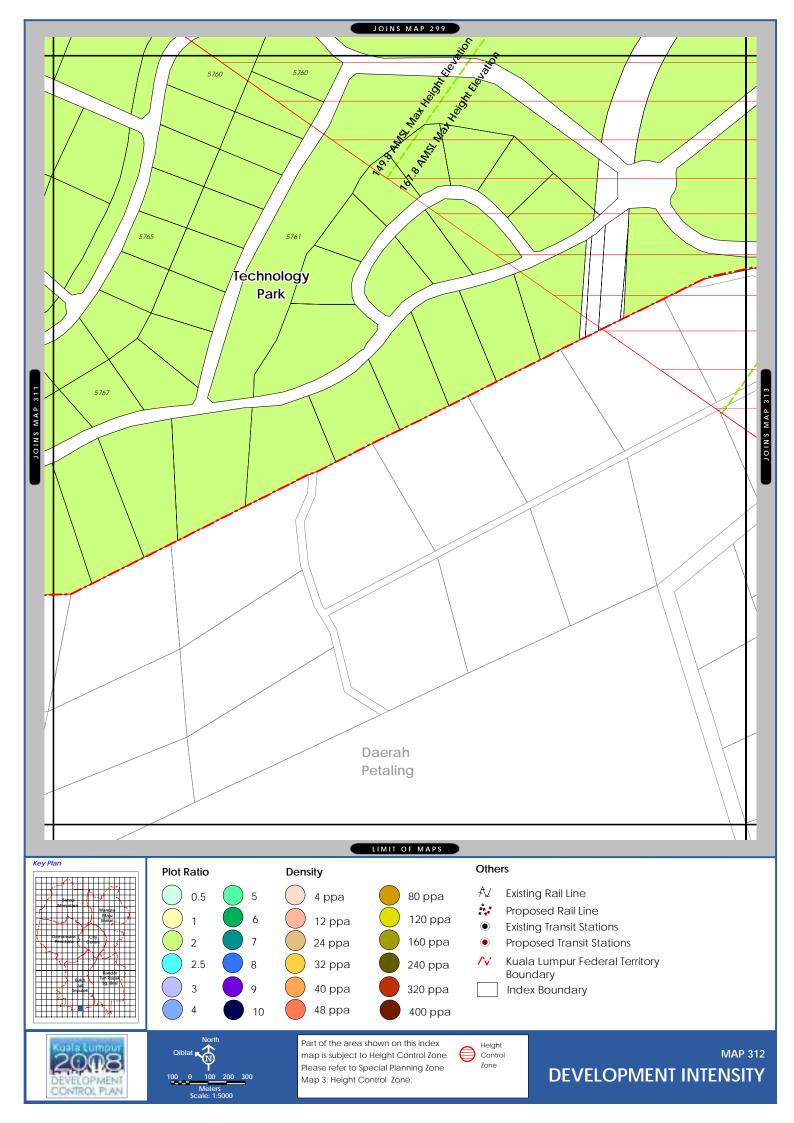


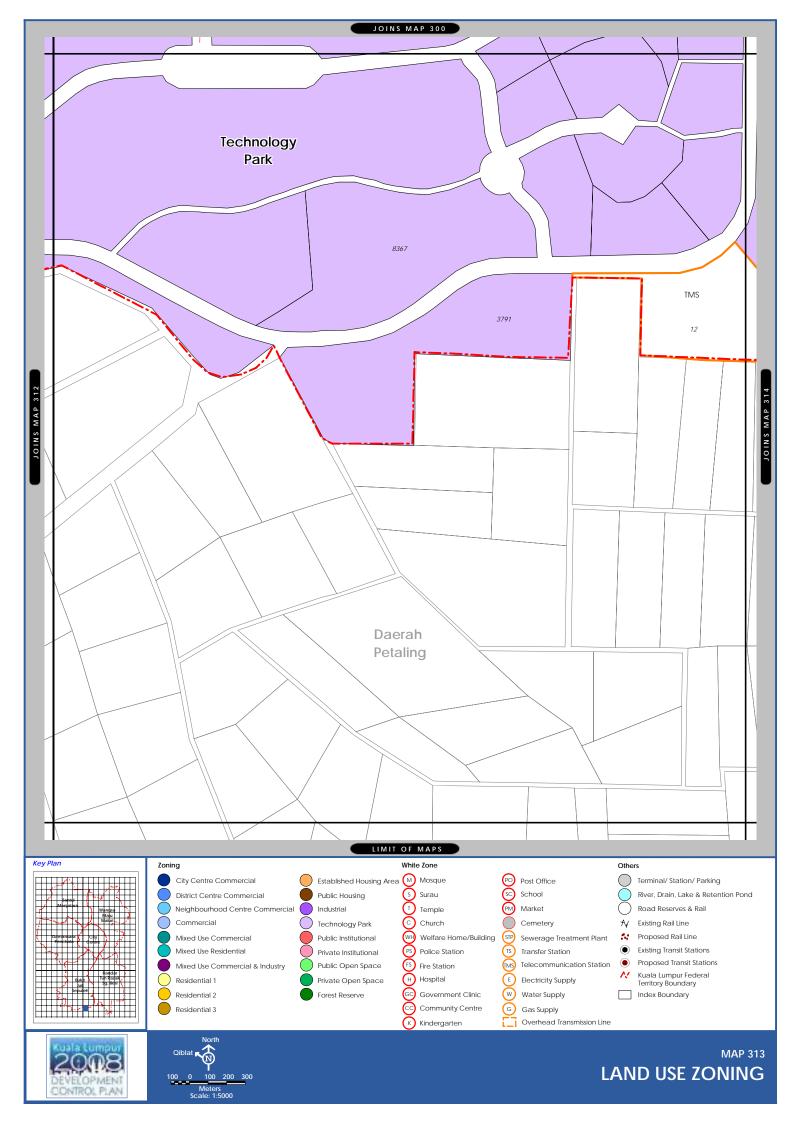


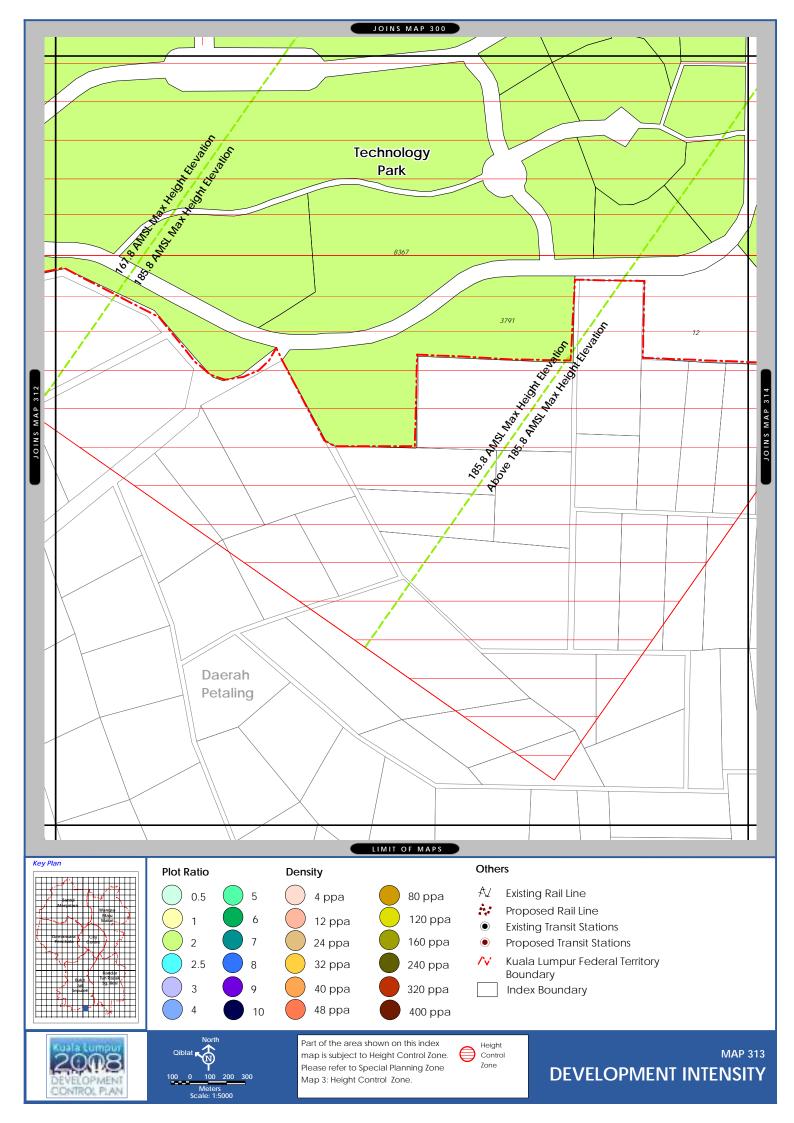


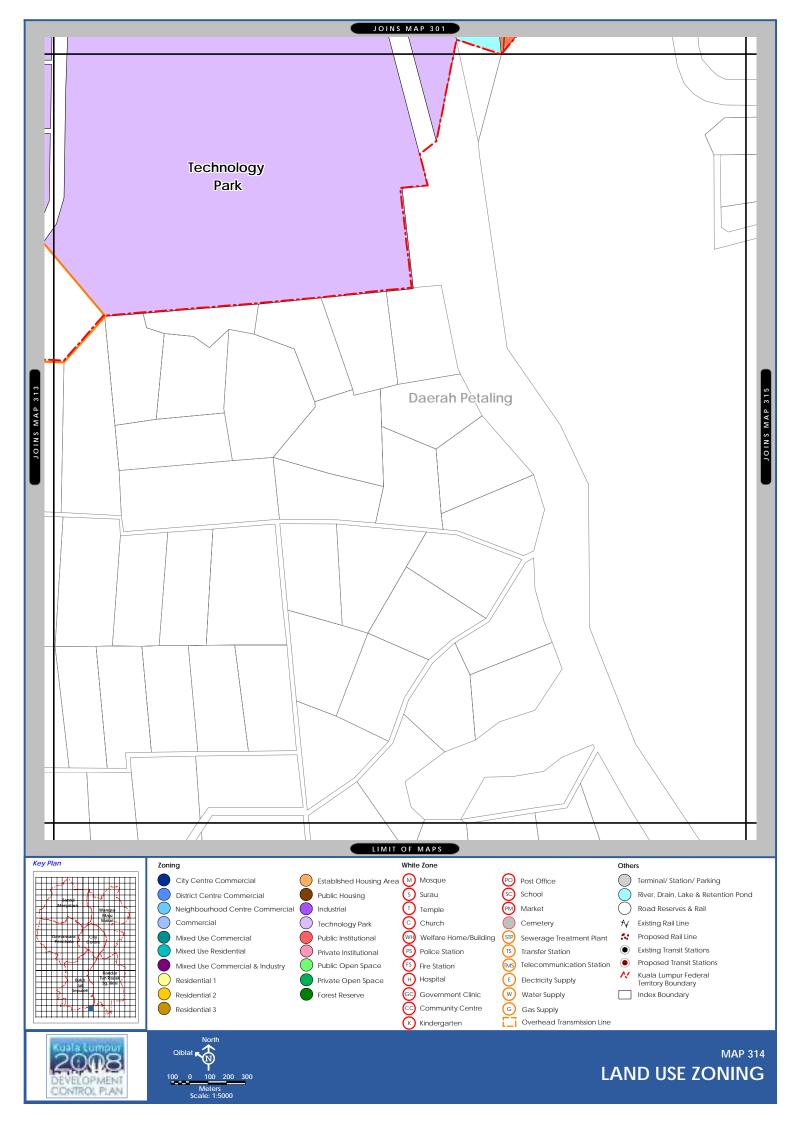


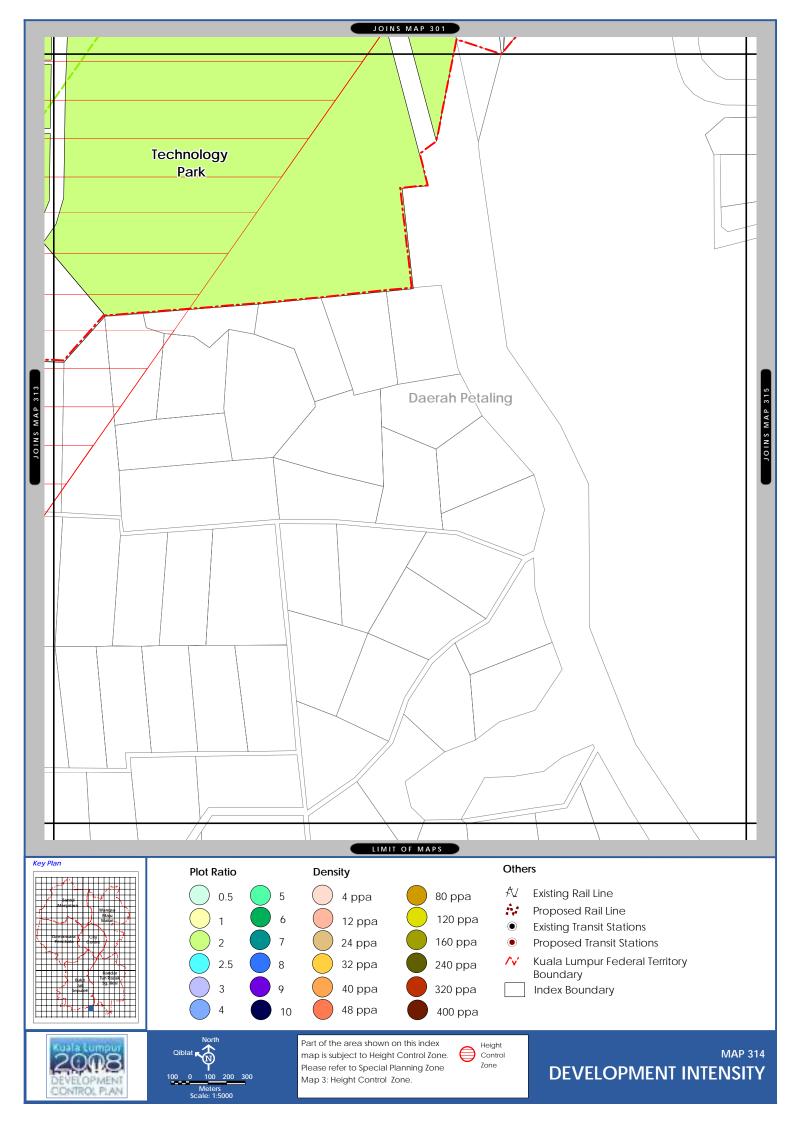


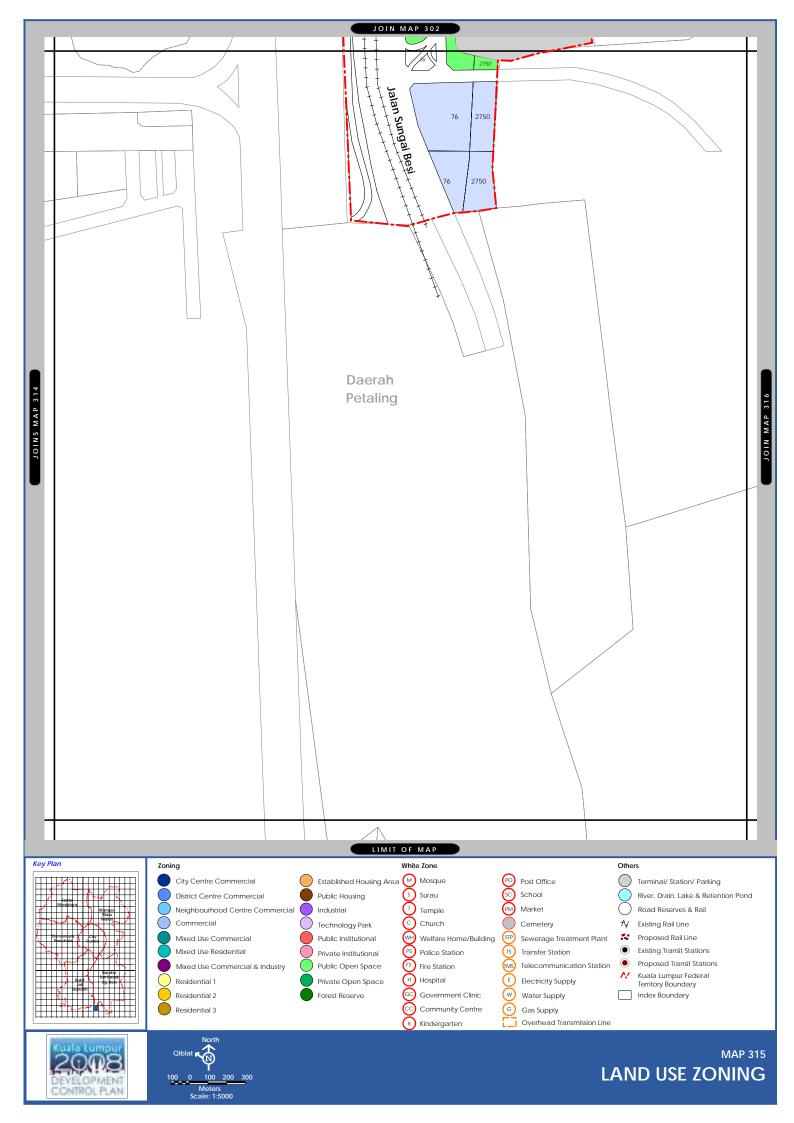


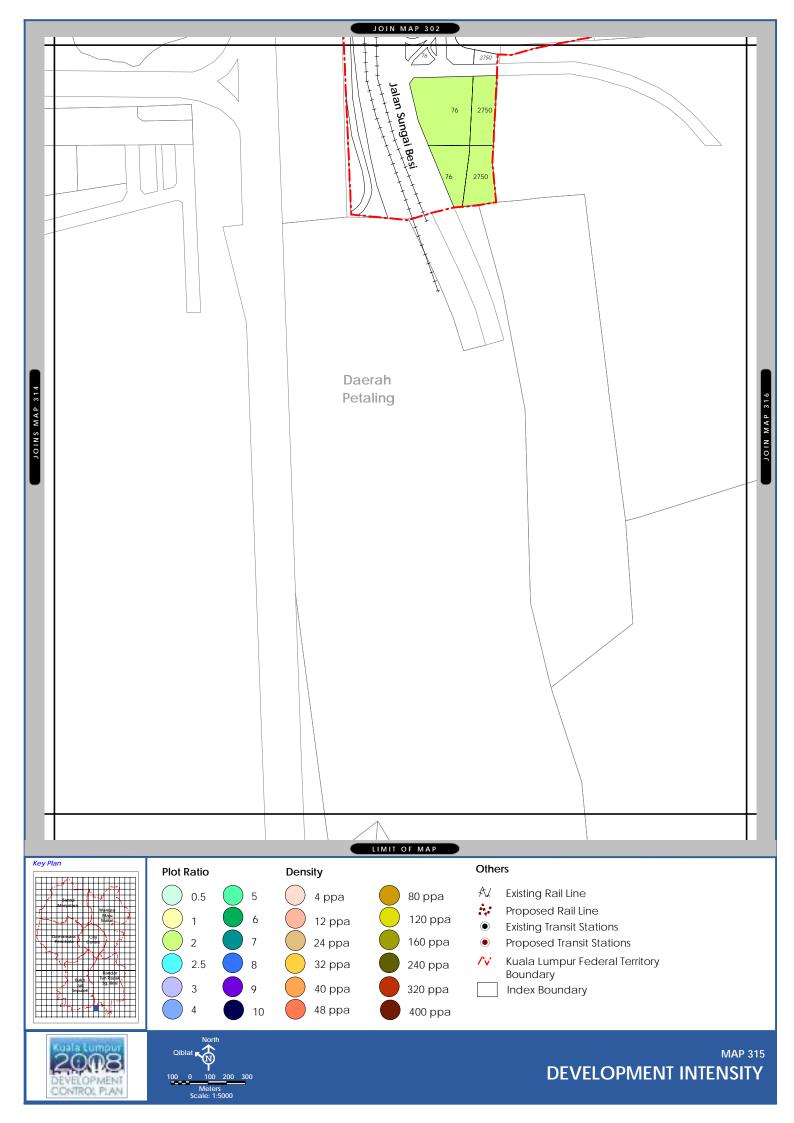


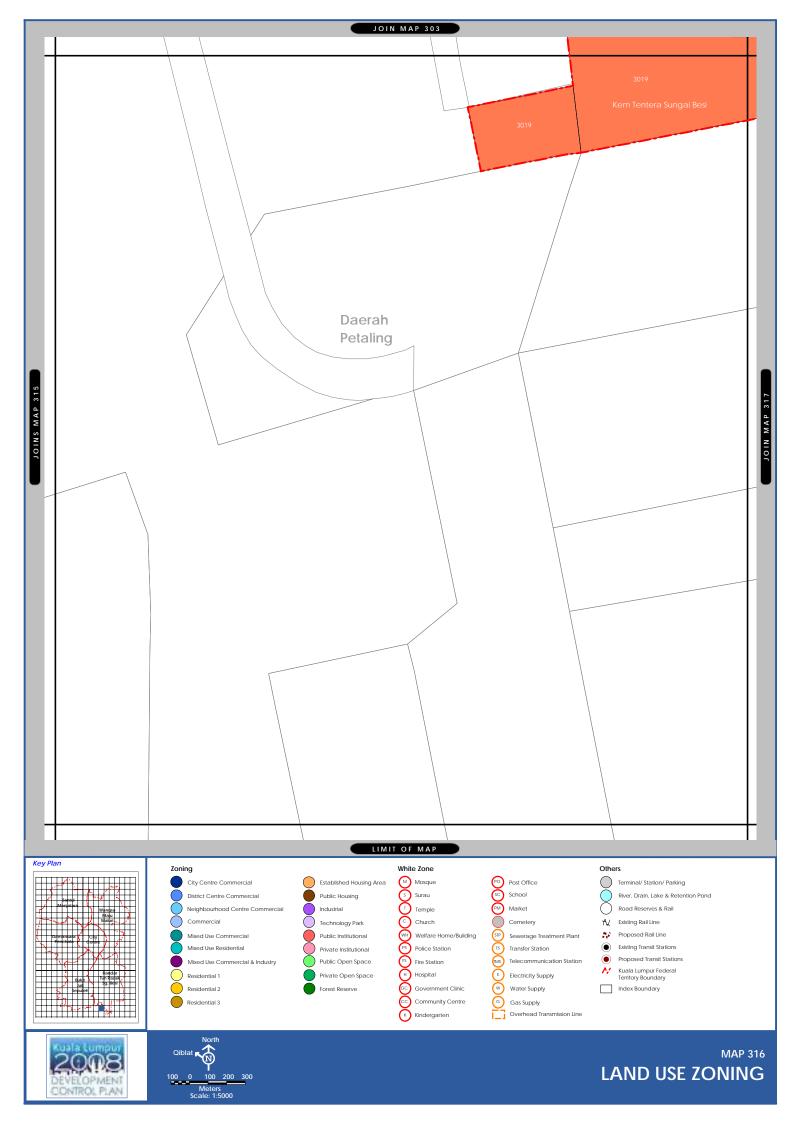


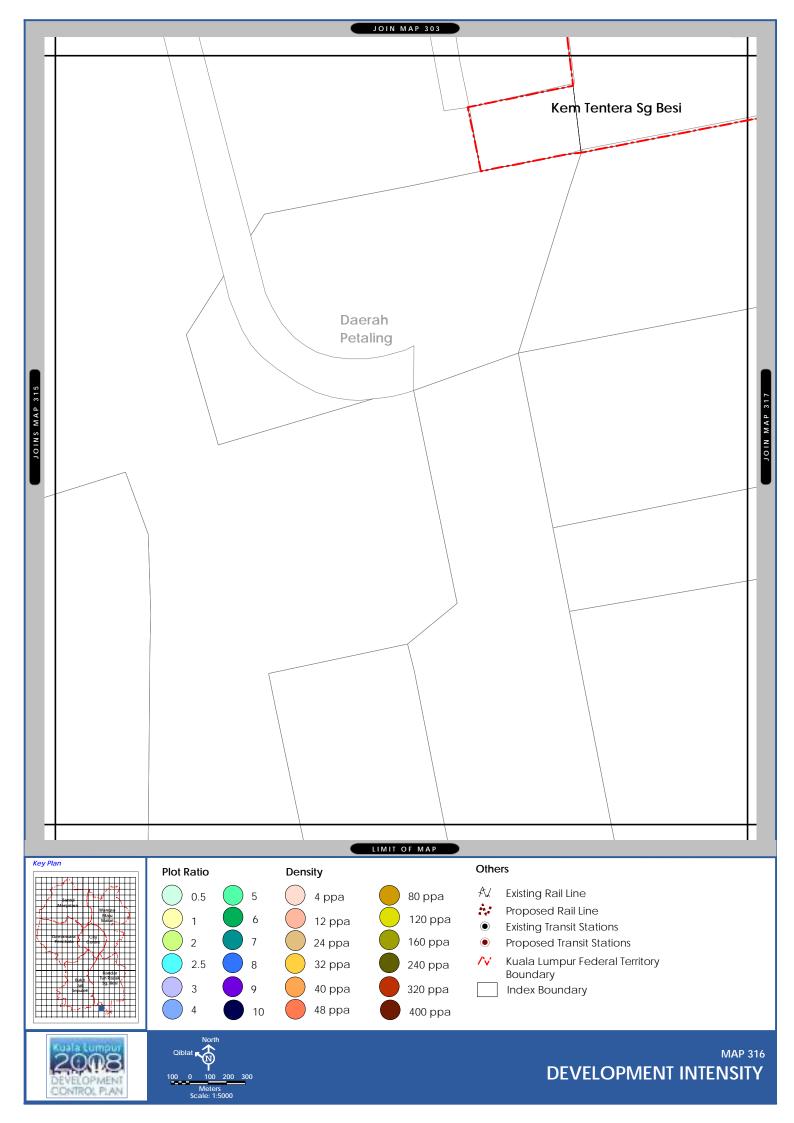


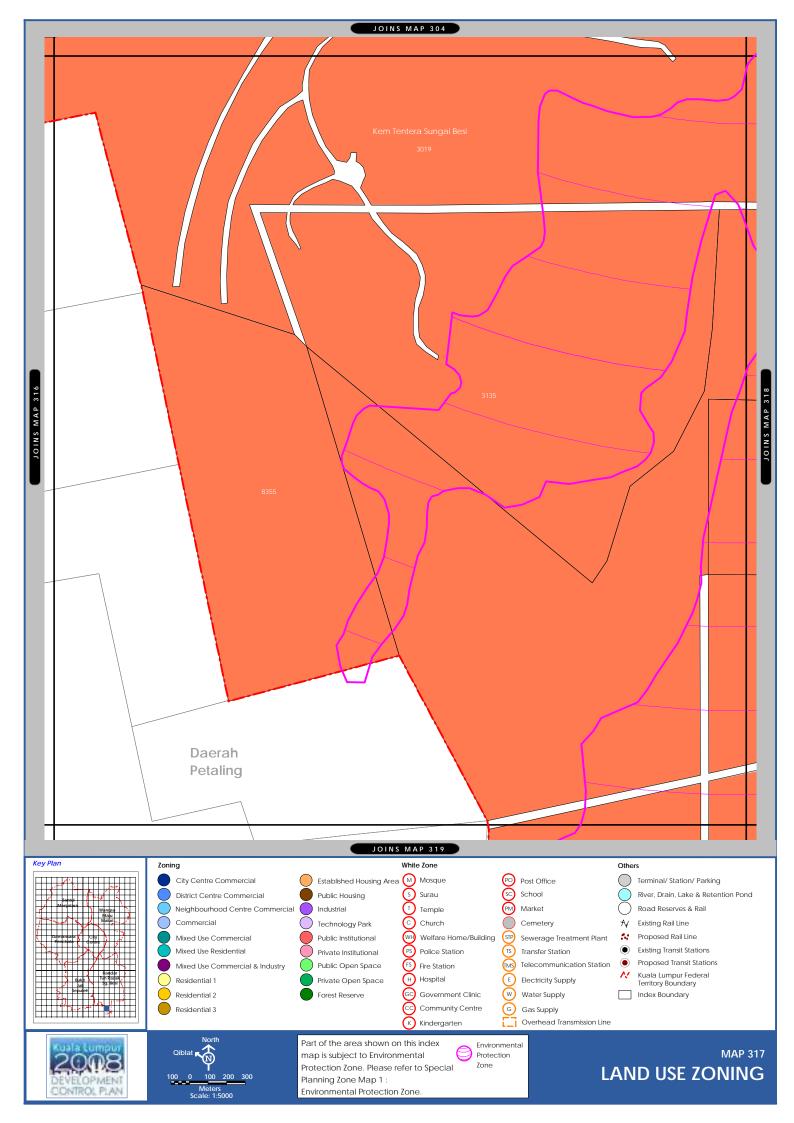


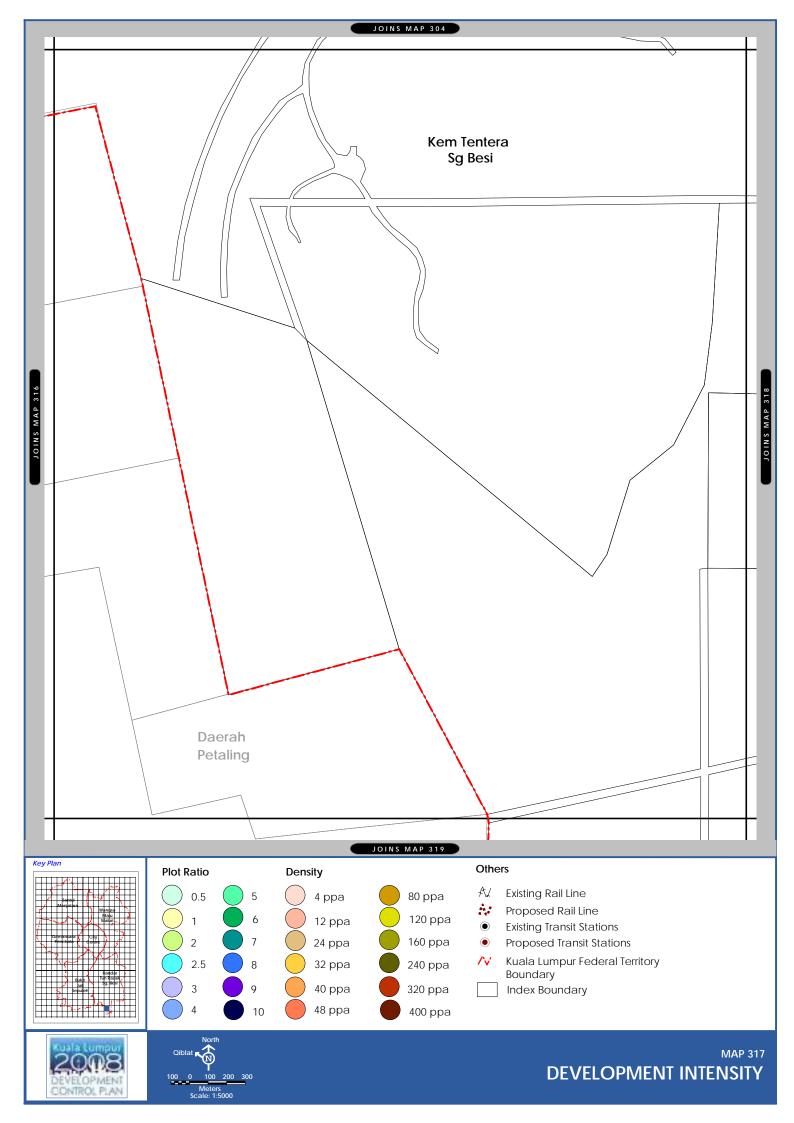


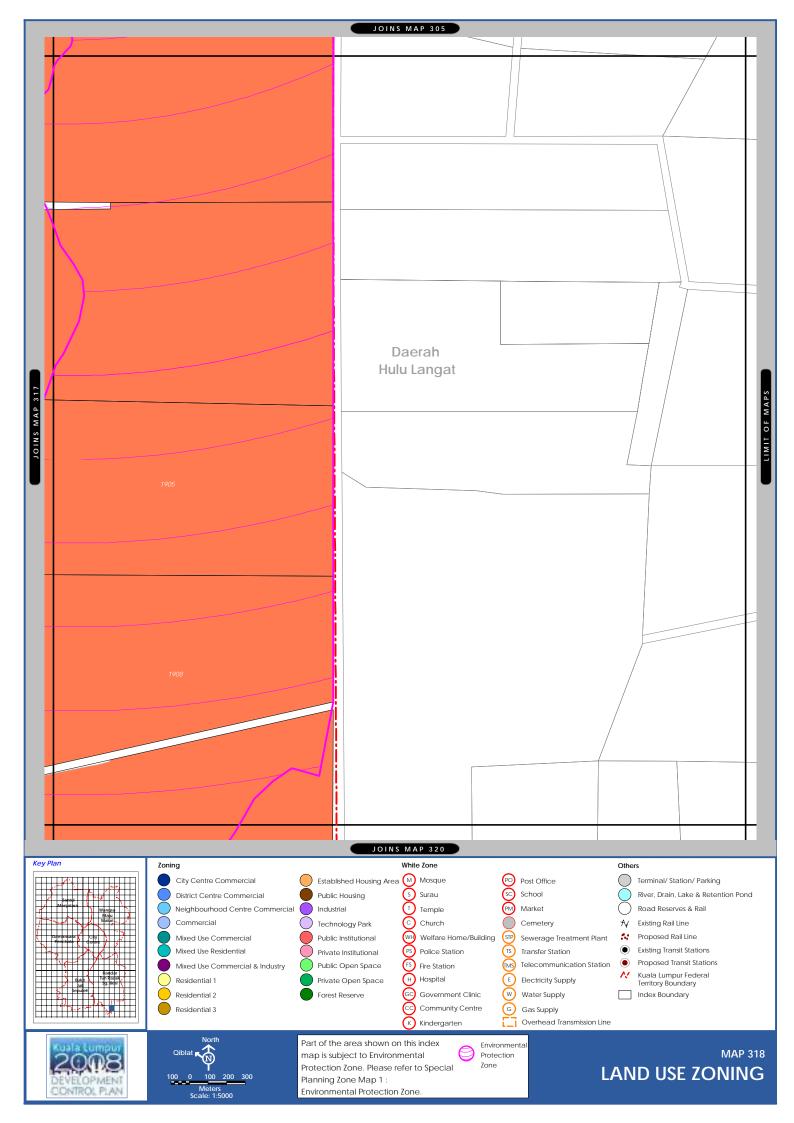


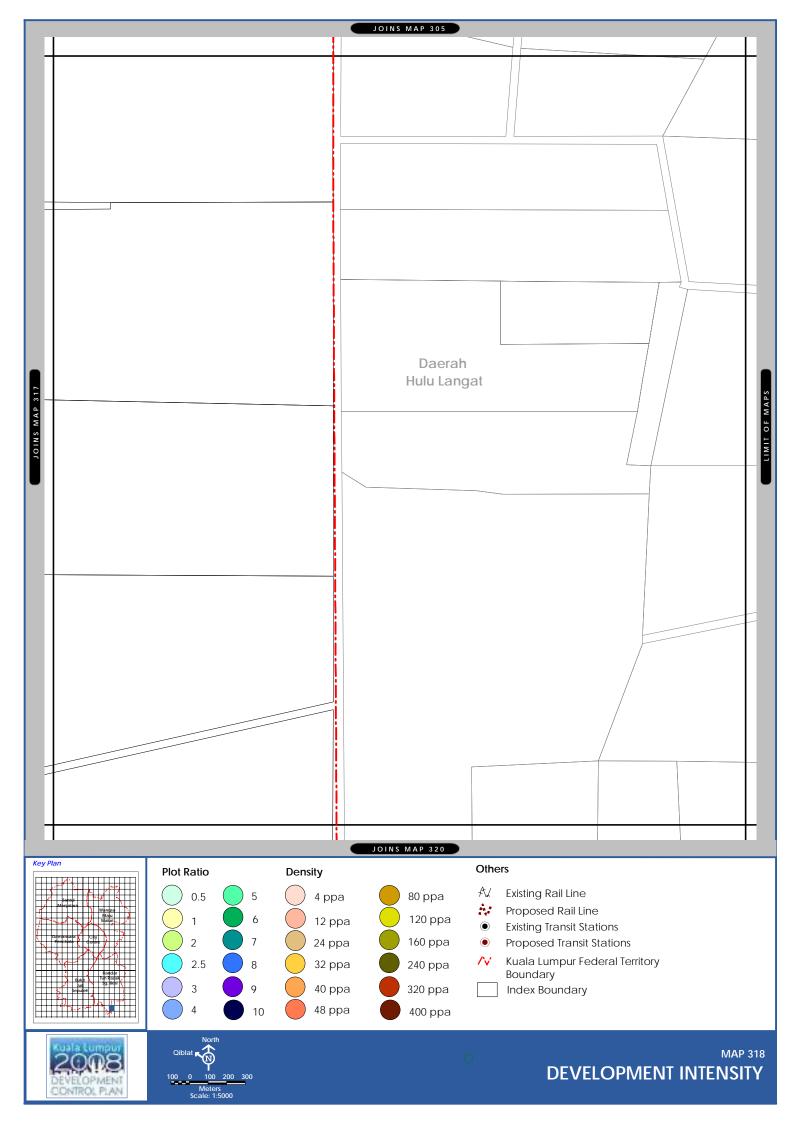


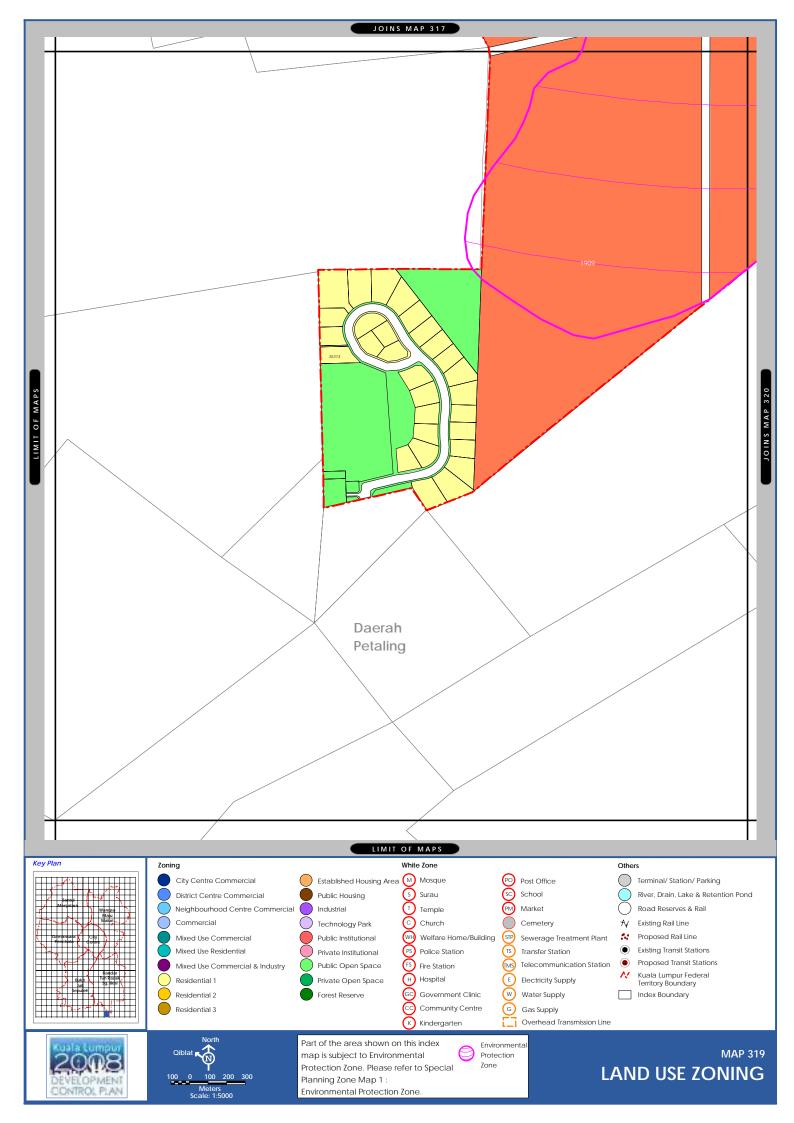


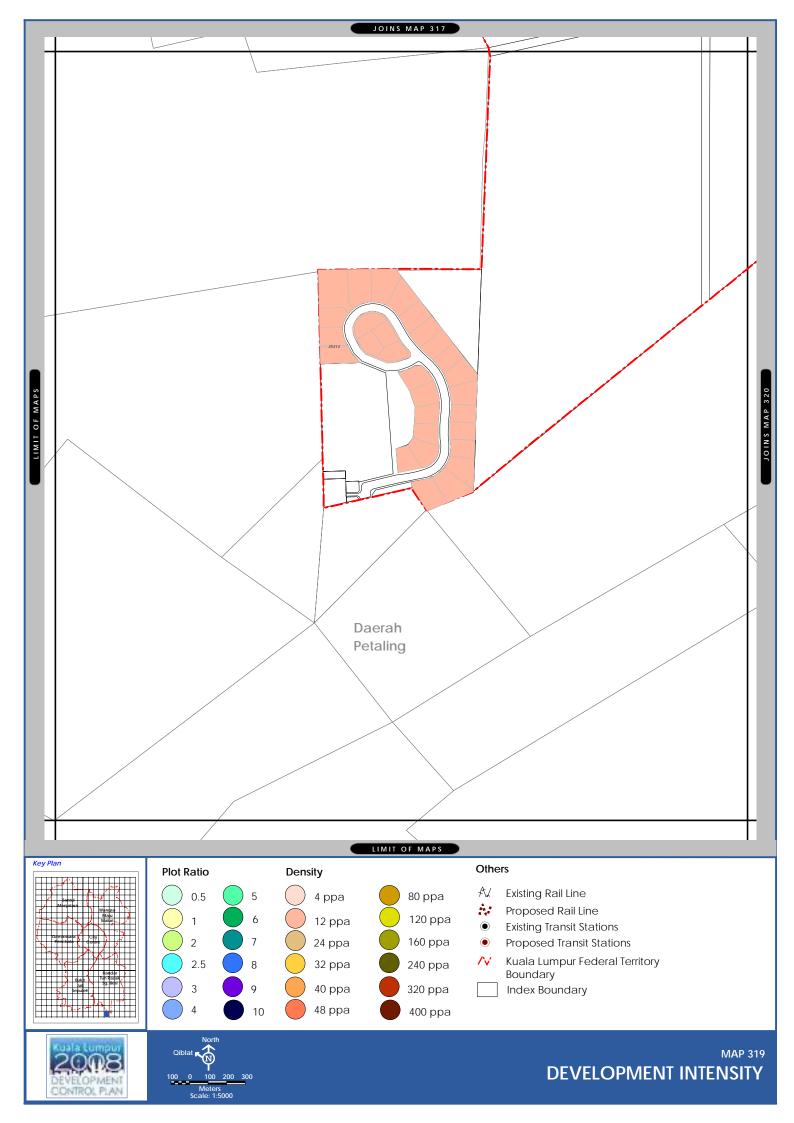


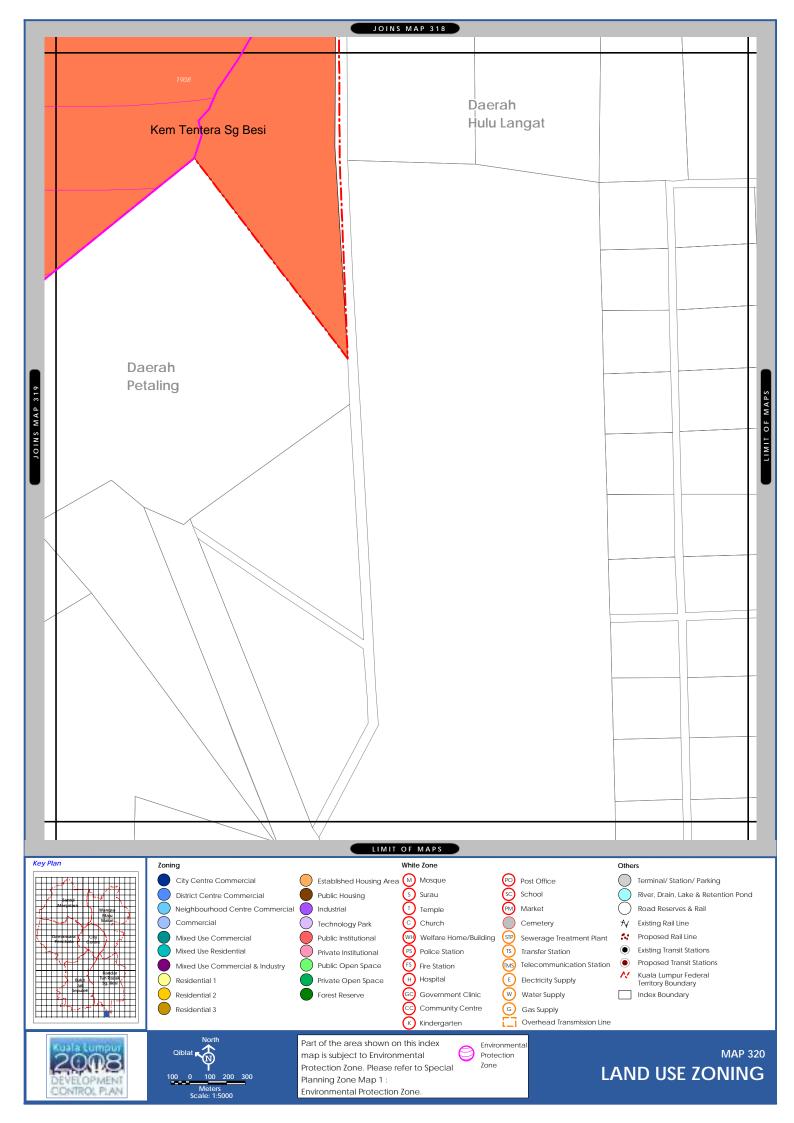


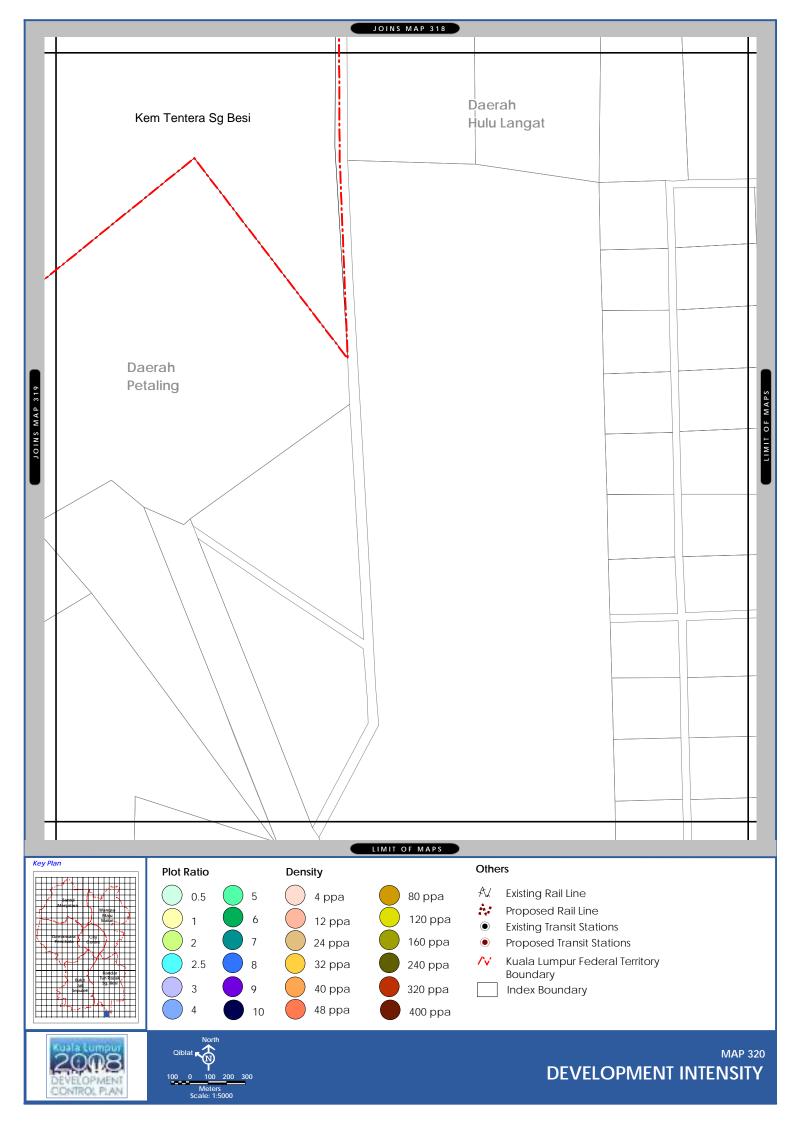














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